STATE OF NORTH CAROLINA DIVISION OF HIGHWAYS WASHINGTON COUNTY 094

N.C. <u>2021CPT.01.03.10941.1, ETC.</u> 2021CPT.01.03.10941.1 PE, CONST 2021CPT.01.03.20941.1 PE, CONST

LOCATION: MAP #1 & #2 - US 64 EBL & WBL FROM END 5 LANE TO SR 1125 OVERPASS

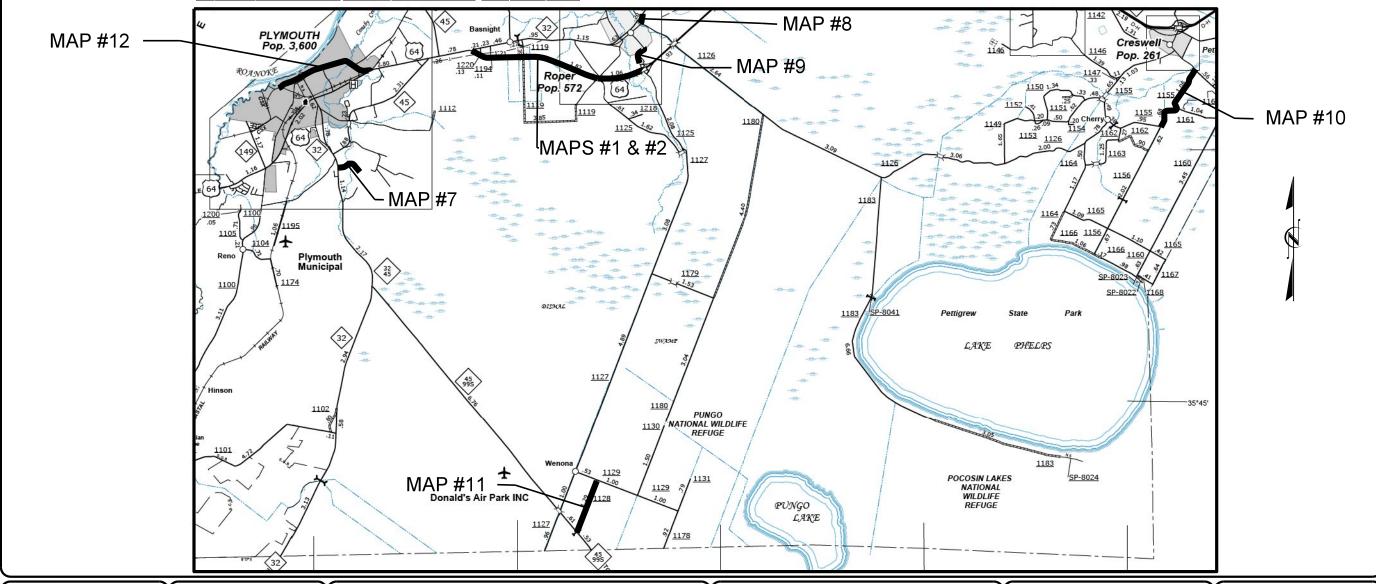
MAP #7 - SR 1117 (STYONS RD.) FROM DEAD END TO NC 32

MAP #8 - SR 1122 (OLD US 64) FROM BRIDGE OVER MAIN CANAL TO NC 32

MAP #9 - SR 1125 (MILL POND RD.) FROM PAV. CHANGE AT M. P. 0.3 TO SR 1175

MAP #10 - SR 1155 (OLD CHERRY RD.) FROM SR 1142 TO SR 1156 MAP #11 - SR 1128 (A CANAL RD.) FROM SR 1129 TO NC 45 MAP #12 - SR 1325 (MAIN ST.) FROM US 64 TO DEAD END

TYPE OF WORK: AST (DOUBLE SEAL), OGAFC, MILLING & RESURFACING



GRAPHIC SCALES

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MAP #1 = 3.93 MILESMAP #2 = 3.93 MILESMAP # 7 = 0.61 MILESMAP #8 = 0.21 MILES MAP #9 = 0.34 MILES

PROJECT LENGTH

MAP #10 = 1.73 MILESMAP #11 = 1.28 MILES MAP #12 = 2.32 MILES

Prepared in the Office of: **DIVISION OF HIGHWAYS** 113 Airport Dr., Edenton NC, 27944 2018 STANDARD SPECIFICATIONS W. B. HOBBS, PE CHRIS SLACHTA

S. P. FENWICK, PE



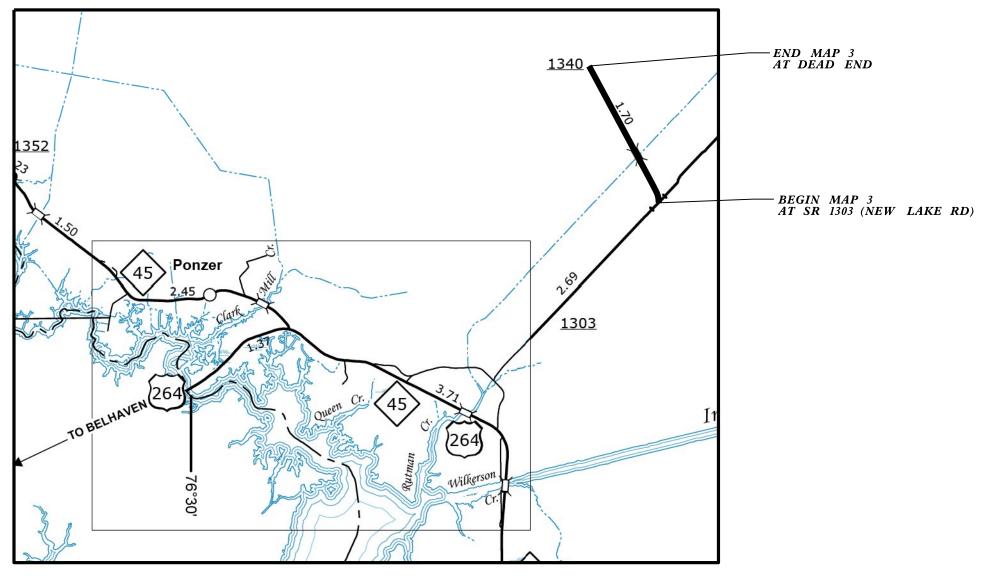
STATE OF NORTH CAROLINA DIVISION OF HIGHWAYS

| STATE | STATE PROJECT REFERENCE NO. | | | | | | | | | | |
|----------------------------------|-----------------------------|--|--|--|--|--|--|--|--|--|--|
| N.C. 2021CPT.01.03.10941.1, ETC. | | | | | | | | | | | |
| | STATE PROJ.NO. DESCRIPTIO | | | | | | | | | | |
| 2021CPT.01.03.20481.1 PE, CON | | | | | | | | | | | |

HYDE COUNTY

LOCATION: SR 1340 (GALL RD) FROM SR 1303 (NEW LAKE RD) TO DEAD END

TYPE OF WORK: RESURFACING



GRAPHIC SCALES

PROJECT LENGTH

DIVISION OF HIGHWAYS 113 AIRPORT DR., EDENTON, NC, 27932

Prepared in the Office of:

2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E DIVISION PROJECT TEAM LEAD

S.P. FENWICK, PLS

DESIGN ENGINEER



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LENGTH ROADWAY MAP #3 = 1.56 MILES

NTS

CHRIS SLACHTA

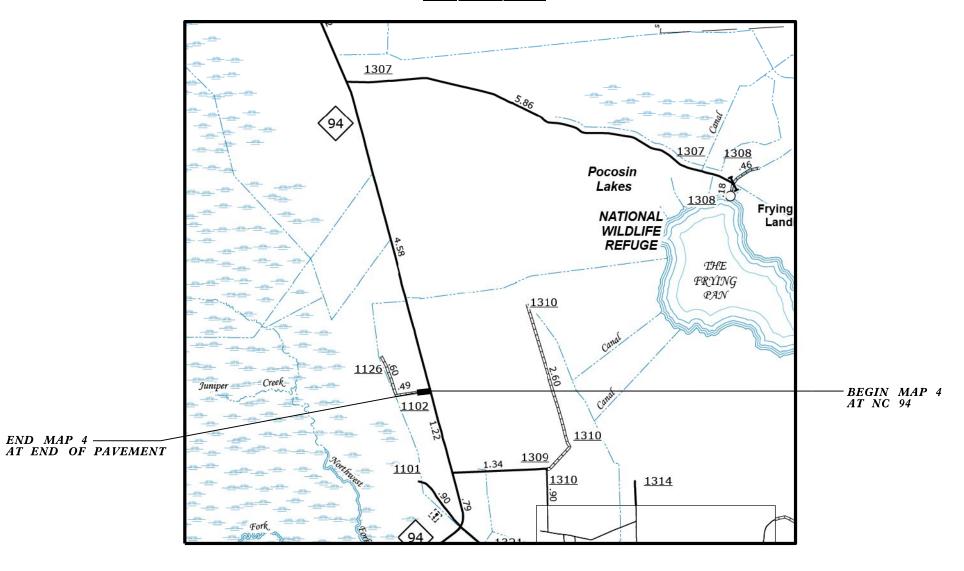
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

| STATE | STATE PROJECT REFERENCE NO. | | | | | | | | | |
|-------|-------------------------------|---------------|---|--|--|--|--|--|--|--|
| N.C. | 2021CPT.01.03.10941.1 | <u>, ETC.</u> | 3 | | | | | | | |
| | STATE PROJ. NO. DESCRIPTI | | | | | | | | | |
| | 2021CPT.01.03.20891.1 PE, CON | | | | | | | | | |

TYRRELL COUNTY

LOCATION: SR 1102 (JONES RD) FROM NC 94 TO END OF PAVEMENT

TYPE OF WORK: RESURFACING



RACT: DA00478

10941

01.03.

2021CP

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GRAPHIC SCALES

NTS

PROJECT LENGTH

LENGTH ROADWAY MAP #4 = 0.1 MILES

Prepared in the Office of:

DIVISION OF HIGHWAYS

113 AIRPORT DR., EDENTON, NC, 27932

2018 STANDARD SPECIFICATIONS

CHRIS SLACHTA

W.B. HOBBS, P.E

DIVISION PROJECT TEAM LEAD

S.P. FENWICK, PLS

DESIGN ENGINEER



STATE OF NORTH CAROLINA DIVISION OF HIGHWAYS

| STATE | STATE PROJECT REPERENCE NO | | SHEET NO. | | | | | |
|-------------------------------|--|--|--------------|--|--|--|--|--|
| N.C. | N.C. 2021CPT.01.03.10941.1, ETC. STATE PROJ.NO. DESCRIPTI | | | | | | | |
| | | | | | | | | |
| 2021CPT.01.03.20891.1 PE, CON | | | | | | | | |

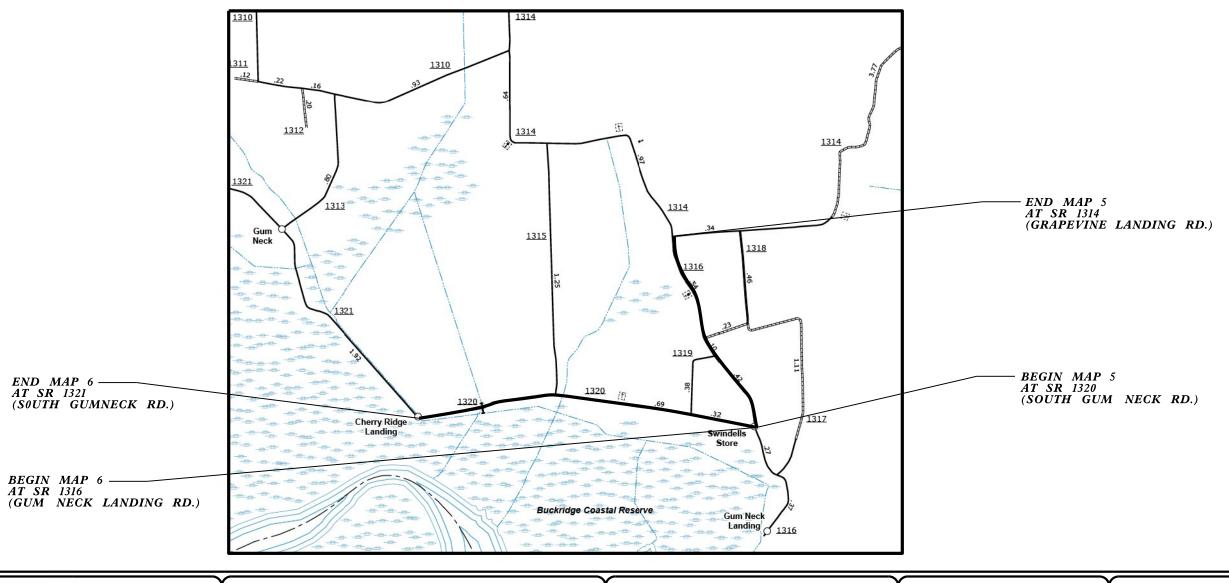
TYRRELL COUNTY

LOCATION: MAP 5 - SR 1316 (GUM NECK LANDING RD) FROM SR 1320 (S GUM NECK RD) TO SR 1314 (GRAPEVINE LANDING RD)

MAP 6 - SR 1320 (S GUM NECK RD) FROM SR 1316 (GUM NECK LANDING RD) TO

SR 1321 (S GUM NECK RD)

TYPE OF WORK: RESURFACING



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A00478

GRAPHIC SCALES

NTS

PROJECT LENGTH

LENGTH ROADWAY MAP #5 = 1.06 MILES LENGTH ROADWAY MAP #6 = 1.71 MILES

Prepared in the Office of: **DIVISION OF HIGHWAYS**

113 AIRPORT DR., EDENTON, NC, 27932 2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E

CHRIS SLACHTA

S.P. FENWICK, PLS DESIGN ENGINEER



| | PAVEMENT SCHEDULE |
|----|--|
| B1 | PROP. APPROX. 0.75" OPEN GRADE ASPHALT FRICTION COURSE, TYPE 1 MODIFIED, AT AN AVERAGE RATE OF 90 LBS. PER SQ. YD. |
| C1 | PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD. |
| C2 | PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH. |
| V1 | MILLING ASPHALT PAVEMENT. 1.5" IN DEPTH. |
| V2 | MILLED RUMBLE STRIPS |
| U | EXISTING PAVEMENT. |

| PROJECT REFERENCE NO. | SHEET NO. |
|-----------------------------|-----------|
| 2021CPT.01.03,10941.1, ETC. | 5 |

NOTES:

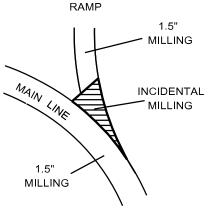
ALL INGRESS AND EGRESS RAMPS TO BE RESURFACED TO THE EDGE OF PAVEMENT OF THE CONNECTING ROAD OR AS DIRECTED BY THE ENGINEER

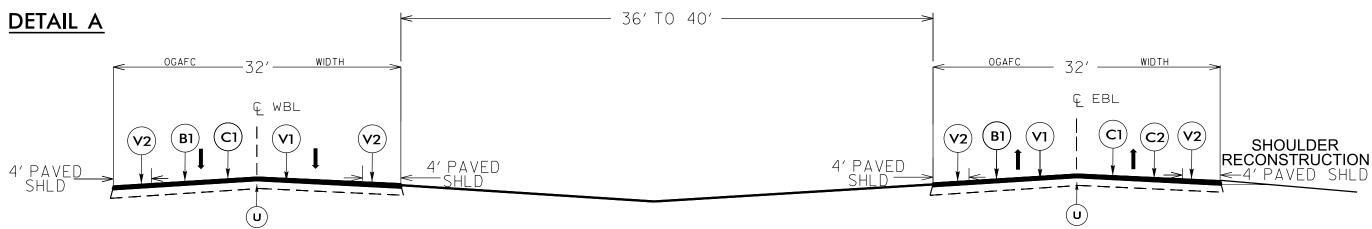
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

1.5" MILLING AND 1.5" OF S9.5C TO BE APPLIED THE FULL WIDTH OF THE ROADWAY

0.75" OGAFC TO BE APPLIED THE FULL WIDTH OF THE ROADWAY

LEVEL COURSE S9.5C TO BE USED ON US 64 EBL FOR APPROXIMATELY 1000' BEGINNING AT STATION 103+89.00 FROM PAVEMENT JOINT (STATION 0+00.00) AT NC 32. SHOULDER RECONSTRUCTION WILL BE NEEDED FOR THIS AREA.





TYPICAL SECTION NO. 1
USE WITH MAPS 1 & 2

PAVEMENT SCHEDULE 202ICPT.01.03.10941.1,ETC. ALL PAVED S.R. ROADS OR RAMPS TO BE RESURFACED TO THE ENDS OF THE RADII OR AS DIRECTED BY THE ENGINEER PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES ASPHALT SURFACE TREATMENT, DOUBLE SEAL ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE. PRIOR TO FINAL SURFACE LAYER BEING INSTALLED EXISTING PAVEMENT. *ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT: BOTTOM LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX 0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY. TOP LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.25 + GAL/SY AND A LAYER OF:5/16" LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX. EARTH MATERIAL. 9+ LBS/SY. LENGTH & WIDTH - EXISTING ASPHALTIC PAVEMENT LENGTH & WIDTH - EXISTING ASPHALTIC PAVEMENT VARIES VARIES EXISTING BASE - EXISTING SUBGRADE MATERIAL 2.5" MAX EXISTING SUBGRADE MATERIAL MILL AND REMOVE EXISTING ASPHALT
PAVEMENT TO NEAT LINES AND REMOVE
EXISTING LOOSE BASE AND/OR SUBGRADE
MATERIAL AND REPLACE WITH ACSC TYPE
S9.5B OR 119.5C AS DIRECTED BY THE ENGINEER SAW AND REMOVE EXISTING ASPHALT
PAVEMENT TO NEAT LINES AND REMOVE
EXISTING LOOSE BASE AND/OR SUBGRADE
MATERIAL AND REPLACE WITH ASIC TYPE 119.0C OR ASBC B25.0C AS DIRECTED BY THE ENGINEER *NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT. ***NOTE:** EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT. MILL PATCHING, 0-2.5" FULL DEPTH PATCHING 0-5" **SHOULDER** SHOULDER RECONSTRUCTION RECONSTRUCTION TYPICAL SECTION NO. 3 U USE WITH: MAP 10 18'-20'-SHOULDER SHOULDER RECONSTRUCTION RECONSTRUCTION TYPICAL SECTION NO. 2 USE WITH: MAPS 3, 4, 5, 6, & 7

NOTES:

PROJECT REFERENCE NO.

SHEET NO.

MILL PATCHING, 0-2.5"

LENGTH & WIDTH

VARIES

EXISTING ASPHALTIC PAVEMENT

VARIES

EXISTING BASE

EXISTING SUBGRADE MATERIAL

PAVEMENT TO NEAT LINES AND REMOVE
EXISTING LOOSE BASE AND/OR SUBGRADE
MATERIAL AND REPLACE WITH ASIC TYPE 119.0C
OR ASBC 825.0C AS DIRECTED BY THE ENGINEER

*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING 0-5"

NOTES:

ALL PAVED S.R. ROADS OR RAMPS TO BE RESURFACED TO THE ENDS OF THE RADII OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE, PRIOR TO FINAL SURFACE LAYER BEING INSTALLED

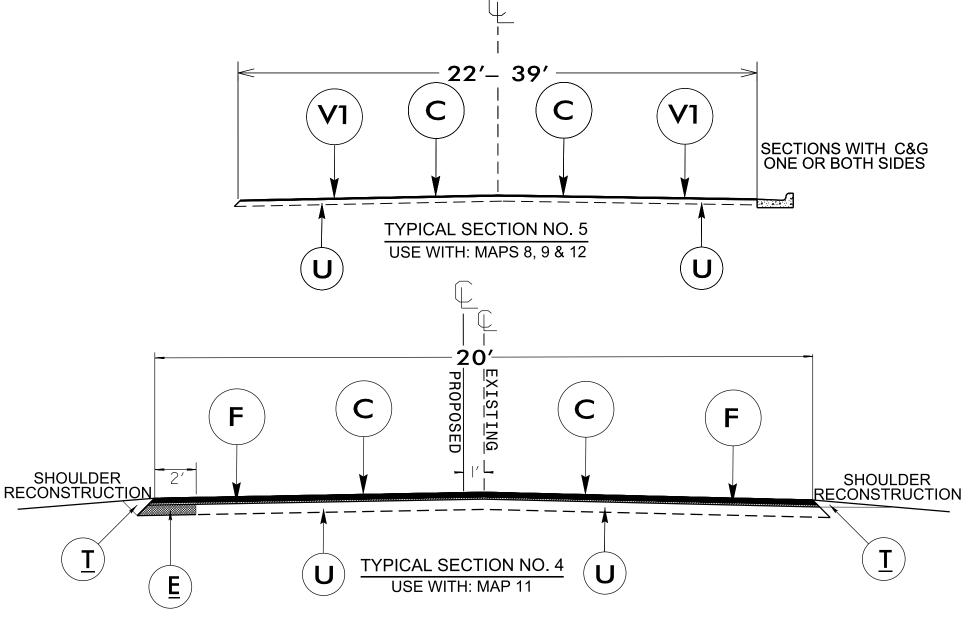
PROJECT REFERENCE NO.

202ICPT.01.03.10941.J.ETC.

2' WIDENING IS IS ON THE WEST SIDE OF ROAD ONLY FOR MAP #11

SOME AREAS OF MAP #12 HAVE PAVEMENT IN THE GUTTER AND SHOULD BE PAVED BACK AS THEY EXIST FOR MAPS #8 & #9 WITH CURB AND GUTTER, MILL 1.5" BELOW THE EDGE OF GUTTER PAN.

ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT:
BOTTOM LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX.
0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY.
TOP LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX.
0.25 + GAL/SY AND A LAYER OF:5/16" LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX.
9+ LBS/SY.



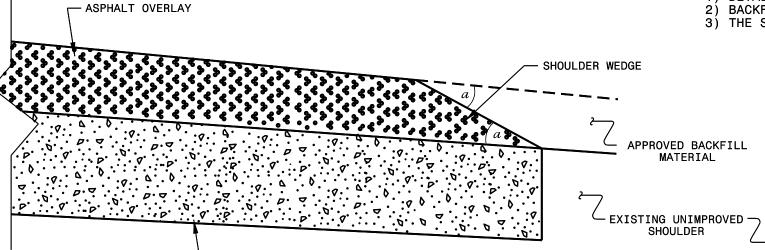
| | | | | | | | | | | | S U M | MAR | Y O | F Q U | JANT | ITIE | S | | | | | | | | | | | | | | | | | |
|-----------------------|------------|--------------------------------|---|---|----------------------|---------------------------------|----------------|--------------|--------|-----|----------------------------|----------------|-----------------------|--------------------------------------|--|--|-----------------------------|---|-----------------------|---------------------------------|--|---|-----------------------------------|-----|-----------------------|--|------------|-----|-------|-------------|---|----|------|----------------------------|
| PROJECT NO | COUNTY | MAP ROUTE NO | DESCRIPTION TYP LANI | ES LANE FINAL TYPE SURFAC TESTING REQUIRE | E ASPHALT G REQUIRED | MATERIAL TRANSFER VEHICLE | LENGTH WIDTH I | MOBILIZATIOI | BORROW | | SHOULDER RECONSTRUCTION | 1½" MILLING | INCIDENTAL MILLING | ASPHALT PAVEMENT BASE COURSE, B25.0C | ASPHALT PAVEMENT SURFACE COURSE, S9.5B | ASPHALT PAVEMENT SURFACE COURSE, S9.5C | ASPHALT FOR PLANT MIX | POLYMER MODIFIED ASPHALT BINDER FOR PLANT MIX | TYPE FC-1 MODIFIED | SURFACE TREATMENT, DOUBLE | EMULSION FOR ASPHALT SURFACE TREATMENT | MILLED RUMBLE STRIPS (ASPHALT CONCRETE) | FULL DEPTH PATCHING 0-5" | | DJUSTMENT MANHOLES | ADJUSTMENT OF METER OR VALVE BOX | SILT FENCE | | | MULCHING F | RESPONSE JU FOR EROSION (ST CONTROL | | LOOP | LEAD-IN CABLE (14-2) |
| | | | | | | | MI FT | LS | CY | TON | SMI | SY | SY | TON | TON | TON | TON | TON | TON | SY | GAL | LF | TON | TON | EA | EA | LF | SY | LF | ACR | EA | EA | LF | LF |
| 2021CPT.01.03.10941.1 | Washington | 1 US 64 EBL WASHINGTON CO. | PAVEMENT JOINT AT NC 32 TO SR 1125 1 2 | MD NO | NO | YES | 3.93 32 | 1 | 300 | 5 | 0.20 | 73,779 | 6,478 | | | 6,730 | 456 | 203 | 3,320 | | | 41,500 | | | | | 200 | 100 | 200 | 0.2 | 1 | | | |
| 2021CPT.01.03.10941.1 | Washington | 2 US 64 WBL WASHINGTON CO. | SR 1125 TO PAVEMENT JOINT AT NC 32 1 2 | MD NO | NO | YES | 3.93 32 | * | | 5 | | 73,779 | 6,229 | | | 6,704 | 402 | 202 | 3,320 | | | 41,500 | | | | | 200 | 100 | 200 | (| | | | |
| 2021CPT.01.03.20481.1 | Hyde | 3 SR 1340 GALL RD. | SR 1303 TO DEAD END 2 2 | 2WU NO | NO | NO | 1.56 18 | * | 468 | 31 | 3.12 | | 125 | | 1,369 | | 92 | | | | | | 197 | 3 | | | 100 | 50 | 100 | 1.60 | 2 | | | |
| 2021CPT.01.03.20891.1 | Tyrrell | 4 SR 1102 JONES RD. | NC 94 TO END OF PAVEMENT 2 2 | 2WU NO | NO | NO | 0.1 18 | * | 30 | 2 | 0.20 | | 200 | | 111 | | 7 | | | | | | 33 | 5 | | | 100 | 25 | 50 | 0.10 | 1 | | | |
| 2021CPT.01.03.20891.1 | Tyrrell | 5 SR 1316 GUM NECK LANDING RD. | SR 1320 TO SR 1314 2 2 | 2WU NO | NO | NO | 1.06 20 | * | 318 | 21 | 2.12 | | 125 | | 1,155 | | 77 | | | | | | 18 | 10 | | | 100 | 50 | 106 | 1.10 | 1 | | | |
| 2021CPT.01.03.20891.1 | Tyrrell | 6 SR 1320 S GUMNECK RD. | SR 1316 TO SR 1321 2 2 | 2WU NO | NO | NO | 1.7 20 | * | 510 | 34 | 3.40 | | 90 | | 1,723 | | 115 | | | | | | 39 | 10 | | | 100 | 50 | 100 | 1.70 | 2 | | | |
| 2021CPT.01.03.20941.1 | Washington | | DEAD END TO NC 32 2 2 | 2WU NO | NO | NO | 0.61 20 | * | 183 | 12 | 1.22 | | 300 | | 626 | | 42 | | | | | | | 10 | | | 50 | 25 | 50 | 0.61 | 1 | | | |
| 2021CPT.01.03.20941.1 | | | BRIDGE OVER MAIN CANAL TO NC 32 5 2 | 2WU NO | NO | NO | 0.21 22 | * | | 5 | | 2,710 | 1,018 | | 293 | | 20 | | | | | | | | | | 50 | 25 | 50 | , | | | | |
| 2021CPT.01.03.20941.1 | | | PAV. CHANGE AT M. P. 0.3 TO SR 1175 5 2 | 2WU NO | NO | NO | 0.34 22-36 | * | | 5 | | 5,210 | 649 | | 498 | | 33 | | | | | | 35 | | 3 | 2 | 50 | 25 | 50 | , | | | | |
| 2021CPT.01.03.20941.1 | | | SR 1142 TO SR 1156 3 2 | 2WU NO | NO | NO | 1.73 18 | * | 519 | 35 | 3.46 | - | 100 | | 1,613 | | 108 | | | 18,269 | 10,500 | | 132 | 10 | | | 100 | 50 | 100 | 1.70 | 2 | + | | |
| 2021CPT.01.03.20941.1 | | | SR 1129 TO NC 45 4 2 | 2WU NO | NO | NO | 1.28 20 | * | 528 | 26 | 2.56 | | | 610 | 1,258 | | 110 | | | 15,269 | 8,419 | | 135 | 10 | | | 100 | 50 | 100 | 1.30 | 2 | + | | |
| 2021CPT.01.03.20941.1 | | | US 64 TO DEAD END 5 2 | 2WU NO | NO | NO | 2.32 24-39 | * | | 5 | | 38,486 | 5,202 | | 3,696 | | 248 | | | , | , | | 4 | | 35 | 25 | 50 | 25 | 50 | , — — — — — | | 4 | 604 | 100 |
| | GRAN | | | | | | 18.77 | 1 | 2,856 | 186 | 16.28 | 193,964 | 20,516 | 610 | 12,342 | 13,434 | 1,710 | 405 | 6,640 | 33,538 | 18,919 | 83,000 | 593 | 58 | 38 | 27 | 1,200 | 575 | 1,156 | 8.31 | 12 | 4 | 604 | 100 |

| PROJECT NO | COUNTY | MAP ROUTE | DESCRIPTION | TYP LANES L | ANE FINAL | WARM N | /IIX MAT | RIAL LEI | IGTH WI | IDTH W | ORK ZONE | TEMPORARY | THERMO | THERMO | GENERIC | GENERIC | GENERIC | GENERIC | THERMO | THERMO | THERMO | THERMO | PAINT LINE, | PAINT LINE, | PAINT LINE | , PAINT | PAINT | PAINT | PAINT | PAINT | PAINT | PAINT | GENERIC | GENERIC | GENERIC |
|------------------|------------|--------------------------------|-------------------------------------|-------------|------------|---------|----------|----------|---------|--------|----------|-----------|-------------|------------------|----------------------|------------------|----------------------|--------------|--------------|----------|----------|------------|-------------|-------------|------------|-----------|-----------|-----------|--------------|-------------|----------|----------|------------|------------|-------------|
| | | NO | | NO 1 | YPE SURFAC | E ASPHA | LT TRAN | ISFER | | | ADV/GEN | TRAFFIC | PAVEMENT | PAVEMENT | PAVEMEN ⁻ | PAVEMENT | PAVEMEN [*] | PAVEMENT | PAVEMENT | PAVEMEN1 | PAVEMENT | T PAVEMEN | 4" WHITE | 4" YELLOW | 16" | LINE, 24" | CHARACTER | CHARACTER | PAVEMENT | PAVEMENT | PAVEMENT | PAVEMENT | PAVEMENT | PAVEMENT | PAVEMENT |
| | | | | | TESTING | REQUIR | ED VEH | ICLE | | \ | WARNING | CONTROL | MARKING | MARKING LINES, 6 | 5" MARKING | MARKING | MARKING | MARKING | MARKING | MARKING | MARKING | MARKING | | | | | (RXR) | (SCHOOL) | MARKING | MARKING | MARKING | MARKING | MARKING | MARKING | MARKING ITE |
| | | | | | REQUIRE | :D | | | | | SIGNING | | LINES, 6" X | X 90 MILS YELLOV | V ITEM | ITEM | ITEM | ITEM | ARROW, | ARROW, | ARROW, | ARROW, | | | | | | | SYMBOL, LEFT | SYMBOL, | SYMBOL, | SYMBOL, | ITEM (HOT | ITEM (HOT | (NON-CAST I |
| | | | | | | | | | | | | | 90 MILS | | (THERMO | (THERMO | (THERMO | (THERMO | LEFT 90 MILS | RIGHT | STRAIGHT | STRAIGHT 8 | k | | | | | | ARROW | RIGHT ARROW | STRAIGHT | STRAIGHT | SPRAY | SPRAY | SNOWPLOW |
| | | | | | | | | | | | | | WHITE | | LINES, 16" | (LINES, 24") | CHARACTE | R CHARACTER | | 90 MILS | 90 MILS | LEFT 90 | | | | | | | | | ARROW | & LEFT | THERMO, 4" | THERMO, 4" | PAVEMEI |
| | | | | | | | | | | | | | | | 90 MILS) | 90 MILS) | RXR 90 MIL | S) SCHOOL 90 | | | | MILS | | | | | | | | | | ARROW | X 50 MILS | X 50 MILS | MARKE |
| | | | | | | | | | | | | | | | | | | MILS) | | | | | | | | | | | | | | | WHITE) | YELLOW) | 1 |
| | | | | | | | | | VII F | FT | SF | LS | LF | LF | LF | LF | EA | EA | EA | EA | EA | EA | LF | LF | LF | LF | EA | EA | EA | EA | EA | EA | LF | LF | EA |
| T.01.03.10941.1 | Washington | 1 US 64 EBL WASHINGTON CO. | PAVEMENT JOINT AT NC 32 TO SR 1125 | 1 2 | MD NO | NO | Y | S 3 | .93 | 32 | 176 | 1 | 25,938 | 20,750 | | | | | | 2 | 4 | | 77,814 | 62,250 | | | | | 2 | | 4 | | | | 260 |
| T.01.03.10941.1 | Washington | 2 US 64 WBL WASHINGTON CO. | SR 1125 TO PAVEMENT JOINT AT NC 32 | 1 2 | MD NO | NO | Υ | S 3 | .93 | 32 | 192 | * | 25,938 | 20,750 | | | | | | 2 | 4 | | 77,814 | 62,250 | | | | | | 2 | 4 | | | | 260 |
| PT.01.03.20481.1 | Hyde | 3 SR 1340 GALL RD. | SR 1303 TO DEAD END | 2 2 2 | WU NO | NO | N | 0 1 | .56 1 | 18 | | * | | | | | | | | | | | 16,474 | 12,355 | | | | | | | | | 16,474 | 12,355 | |
| T.01.03.20891.1 | Tyrrell | 4 SR 1102 JONES RD. | NC 94 TO END OF PAVEMENT | 2 2 2 | WU NO | NO | N | 0 (|).1 1 | 18 | | * | | | | | | | | | | | | | | | | | | | | | | | |
| PT.01.03.20891.1 | Tyrrell | 5 SR 1316 GUM NECK LANDING RD. | SR 1320 TO SR 1314 | 2 2 2 | WU NO | NO | N | 0 1 | .06 2 | 20 | | * | | | | | | | | | | | 11,194 | 8,395 | | | | | | | | | 11,194 | 8,395 | |
| PT.01.03.20891.1 | Tyrrell | 6 SR 1320 S GUMNECK RD. | SR 1316 TO SR 1321 | 2 2 2 | WU NO | NO | N | 0 : | L.7 2 | 20 | | * | | | | | | | | | | | 18,058 | 13,543 | | | | | | | | | 18,058 | 13,543 | |
| PT.01.03.20941.1 | Washington | 7 SR 1117 STYONS RD. | DEAD END TO NC 32 | 2 2 2 | WU NO | NO | N | 0 0 | .61 2 | 20 | | * | | | | | | | | | | | 6,336 | 4,752 | | | | | | | | | 6,336 | 4,752 | |
| PT.01.03.20941.1 | Washington | 8 SR 1122 OLD US 64 | BRIDGE OVER MAIN CANAL TO NC 32 | 5 2 2 | WU NO | NO | N | 0 0 | .21 2 | 22 | | | | | | 50 | | | | 1 | | 1 | 2,218 | 1,663 | | 50 | | | | 1 | | 1 | 2,218 | 1,663 | |
| PT.01.03.20941.1 | Washington | 9 SR1125 MILL POND RD | PAV. CHANGE AT M. P. 0.3 TO SR 1175 | 5 2 2 | WU NO | NO | N | 0 0 | .34 22 | 2-36 | | * | | | | 40 | | 12 | | | | | 3,590 | 2,693 | | 40 | | 12 | | | | | 3,590 | 2,693 | |
| PT.01.03.20941.1 | Washington | 10 SR 1155 OLD CHERRY RD. | SR 1142 TO SR 1156 | 3 2 2 | WU NO | NO | N | 0 1 | .73 1 | 18 | 40 | * | | | | | | | | | | | 18,269 | 13,702 | | | | | | | | | 18,269 | 13,702 | |
| PT.01.03.20941.1 | Washington | 11 SR 1128 "A" CANAL RD. | SR 1129 TO NC 45 | 4 2 2 | WU NO | NO | N | 0 1 | .28 2 | 20 | 40 | * | | | | | | | | | | | 13,517 | 10,138 | | | | | | | | | 13,517 | 10,138 | |
| PT.01.03.20941.1 | | | US 64 TO DEAD END | 5 2 2 | WU NO | NO | N | O 2 | .32 24 | 1-39 | 834 | * | | | 50 | 340 | 2 | 12 | 12 | | | | 12,000 | 49,000 | 50 | 340 | 2 | 12 | 12 | | | | 12,000 | 24,500 | |
| | _ | D TOTAL | | | | | | 19 | 3.77 | | 1.282 | 1 | 51,876 | 41.500 | 50 | 430 | 2 | 2/ | 12 | E | Q | 1 | 257,284 | 240,741 | 50 | 430 | 2 | 2/ | 1/1 | 2 | Q | 1 | 101,656 | 91.741 | 520 |

PROJECT REFERENCE NO 2021CPT.01.03.10941.1, ETC

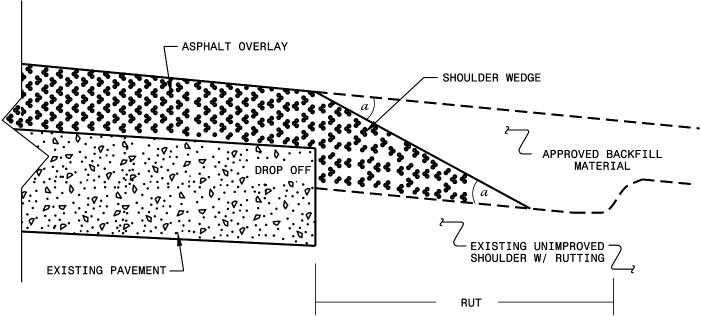
NOTES:

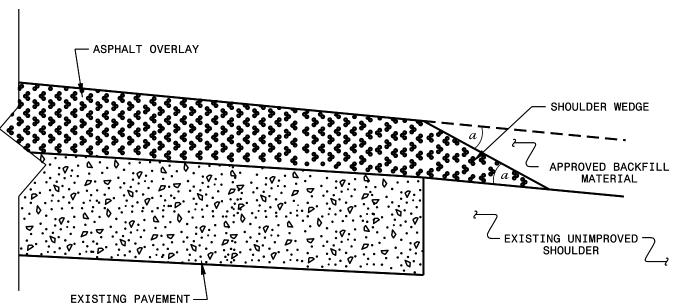
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)





SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT Office 919-707-6950 FAX 919-250-4119

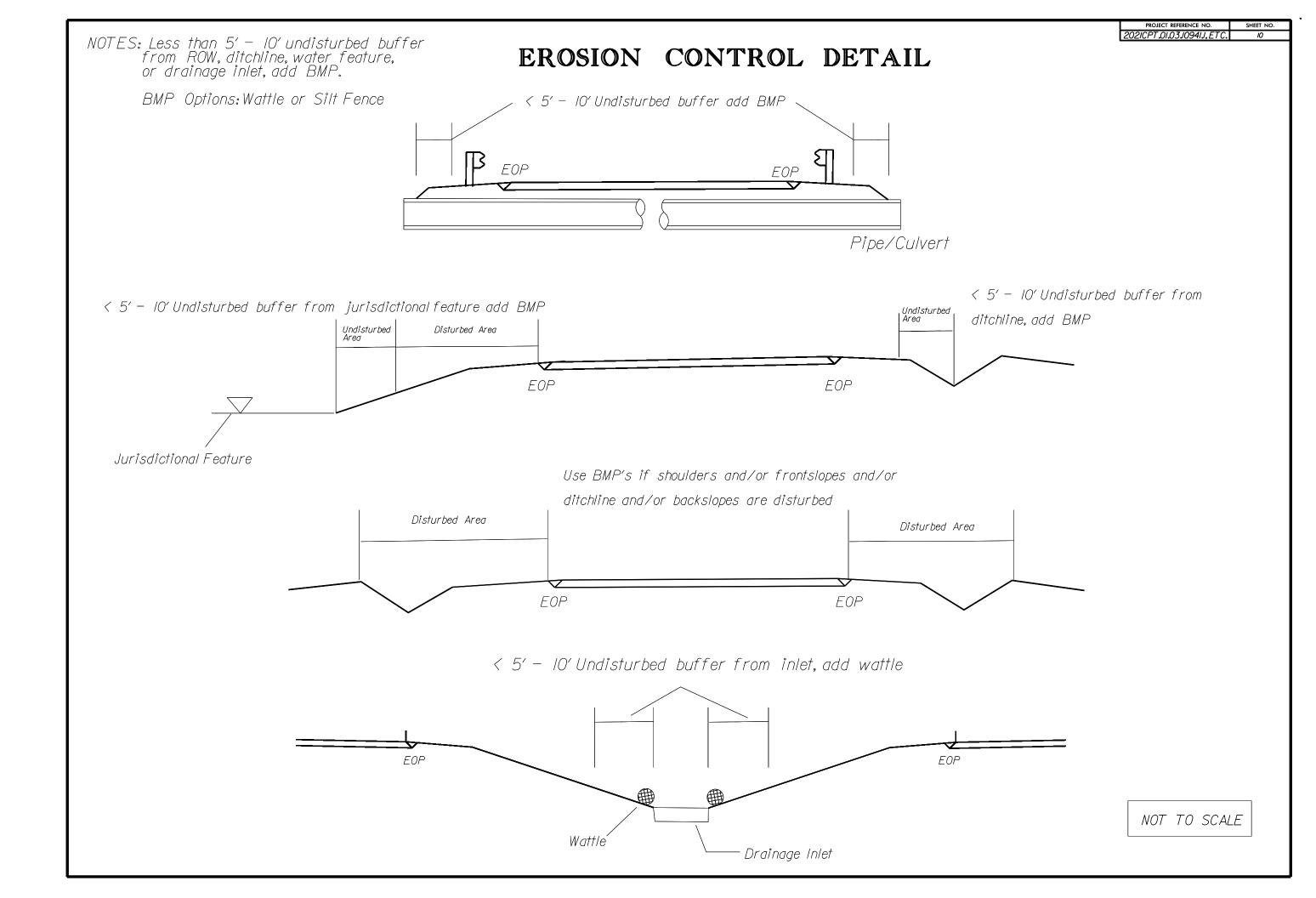
SHOULDER WEDGE **DETAILS**

| ORIGINAL BY: | T.SPELL DATE: | 7-19-11 |
|--------------|---------------------------------|--------------|
| MODIFIED BY: | DATE | 10/16/12 |
| CHECKED BY:_ | DATE: | : |
| FILE SPEC.: | s:usr/details/stand/shoulderwed | gedetail.dgn |

SHOULDER WEDGE DETAIL

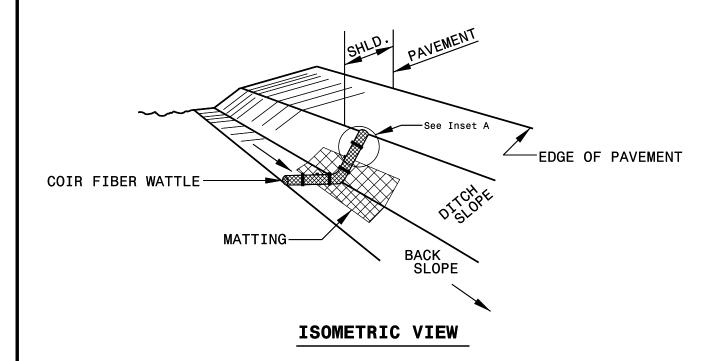
(Resurfacing Adjacent to Rutted Shoulder)

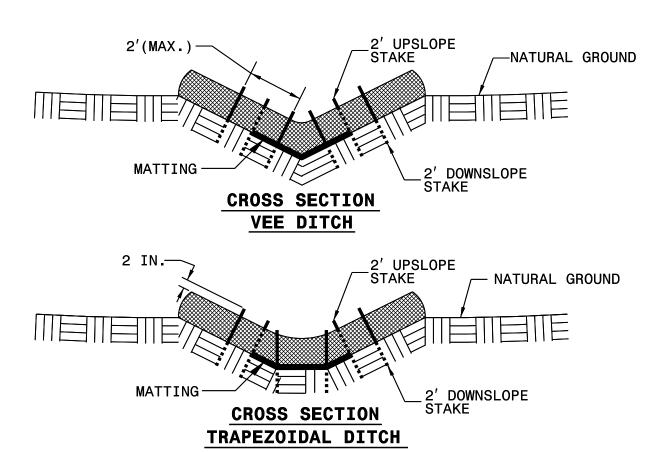
PROPOSED PAVEMENT -



PROJECT REFERENCE NO. SHEET NO. 2021CPT.01.03.10941.J, ETC. 11

COIR FIBER WATTLE DETAIL





NOTES:

USE MINIMUM 12 IN. DIAMETER COIR FIBER (COCONUT FIBER) WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

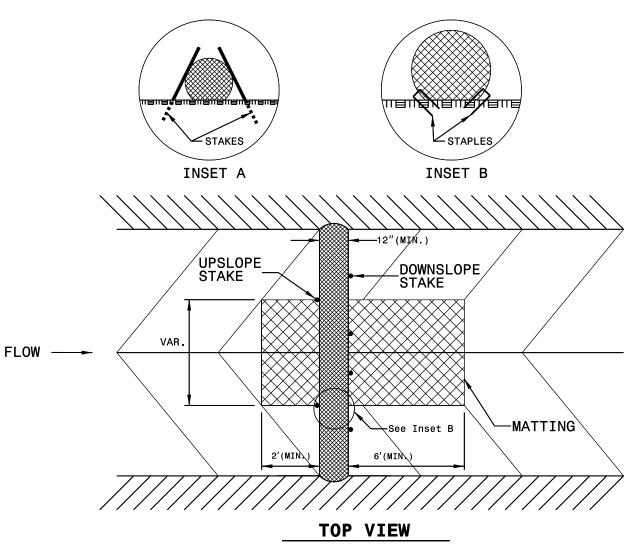
 $\underline{\text{ONLY}}$ Install wattle(s) to a height in ditch so flow will not wash around wattle and scour ditch slopes and as directed.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



PROJECT REFERENCE NO. SHEET NO. 02ICPT.01.03.10941J.ETC. 12

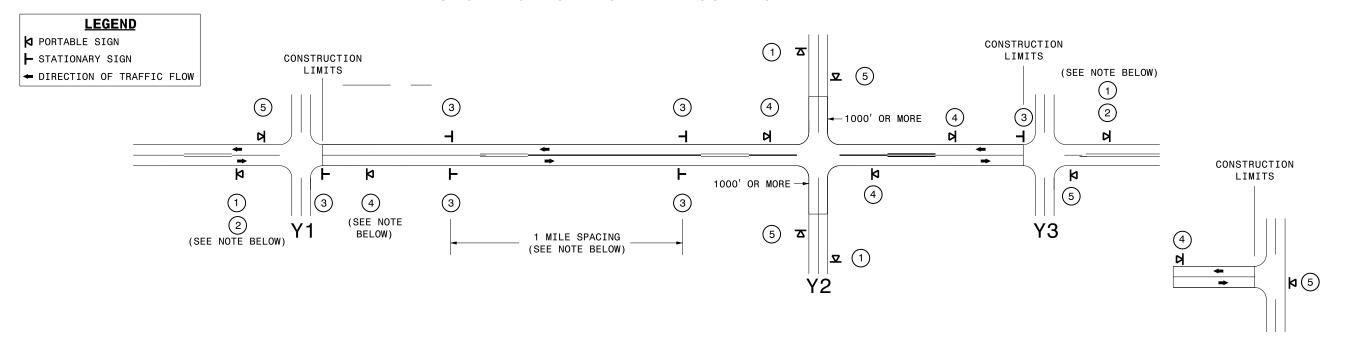
DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA

SOIL STABILIZATION TIMEFRAMES

| SITE DESCRIPTION | STABILIZATION TIME | TIMEFRAME EXCEPTIONS |
|--|--------------------|--|
| PERIMETER DIKES, SWALES, DITCHES AND SLOPES | 7 DAYS | NONE |
| HIGH QUALITY WATER (HQW) ZONES | 7 DAYS | NONE |
| SLOPES STEEPER THAN 3:1 | 7 DAYS | IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED. |
| SLOPES 3:1 OR FLATTER | I4 DAYS | 7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH. |
| ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1 | I4 DAYS | NONE, EXCEPT FOR PERIMETERS AND HOW ZONES. |

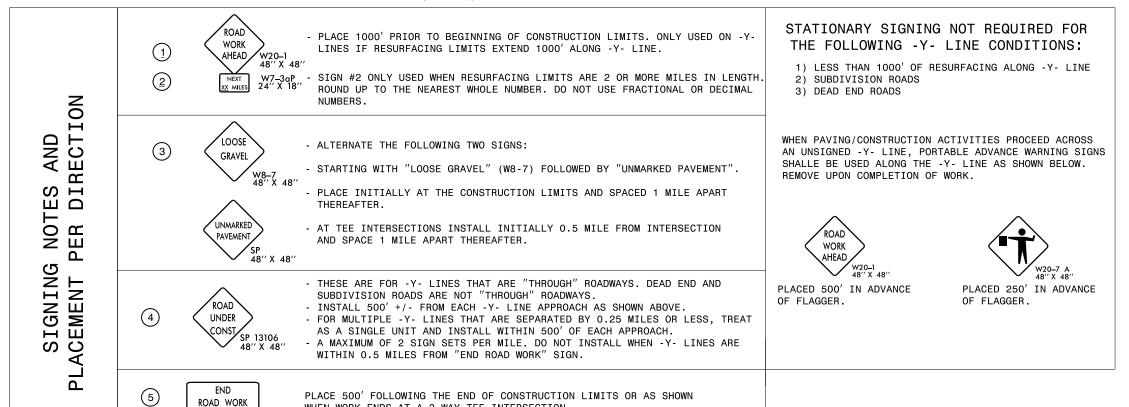
PROJ. REFERENCE NO. SHEET NO. 221CPT.01.03.10941.1, ETC. 13

SIGNING FOR ASPHALT SURFACE TREATMENT



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING





ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT

TEE INTERSECTION

06-AUG-2020 08:05

MAPS LESS THAN 2 MILES G20-2 A 48" X 24"

START OF CONTRACT WORK.

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE

WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

PROJ. REFERENCE NO. SHEET NO. 2021CPT.01.03.10941.1, ETC. 14

SIGNING FOR RESURFACING PROJECTS

<u>LEGEND</u>

→ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW

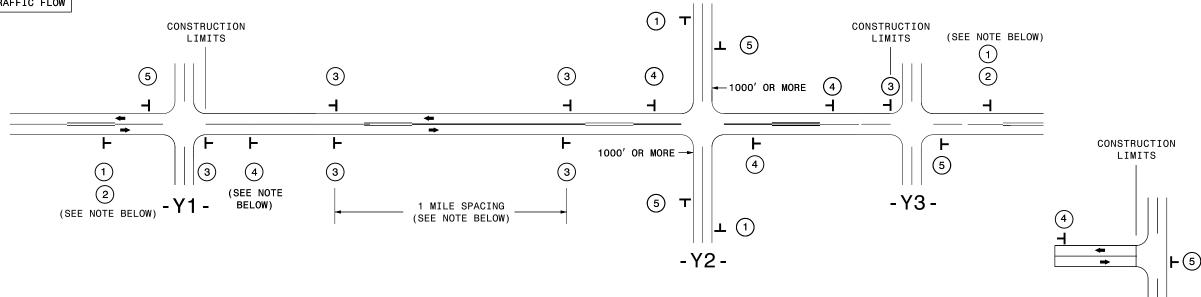
TES AND DIRECTION

NOT

SIGNING

PER

PLACEMENT



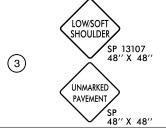
TEE INTERSECTION -Y- LINE SIGNING

MAINLINE (-L-) SIGNING

ROAD PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS.
ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.

AHEAD W20-1
48" X 48"

#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)



ROAD

UNDER

G20-2 A

W7_3aP

(1)

(2)

(4)

- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.
- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.
- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.
- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.
- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS,
- TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.
 A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES
- ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.
- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.

5 END PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

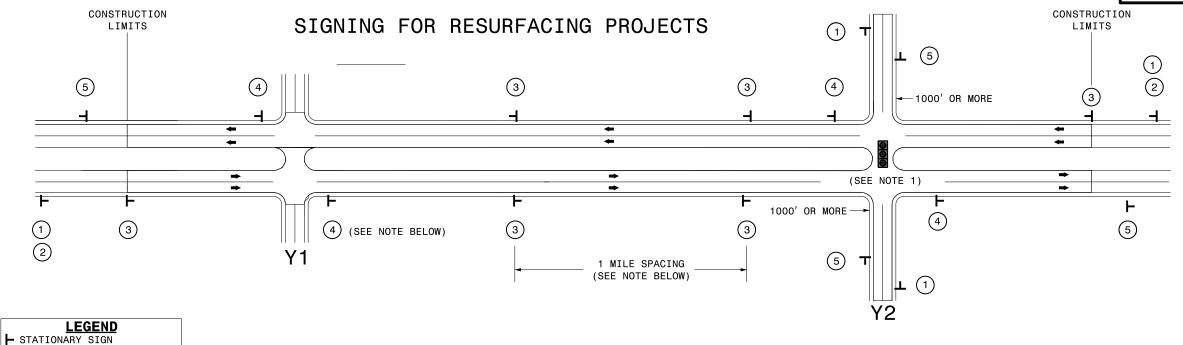


PLACED 500' IN ADVANCE OF FLAGGER.



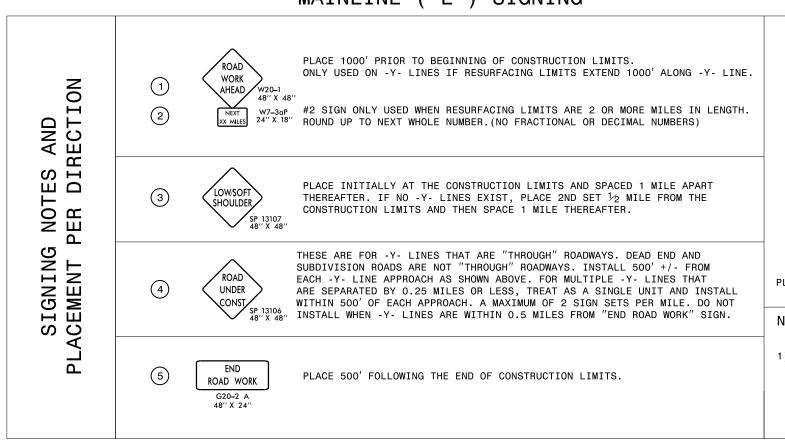


ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2-LANE ROADWAY
RESURFACING



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING



NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.





PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER

NOTES:

1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.



RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS

- DIRECTION OF TRAFFIC FLOW