



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

MICHAEL F. EASLEY
GOVERNOR

LYNDO TIPPETT
SECRETARY

November 5, 2004

Addendum No. 1

RE: Emergency Bridge Contract
Contract ID: C201345
WBS # DF112.2044017 (Bridge 180) & 112.1044014 (Bridge 111)
Haywood County
Bridge 180 on SR 1123 over West Fork of Pigeon River
Bridge 111 on US 276 over East Fork of Pigeon River

November 24, 2004 Letting

To Whom It May Concern:

Reference is made to the Request for Proposal recently furnished to you on the above project. The following revisions have been made to the Request for Proposal:

On the RFP Cover Sheet, the dates for the Technical and Price Proposal Submission and Price Proposal Opening have been revised. Please void Cover Sheet in your proposal and staple the revised Cover Sheet thereto.

On the second page of the Proposal Form, the date for submitting the payment and performance bonds has been revised. Please void second page of the Proposal Form in your proposal and staple the revised second page of the Proposal Form thereto.

On page 1, *Contract Time and Liquidated Damages* has been revised and *Intermediate Contract Time #1 and Liquidated Damages* has been deleted. Please void Page No. 1 in your proposal and staple the revised Page No. 1 thereto.

On page 21, *Submittal of Proposals* has been revised. Please void Page No. 21 in your proposal and staple the revised Page No. 21 thereto.

On page 24, *Technical Proposal Evaluation Criteria* has been revised. Please void Page No. 24 in your proposal and staple the revised Page No. 24 thereto.

On page 25, *Technical Proposal Evaluation Criteria* has been revised. Please void Page No. 25 in your proposal and staple the revised Page No. 25 thereto.

On page 29, *Roadway Design Scope of Work* has been revised. Please void Page No. 29 in your proposal and staple the revised Page No. 29 thereto.

MAILING ADDRESS:
NC DEPARTMENT OF TRANSPORTATION
PROJECT SERVICES UNIT
1591 MAIL SERVICE CENTER
RALEIGH NC 27699-1591

TELEPHONE: 919-250-4128
FAX: 919-250-4119

WEBSITE: WWW.DOH.DOT.STATE.NC.US

LOCATION:
CENTURY CENTER COMPLEX
ENTRANCE B-2
1020 BIRCH RIDGE DRIVE
RALEIGH NC

On page 32, *Structure Scope of Work* has been revised. Please void Pages No. 32 – 33 in your proposal and staple the revised Pages No. 32 – 33 thereto.

On page 34, *Hydraulics Design Scope of Work* has been revised. Please void Page No. 34 in your proposal and staple the revised Page No. 34 thereto.

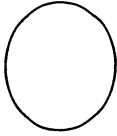
On page 41 - 42, *Erosion and Sedimentation Control Scope of Work* has been revised. Please void Pages No. 41 – 42 in your proposal and staple the revised Pages No. 41 – 42 thereto.

In addition, the timeline for this project has been revised and is attached for you use. The Department is also requesting that **one copy**, of the eight Technical Proposals requested on Page 22 of the Final RFP, be delivered to Jamie Wilson, at the Division 14 Office in Sylva, NC, no later than 4:00 pm on November 22, 2004. Your cooperation is appreciated.

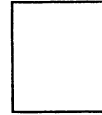
Sincerely,

R.A. Garris, P.E.
Contract Officer

c: Mr. Len Sanderson, PE
Mr. Steve Varnedoe, PE
Ms. Deborah Barbour, P.E.
Mr. Steve Dewitt, PE (w/attachment)
Mr. Joel Setzer, PE
Mr. Victor Barbour, PE (w/attachment)
Mr. Art McMillan, P.E.
Mr. Rodger Rochelle, PE (w/attachment)
Ms. Emily Lawton, FHWA (w/attachment)
Mr. Jay Bennett, PE (w/attachment)
Mr. Andy Gay, PE (w/attachment)
Mr. Ron Davenport, PE (w/attachment)
Ms. Marsha Sample (w/attachment)
Mr. Chris Smitherman (w/attachment)
Mr. Njorge W. Wainaina, PE (w/attachment)
Mr. Mark Davis, PE (with attachment)
Mr. Rick Styles, PE (with attachment)
Mr. John Emerson, PE (with attachment)
Mr. Mike Summers, PE (with attachment)
Mr. Greg Perfetti, PE
Technical Review Committee Members (w/attachment)
File (w/attachment)



-- STATE OF NORTH CAROLINA--
DEPARTMENT OF TRANSPORTATION
RALEIGH, N.C.



DESIGN-BUILD PACKAGE

FINAL RFP

OCTOBER 25, 2004

VOID FOR BIDDING

DATE AND TIME OF TECHNICAL AND PRICE PROPOSAL SUBMISSION: **November 22, 2004 AT 4:00 PM**

DATE AND TIME OF PRICE PROPOSAL OPENING: **November 24, 2004 AT 10:00 AM**

CONTRACT ID: C 201345 MILES: .133

WBS ELEMENT NO. DF112.2044017 (BRIDGE 180) and 112.1044014 (BRIDGE 111)

FEDERAL-AID NO.

COUNTY: HAYWOOD

ROUTE NO. SR 1123 AND US 276

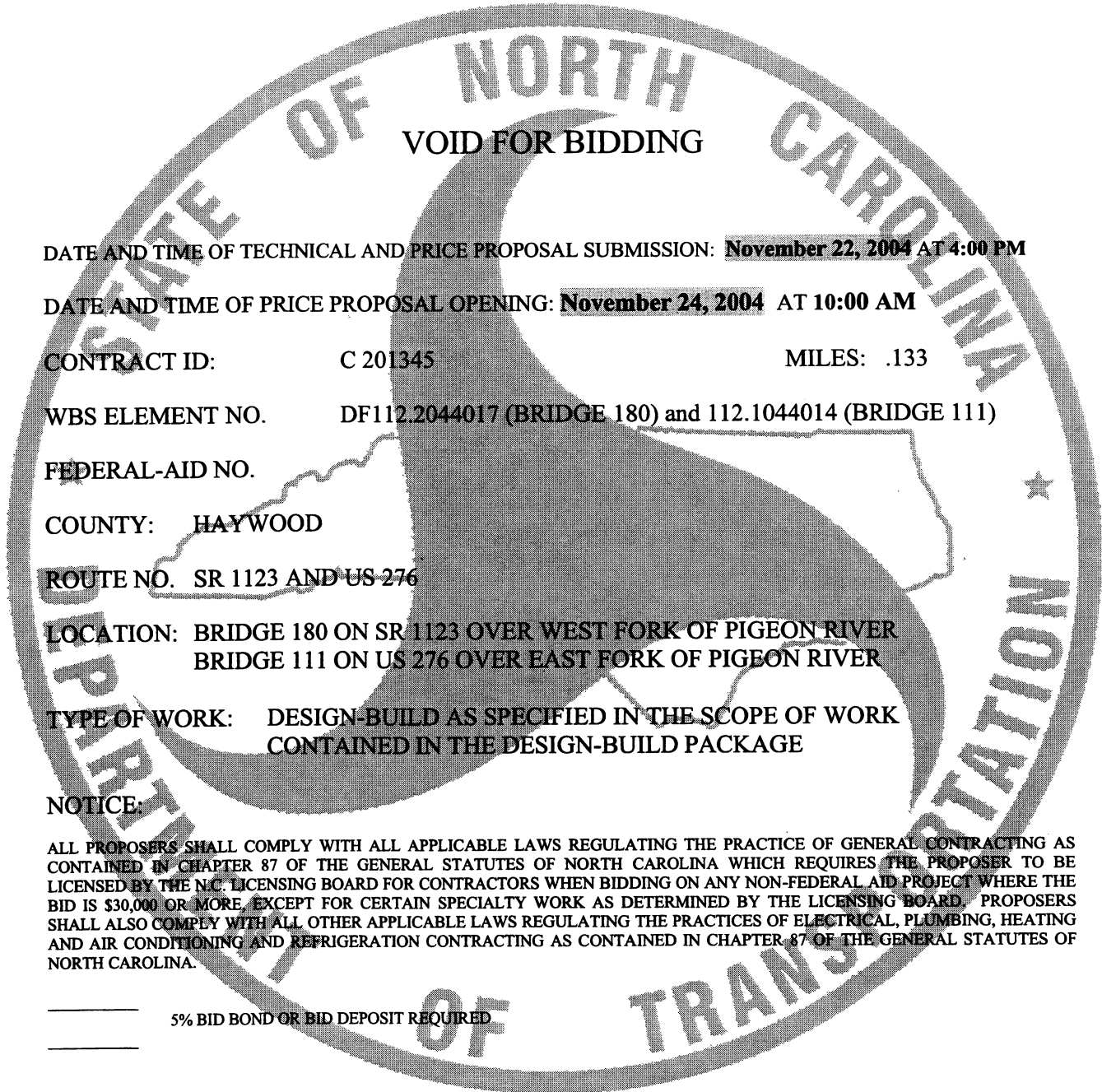
LOCATION: BRIDGE 180 ON SR 1123 OVER WEST FORK OF PIGEON RIVER
BRIDGE 111 ON US 276 OVER EAST FORK OF PIGEON RIVER

TYPE OF WORK: DESIGN-BUILD AS SPECIFIED IN THE SCOPE OF WORK
CONTAINED IN THE DESIGN-BUILD PACKAGE

NOTICE:

ALL PROPOSERS SHALL COMPLY WITH ALL APPLICABLE LAWS REGULATING THE PRACTICE OF GENERAL CONTRACTING AS CONTAINED IN CHAPTER 87 OF THE GENERAL STATUTES OF NORTH CAROLINA WHICH REQUIRES THE PROPOSER TO BE LICENSED BY THE N.C. LICENSING BOARD FOR CONTRACTORS WHEN BIDDING ON ANY NON-FEDERAL AID PROJECT WHERE THE BID IS \$30,000 OR MORE, EXCEPT FOR CERTAIN SPECIALTY WORK AS DETERMINED BY THE LICENSING BOARD. PROPOSERS SHALL ALSO COMPLY WITH ALL OTHER APPLICABLE LAWS REGULATING THE PRACTICES OF ELECTRICAL, PLUMBING, HEATING AND AIR CONDITIONING AND REFRIGERATION CONTRACTING AS CONTAINED IN CHAPTER 87 OF THE GENERAL STATUTES OF NORTH CAROLINA.

5% BID BOND OR BID DEPOSIT REQUIRED

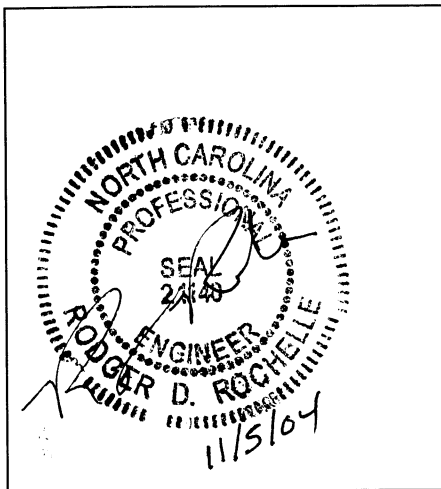


The published volume entitled *North Carolina Department of Transportation, Raleigh, Standard Specifications for Roads and Structures, JANUARY 2002* with all amendments and supplements thereto, is by reference, incorporated into and made a part of this contract; that, except as herein modified, all the Construction and work included in this contract is to be done in accordance with the specifications contained in said volume, and amendments and supplements thereto, under the direction of the Engineer.

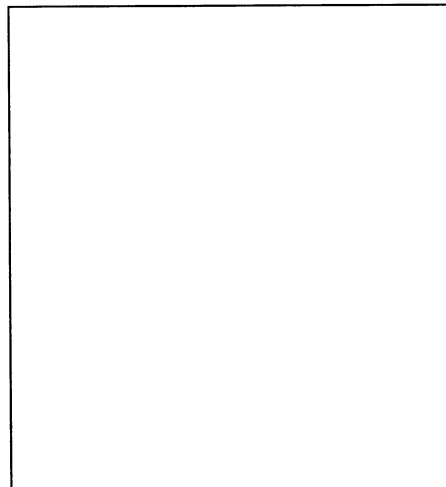
All design manuals, policy and procedures manuals, and AASHTO publications and guidelines referenced in the Request For Proposal, are by reference, incorporated and made part of this contract.

If the proposal is accepted and the award is made, the technical proposal submitted by the proposer is by reference, incorporated and made part of this contract. The contract is valid only when signed either by the Contract Officer or such other person as may be designated by the Secretary to sign for the Department of Transportation. The conditions and provisions herein cannot be changed except by written approval as allowed by the Request For Proposal.

Accompanying this Proposal is a bid bond secured by a corporate surety, or certified check payable to the order of the Department of Transportation, for five percent of the total bid price, which deposit is to be forfeited as liquidated damages in case this bid is accepted and the Design-Build Team shall fail to provide the required payment and performance bonds with the Department of Transportation, under the condition of this proposal, no later than 10:00 am December 1, 2004 as provided in the Standard Specifications; otherwise said deposit will be returned to the Design Build Team.



*State Alternative Delivery
Systems Engineer*



State Contract Officer

PROJECT SPECIAL PROVISIONS

CONTRACT TIME AND LIQUIDATED DAMAGES (Projects with Permits)

The date of availability for this contract is **December 2, 2004** except that work in jurisdictional waters and wetlands shall not begin until a meeting between the DOT, Regulatory Agencies, and the Design Build Team is held as stipulated in the permits contained elsewhere in this proposal. The Design Build Team shall consider this factor in determining the proposed completion date for this project.

The completion date for this contract is defined as the date proposed in the Technical Proposal by the proposer who is awarded the project. The completion date thus proposed shall not be later than **September 15, 2005**.

When observation periods are required by the special provisions, they are not a part of the work to be completed by the completion date and/or intermediate contract times. Should an observation period extend beyond the final completion date, the acceptable completion of the observation period shall be a part of the work covered by the performance and payment bonds.

The liquidated damages for this contract are **Five Hundred Dollars (\$500.00)** per calendar day.

DB1G04

****NOTE** Removed Intermediate Contract Time for Bridge #111**

SPECIAL REQUIREMENTS FOR WORK IN NATIONAL FOREST

7/1/95

In addition to other requirements in this proposal with respect to clearing, erosion control, protection of environment, etc., comply with the following requirements:

1. Comply with the portions of these Special Requirements, entitled "Fire Plan", Clearing Plan" and "Landscape and Erosion Control Plan". Note the fact that merchantable timber within Forest Service Property will become the property of the Contractor.
2. Comply with the following recommendations of the State Fish and Game Department and Forest Service for wildlife and fish management:

The Design Build Team or any subcontractor for the Design Build Team which are employed to provide services for this project shall not discuss employment opportunities or engage the services of any person or persons, now in the employment of the State during the time of this contract, without written consent of the State.

In the event of engagement, the Design Build Team or their subcontractors shall restrict such person or persons from working on any of the Design Build Team's contracted projects in which the person or persons were "formerly involved" while employed by the State. The restriction period shall be for the duration of the contracted project with which the person was involved. *Former Involvement* shall be defined as active participation in any of the following activities:

- Drafting the contract
- Defining the scope of the contract
- Selection of the Design Build Team
- Negotiation of the cost of the contract (including calculating manhours or fees); and
- Administration of the contract.

An exception to these terms may be granted when recommended by the Secretary and approved by the Board of Transportation.

Failure to comply with the terms stated above in this section shall be grounds for termination of this contract and/or not being considered for selection of work on future contracts for a period of one year.

SUBMITTAL OF PROPOSALS

GENERAL

Technical and Price Proposals will be accepted until **4:00 P.M. Local Time on Monday November 22, 2004**, at the office of the Contract Officer, 1020 Birch Ridge Drive, Century Center Complex Bldg. B, Raleigh, NC. No Proposals will be accepted after the time specified.

Proposals shall be submitted in 2 separate, sealed parcels containing the Technical Proposal in one and the Price Proposal in the other parcel.

Construction Management –8 points

- Provide a brief narrative description of the Design Build Team’s proposed plan for performing construction on the project. This description shall include at least the following:
 - A construction organization chart for the project, showing the relationships between functions shown on the chart and the functional relationships with subcontractors.
 - The chart shall indicate how the Design Build Team intends to divide the project into work segments to enable optimum construction performance.
 - The Design Build Team’s plans and procedures to insure timely deliveries of materials to achieve the project schedule.
 - Describe the overall strengths of the construction team and their ability to fulfill the construction management requirements of this project.

Disadvantaged or Minority and Women’s Business Enterprises (DB/MB/WB) – 3 points

- Describe the Design Build Team’s approach to ensuring that DB/MB/WB will have opportunity to participate in the design and in the Construction Engineering and Inspection (CEI) aspect of the project.
- DB/MB/WB firms to be utilized in the design and the CEI work shall be noted in the submittal for this RFP.

Natural Environmental Responsibility – 8 Points

- Describe the Design Build Team’s approach to addressing environmental concerns within the project boundaries.
- Identify innovative approaches to minimize any impacts in environmentally sensitive areas. Describe any temporary impacts and associated minimization approaches.
- Describe how the bridges will be built to minimize impacts.
- Describe the demolition plan for bridges so as to minimize environmental impact.

Design Features – 23 points

- Show plan view of design concepts with key elements noted.
- Identify preliminary horizontal and vertical alignment of all roadway elements.
- Identify the appropriate design criteria for each feature if not provided.
- Describe any Geotechnical investigations to be performed by the Design Build Team.
- Describe how any utility conflicts will be addressed and any special utility design considerations.
- Provide conceptual bridge plans or details
- Identify any special bridge design features to be constructed.
- Discuss the long term durability of the proposed structures.

2. Schedule and Milestones – 30 points

Provide a schedule for the project including both design and construction. The schedule shall show the sequence and continuity of operations, as well as the month of delivery of usable segments of the project.

The schedule shall also include the Design Build Team’s final completion date. **This date shall be clearly indicated on the Project Schedule and labeled “Final Completion Date”.**

3. Innovation – 5 points

Identify any aspects of the design or construction elements that the firm considers innovative. Include a description of alternatives that were considered whether implemented or not.

4. Safety Plan – 5 points

- Specifically describe how residential access will be maintained.
- Describe the safety considerations specific to the project.
- Discuss the Design Build Team's overall approach to safety.

No oral interview will be required for this project. At least one member of the Design Build Team shall be available, by phone, to answer questions related to their Technical Proposal. The representative shall be available **between 1:00 pm and 3:00 pm on November 23, 2004**. The Department will use the information provided in this discussion to assist in the evaluation of the technical proposal.

SELECTION PROCEDURE

There will be a Technical Review Committee (TRC) composed of Project Managers, and three or more senior personnel from involved engineering groups that will evaluate the Technical Proposal on the basis of the criteria provided in the Design Build Package.

The selection of a Design Build Team will involve both technical quality and price. The technical proposals will be presented to the TRC for evaluation. The TRC shall first determine whether the proposals are responsive to the requirements of the Design Build Package. Each responsive technical proposal shall be evaluated based on the rating criteria provided in the Design Build Package. The TRC will submit an overall technical proposal score for each firm to the Manager of the Contract Office. A maximum quality credit percentage will be assigned for each project, as determined by the TRC.

Quality Credit Evaluation Factors for Technical Proposals

Responsiveness to Request for Proposal	60
Schedule and Milestones	30
Innovation	5
Safety Plan	5
Maximum Score	100

ROADWAY DESIGN SCOPE OF WORK**I. DESCRIPTION OF WORK:**

- Design and construct Bridge No. 111 and approaches on US-276 over the East Fork Pigeon River and Bridge No. 180 and approaches on SR 1123 over the West Fork Pigeon River.
- The Design Build Team shall provide a resurfacing grade for the mainline and all affected -Y-lines. The design and construction limits shall be of sufficient length to properly tie into the existing grades and be in accordance with all NCDOT guidelines and standards. The improvements to SR 1123 shall extend a minimum of 100 ft. to the northeast as measured along the new alignment proposed by the Design Build Team.
- US-276 shall meet rural major collector standards with a design speed of 55 mph. SR 1123 shall meet local standards with a design speed of 35 mph. Each project will be designed as a 2-lane roadway with 12-ft. travel lanes.
- The Design Build Team will be responsible for submitting proposed design criteria as part of their Technical Proposal. All design criteria must be approved by the Department prior to submittal of the Preliminary Plans.
- The Design Build Team shall be responsible for submittal of Structure Recommendations, which must be approved by the Department prior to submittal of the Preliminary Plans.
- All Design Exceptions needed shall be clearly noted in the Technical Proposal and will be subject to review and approval by the Department.
- The replacement structure for Bridge No. 111 shall be built as a tangent structure but wide enough to accommodate the current alignment with a painted curve.
- All existing driveway connections shall be maintained during construction.
- If the Design Build Team discovers any unanticipated historic or archeological remains while conducting the authorized work, they shall immediately notify the Archaeology Unit within the Office of Human Environment at (919)-715-1561.

II. GENERAL :

The design shall be in accordance with the 2001 *AASHTO A Policy on Geometric Design of Highways and Streets*, January 2002 *NCDOT Roadway Standard Drawings*, *NCDOT 2002 Roadway Design Manual*, *Roadway Design Policy and Procedure Manual*, *NCDOT Guidelines for Roadway Design Activities*, January 2002 *North Carolina Standard Specifications for Roads and Structures*, and the *AASHTO Roadside Design Guide 2002*.

The Design-Build Team shall identify the need for any special roadway design details (e.g. drainage structures, guardrail, retaining walls, concrete barriers, etc.) and shall

STRUCTURES SCOPE OF WORK**I. DESCRIPTION OF WORK:**

- The Design Build Team shall replace Bridge No. 111 on US-276 over the East Fork Pigeon River and Bridge No. 180 on SR 1123 over the West Fork Pigeon River.
- 12 ft. approach slabs are acceptable. Reinforced Bridge Approach Fills are not required. Weathering steel is required, if applicable. No deck drains will be allowed over water and all bridge/roadway drainage must pass through a vegetative buffer before entering the rivers.
- Bridge No. 111 shall not have bents located in the water. If using a vertical abutment at the south end of this bridge, then a retaining wall or wing wall must be used on the west side of US276 and extend back from the south end of the bridge to fully contain the existing embankment. This wall shall be designed and constructed to minimize impact to the water below and shall be constructed within all permit restrictions.
- Bridge No. 180 shall be realigned towards the southeast as shown on the Preliminary Hydraulic Bridge Survey Report. As an option, a combination of re-alignment and rock cut excavation may be performed. A maximum of two interior bents is allowed; however, these bents may not be located further away from the streambank than the existing northernmost and southernmost interior bent footings. The foundation for the interior bent near the north end of this bridge, if used, shall not be located further downstream than the end of the existing northern abutment wing wall.
- The Design Build Team will be responsible for removal and disposal of the existing substructure and superstructure at both locations. The removal of structures over waterways shall be done in accordance with all permit requirements. The existing abutment at north end of Bridge #180 shall be removed down to the rock line and the abutment at north end of Bridge #111 shall be removed down to one foot below natural ground. All other bents and abutments shall be removed in their entirety. The removal of foundation components below the streambed in water may remain, at the discretion of the Resident Engineer, if needed to minimize disturbance of the streambed. The removal of the interior bents at Bridge #111 may be accomplished by collapsing the bents toward the closest streambank. A rock causeway may be employed between each interior bent and the closest abutment for this purpose. The removal of the bridge deck and barrier rail shall be done using non-shattering methods. The demolition plan shall be clearly described in the Technical Proposal and will be subject to review and approval by the NCDOT.
- All bridges shall meet approved Roadway typical sections and grades. Bridge geometry (width, length, skew, span arrangement, etc.) shall be in accordance with an approved Structure Recommendation prepared by the team.

II. GENERAL:

The team's primary design firm shall be on the Highway Design Branch list of firms qualified for Structure Design and maintain an office in North Carolina.

Design shall be in accordance with the Seventeenth Edition AASHTO *Standard Specifications for Highway Bridges*, NCDOT *Structure Design Manual* (including policy memos), and NCDOT *Bridge Policy Manual*. Construction and Materials shall be in

accordance with 2002 *NCDOT Standard Specifications For Roads and Structures*, *NCDOT Structure Design Unit Project Special Provisions*, and *NCDOT Structure Design Unit Standard Drawings*.

III. **SUBMITTALS:**

Sufficient data, including items previously approved by other NCDOT units (Roadway, Geotechnical, Hydraulics, Traffic, etc.) shall be submitted with (or prior to) all plan submittals to facilitate review.

A. Submittals for Review:

For bridges, the required plan submittals for review are six half size (11”X17”) sets of Preliminary General Drawings and six half size sets of final plans. Two complete sets of project special provisions shall also be submitted for review.

Preliminary General Drawings shall contain sufficient details (drawings or narrative) to explain the scope of design and construction intended for the bridge, and shall list all anticipated special provisions and notes describing design data and material properties (for guidance, refer to *NCDOT Structure Design Manual* Section 5, General Drawings). Final Plans are expected to have all plan details and notes completed for final review. The RFC’s (optional) and Final Plans submittals may be separated into substructure and superstructure or other submittals as necessary to accommodate construction schedules.

All comments by the Department or FHWA on all submittals shall be addressed in writing and by making appropriate changes to designs or drawings before construction of those elements begins. Re-submittal of plans may be required.

B. Submittals for Record Keeping:

One complete full size original set of Final plans shall be submitted to the structure design unit for record keeping along with the complete set of original design files and one complete set of project special provisions. The record plan set, design files, and project special provisions shall bear the seal of a North Carolina registered Professional Engineer.

C. Working Drawing Submittals:

Working drawing submittals shall be in accordance with the “Submittal of Working Drawings” project special provision. Sufficient data shall be submitted prior to or with the working drawings to facilitate review. This data shall include one half- size copy of the appropriate RFC drawing or drawings related to the submittal

D. Other:

The Contractor shall be responsible for all additional copies of structure plans for other units as requested (including but not limited to As Built Plans).

HYDRAULICS DESIGN SCOPE OF WORK

- The Design-Build team shall employ a private engineering firm to perform all hydraulic design work required under this contract. The private engineering firm must be prequalified under the Department's normal pre-qualification procedures prior to bid submission.
- The Design Build Team has been provided with a preliminary Bridge Survey Report and shall prepare a Bridge Survey Report for each structure that is modified from the report provided.
- The Design Build Team is responsible for performing hydraulic analyses to ensure that the proposed bridge designs do not negatively impact the 100 year flood elevation. The Design Build Team shall certify this no-rise condition by completing and sealing the form provided by the Department. The Design Build Team shall submit the hydraulic model runs supporting this certification to the Department.
- Bridge No. 111 is located in a Flood Hazard Zone with a detailed study.
- Bridge No. 180 is located in a Flood Hazard Zone without a detailed study.
- The Design Build Team will be required to provide all necessary drainage design for this project.
- The Design Build Team will be required to comply with all permit requirements. A copy of the permits will be provided to the team.
- All design shall be in accordance with criteria provided in the North Carolina Division of Highways "*Guidelines for Drainage Studies and Hydraulics Design-1999*" and the addendum "*Handbook of Design for Highway Drainage Studies-1973*"

EROSION AND SEDIMENTATION CONTROL SCOPE OF WORK

I. DESCRIPTION OF WORK:

The Design Build Team shall prepare Erosion and Sedimentation Control Plans in accordance with NCDENR-Land Quality's *Erosion and Sediment Control Planning and Design Manual*. The Design Build Team shall also comply with the North Carolina Administrative Code Title 15A Department of Environment and Natural Resources Chapter 4, Sediment Control. Borrow or waste areas that are part of the project will require a separate Erosion and Sedimentation Control Plan, unless the borrow or waste activity is regulated under the *Mining Act of 1971*, or is a landfill regulated by the Division of Solid Waste Management (NCDENR).

The NCDOT Roadside Environmental Unit and/or the Division Environmental Officer must approve all Erosion and Sedimentation Control Plans prior to construction.

Whenever the Engineer determines that significant erosion and sedimentation continues despite the installation of approved protective practices, the Design Build Team will be required to take additional protective action. An approved Erosion and Sedimentation Control Plan Does Not Exempt the Builder from making every effort to contain sediment onsite.

II. GENERAL INFORMATION

- Plans must address any environmental issues raised during the permitting process.
- Utilize adequate perimeter controls. (temporary diversions, silt fence, etc.)
- Provide adequate silt storage for 2400 cubic feet per disturbed acre. Sediment basins should be sized with surface area equal to .01 times the peak inflow rate using 10-peak runoff data .
- Design Riser Basins to the following standards:
 1. Surface Area should be determined by Equation A (sq. ft.) = $Q_{10} \text{ (cfs)} * 435.6$
 2. Riser Pipe should have a cross-sectional area 1.5 times that of the barrel pipe
 3. Perforations in the riser pipe should be reduced to increase dewatering time to twenty-four (24) hours.
- Protect proposed inlets with RIST-A, RIST-C, PIST-A, etc.
- Devices at all drainage turnouts should utilize sediment control stone. (TRSD-B, TRSC-A, etc.)
- Show matting on all ditch lines (non-jurisdictional streams) with 1.25% grade or larger and all cut/fill slopes 2:1 or greater where it is difficult to establish vegetation and/or slope failure is occurring
- Seeding and mulching shall be performed immediately following final grading.
- Sediment and erosion control measures shall follow NCDOT standards for sensitive and high quality watersheds.

- Grubbing and removal of vegetation shall be minimized to the greatest extent possible. Vegetation, native trees and shrubs shall be used to re-vegetate streambanks.

Erosion Control Liquidated Damages:

The Design-Builder shall take all reasonable precaution to comply with all regulations of all authorities having jurisdiction over public and private land governing the protection of erosion and sedimentation. Any fines, remediation required, or charges levied against the Department for failing to comply with all rules and regulations concerning erosion and sediment control, due to the Design-Builder's negligence, carelessness, or failure to implement the erosion and sediment control plan and specifications, will be deducted from monies due the Design-Builder on his contract. In addition to said fines, remediation required, or charges levied, any associated engineering costs or actions taken by the Department in order for the Department to comply with rules and regulations, as a result of the Design-Builder's negligence, carelessness, or failure to implement the erosion and sediment control plan and specifications, will be deducted from the monies due to the Design-Builder.

Open Burning:

Open burning is not permitted on any portion of the right-of-way limits established for this project. Do not burn the clearing, grubbing or demolition debris designated for disposal and generated from the project at locations within the project limits, off the project limits or at any waste or borrow sites in this county. Dispose of the clearing, grubbing and demolition debris by means other than burning, according to state or local rules and regulations.

III. INFORMATION PROVIDED BY NCDOT:

- The NCDOT Roadside Environment Unit (REU) will provide a sample set of Erosion and Sedimentation Control plans (including any special details or special provisions used by the NCDOT REU) and MicroStation Erosion Control tool palette to the Designer/Planner for reference.
- Erosion Control Special Provisions are available at the following web-site:
http://stage.dot.state.nc.us/dohweb/operations/dp_chief_eng/roadside/soil&water/provisions/e&scprov.html