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STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

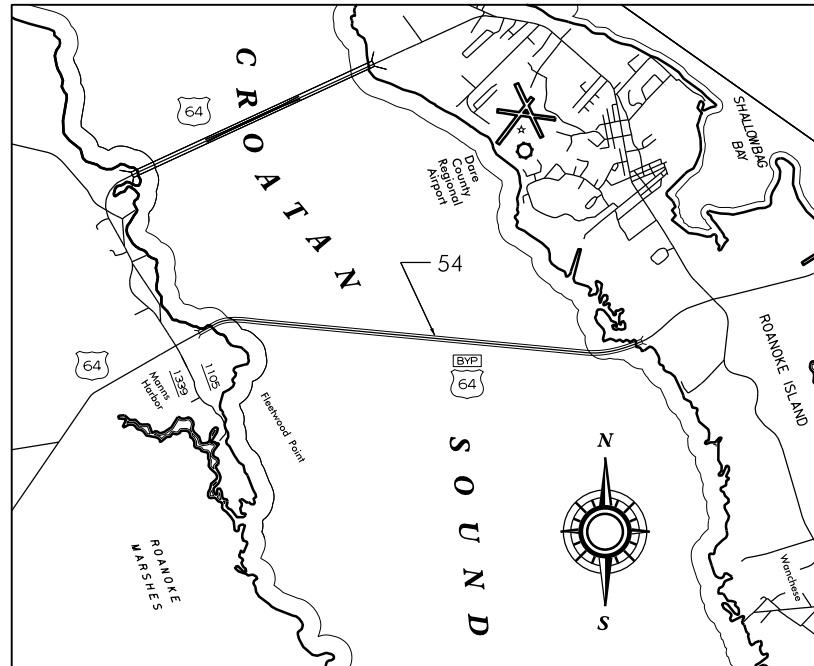
DARE COUNTY



STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	B-5943	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
46482.1.1		P.E.	
46482.3.1		CONST.	

LOCATION: DARE COUNTY:
BRIDGE #54 ON US64 & US264 OVER THE CROATAN SOUND.

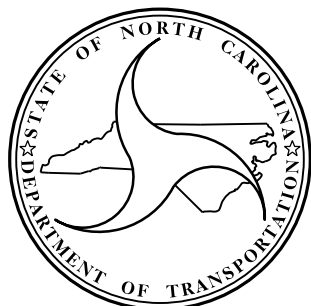
TYPE OF WORK: BRIDGE PRESERVATION WITH CONCRETE BRIDGE DECK CRACK SEAL.



VICINITY MAP - DARE CO.

PROJECT: B-5943

CONTRACT: DA00337



DESIGN DATA

DARE COUNTY
BRIDGE #54 ADT 2010 = 4000

PROJECT LENGTH

DARE COUNTY
BRIDGE #54 = 5.23 MILES

Prepared In the Office of:
**DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**
STRUCTURES MANAGEMENT UNIT - PRESERVATION & REPAIR GROUP
1000 BIRCH RIDGE DR. RALEIGH, N.C. 27610

ERIC B. NELSON, P.E.
PROJECT ENGINEER

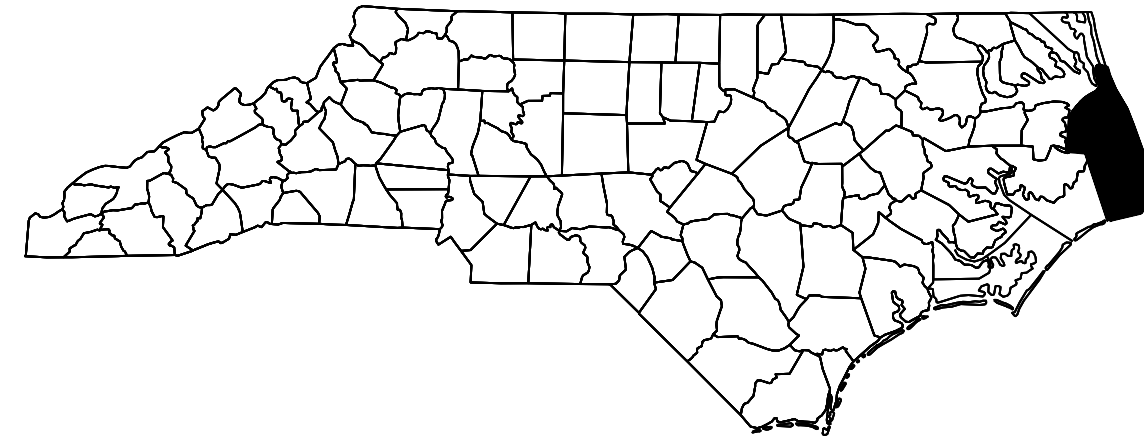
2012 STANDARD SPECIFICATIONS

LETTING DATE:
MAY 17, 2017

DocuSigned by:
T. SHERRILL
AD5031D87294CC...
SEAL
18565
ENGINEER
TIMOTHY M. SHERRILL
3/8/2017
TIMOTHY M. SHERRILL, P.E.
PROJECT DESIGN ENGINEER

PROJECT: B-5943

CONTRACT: DA00337



STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

DARE COUNTY

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	B-5943	1	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	
46482.1.1		P.E.	
46482.3.1		CONST.	

LOCATION: DARE COUNTY:
BRIDGE #54 ON US64 & US264 OVER THE CROATAN SOUND.

TYPE OF WORK: BRIDGE PRESERVATION WITH CONCRETE BRIDGE DECK CRACK SEAL.

INDEX OF SHEETS

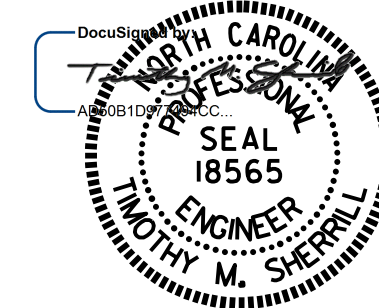
I
IA
S-1
S-2 THRU S-10
SN
TMP-1 THRU TMP-5

TITLE SHEET
INDEX OF SHEETS
TOTAL BILL OF MATERIAL
STRUCTURAL PLANS - DARE #54
STANDARD NOTES
TRAFFIC MANAGEMENT PLANS

TOTAL BILL OF MATERIAL				
MOBILIZATION	VOLUMETRIC MIXER	CONCRETE FOR DECK REPAIR	SHOTBLASTING BRIDGE DECK	CONCRETE BRIDGE DECK CRACK SEALING
LUMP SUM	LUMP SUM	CU.FT.	SO.YDS.	SO.YDS.
LUMP SUM	LUMP SUM	2	193,060	193,060

NOTE: MINIMAL QUANTITIES OF CONCRETE FOR DECK REPAIR ARE ANTICIPATED AND THE QUANTITIES INDICATED ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT INDICATED ON THE DRAWINGS ARE DEEMED NECESSARY BY THE ENGINEER, THE CONTRACTOR SHALL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATION AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUANTITIES ENTERED INTO THE QUANTITY TABLE.

PROJECT NO. B-5943
DARE COUNTY
BRIDGE NO. 54



1/10/2017

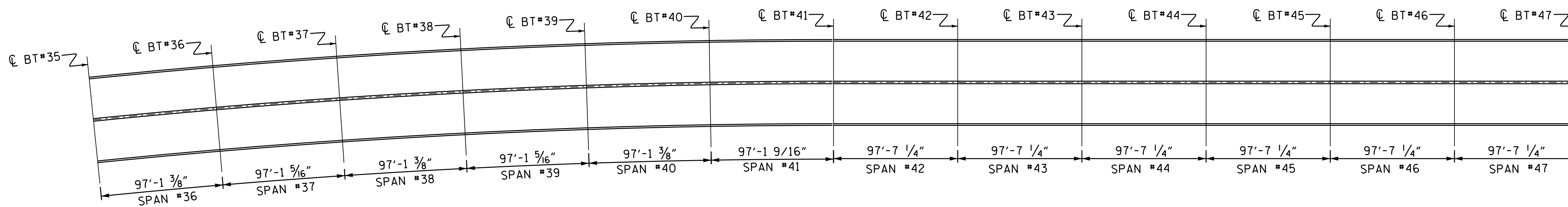
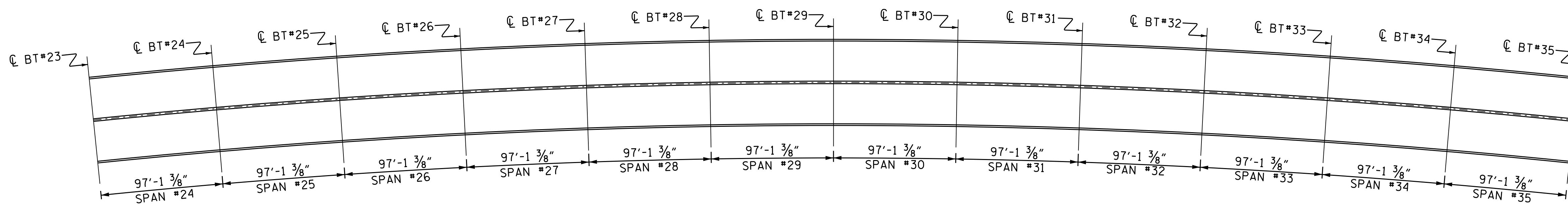
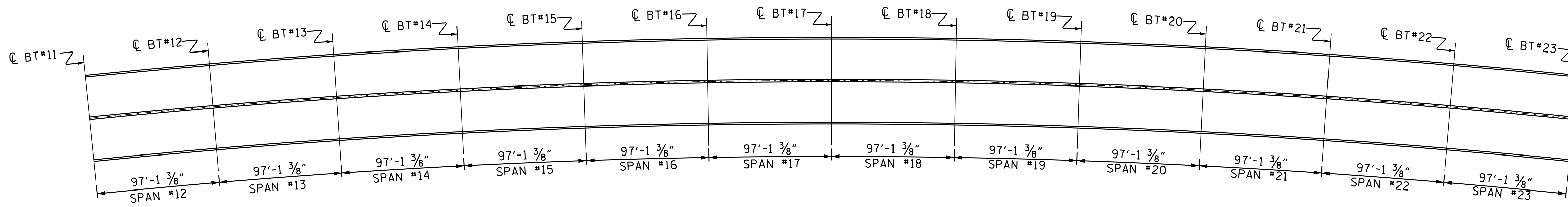
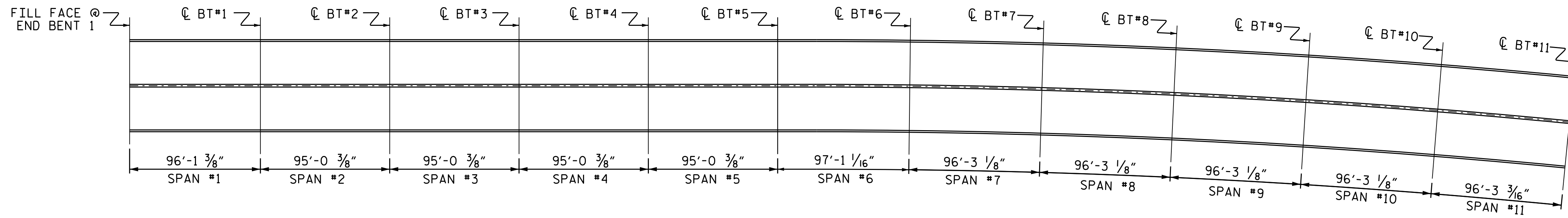
STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION
RALEIGH

TOTAL BILL OF MATERIAL

DRAWN BY : M.A.LEE DATE : 12/2016
CHECKED BY : T.M.SHERRILL DATE : 12/2016

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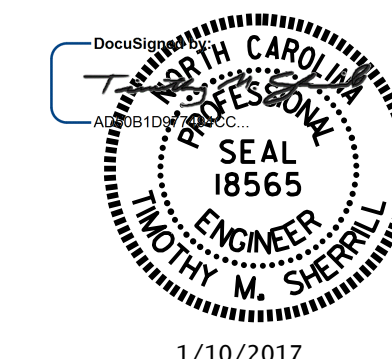
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NO.	BY:	DATE:	NO.	BY:	DATE:	S-1
1			3			TOTAL SHEETS
2			4			10



SCOPE OF WORK
 - SHOTBLAST EXISTING CONCRETE DECK.
 - APPLY CONCRETE DECK CRACK SEALANT.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54

SHEET 1 OF 6

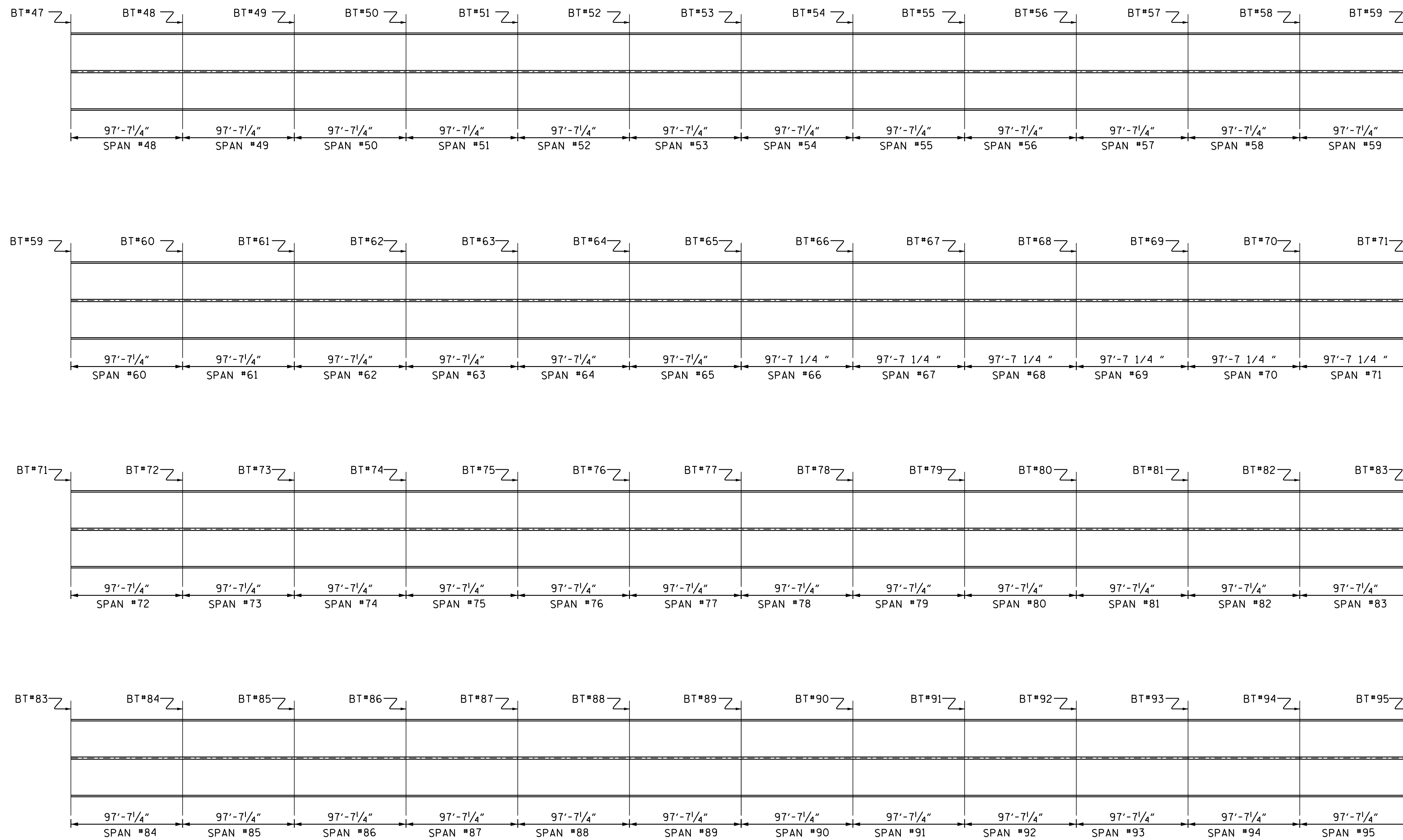


STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 GENERAL DRAWING
 BRIDGE 54 ON US 64
 OVER CHOWAN SOUND

DRAWN BY : P. D. BRYANT DATE : 12/2016
 CHECKED BY : T. M. SHERRILL DATE : 12/2016

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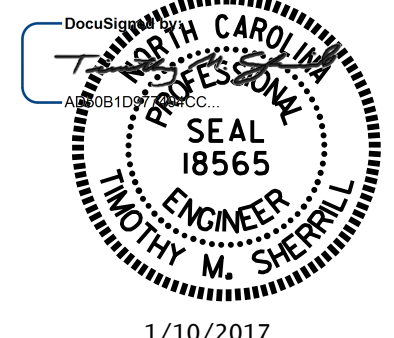
REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-2
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2			4			10



SCOPE OF WORK
 - SHOTBLAST EXISTING CONCRETE DECK.
 - APPLY CONCRETE DECK CRACK SEALANT.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54

SHEET 2 OF 6

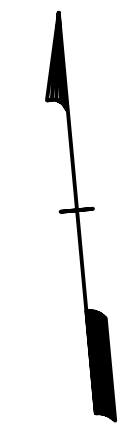
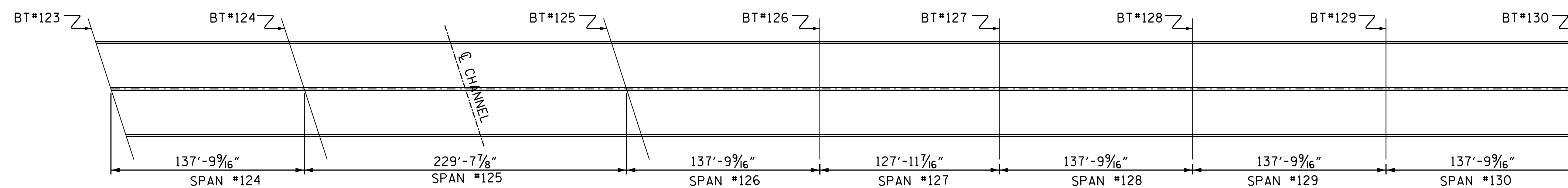
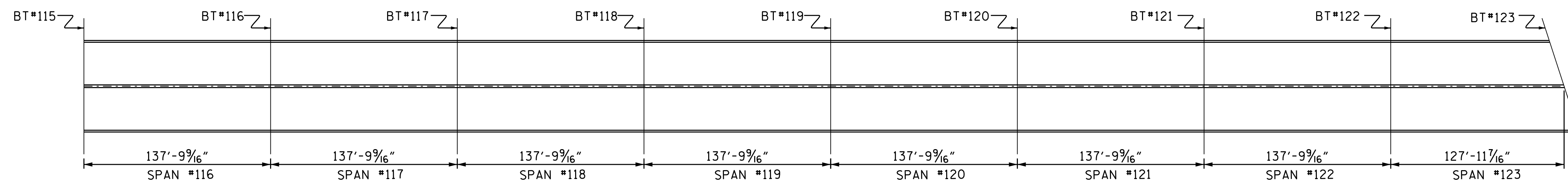
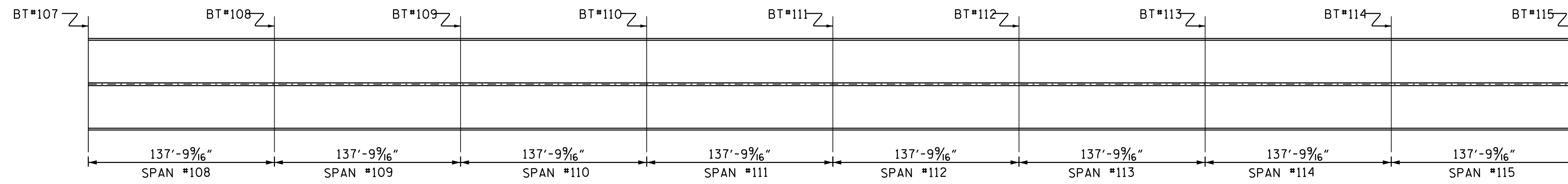
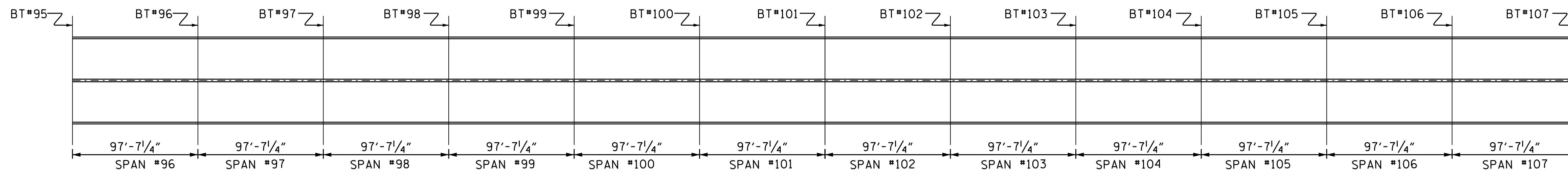


STATE OF NORTH CAROLINA
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 RALEIGH
 GENERAL DRAWING
 BRIDGE 54 ON US 64
 OVER CHOWAN SOUND

DRAWN BY : P. D. BRYANT DATE : 12/2016
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SCOPE OF WORK

- SHOTBLAST EXISTING CONCRETE DECK.
- APPLY CONCRETE DECK CRACK SEALANT.

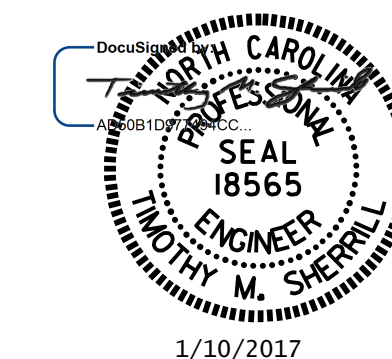
PROJECT NO. B-5943

DARE COUNTY

BRIDGE NO. 54

SHEET 3 OF 6

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 GENERAL DRAWING
 BRIDGE 54 ON US 64
 OVER CHOWAN SOUND

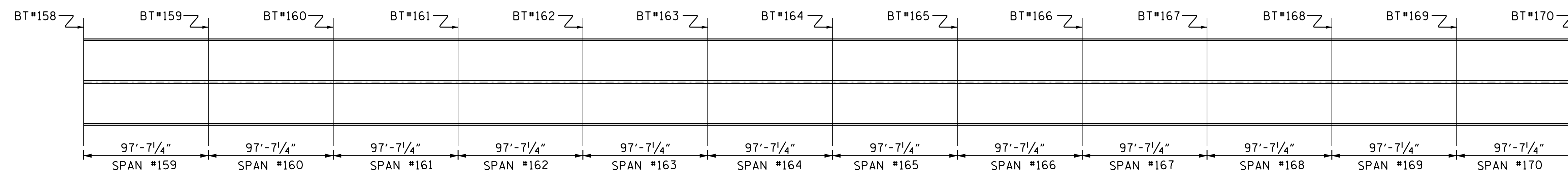
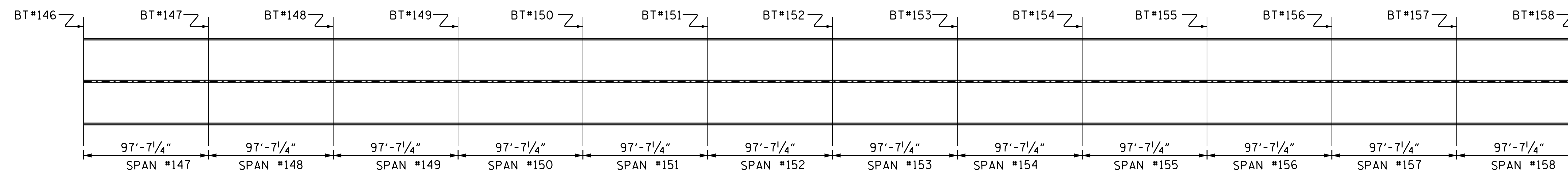
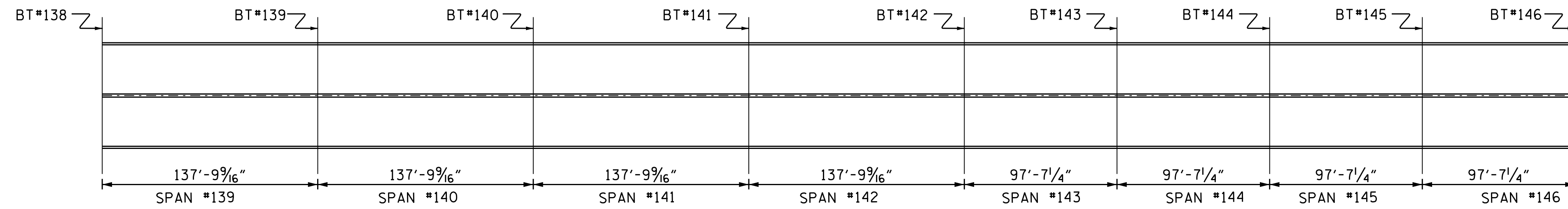
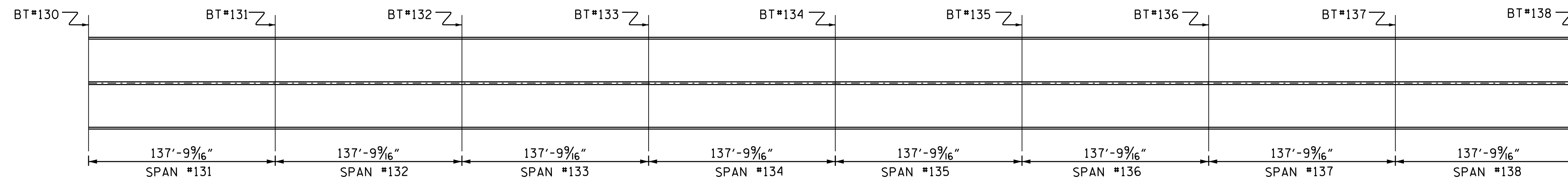


1/10/2017

DRAWN BY : P. D. BRYANT DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

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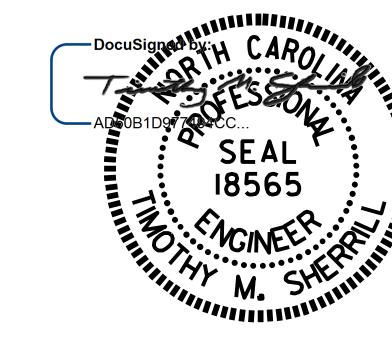
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SCOPE OF WORK
 - SHOTBLAST EXISTING CONCRETE DECK.
 - APPLY CONCRETE DECK CRACK SEALANT.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54

SHEET 4 OF 6



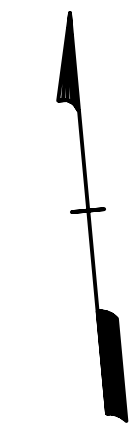
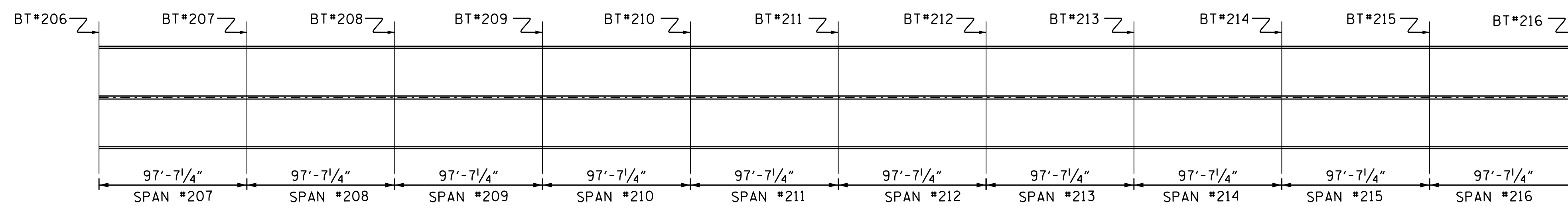
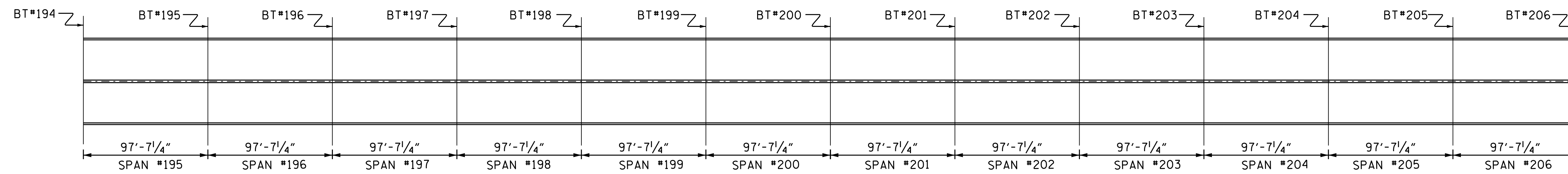
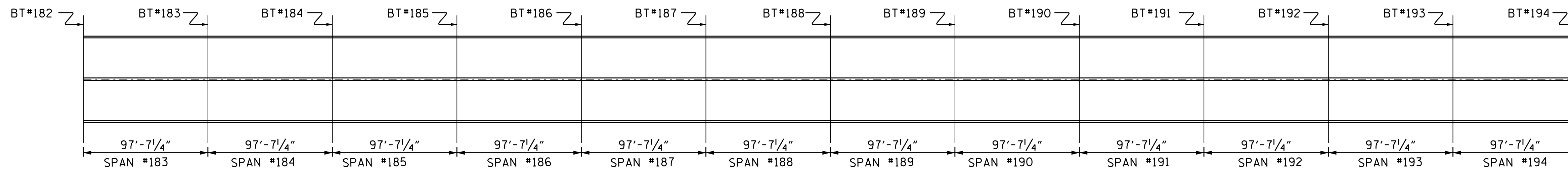
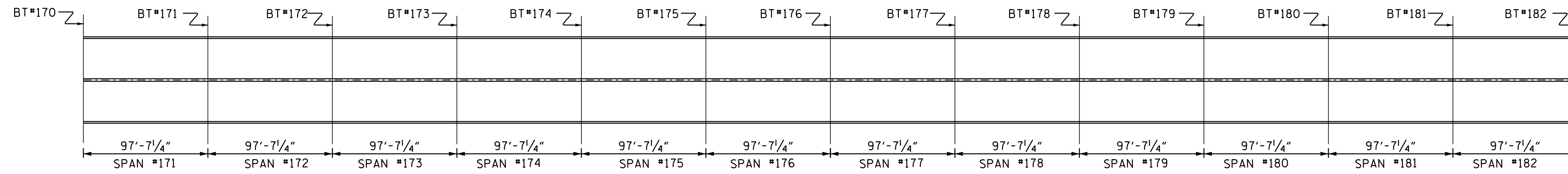
STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**BRIDGE 54 ON US 64
 OVER CHOWAN SOUND**

DRAWN BY : P. D. BRYANT DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

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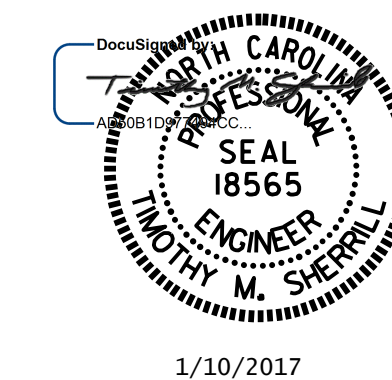


SCOPE OF WORK

- SHOTBLAST EXISTING CONCRETE DECK.
- APPLY CONCRETE DECK CRACK SEALANT.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54

SHEET 5 OF 6



1/10/2017

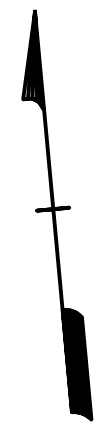
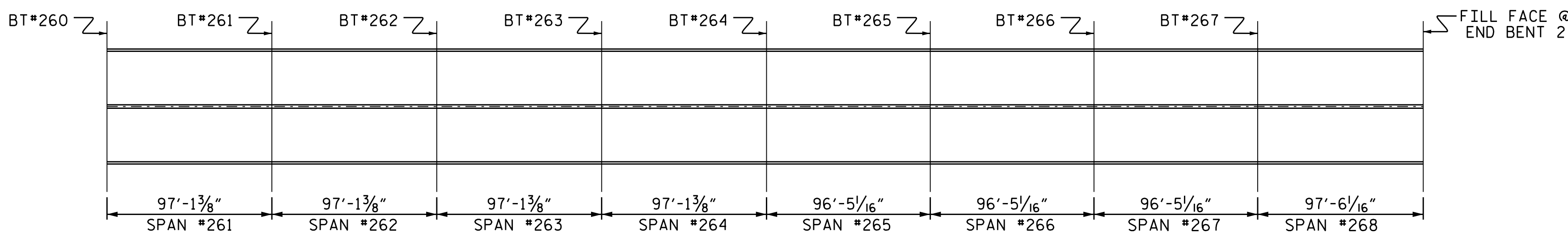
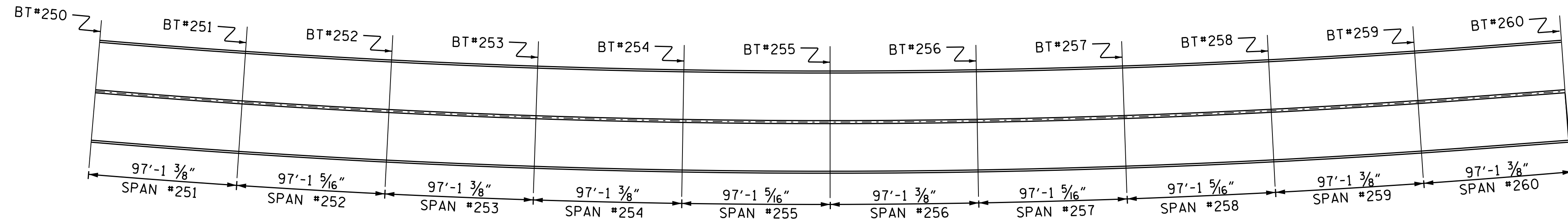
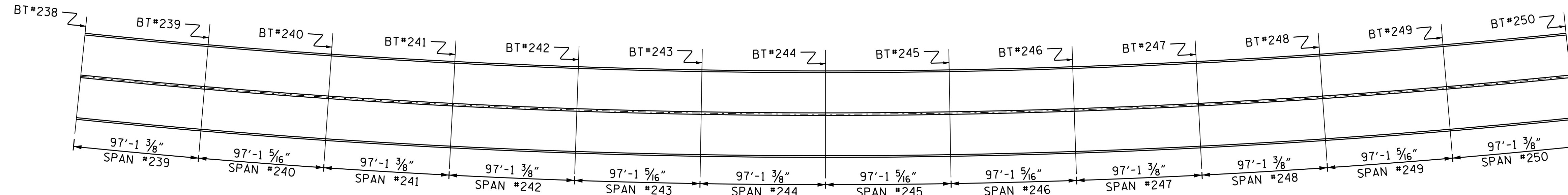
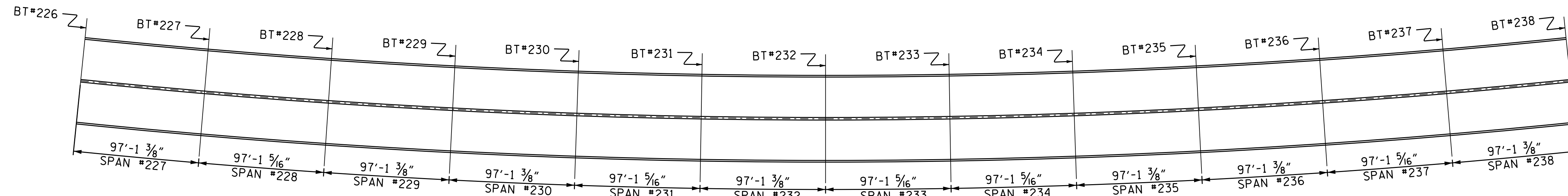
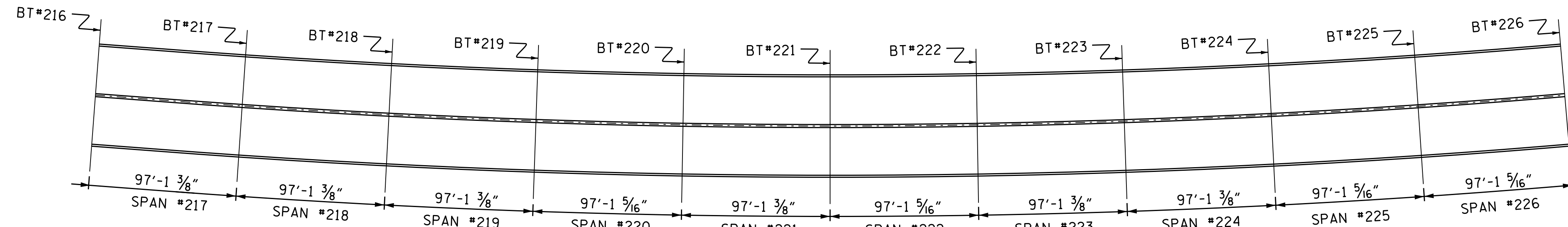
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 DEPARTMENT OF TRANSPORTATION
 RALEIGH

BRIDGE 54 ON US 64
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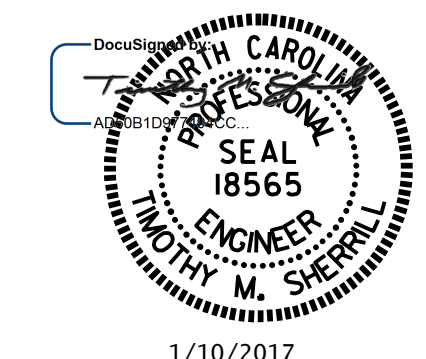
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2			4			10



SCOPE OF WORK
 - SHOTBLAST EXISTING CONCRETE DECK.
 - APPLY CONCRETE DECK CRACK SEALANT.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54

SHEET 6 OF 6



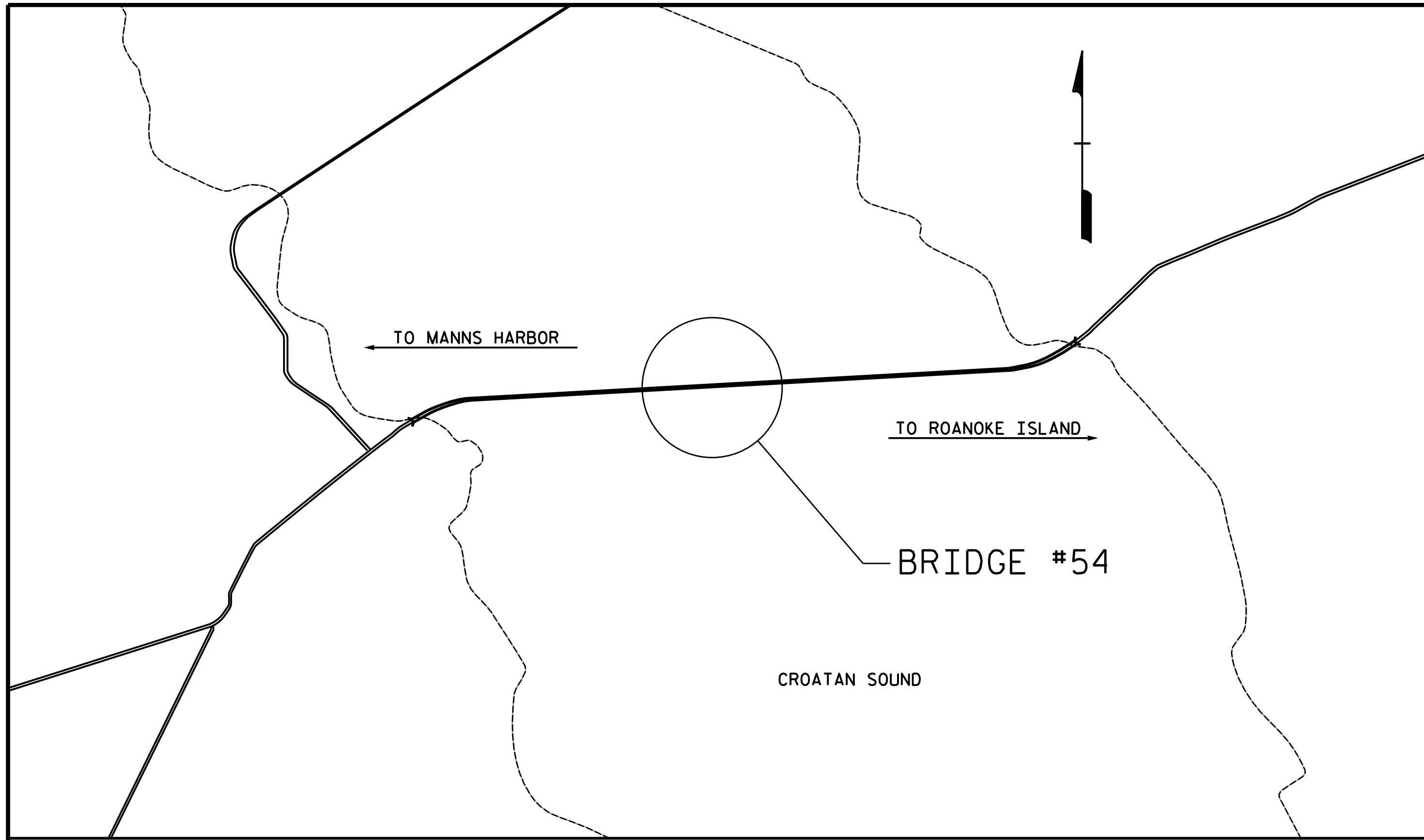
STATE OF NORTH CAROLINA
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 RALEIGH

**BRIDGE 54 ON US 64
 OVER CHOWAN SOUND**

DRAWN BY : P. D. BRYANT DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

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REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	S-7
1			3			TOTAL SHEETS
2			4			10



LOCATION SKETCH

INFORMATION INDICATED ON THE LOCATION SKETCH SHALL BE CONSIDERED GENERAL INFORMATION ONLY. CONTRACTOR SHALL CONFIRM, THROUGH OTHER SOURCES, SPECIFIC INFORMATION REGARDING THE BRIDGES, ROADWAYS, UTILITIES, THE SURROUNDING AREA, AND ANY OTHER ASPECTS THAT MAY BE NECESSARY TO PERFORM AND COMPLETE THE PROJECT.

GENERAL NOTES

EXISTING DIMENSIONS AND BRIDGE CONDITION ARE FROM BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL FIELD VERIFY THE INFORMATION SHOWN ON THE PLANS AND NOTIFY THE ENGINEER IF ACTUAL DIMENSIONS AND CONDITIONS DIFFER.

EXISTING JOINTS AND DECK DRAINS SHALL BE SEALED PRIOR TO BEGINNING SURFACE PREPARATION OF BRIDGE DECK.

THE CONTRACTOR SHALL PROVIDE A METHOD OF HANDLING UNEXPECTED BLOW THROUGH OF THE DECK.

FOR SHOTBLASTING BRIDGE DECK, SEE SPECIAL PROVISIONS.

FOR CONCRETE BRIDGE DECK CRACK SEALING, SEE SPECIAL PROVISIONS.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

FOR GROUT FOR STRUCTURES, SEE SPECIAL PROVISIONS.

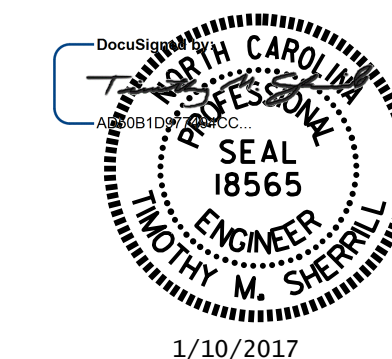
FOR CONCRETE FOR DECK REPAIRS, SEE SPECIAL PROVISIONS.

FOR VOLUMETRIC MIXER, SEE SPECIAL PROVISIONS.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.

FOR CONTROL OF TRAFFIC AND LIMITS ON PHASING OF CONSTRUCTION, SEE TRANSPORTATION MANAGEMENT PLANS.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH
 GENERAL DRAWING
 BRIDGE #54 ON US 64
 OVER CROATAN SOUND

DRAWN BY : M.A. LEE DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

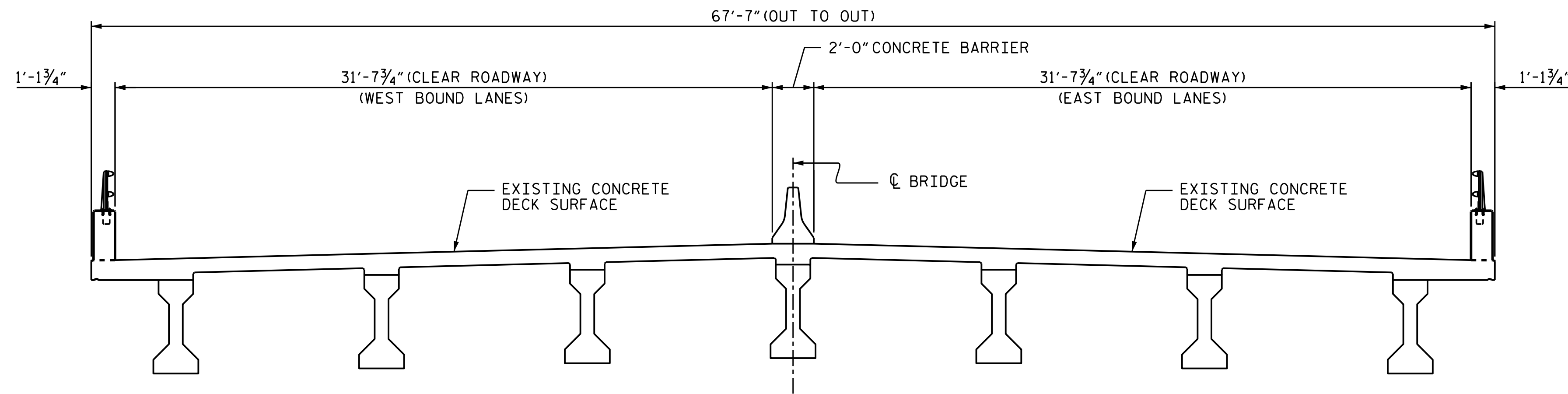
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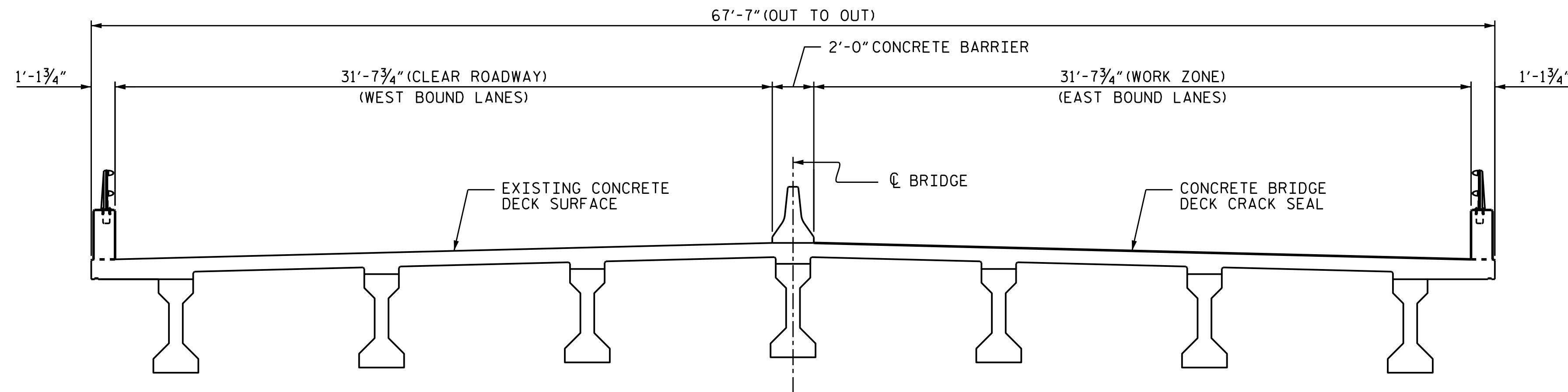
NOTE:

THE WORK STAGING ON THIS PLAN SHEET INDICATES THAT THE EAST BOUND LANE CONCRETE BRIDGE DECK CRACK SEAL IS PERFORMED FIRST, FOLLOWED BY THE WEST BOUND CONCRETE BRIDGE DECK CRACK SEAL WORK. THE CONTRACTOR MAY ELECT TO SEQUENCE THE WORK DIFFERENTLY, BUT THE DIMENSIONS OF THE WORK ZONE AND CLEAR ROADWAY AREAS SHALL MATCH THAT INDICATED ON THIS PLAN SHEET, RESPECTIVE TO THE LANE WHERE THE CONCRETE BRIDGE DECK CRACK SEAL IS BEING PERFORMED.



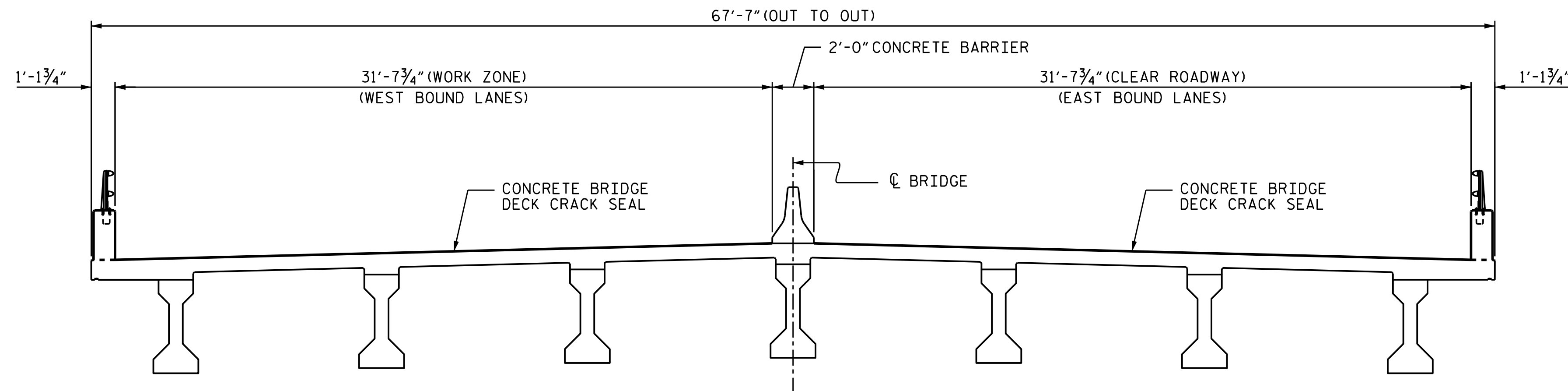
TYPICAL SECTION

(EXISTING - LOOKING EAST)
(SPAN 1 SHOWN - OTHERS SIMILAR)



TYPICAL SECTION

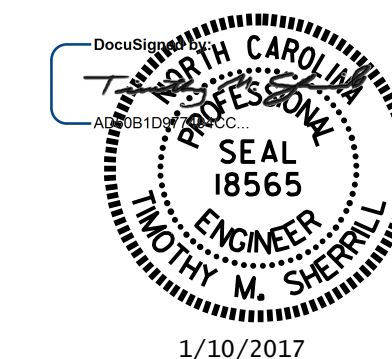
(WEST BOUND WORK - LOOKING EAST)
(SPAN 1 SHOWN - OTHERS SIMILAR)



TYPICAL SECTION

(EAST BOUND WORK - LOOKING EAST)
(SPAN 1 SHOWN - OTHERS SIMILAR)

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

**TYPICAL SECTION
 AND DECK CRACK SEAL
 PREPARATION**

DRAWN BY : M.A. LEE DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

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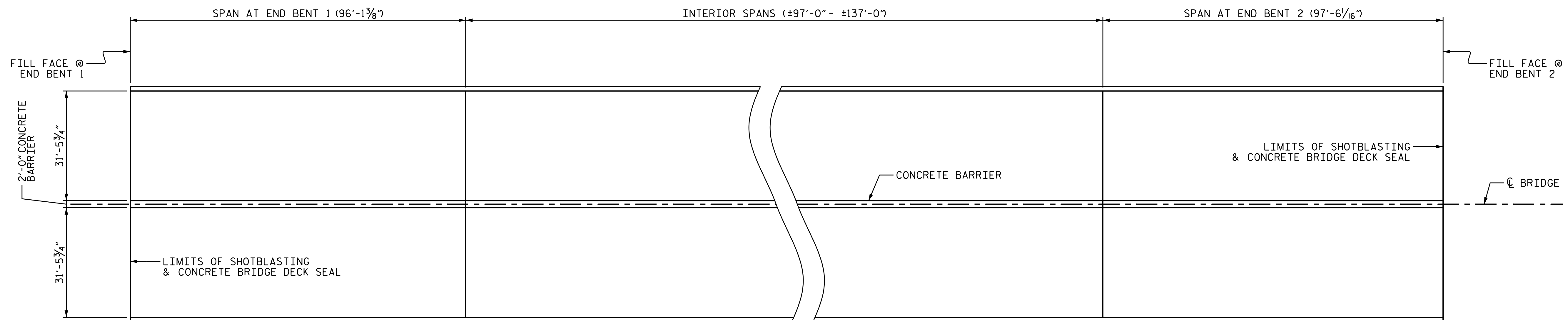
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NO.	BY:	DATE:	NO.	BY:	DATE:	S-9
1			3			TOTAL SHEETS
2			4			10

SUMMARY OF QUANTITIES

	ESTIMATE	ACTUAL
SHOTBLASTING BRIDGE DECK	193,060 S.Y.	
CONCRETE FOR DECK REPAIR	2 C.F.	
CONCRETE BRIDGE DECK CRACK SEAL	193,060 S.Y.	

NOTES:

MINIMAL QUANTITIES OF CONCRETE FOR DECK REPAIR ARE ANTICIPATED AND THE QUANTITIES INDICATED ARE GIVEN WITH THE BEST INFORMATION AVAILABLE. IF ADDITIONAL REPAIRS NOT INDICATED ON THE DRAWINGS ARE DEEMED NECESSARY BY THE INSPECTOR OR THE ENGINEER, THE CONTRACTOR SHALL NOTE ON THE DRAWINGS THE APPROXIMATE LOCATION AND DESCRIPTION OF THE REPAIRS AND ADJUST THE ACTUAL QUANTITIES ENTERED INTO THE QUANTITY TABLE.

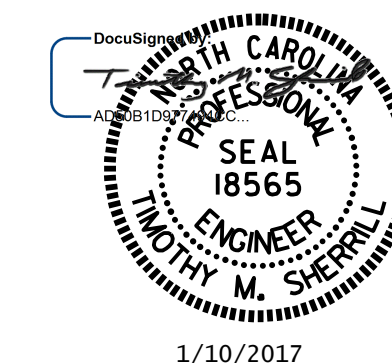


PLAN

ANTICIPATED LOCATIONS FOR CONCRETE REPAIR

SPAN	AREA
48	1 S.F.
58	1 S.F.
71	2 S.F.
82	1 S.F.
86	1 S.F.
92	6 S.F.
124	1 S.F.
144	3 S.F.
193	1 S.F.
231	1 S.F.

PROJECT NO. B-5943
DARE COUNTY
 BRIDGE NO. 54



STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

PREPARATION FOR
 CONCRETE BRIDGE
 DECK CRACK SEAL

DRAWN BY : M.A. LEE DATE : 12/2016
 CHECKED BY : T.M. SHERRILL DATE : 12/2016

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REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	TOTAL SHEETS
1			3			S-10
2			4			10

STANDARD NOTES

DESIGN DATA:

SPECIFICATIONS	-----	A.A.S.H.T.O. (CURRENT)
LIVE LOAD	-----	SEE PLANS
IMPACT ALLOWANCE	-----	SEE A.A.S.H.T.O.
STRESS IN EXTREME FIBER OF		
STRUCTURAL STEEL - AASHTO M270 GRADE 36	-	20,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50W	-	27,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50	-	27,000 LBS. PER SQ. IN.
REINFORCING STEEL IN TENSION		
GRADE 60	--	24,000 LBS. PER SQ. IN.
CONCRETE IN COMPRESSION	-----	1,200 LBS. PER SQ. IN.
CONCRETE IN SHEAR	-----	SEE A.A.S.H.T.O.
STRUCTURAL TIMBER - TREATED OR		
UNTREATED - EXTREME FIBER STRESS	-----	1,800 LBS. PER SQ. IN.
COMPRESSION PERPENDICULAR TO GRAIN OF TIMBER	-----	375 LBS. PER SQ. IN.
EQUIVALENT FLUID PRESSURE OF EARTH	-----	30 LBS. PER CU. FT. (MINIMUM)

MATERIAL AND WORKMANSHIP:

EXCEPT AS MAY OTHERWISE BE SPECIFIED ON PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE 2012 "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" OF THE N. C. DEPARTMENT OF TRANSPORTATION.

STEEL SHEET PILING FOR PERMANENT OR TEMPORARY APPLICATIONS SHALL BE HOT ROLLED.

CONCRETE:

UNLESS OTHERWISE REQUIRED ON PLANS, CLASS A CONCRETE SHALL BE USED FOR ALL PORTIONS OF ALL STRUCTURES WITH THE EXCEPTION THAT: CLASS AA CONCRETE SHALL BE USED IN BRIDGE SUPERSTRUCTURES, ABUTMENT BACKWALLS, AND APPROACH SLABS; AND CLASS B CONCRETE SHALL BE USED FOR SLOPE PROTECTION AND RIP RAP.

CONCRETE CHAMFERS:

UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED CORNERS ON STRUCTURES SHALL BE CHAMFERED 3/4" WITH THE FOLLOWING EXCEPTIONS: TOP CORNERS OF CURBS MAY BE ROUNDED TO 1-1/2" RADIUS WHICH IS BUILT INTO CURB FORMS; CORNERS OF TRANSVERSE FLOOR EXPANSION JOINTS SHALL BE ROUNDED WITH A 1/4" FINISHING TOOL UNLESS OTHERWISE REQUIRED ON PLANS; AND CORNERS OF EXPANSION JOINTS IN THE ROADWAY FACES AND TOPS OF CURBS AND SIDEWALKS SHALL BE ROUNDED TO A 1/4" RADIUS WITH A FINISHING STONE OR TOOL UNLESS OTHERWISE REQUIRED ON PLANS.

DOWELS:

DOWELS WHEN INDICATED ON PLANS AS FOR CULVERT EXTENSIONS, SHALL BE EMBEDDED AT LEAST 12" INTO THE OLD CONCRETE AND GROUTED INTO PLACE WITH 1:2 CEMENT MORTAR.

ALLOWANCE FOR DEAD LOAD DEFLECTION, SETTLEMENT, ETC. IN CASTING SUPERSTRUCTURES:

BRIDGES SHALL BE BUILT ON THE GRADE OR VERTICAL CURVE SHOWN ON PLANS. SLABS, CURBS AND PARAPETS SHALL CONFORM TO THE GRADE OR CURVE.
ALL DIMENSIONS WHICH ARE GIVEN IN SECTION AND ARE AFFECTED BY DEAD LOAD DEFLECTIONS ARE DIMENSIONS AT CENTER LINE OF BEARING UNLESS OTHERWISE NOTED ON PLANS. IN SETTING FORMS FOR STEEL BEAM BRIDGES AND PRESTRESSED CONCRETE GIRDER BRIDGES, ADJUSTMENTS SHALL BE MADE DUE TO THE DEAD LOAD DEFLECTIONS FOR THE ELEVATIONS SHOWN. WHERE BLOCKS ARE SHOWN OVER BEAMS FOR BUILDING UP TO THE SLAB, THE VERTICAL DIMENSIONS OF THE BLOCKS SHALL BE ADJUSTED BETWEEN BEARINGS TO COMPENSATE FOR DEAD LOAD DEFLECTIONS, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER. WHERE BOTTOM OF SLAB IS IN LINE WITH BOTTOM OF TOP FLANGES, DEPTH OF SLAB BETWEEN BEARINGS SHALL BE ADJUSTED TO COMPENSATE FOR DEAD LOAD DEFLECTION, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER.
IN SETTING FALSEWORK AND FORMS FOR REINFORCED CONCRETE SPANS, AN ALLOWANCE SHALL BE MADE FOR DEAD LOAD DEFLECTIONS, SETTLEMENT OF FALSEWORK, AND PERMANENT CAMBER WHICH SHALL BE PROVIDED FOR IN ADDITION TO THE ELEVATIONS SHOWN. AFTER REMOVAL OF THE FALSEWORK, THE FINISHED STRUCTURES SHALL CONFORM TO THE PROFILE AND ELEVATIONS SHOWN ON THE PLANS AND CONSTRUCTION ELEVATIONS FURNISHED BY THE ENGINEER.
DETAILED DRAWINGS FOR FALSEWORK OR FORMS FOR BRIDGE SUPERSTRUCTURE AND ANY STRUCTURE OR PARTS OF A STRUCTURE AS NOTED ON THE PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE CONSTRUCTION OF THE FALSEWORK OR FORMS IS STARTED.

REINFORCING STEEL:

ALL REINFORCING STEEL SHALL BE DEFORMED. DIMENSIONS RELATIVE TO PLACEMENT OF REINFORCING ARE TO CENTERS OF BARS UNLESS OTHERWISE INDICATED IN THE PLANS. DIMENSIONS ON BAR DETAILS ARE TO CENTERS OF BARS OR ARE OUT TO OUT AS INDICATED ON PLANS.
WIRE BAR SUPPORTS SHALL BE PROVIDED FOR REINFORCING STEEL WHERE INDICATED ON THE PLANS. WHEN BAR SUPPORT PIECES ARE PLACED IN CONTINUOUS LINES, THEY SHALL BE SO PLACED THAT THE ENDS OF THE SUPPORTING WIRES SHALL BE LAPPED TO LOCK LEGS ON ADJOINING PIECES.

STRUCTURAL STEEL:

AT THE CONTRACTOR'S OPTION, HE MAY SUBSTITUTE 7/8" Ø SHEAR STUDS FOR THE 3/4" Ø STUDS SPECIFIED ON THE PLANS. THIS SUBSTITUTION SHALL BE MADE AT THE RATE OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS, AND STUD SPACING CHANGES SHALL BE MADE AS NECESSARY TO PROVIDE THE SAME EQUIVALENT NUMBER OF 7/8" Ø STUDS ALONG THE BEAM AS SHOWN FOR 3/4" Ø STUDS BASED ON THE RATIO OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS. STUDS OF THE LENGTH SPECIFIED ON THE PLANS MUST BE PROVIDED. THE MAXIMUM SPACING SHALL BE 2'-0".
EXCEPT AT THE INTERIOR BEARING SUPPORTS OF CONTINUOUS BEAMS WHERE THE COVER PLATE IS IN CONTACT WITH BEARING PLATE, THE CONTRACTOR MAY, AT HIS OPTION, SUBSTITUTE FOR THE COVER PLATES DESIGNATED ON THE PLANS COVER PLATES OF THE EQUIVALENT AREA PROVIDED THESE PLATES ARE AT LEAST 5/16" IN THICKNESS AND DO NOT EXCEED A WIDTH EQUAL TO THE FLANGE WIDTH LESS 2" OR A THICKNESS EQUAL TO 2 TIMES THE FLANGE THICKNESS. THE SIZE OF FILLET WELDS SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT ANSI/AASHTO/AWS "BRIDGE WELDING CODE". ELECTROSLAG WELDING WILL NOT BE PERMITTED.
WITH THE SOLE EXCEPTION OF EDGES AT SURFACES WHICH BEAR ON OTHER SURFACES, ALL SHARP EDGES AND ENDS OF SHAPES AND PLATES SHALL BE SLIGHTLY ROUNDED BY SUITABLE MEANS TO A RADIUS OF APPROXIMATELY 1/16 INCH OR EQUIVALENT FLAT SURFACE AT A SUITABLE ANGLE PRIOR TO PAINTING, GALVANIZING, OR METALLIZING.

HANDRAILS AND POSTS:

METAL STANDARDS AND FACES OF THE CONCRETE END POSTS FOR THE METAL RAIL SHALL BE SET NORMAL TO THE GRADE OF THE CURB, UNLESS OTHERWISE SHOWN ON PLANS. THE METAL RAIL AND TOPS OF CONCRETE POSTS USED WITH THE ALUMINUM RAIL SHALL BE BUILT PARALLEL TO THE GRADE OF THE CURB.
METAL HANDRAILS SHALL BE IN ACCORDANCE WITH THE PLANS. RAILS SHALL BE AS MANUFACTURED FOR BRIDGE RAILING. CASTINGS SHALL BE OF A UNIFORM APPEARANCE. FINIS AND OTHER DEFORMATIONS RESULTING FROM CASTING OR OTHERWISE SHALL BE REMOVED IN A MANNER SO THAT A UNIFORM COLORING OF THE COMPLETED CASTING SHALL BE OBTAINED. CASTINGS WITH DISCOLORATIONS OR OF NON-UNIFORM COLORING WILL NOT BE ACCEPTED. CERTIFIED MILL REPORTS ARE REQUIRED FOR METAL RAILS AND POSTS.

SPECIAL NOTES:

GENERALLY, IN CASE OF DISCREPANCY, THIS STANDARD SHEET OF NOTES SHALL GOVERN OVER THE SPECIFICATIONS, BUT THE REMAINDER OF THE PLANS SHALL GOVERN OVER NOTES HEREON, AND SPECIAL PROVISIONS SHALL GOVERN OVER ALL. SEE SPECIFICATIONS ARTICLE 105-4.

ENGLISH

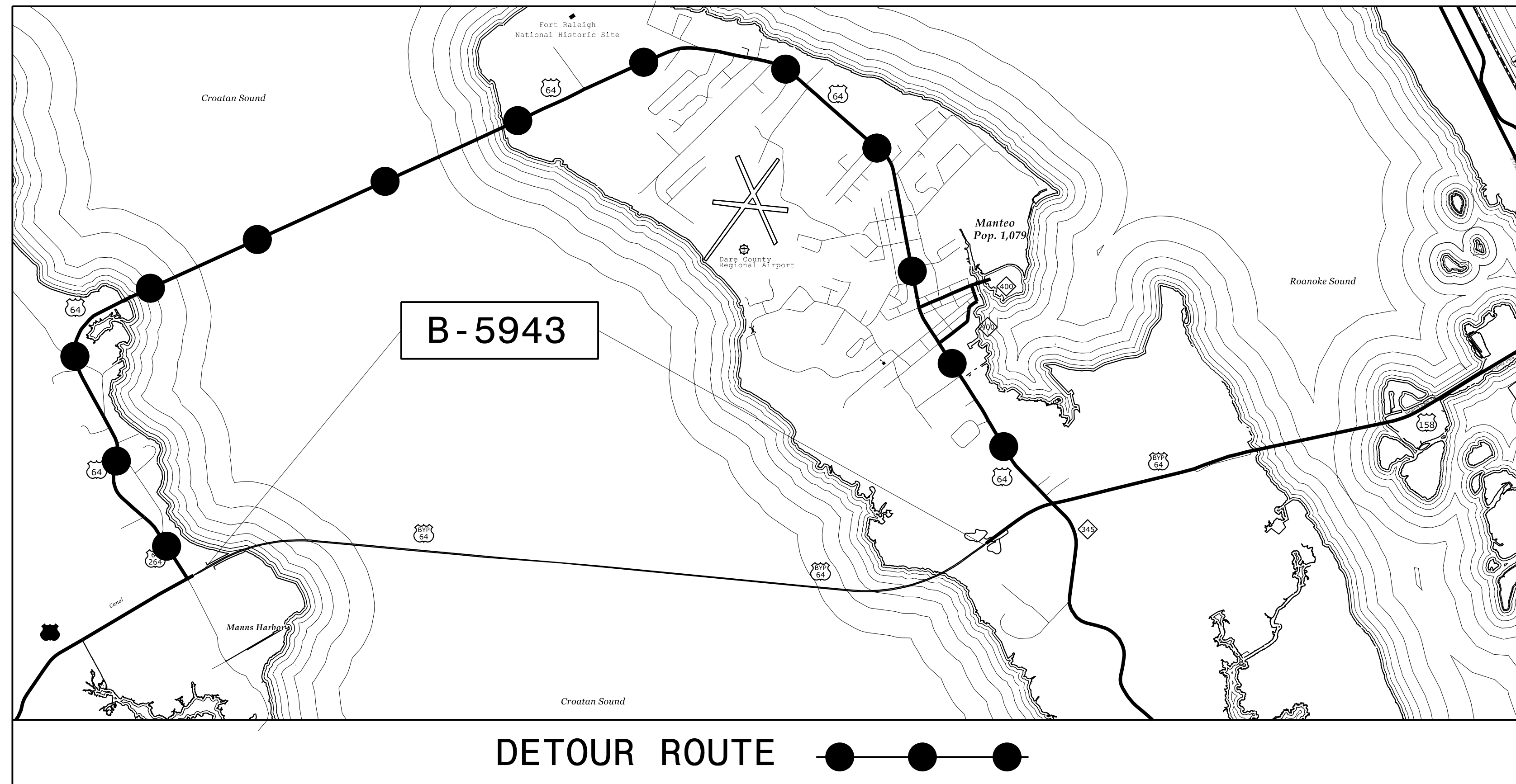
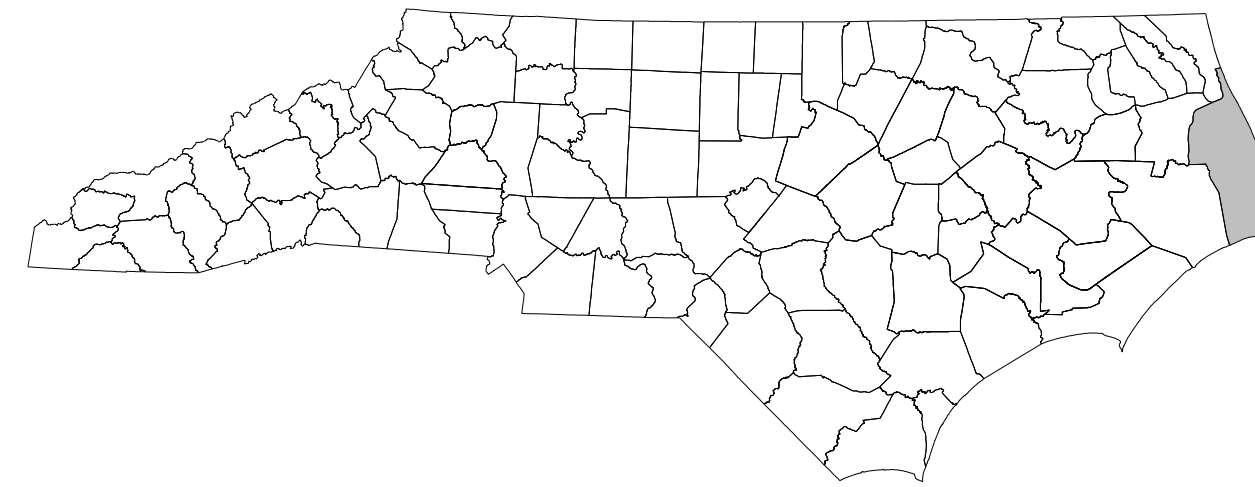
JANUARY, 1990

STD. NO. SN

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

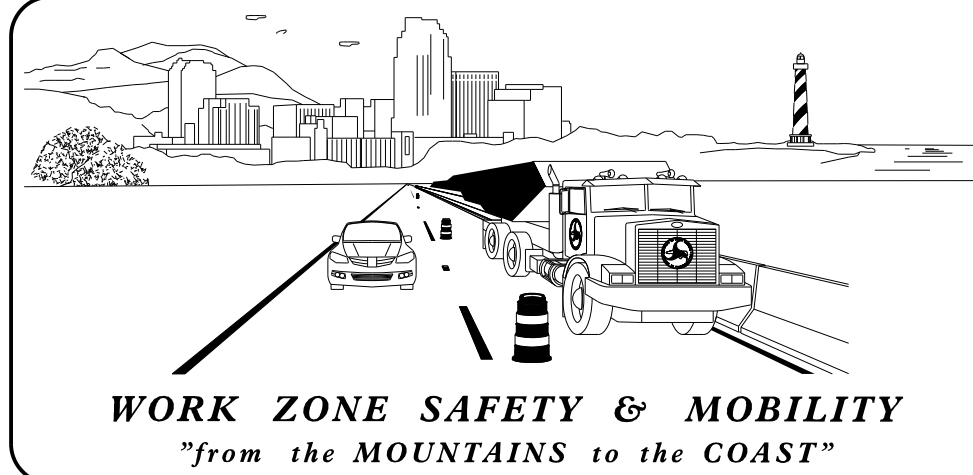
TRANSPORTATION MANAGEMENT PLAN

DARE COUNTY



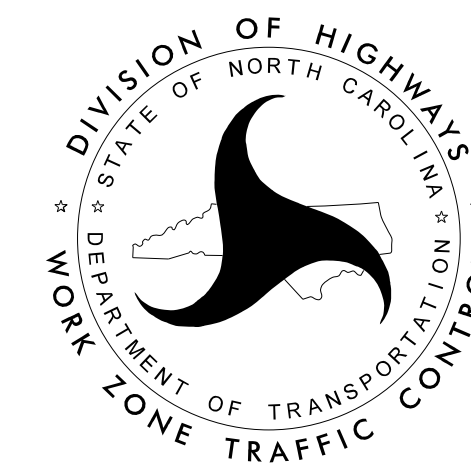
LOCATION: BRIDGE #54 ON US 64 BYPASS IN DARE COUNTY

TYPE OF WORK: BRIDGE PRESERVATION
(SHOTCRETE REPAIRS, SUBSTRUCTURE REPAIR,
AND DECK SEALING)



N.C.D.O.T. WORK ZONE TRAFFIC CONTROL
1561 MAIL SERVICE CENTER (MSC) RALEIGH, NC 27699-1561
750 N. GREENFIELD PARKWAY, GARNER, NC 27529 (DELIVERY)
PHONE: (919) 773-2800 FAX: (919) 771-2745

J. S. BOURNE, P.E. STATE TRAFFIC MANAGEMENT ENGINEER
STEVE KITE, P.E. TRAFFIC CONTROL PROJECT ENGINEER
MATT SPRINGER, P.E. TRAFFIC CONTROL PROJECT DESIGN ENGINEER
SPENCER JENNINGS TRAFFIC CONTROL DESIGN ENGINEER



INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP, AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, MANAGEMENT STRATEGIES AND LEGEND
TMP-2	TRANSPORTATION OPERATIONS PLAN: (GENERAL NOTES, AND PHASING)
TMP-3	US 64 BYP OFF-SITE DETOUR
TMP-4	US 64 EAST BYP ROAD CLOSURE DETAIL
TMP-5 & TMP-6	US 64 WEST BYP ROAD CLOSURE DETAIL

SHEET NO.
TMP-1

B-5943

TIP PROJECT:

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UNLESS ALL SIGNATURES COMPLETED

APPROVED: *Steve Kite*

DATE: 1/31/2017

SEAL



1/26/2017 R:\Traffic\TrafficControl\CP\B-5943_TC_TMP_Ldgn User: sbjennings

ROADWAY STANDARD DRAWINGS

THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.03	TEMPORARY ROAD CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.06	WARNING SIGNS FOR BLASTING ZONES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	POSITIVE PROTECTION
1180.01	SKINNY-DRUM
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTI-LANE ROADWAYS
1205.03	PAVEMENT MARKINGS - EXITS AND ENTRANCE RAMP
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1205.10	PAVEMENT MARKINGS - SCHOOL AREAS
1205.11	PAVEMENT MARKINGS - RAILROAD CROSSINGS
1205.12	PAVEMENT MARKINGS - BRIDGES
1205.13	PAVEMENT MARKINGS - LANE REDUCTIONS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1261.01	GUARDRAIL AND BARRIER DELINEATORS - INSTALLATION SPACING
1261.02	GUARDRAIL AND BARRIER DELINEATORS - TYPES AND MOUNTING
1262.01	GUARDRAIL END DELINEATION
1264.01	OBJECT MARKERS - TYPES
1264.02	OBJECT MARKERS - INSTALLATION

MANAGEMENT STRATEGIES

THIS PROJECT WILL REHABILITATE BRIDGE #54 ON US 64 BYPASS IN DARE COUNTY.

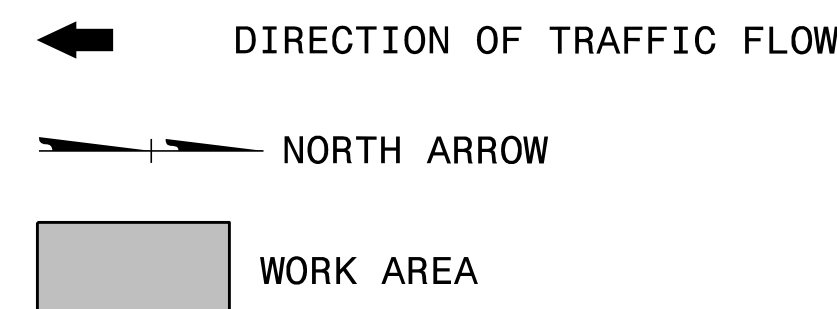
FOR THE BRIDGE SUBSTRUCTURE REPAIR TRAFFIC WILL BE MAINTAINED BY TEMPORARY SHORT TERM SINGLE LANE CLOSURES. THESE LANE CLOSURES WILL ONLY BE ALLOWED IN ONE DIRECTION AT A TIME.

DURING BRIDGE DECK SEALING TRAFFIC WILL BE MAINTAINED BY DIRECTIONAL OFFSITE DETOURS VIA US 64.

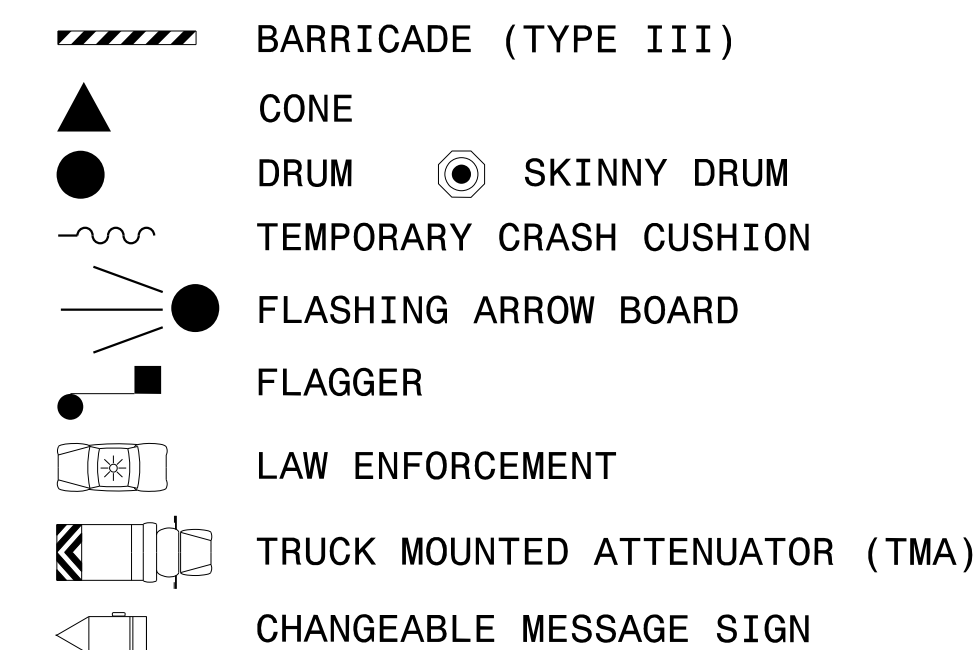
TIME RESTRICTIONS WILL BE USED TO LIMIT THE AMOUNT OF DRIVER DELAY DURING LANE CLOSURES.

LEGEND

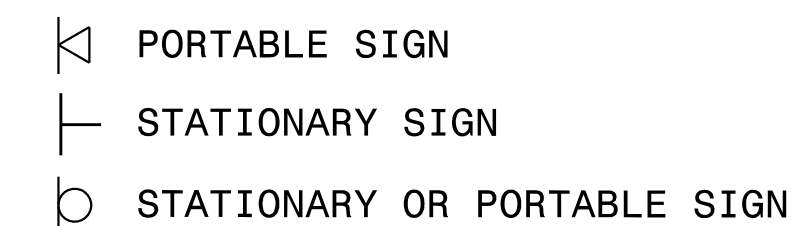
GENERAL



TRAFFIC CONTROL DEVICES



TEMPORARY SIGNING



PAVEMENT MARKING SYMBOLS



TEMPORARY PAVEMENT MARKING

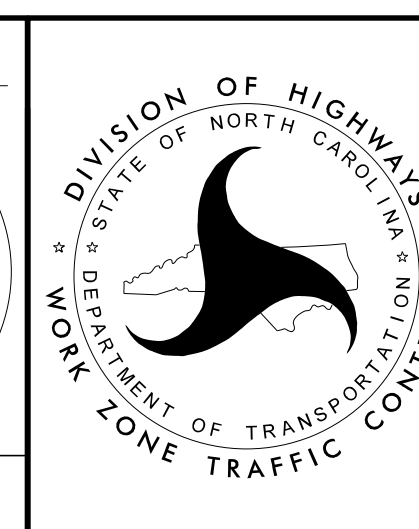
4" PAINT

PA	WHITE EDGELINE
PB	YELLOW EDGELINE
PC	10 FT. WHITE SKIP

APPROVED: *Steve Kite*
DESIGNED BY: E27C30E10FC442

DATE: 1/31/2017

**DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED**



ROADWAY STANDARD
DRAWINGS, MANAGEMENT
STRATEGIES & LEGEND

GENERAL NOTES

CHANGES MAY BE REQUIRED WHEN PHYSICAL DIMENSIONS IN THE DETAIL DRAWINGS, STANDARD DETAILS, AND ROADWAY DETAILS ARE NOT ATTAINABLE TO MEET FIELD CONDITIONS OR RESULT IN DUPLICATE OR UNDESIRED OVERLAPPING OF DEVICES. MODIFICATION MAY INCLUDE: MOVING, SUPPLEMENTING, COVERING, OR REMOVAL OF DEVICES AS DIRECTED BY THE ENGINEER.

THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT EXCEPT WHEN OTHERWISE NOTED IN THE PLAN OR DIRECTED BY THE ENGINEER.

TIME RESTRICTIONS

A) DO NOT CLOSE OR NARROW TRAVEL LANES AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
US 64 & US 64 BYP	MAY 1 - SEPT 30 MONDAY THRU THURSDAY 6:00 A.M. - 7:00 P.M. AND FRIDAY - 6:00 A.M. TO SUNDAY - 7:00 P.M.

B) DO NOT CLOSE OR NARROW TRAVEL LANES DURING HOLIDAYS AND SPECIAL EVENTS AS FOLLOWS:

<u>ROAD NAME</u>
US 64 & US 64 BYP

HOLIDAY

- FOR ANY UNEXPECTED OCCURRENCE THAT CREATES UNUSUALLY HIGH TRAFFIC VOLUMES, AS DIRECTED BY THE ENGINEER.
- FOR NEW YEAR'S, BETWEEN THE HOURS OF 6:00 A.M. DECEMBER 31st TO 7:00 P.M. JANUARY 2ND. IF NEW YEAR'S DAY IS ON A FRIDAY, SATURDAY, SUNDAY, OR MONDAY THEN UNTIL 7:00 P.M. THE FOLLOWING TUESDAY.
- FOR EASTER, BETWEEN THE HOURS OF 6:00 A.M. THURSDAY AND 7:00 P.M. MONDAY.
- FOR MEMORIAL DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY TO 7:00 P.M. TUESDAY.
- FOR INDEPENDENCE DAY, BETWEEN THE HOURS OF 6:00 A.M. THE DAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE DAY AFTER INDEPENDENCE DAY.

IF INDEPENDENCE DAY IS ON A FRIDAY, SATURDAY, SUNDAY OR MONDAY THEN BETWEEN THE HOURS OF 6:00 A.M. THE THURSDAY BEFORE INDEPENDENCE DAY AND 7:00 P.M. THE TUESDAY AFTER INDEPENDENCE DAY.
- FOR LABOR DAY, BETWEEN THE HOURS OF 6:00 A.M. FRIDAY AND 7:00 P.M. TUESDAY.
- FOR THANKSGIVING DAY, BETWEEN THE HOURS OF 6:00 A.M. TUESDAY TO 7:00 P.M. MONDAY.
- FOR CHRISTMAS, BETWEEN THE HOURS OF 6:00 A.M. THE FRIDAY BEFORE THE WEEK OF CHRISTMAS DAY AND 7:00 P.M. THE FOLLOWING TUESDAY AFTER THE WEEK OF CHRISTMAS.

C) DO NOT CLOSE ROADS AS FOLLOWS:

<u>ROAD NAME</u>	<u>DAY AND TIME RESTRICTIONS</u>
US 64 BYP	MAY 1 - SEPT 30 MONDAY THRU THURSDAY 6:00 A.M. - 10:00 P.M. AND FRIDAY - 6:00 A.M. TO SUNDAY - 10:00 P.M.
	OCT 1 - APR 30 FRIDAY - 6:00 A.M. TO SUNDAY - 10:00 P.M.

LANE AND SHOULDER CLOSURE REQUIREMENTS

- D) REMOVE LANE CLOSURE DEVICES FROM THE LANE WHEN WORK IS NOT BEING PERFORMED BEHIND THE LANE CLOSURE OR WHEN A LANE CLOSURE IS NO LONGER NEEDED OR AS DIRECTED BY THE ENGINEER.
- E) WHEN PERSONNEL AND/OR EQUIPMENT ARE WORKING WITHIN A LANE OF TRAVEL OF AN UNDIVIDED OR DIVIDED FACILITY, CLOSE THE LANE ACCORDING TO THE TRAFFIC CONTROL PLANS, ROADWAY STANDARD DRAWINGS, OR AS DIRECTED BY THE ENGINEER. CONDUCT THE WORK SO THAT ALL PERSONNEL AND/OR EQUIPMENT REMAIN WITHIN THE CLOSED TRAVEL LANE.
- F) DO NOT INSTALL MORE THAN ONE LANE CLOSURE IN ANY ONE DIRECTION ON US 64 BYP.

TRAFFIC PATTERN ALTERATIONS

- G) NOTIFY THE ENGINEER TWENTY ONE (21) CALENDAR DAYS PRIOR TO ANY TRAFFIC PATTERN ALTERATION.

SIGNING

- H) INSTALL ADVANCE WORK ZONE WARNING SIGNS WHEN WORK IS WITHIN 40 FT FROM THE EDGE OF TRAVEL LANE AND NO MORE THAN THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- I) PROVIDE SIGNING AND DEVICES REQUIRED TO CLOSE THE ROAD ACCORDING TO THE ROADWAY STANDARD DRAWINGS AND TRAFFIC CONTROL PLANS.

PROVIDE SIGNING REQUIRED FOR THE OFF-SITE DETOUR ROUTE AS SHOWN IN THE TRAFFIC CONTROL PLANS.
- J) COVER OR REMOVE ALL SIGNS AND DEVICES REQUIRED TO CLOSE THE ROAD WHEN ROAD CLOSURE IS NOT IN OPERATION.

COVER OR REMOVE ALL SIGNS REQUIRED FOR THE OFF-SITE DETOUR WHEN THE DETOUR IS NOT IN OPERATION.
- K) ENSURE ALL NECESSARY SIGNING IS IN PLACE PRIOR TO ALTERING ANY TRAFFIC PATTERN.

TRAFFIC CONTROL DEVICES

- L) WHEN LANE CLOSURES ARE NOT IN EFFECT SPACE CHANNELIZING DEVICES IN WORK AREAS NO GREATER IN FEET THAN TWICE THE POSTED SPEED LIMIT (MPH) EXCEPT, 10 FT ON-CENTER IN RADII, AND 3 FT OFF THE EDGE OF AN OPEN TRAVELWAY. REFER TO STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES SECTIONS 1130 (DRUMS), 1135 (CONES) AND 1180 (SKINNY DRUMS) FOR ADDITIONAL REQUIREMENTS.
- M) PLACE TYPE III BARRICADES, WITH "ROAD CLOSED" SIGN R11-2 ATTACHED, OF SUFFICIENT LENGTH TO CLOSE ENTIRE ROADWAY.
- N) PLACE ADDITIONAL SETS OF THREE CHANNELIZING DEVICES PERPENDICULAR TO THE EDGE OF TRAVELWAY ON 500 FT CENTERS WHEN UNOPENED LANES ARE CLOSED TO TRAFFIC.

PAVEMENT MARKINGS AND MARKERS

- O) INSTALL TEMPORARY PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKERS AS FOLLOWS:
- | <u>ROAD NAME</u> | <u>MARKING</u> | <u>MARKER</u> |
|------------------|----------------|---------------|
| US 64 BYP | PAINT | TEMP RAISED |
- P) PLACE ONE APPLICATION OF PAINT FOR TEMPORARY TRAFFIC PATTERNS, WHEN APPLICABLE AND AS DIRECTED BY THE ENGINEER.
- Q) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
- R) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS BY THE END OF EACH DAY'S OPERATION.

MISCELLANEOUS

- S) MAY USE LAW ENFORCEMENT TO DIRECT TRAFFIC AND ENFORCE ROAD CLOSURE LOCATIONS AS DIRECTED BY THE ENGINEER.

PHASING

STEP 1: INSTALL WORK ZONE ADVANCE WARNING SIGNS ON US 64 BYP ACCORDING TO RSD 1101.01, SHEET 2 OF 3.

STEP 2: USING RSD 1101.02, SHEET 3 OF 15, COMPLETE SUBSTRUCTURE REPAIRS ON BRIDGE NO. 54.

NOTE: LANE CLOSURES SHALL BE INSTALLED IN ONE DIRECTION AT A TIME. COMPLETE THE WORK REQUIRED IN STEPS 2A THRU 2E

COMPLETE THE WORK OF STEPS 2A-2D, OVER SUCCESSIVE WORK PERIODS, USING ROAD CLOSURE TIME RESTRICTIONS. SEE GENERAL NOTE (C) ON TMP-2 AND TMP-3 THRU TMP-6 FOR DETOUR ROUTE AND ROAD CLOSURE SIGNING.

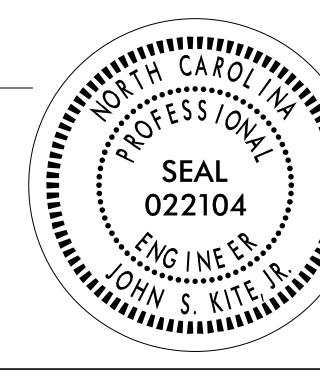
- 2A) INSTALL ROAD CLOSURE SIGNS AND DEVICES FOR DIRECTIONAL CLOSURES.
- 2B) USING TMP-3 THRU TMP-6 CLOSE ONE DIRECTION OF US 64 BYPASS TO THRU TRAFFIC.
- 2C) SEAL THE LENGTH OF DECK SURFACE THAT CAN BE APPLIED DURING A SINGLE ROAD CLOSURE PERIOD.
- 2D) PLACE TEMPORARY PAVEMENT MARKING IN ORIGINAL PATTERN.
- 2E) REMOVE ALL ROAD CLOSURES SIGNS AND TRAFFIC CONTROL DEVICES AND OPEN TRAFFIC TO THE EXISTING PATTERN.

STEP 3: USING RSD 1101.02, SHEET 2 OF 9 PLACE FINAL PAVEMENT MARKINGS AND MARKERS IN ORIGINAL PATTERN.

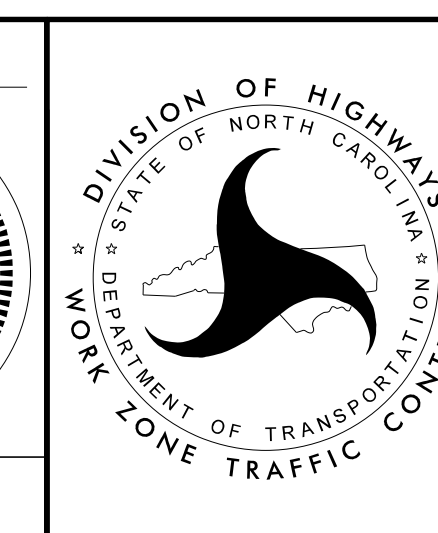
STEP 4: REMOVE ALL TEMPORARY TRAFFIC CONTROL DEVICES AND OPEN ALL LANES TO TRAFFIC.

APPROVED: *Steve Kite*
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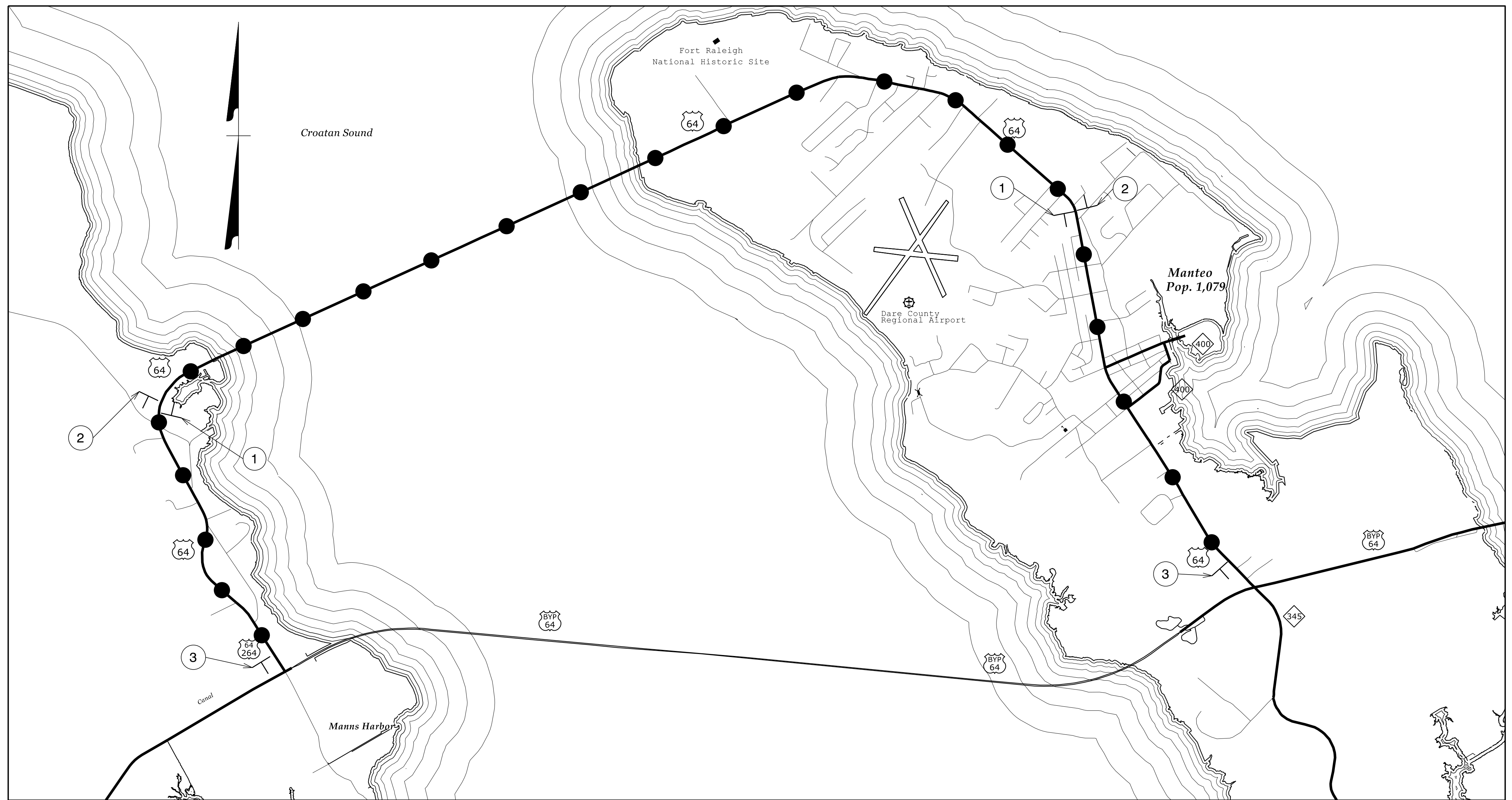
DATE: 1/31/2017


 SEAL
 022104
 ENGINEER
 JOHN S. KITE, II
 NORTH CAROLINA

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GENERAL NOTES AND PHASING

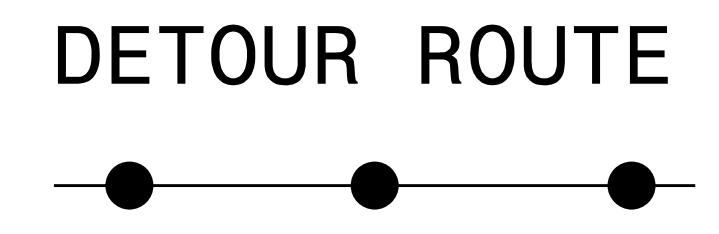
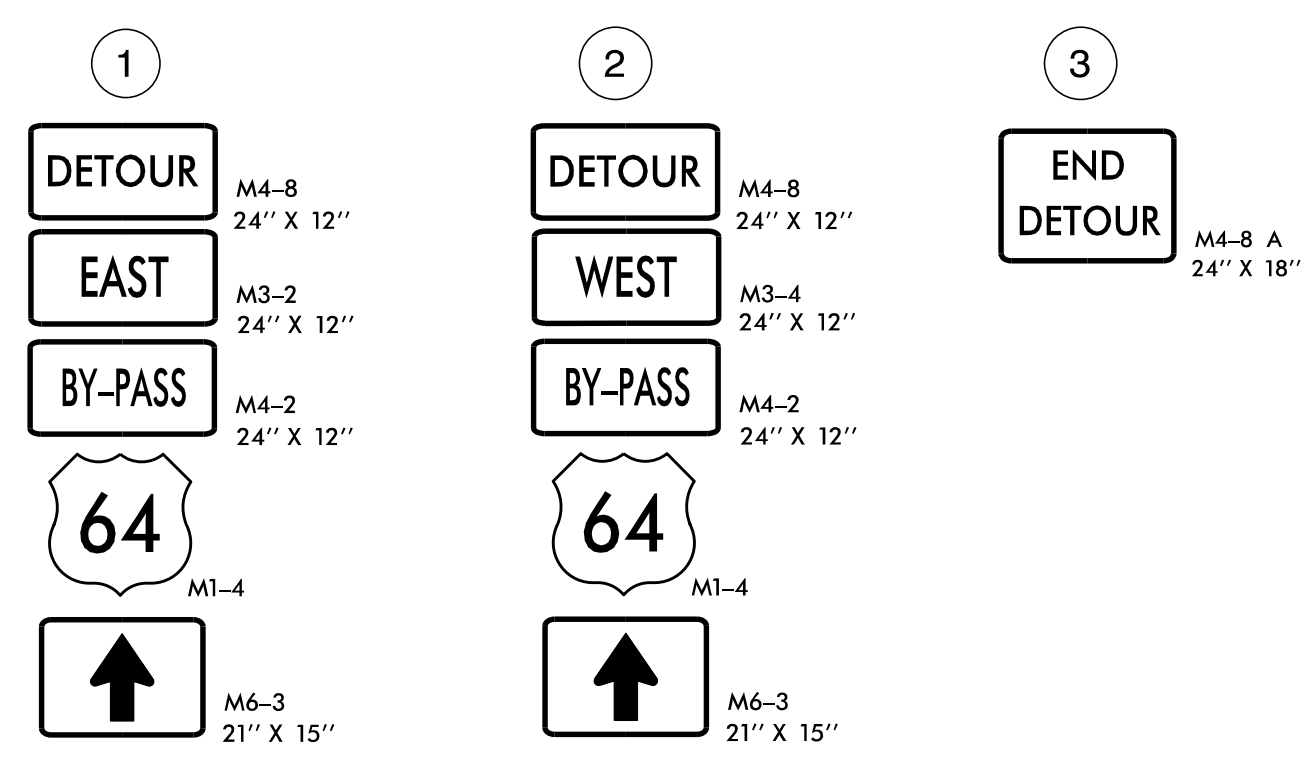


NOTES:

CLOSE ONLY ONE DIRECTION OF US 64 BYP AT A TIME.

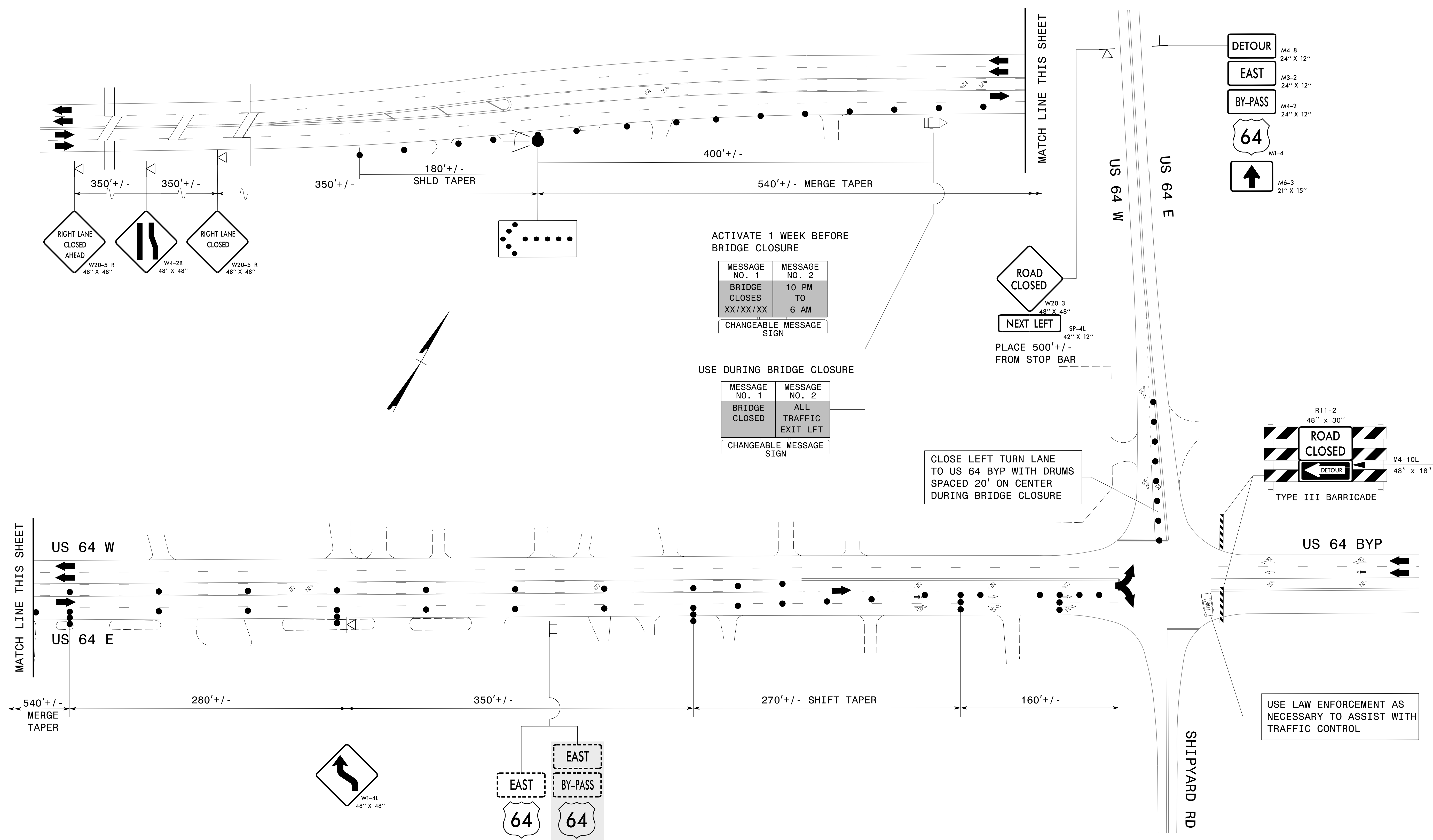
TRAFFIC CONTROL DEVICES ① THROUGH ③ SHALL BE INSTALLED AS SHOWN ON THE PLAN FOR AS DIRECTED BY THE ENGINEER.

SEE TMP-4 AND TMP-5 FOR ADDITIONAL TRAIL BLAZING SIGNS.



APPROVED: <i>Steve Kite</i> DATE: 1/31/2017 		<p>US 64 BYP OFF-SITE DETOUR</p>
<p>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</p>		

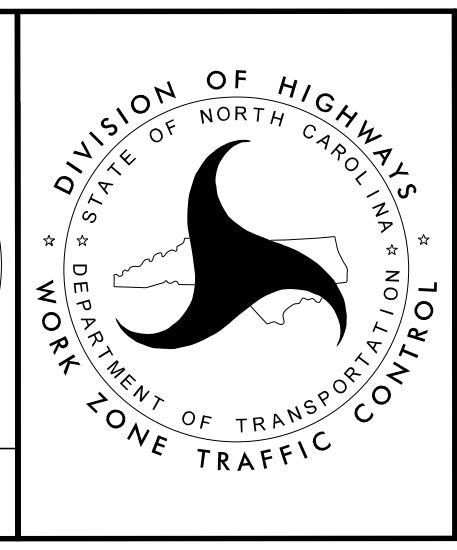
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 User: sbjennings



1/26/2017
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APPROVED: *Steve Kite*
 DATE: 1/31/2017

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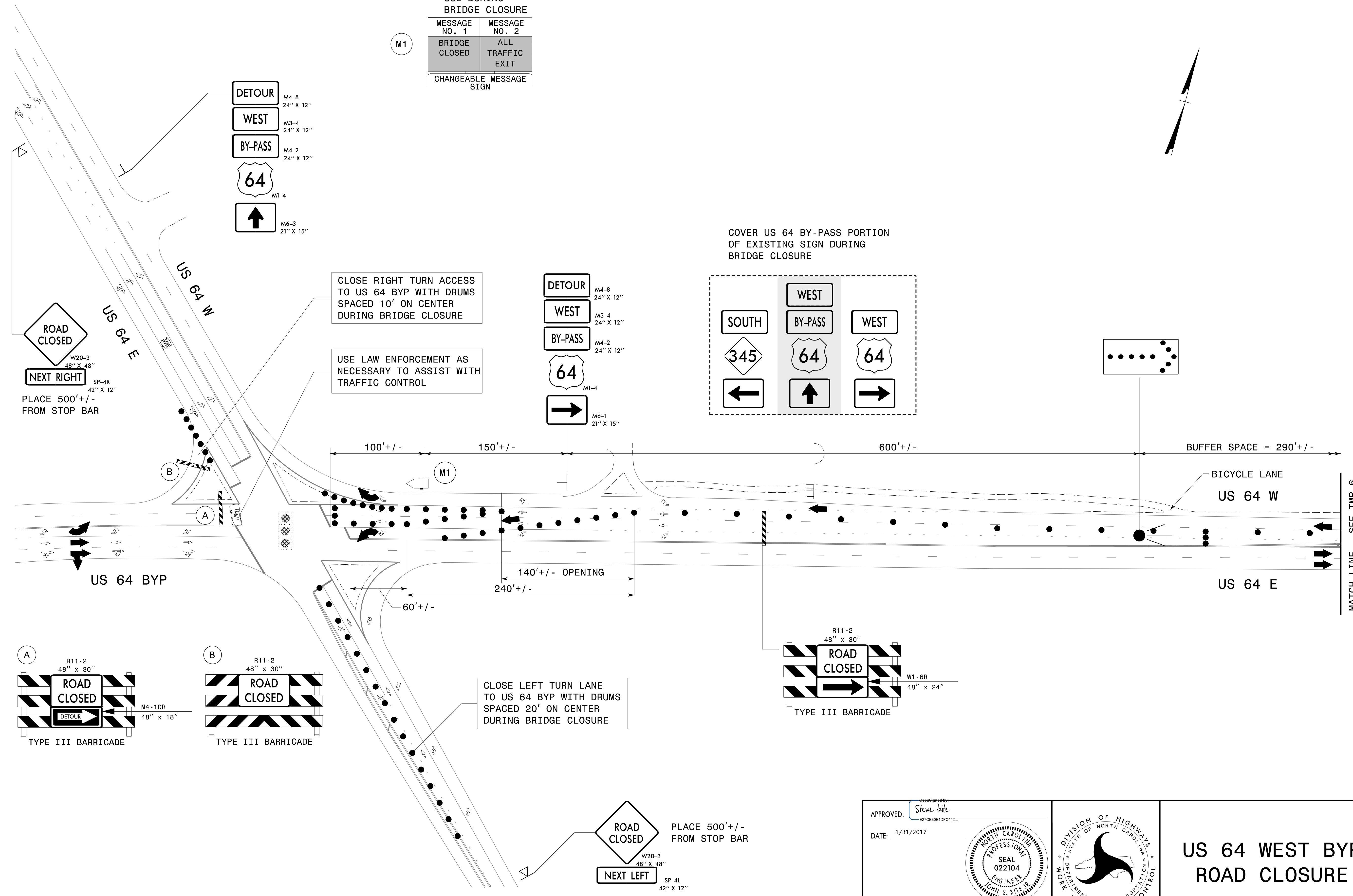


US 64 EAST BYP ROAD CLOSURE DETAIL

USE DURING
BRIDGE CLOSURE

MESSAGE NO. 1	MESSAGE NO. 2
BRIDGE CLOSED	ALL TRAFFIC EXIT

CHANGEABLE MESSAGE SIGN



ROAD CLOSED
W20-3
48" X 48"

NEXT RIGHT
SP-4R
42" X 12"

PLACE 500' +/-
FROM STOP BAR

CLOSE RIGHT TURN ACCESS
TO US 64 BYP WITH DRUMS
SPACED 10' ON CENTER
DURING BRIDGE CLOSURE

USE LAW ENFORCEMENT AS
NECESSARY TO ASSIST WITH
TRAFFIC CONTROL

COVER US 64 BY-PASS PORTION
OF EXISTING SIGN DURING
BRIDGE CLOSURE

A R11-2
48" X 30"

ROAD CLOSED

DETOUR

M4-10R
48" X 18"

TYPE III BARRICADE

B R11-2
48" X 30"

ROAD CLOSED

TYPE III BARRICADE

CLOSE LEFT TURN LANE
TO US 64 BYP WITH DRUMS
SPACED 20' ON CENTER
DURING BRIDGE CLOSURE

R11-2
48" X 30"

ROAD CLOSED

W1-6R
48" X 24"

TYPE III BARRICADE

ROAD CLOSED
W20-3
48" X 48"

NEXT LEFT
SP-4L
42" X 12"

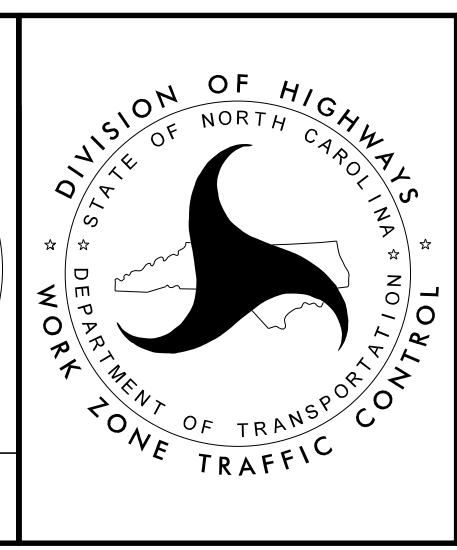
PLACE 500' +/-
FROM STOP BAR

APPROVED: *Steve Kite*
E27CE30E10FC442

DATE: 1/31/2017

SEAL
022104
ENGINEER
JOHN S. KITE, II

DOCUMENT NOT CONSIDERED FINAL
UNLESS ALL SIGNATURES COMPLETED



US 64 WEST BYP
ROAD CLOSURE

I:\26\2017
 R:\TrafficControl\CPA\B-5943_TC_TMP-5.dgn
 User:tbjennings

ACTIVATE 1 WEEK BEFORE BRIDGE CLOSURE

MESSAGE NO. 1	MESSAGE NO. 2
BRIDGE CLOSURES TO XX/XX/XX	10 PM TO 6 AM

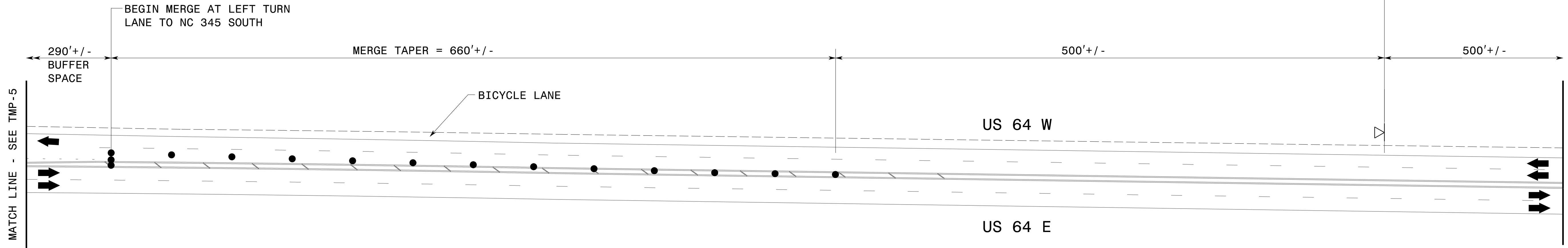
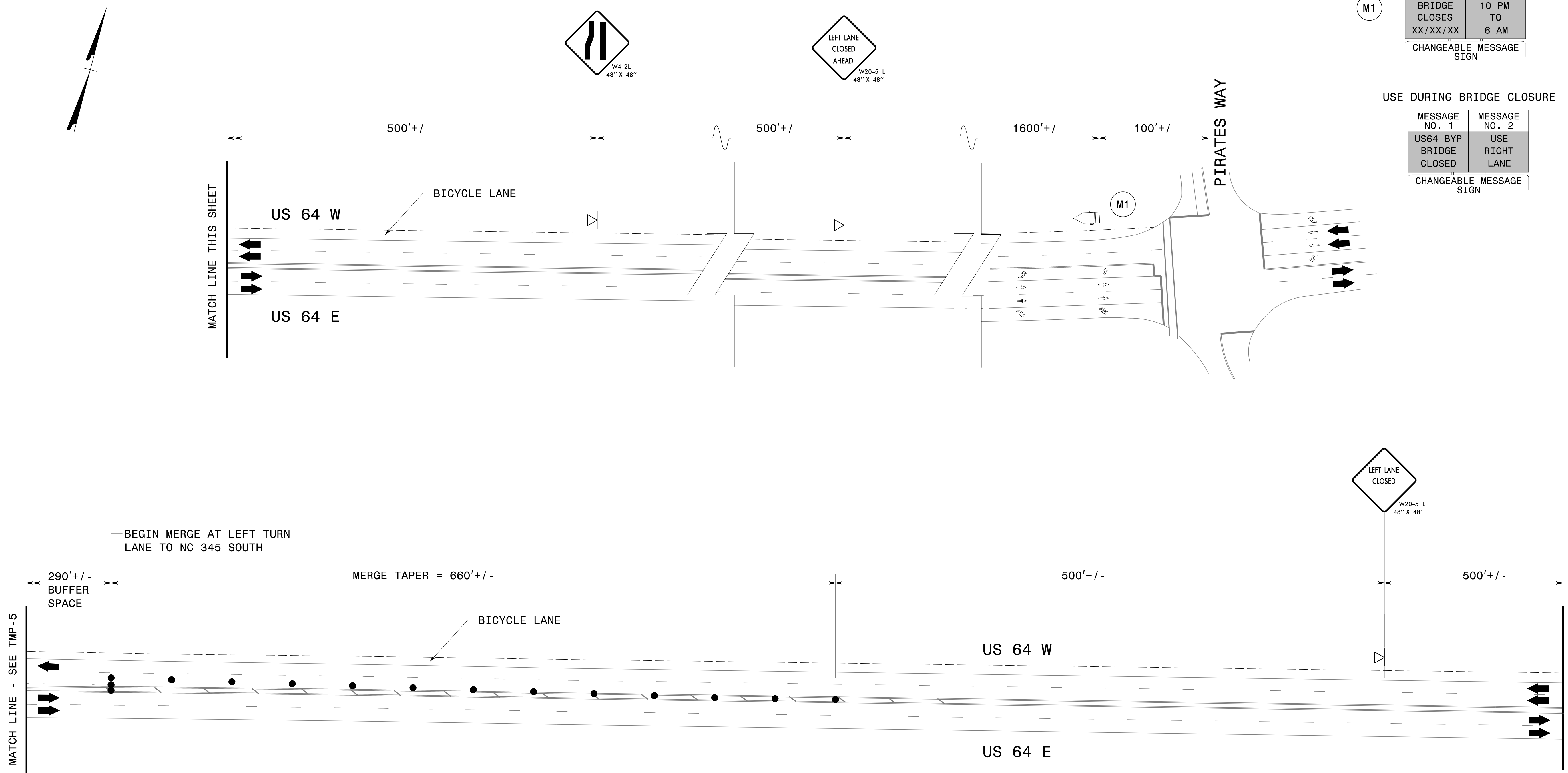
CHANGEABLE MESSAGE SIGN

M1

USE DURING BRIDGE CLOSURE

MESSAGE NO. 1	MESSAGE NO. 2
US64 BYP BRIDGE CLOSED	USE RIGHT LANE

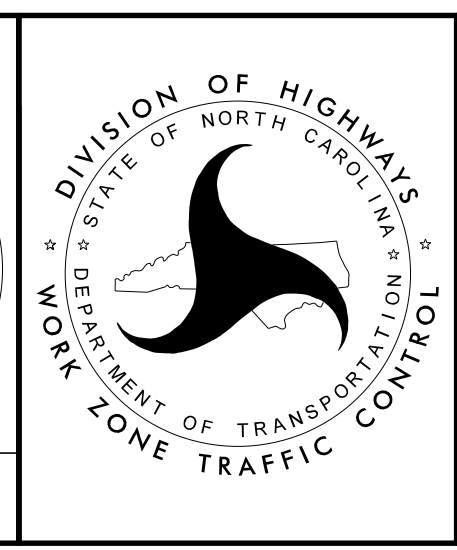
CHANGEABLE MESSAGE SIGN



1/26/2017
 R:\TrafficControl\CP\B-5943_TC_TMP_6.dgn
 User: sbjennings

APPROVED: *Steve Kite*
 DATE: 1/31/2017

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**US 64 WEST BYP
ROAD CLOSURE**