STATE	STATE PROJECT REFERENCE NO.	SHEET NO.
N.C.	2021CPT.01.04.10661.1, ETC.	1

STATE PROJ. NO.	DESCRIPTION
2021CPT.01.04.10661.1, ETC,	PE, CONST
2021CPT.01.04.20661.1, ETC,	PE, CONST

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

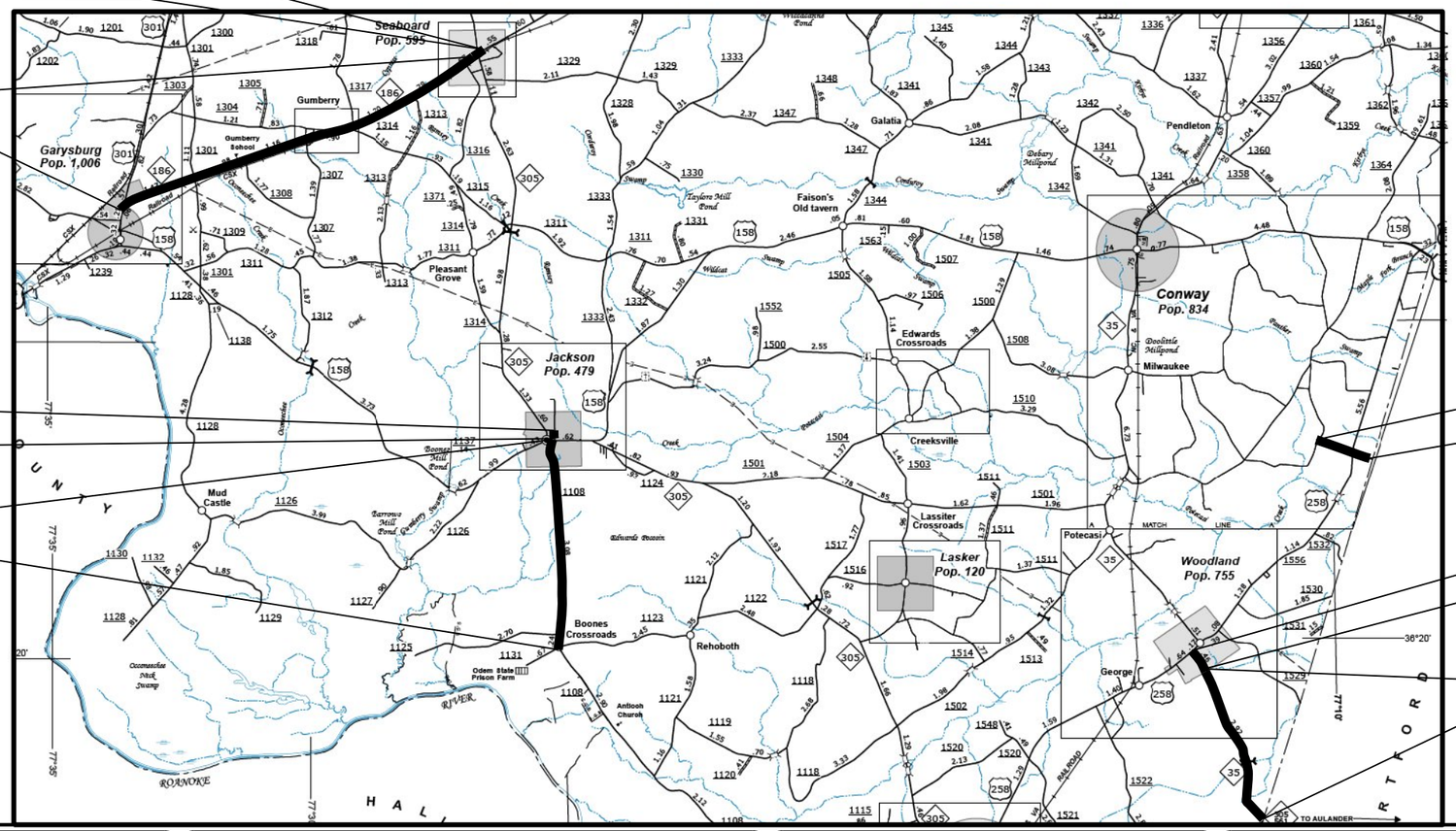
NORTHAMPTON COUNTY

LOCATION:

MAP 1 – NC 186 FROM US 301 TO SEABOARD CURB & GUTTER
MAP 2 – NC 186 FROM CURB & GUTTER TO END C&G
MAP 3 – NC 35 FROM NC 305 TO WOODLAND CURB & GUTTER
MAP 4 – NC 35 FROM CURB & GUTTER TO US 258
MAP 5 – SR 1108(BRYANTTOWN RD) FROM SR 1123(REHOBOTH CHURCH RD) TO US 158
MAP 6 – SR 1368(ATHERTON ST) FROM CALHOUN ST TO US 158
MAP 7 – SR 1542(PINETOPS RD) FROM HERTFORD COUNTY LINE TO SR 1541 (BURNT BRIDGES RD)

TYPE OF WORK: AST (SINGLE/DOUBLE SEAL), MILLING & RESURFACING

END MAP 2
BEGIN MAP 2
END MAP 1
BEGIN MAP 1



BEGIN MAP 6
END MAP 6
END MAP 5
BEGIN MAP 5

END MAP 7
BEGIN MAP 7
END MAP 4
BEGIN MAP 4
END MAP 3
BEGIN MAP 3

GRAPHIC SCALES

NTS

LENGTH ROADWAY MAP #1 =	6.94	MILES
LENGTH ROADWAY MAP #2 =	0.22	MILES
LENGTH ROADWAY MAP #3 =	2.95	MILES
LENGTH ROADWAY MAP #4 =	0.39	MILES
LENGTH ROADWAY MAP #5 =	3.82	MILES
LENGTH ROADWAY MAP #6 =	0.08	MILES
LENGTH ROADWAY MAP #7 =	1.05	MILES

Prepared In the Office of:
DIVISION OF HIGHWAYS
113 AIRPORT DR., EDENTON, NC, 27932

2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E
DIVISION PROJECT TEAM LEAD

CHRIS SLACHTA
DIVISION PROPOSALS ENGINEER

S.P. FENWICK, PLS
DESIGN ENGINEER

STATE OF NORTH CAROLINA • DEPARTMENT OF TRANSPORTATION •

CONTRACT: DA00479 WBS PROJECT: 2021CPT.01.04.10661.1, ETC.

09/08/99

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CONTRACT: DA00479 WBS PROJECT: 2021CPT.01.04.10661.1, ETC.

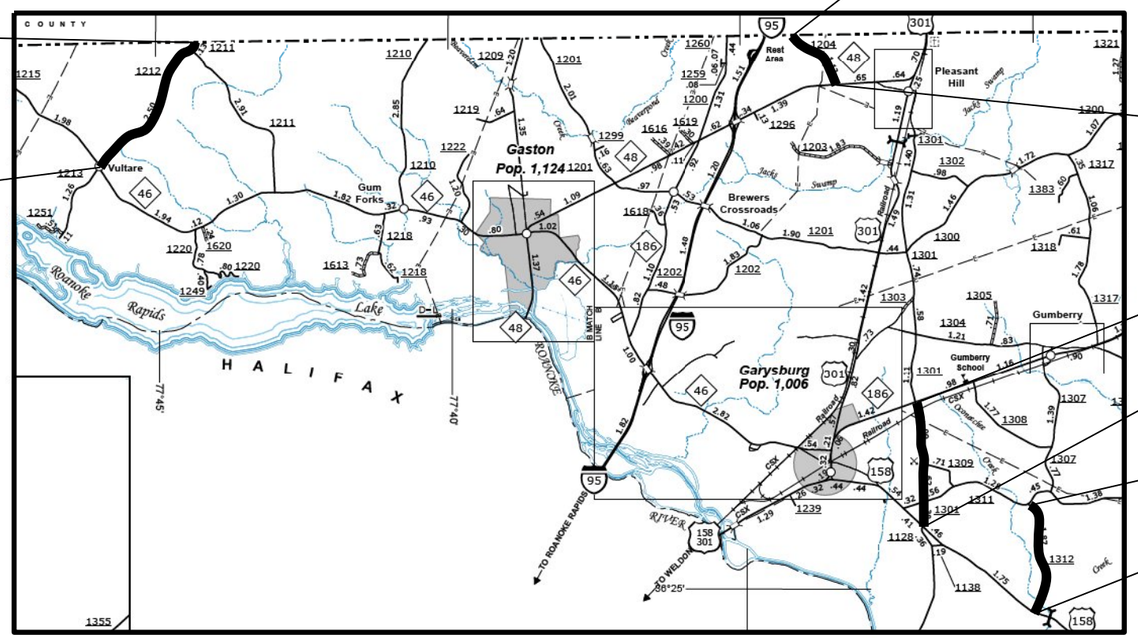
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.
N.C.	2021CPT.01.04.10661.1, ETC.	2
STATE PROJ. NO.		DESCRIPTION
2021CPT.01.04.20661.1		PE, CONST

NORTHAMPTON COUNTY

LOCATION: MAP 8 – SR 1212 (OAK GROVE CHURCH RD) FROM VA STATE LINE TO NC 46
MAP 9 – SR 1204 (JAMES MASSEY RD) FROM VA STATE LINE TO NC 48
MAP 10 – SR 1301 (CORNWALLIS RD) FROM NC 186 TO US 158
MAP 11 – SR 1312 (ST JOHNS CHURCH ROAD) FROM SR 1311 (JACKSON BYPASS RD) TO US 158

TYPE OF WORK: AST (SINGLE/DOUBLE SEAL), MILLING & RESURFACING



GRAPHIC SCALES		Prepared in the Office of: DIVISION OF HIGHWAYS, 113 AIRPORT DR., EDENTON, NC, 27932		
NTS	LENGTH ROADWAY MAP #8 = 2.63 MILES LENGTH ROADWAY MAP #9 = 1.12 MILES LENGTH ROADWAY MAP #10 = 1.99 MILES LENGTH ROADWAY MAP #11 = 1.87 MILES	2018 STANDARD SPECIFICATIONS W.B. HOBBS, P.E, DIVISION PROJECT TEAM LEAD CHRIS SLACHTA, DIVISION PROPOSALS ENGINEER	S.P. FENWICK, PLS, DESIGN ENGINEER	STATE OF NORTH CAROLINA · DEPARTMENT OF TRANSPORTATION ·

07-OCT-2020 11:18
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$$$$USERNAME$$$$
09/08/99

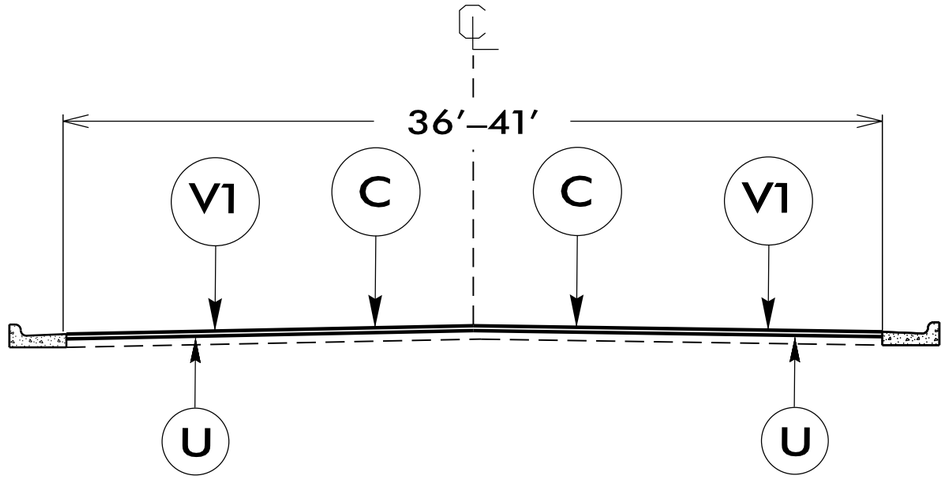
PAVEMENT SCHEDULE

C	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C1	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
T	EARTH MATERIAL
U	EXISTING PAVEMENT.
V1	MILLING ASPHALT PAVEMENT. 1.5″ IN DEPTH.

NOTES:

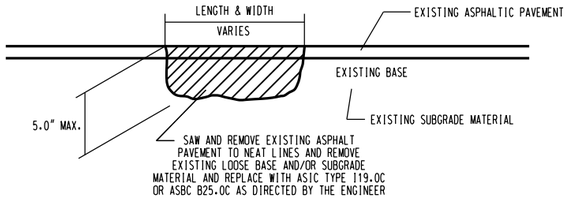
ALL PAVED S. R. ROADS OR RAMPS TO BE RESURFACED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES



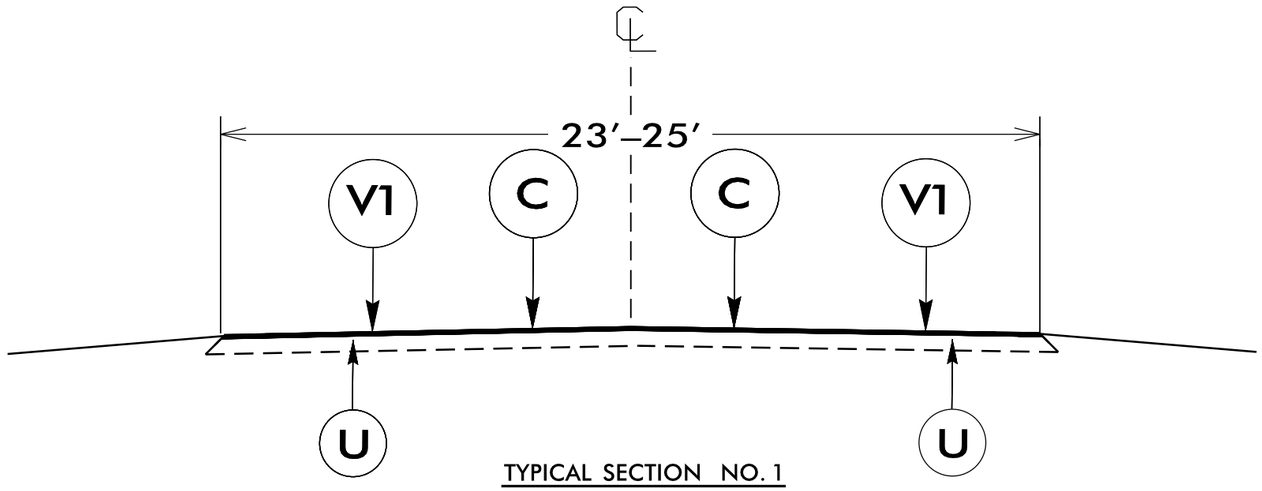
TYPICAL SECTION NO. 2

USE WITH: MAP 2 & MAP 4



*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING 0 – 5″



TYPICAL SECTION NO. 1

USE WITH: MAP 1 & MAP 3

PROJECT REFERENCE NO.	SHEET NO.
2021CPT.01.04.10661J, ETC.	4

PAVEMENT SCHEDULE

C	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C1	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
T	EARTH MATERIAL
U	EXISTING PAVEMENT.
V1	MILLING ASPHALT PAVEMENT. 1.5″ IN DEPTH.

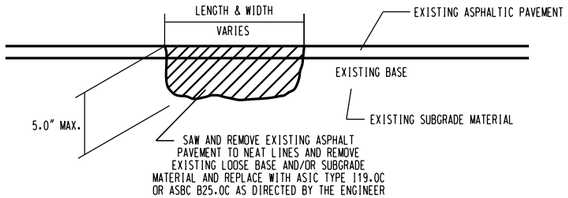
NOTES:

ALL PAVED S. R. ROADS OR RAMPS TO BE RESURFACED AND / OR TREATED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE, PRIOR TO FINAL SURFACE LAYER BEING INSTALLED

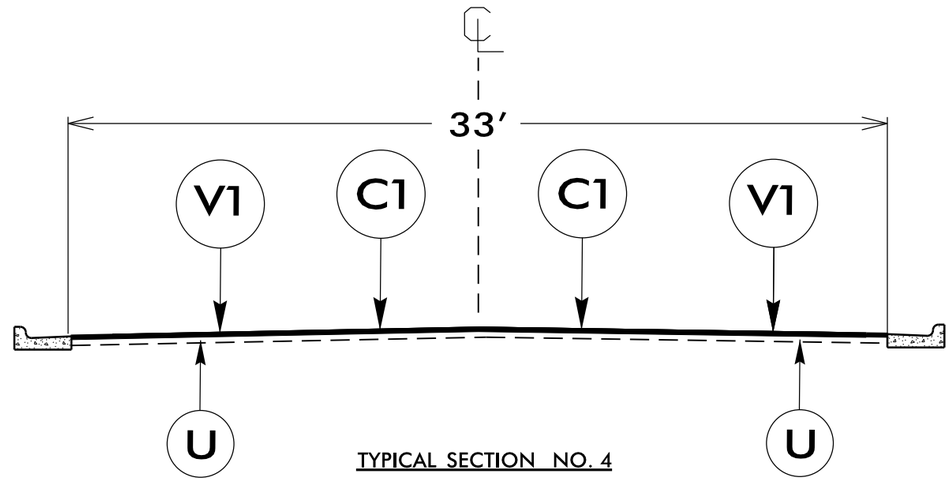
ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT:
BOTTOM LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY.
TOP LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.25 + GAL/SY AND A LAYER OF:5/16″ LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX. 9+ LBS/SY.



*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

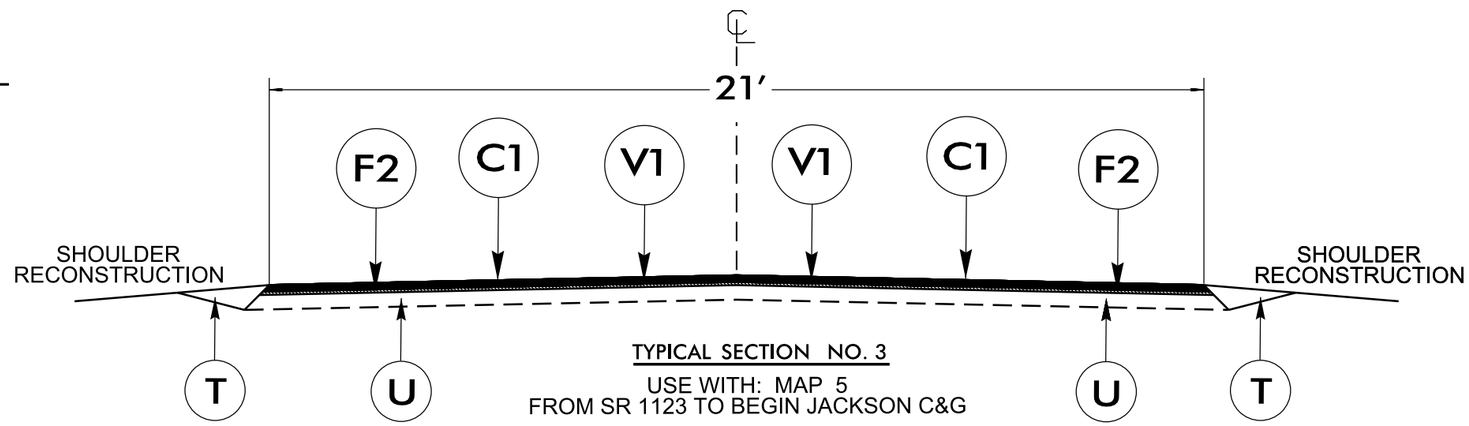
FULL DEPTH PATCHING, 0 - 5″

*MAP #6 HAS PAVEMENT IN THE GUTTER AND SHOULD BE PAVED BACK AS IT EXISTS



TYPICAL SECTION NO. 4

USE WITH: MAP 5 - FROM BEGIN JACKSON C&G TO US 158
MAP 6



TYPICAL SECTION NO. 3

USE WITH: MAP 5
FROM SR 1123 TO BEGIN JACKSON C&G

PAVEMENT SCHEDULE

C	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C1	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
T	EARTH MATERIAL
U	EXISTING PAVEMENT.
V1	MILLING ASPHALT PAVEMENT. 1.5″ IN DEPTH.

NOTES:

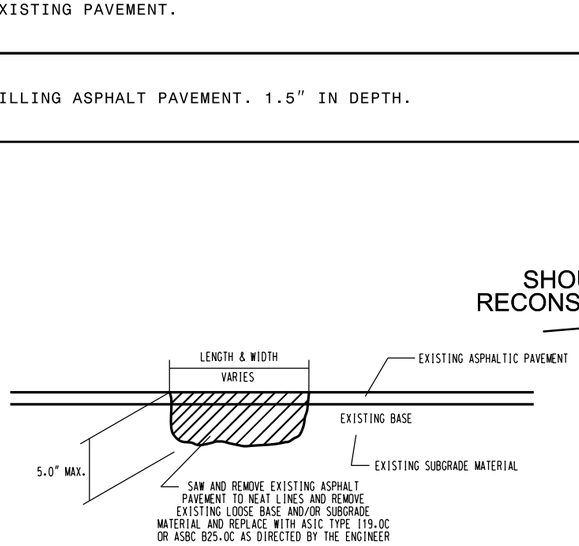
ALL PAVED S. R. ROADS OR RAMPS TO BE RESURFACED AND / OR TREATED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE, PRIOR TO FINAL SURFACE LAYER BEING INSTALLED

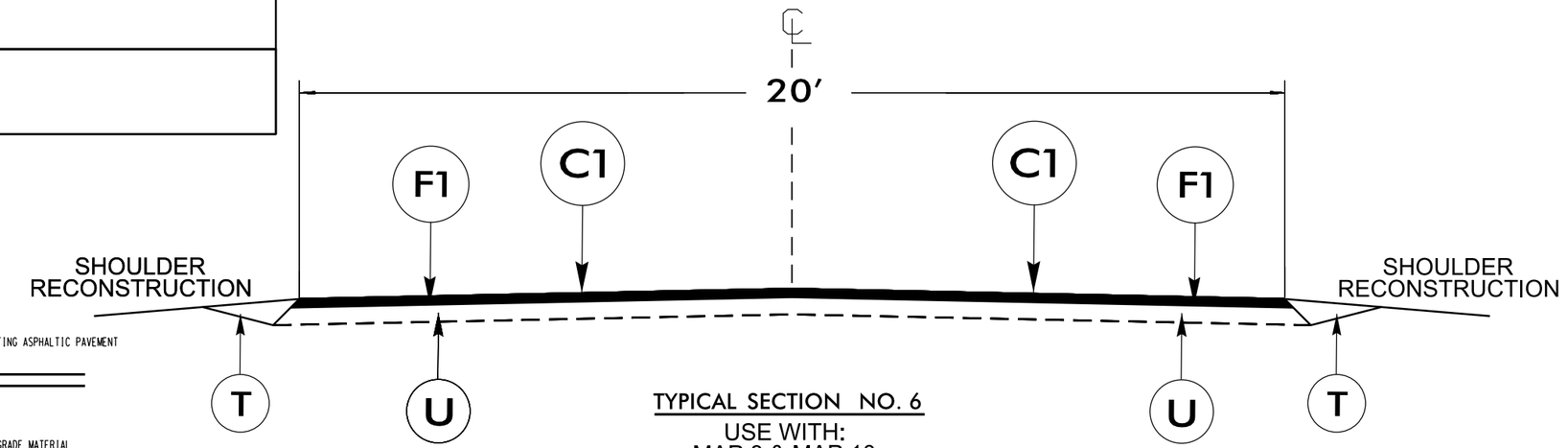
ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT:
BOTTOM LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY.
TOP LAYER CONSISTS OF EMULISIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.25 + GAL/SY AND A LAYER OF:5/16″ LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX. 9+ LBS/SY.

ASPHALT SURFACE TREATMENT (SINGLE SEAL) CONSISTS OF: ONE LAYER OF EMULSIFIED ASPHALT GRADE CRS-2L AT A RATE OF 0.28 GAL/SY AND ONE LAYER OF 5/16 LIGHTWEIGHT AGGREGATE AT A RATE OF 9 LBS/SY.

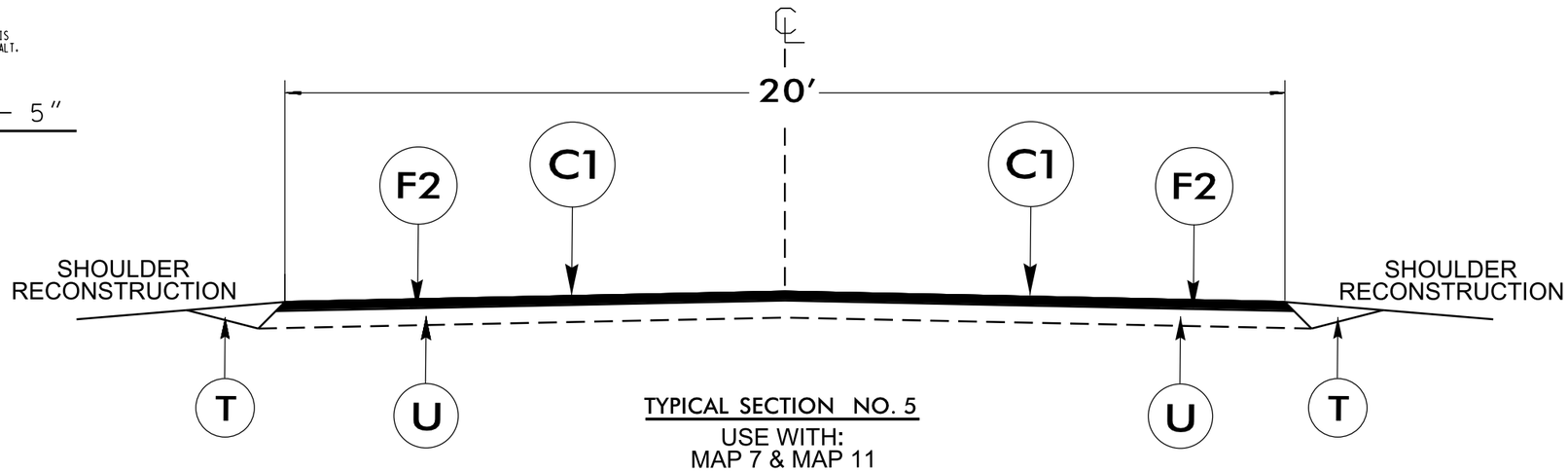


*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING, 0 - 5″



TYPICAL SECTION NO. 6
USE WITH:
MAP 8 & MAP 10



TYPICAL SECTION NO. 5
USE WITH:
MAP 7 & MAP 11

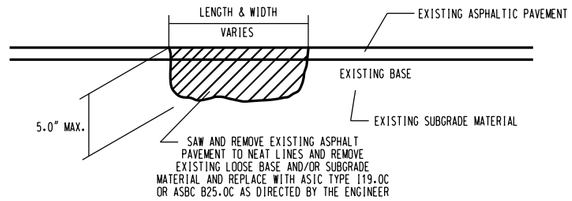
PAVEMENT SCHEDULE

C	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C1	PROP. APPROX. 1.5″ ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F1	ASPHALT SURFACE TREATMENT, SINGLE SEAL
F2	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
T	EARTH MATERIAL
U	EXISTING PAVEMENT.
V1	MILLING ASPHALT PAVEMENT. 1.5″ IN DEPTH.

NOTES:

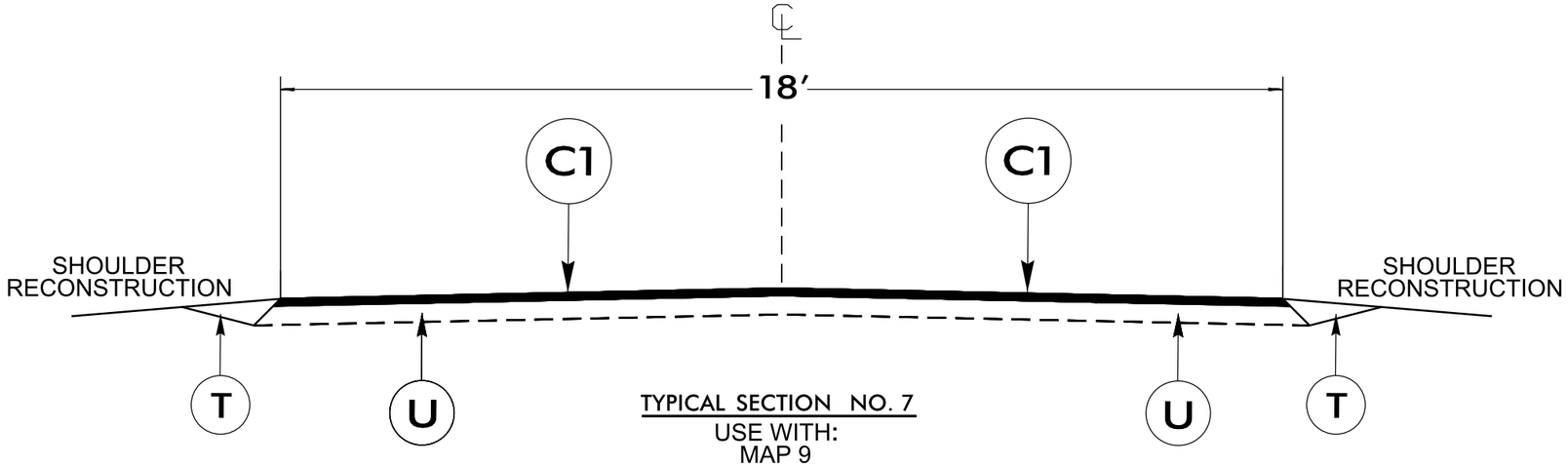
ALL PAVED S. R. ROADS OR RAMPS TO BE RESURFACED AND / OR TREATED TO THE ENDS OF THE RADII, OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES



*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING, 0 – 5″



TYPICAL SECTION NO. 7

USE WITH:
MAP 9

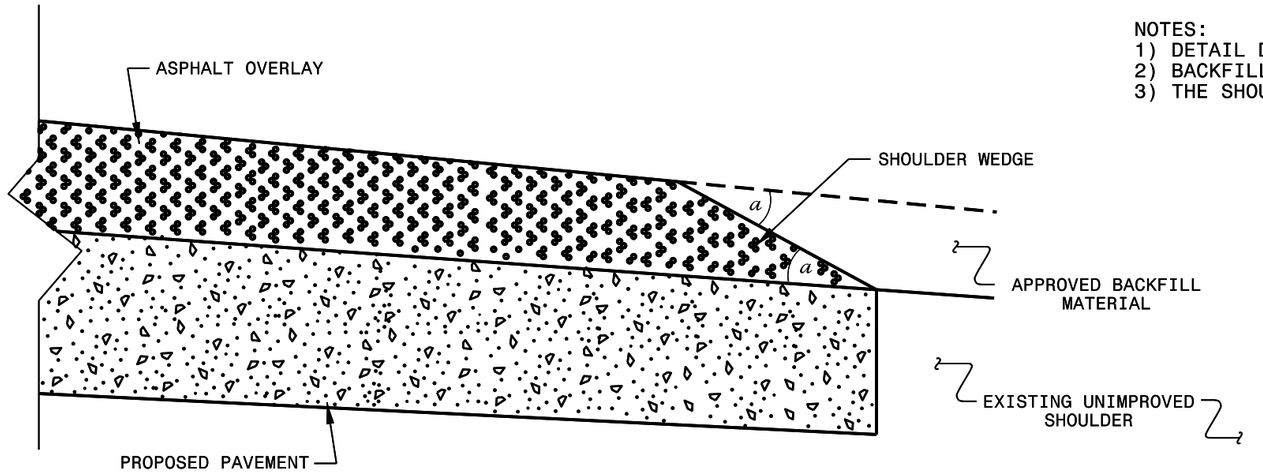
PROJECT NO.	SHEET NO.
2021CPT.01.04.10661.1, ETC.	6

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	MATERIAL TRANSFER VEHICLE	LENGTH	WIDTH	MOBILIZATION	BORROW	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1½" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5C	ASPHALT FOR PLANT MIX	ASPHALT SURFACE TREATMENT, DOUBLE SEAL	ASPHALT SURFACE TREATMENT, SINGLE SEAL	EMULSION FOR ASPHALT SURFACE TREATMENT	FULL DEPTH PATCHING, 0-5"	ADJUSTMENT OF MANHOLES	ADJUSTMENT OF METER OR VALVE BOX	STEEL BEAM GUARDRAIL	GUARDRAIL END UNITS, TYPE TL-2	REMOVE EXISTING GUARDRAIL
											MI	FT	LS	CY	TONS	SMI	SY	SY	TONS	TONS	TONS	SY	SY	GAL	TON	EA	EA	LF	EA	LF
2021CPT.01.04.10661.1	Northampton	1	NC 186	US 301 TO BEGIN SEABOARD C&G	1	2	2WU	NO	NO	YES	6.94	25	1		140		102,479	3,848		8,885	533						3			
2021CPT.01.04.10661.1	Northampton	2	NC 186 (C&G)	BEGIN AT SEABOARD C&G TO END C&G	2	2	2WU	NO	NO	YES	0.22	41	*		4		5,292	797		513	31				47	3	2			
2021CPT.01.04.10661.1	Northampton	3	NC 35	NC 305 TO WOODLAND CITY LIMITS C&G	1	2	2WU	NO	NO	YES	2.95	23	*		59		39,805			3,364	202				40			713	4	813
2021CPT.01.04.10661.1	Northampton	4	NC 35 (C&G)	WOODLAND CITY LIMITS C&G TO US 258	2	2	2WU	NO	NO	YES	0.39	36	*		8		8,237	282		718	43				30	18				
2021CPT.01.04.20661.1	Northampton	5	SR 1108 BRYANTTOWN RD	FROM SR 1123 TO US 158	3&8	2	2WU	NO	NO	NO	3.82	21-33	*	996	66	6.64	8,213	195	4,210		336	41,702		22,936		3	3			
2021CPT.01.04.20661.1	Northampton	6	SR 1368 ATHERTON ST	FROM CALHOUN ST TO US 158	4	2	2WU	NO	NO	NO	0.08	33	*				1,549	230	156		10					2	3			
2021CPT.01.04.20661.1	Northampton	7	SR 1542 PINETOPS RD	HERTFORD CO. LINE TO SR 1541	5	2	2WU	NO	NO	NO	1.05	20	*	315	21	2.10		125	1,023		69	12,320		6,647	2					
2021CPT.01.04.20661.1	Northampton	8	SR 1212 OAK GROVE CHURCH RD	FROM VA LINE TO NC 46	6	2	2WU	NO	NO	NO	2.63	20	*	789	53	5.26		25	2,606		175		30,859	9,348	5					
2021CPT.01.04.20661.1	Northampton	9	SR 1204 JAMES MASSEY RD	FROM VA LINE TO NC 48	7	2	2WU	NO	NO	NO	1.12	18	*	339	22	2.24		50	991		66				2					
2021CPT.01.04.20661.1	Northampton	10	SR 1301 CORNWALLIS RD	FROM NC 186 TO US 158	6	2	2WU	NO	NO	NO	1.99	20	*	591	40	3.98			1,990		133		23,648	7,024	9					
2021CPT.01.04.20661.1	Northampton	11	SR 1312 ST JOHNS CHURCH RD	FROM SR 1311 TO US 158	5	2	2WU	NO	NO	NO	1.87	20	*	558	37	3.74		100	1,847		124	21,941		12,003						
GRAND TOTAL											23.06		1	3,588	450	23.96	165,575	5,652	12,823	13,480	1,722	75,963	54,507	57,958	135	26	11	713	4	813

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	WORK ZONE ADV/GEN WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	PAVEMENT MARKING LINE, THERMO 6" X 90 MILS WHITE	PAVEMENT MARKING LINE, THERMO 6" X 90 MILS YELLOW	PAVEMENT MARKING LINE, 4" PAINT WHITE	PAVEMENT MARKING LINE, 4" PAINT YELLOW	PAVEMENT MARKING LINE, 16" PAINT	PAVEMENT MARKING LINE, 24" PAINT	PAVEMENT MARKING CHARACTER, PAINT (RXR)	GENERIC PAVEMENT MARKING ITEM (HOT SPRAY THERMO, 4" X 50 MILS WHITE)	GENERIC PAVEMENT MARKING ITEM (HOT SPRAY THERMO, 4" X 50 MILS YELLOW)	GENERIC PAVEMENT MARKING ITEM (NON-CAST IRON SNOWPLOWABLE MARKER)	GENERIC PAVEMENT MARKING ITEM (THERMO LINE, 16" X 90 MILS)	GENERIC PAVEMENT MARKING ITEM (THERMO LINE, 24" X 90 MILS)	GENERIC PAVEMENT MARKING ITEM (THERMO CHARACTER, 90 MILS)	SEEDING & MULCHING	RESPONSE FOR EROSION CONTROL	TEMPORARY SILT FENCE	COIR FIBER WATTLE	JUNCTION BOX (STANDARD SIZE)
								MI	FT	SF	LS	LF	LF	LF	LF	LF	LF	EA	LF	LF	EA	LF	LF	EA	ACR	EA	LF	LF	EA
2021CPT.01.04.10661.1	Northampton	1	NC 186	US 301 TO BEGIN SEABOARD C&G	1	2	2WU	6.94	25	936	1	74,674	54,965	149,349	91,608						451									
2021CPT.01.04.10661.1	Northampton	2	NC 186 (C&G)	BEGIN AT SEABOARD C&G TO END C&G	2	2	2WU	0.22	41		*		1,452		1,452						14									1
2021CPT.01.04.10661.1	Northampton	3	NC 35	NC 305 TO WOODLAND CITY LIMITS C&G	1	2	2WU	2.95	23	228	*	31,742	19,470	31,742	19,470						186									
2021CPT.01.04.10661.1	Northampton	4	NC 35 (C&G)	WOODLAND CITY LIMITS C&G TO US 258	2	2	2WU	0.39	36		*	5,148			2,574						25									1
2021CPT.01.04.20661.1	Northampton	5	SR 1108 BRYANTTOWN RD	FROM SR 1123 TO US 158	3\4	2	2WU	3.82	21\33	350	*			71,446	50,424				37,171	25,212					3.3	2	100	40		
2021CPT.01.04.20661.1	Northampton	6	SR 1368 ATHERTON ST	FROM CALHOUN ST TO US 158	4	2	2WU	0.08	33		*																			
2021CPT.01.04.20661.1	Northampton	7	SR 1542 PINETOPS RD	FROM HERTFORD CO LINE TO SR 1541	5	2	2WU	1.05	20		*			22,166	13,860				11,083	6,798					1.0	1	50	20		
2021CPT.01.04.20661.1	Northampton	8	SR 1212 OAK GROVE CHURCH RD	FROM VA LINE TO NC 46	6	2	2WU	2.63	20	200	*			56,382	35,584				28,191	17,292					2.6	2	50	20		
2021CPT.01.04.20661.1	Northampton	9	SR 1204 JAMES MASSEY RD	FROM VA LINE TO NC 48	7	2	2WU	1.12	18		*			24,318	14,916				12,159	7,458					1.1	1	50	20		
2021CPT.01.04.20661.1	Northampton	10	SR 1301 CORNWALLIS RD	FROM NC 186 TO US 158	6	2	2WU	1.99	20		*			42,029	26,268	50	25	2	21,015	13,134		50	25	2	2.0	1	50	20		
2021CPT.01.04.20661.1	Northampton	11	SR 1312 ST JOHNS CHURCH RD	FROM SR 1311 TO US 158	5	2	2WU	1.87	20		*			39,494	24,684				19,975	12,342					1.9	1	50	20		
GRAND TOTAL								23.06		1,714	1	111,564	75,887	436,926	280,840	50	25	2	129,594	82,236	676	50	25	2	11.9	8	350	140	2
												187,451		717,766					211,830										

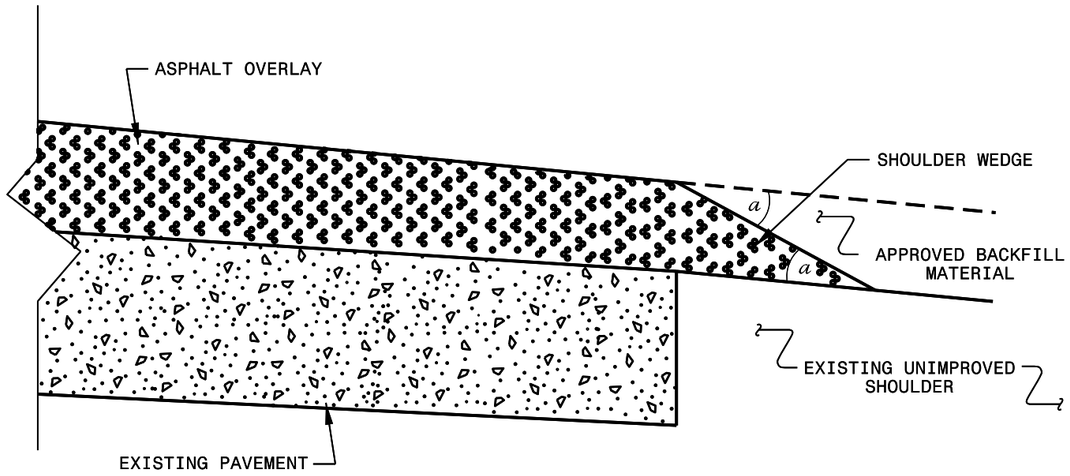
PROJECT REFERENCE NO.	SHEET NO.
2021CPT.01.04.10661.1, ETC.	7



NOTES:
1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.

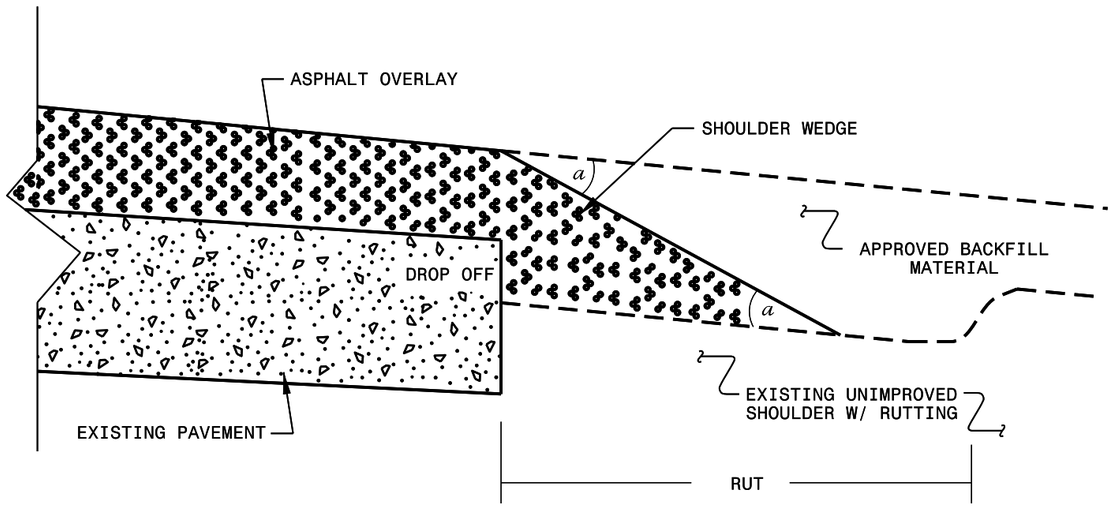
SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

SHOULDER WEDGE DETAILS

ORIGINAL BY: T.SPELL DATE: 7-19-11
MODIFIED BY: DATE: 10/16/12
CHECKED BY: DATE:
FILE SPEC.: s:usr/details/stand/shoulderwedgedetail.dgn

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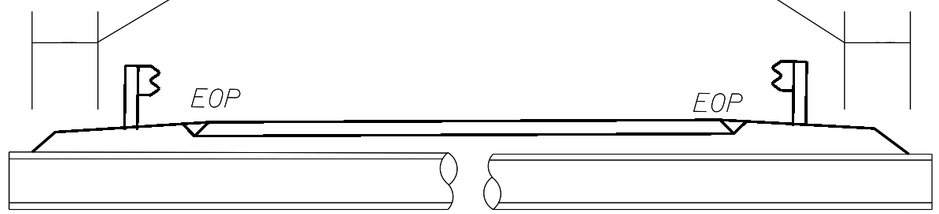
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

EROSION CONTROL DETAIL

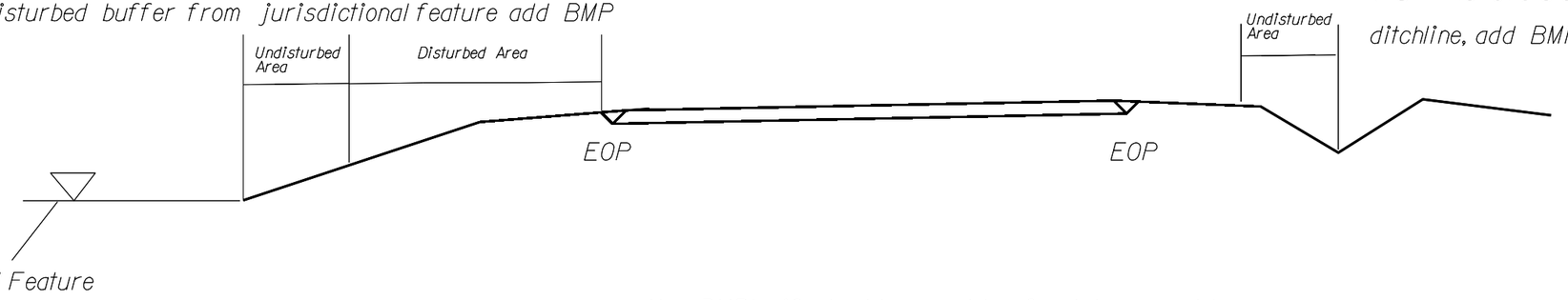
< 5' - 10' Undisturbed buffer add BMP



Pipe/Culvert

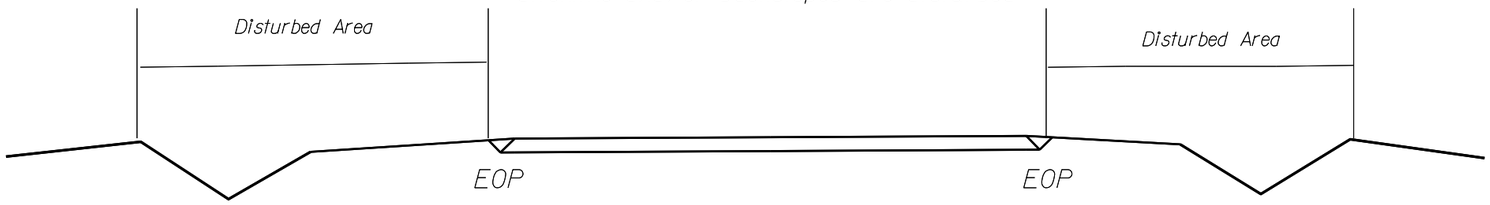
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP

< 5' - 10' Undisturbed buffer from ditchline, add BMP

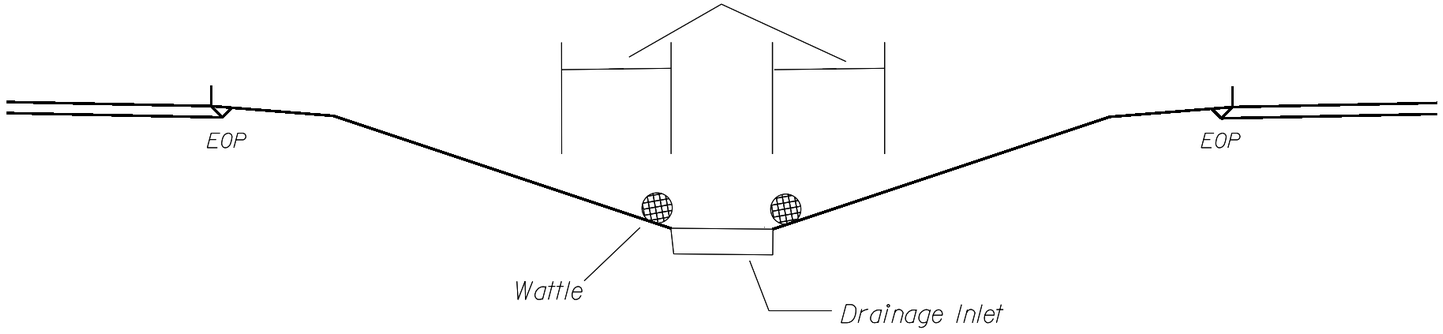


Jurisdictional Feature

Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

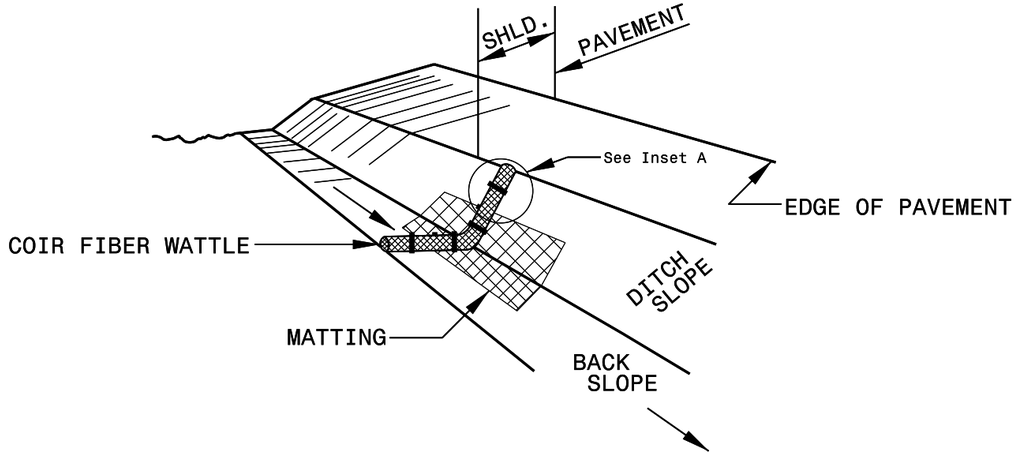


< 5' - 10' Undisturbed buffer from inlet, add wattle

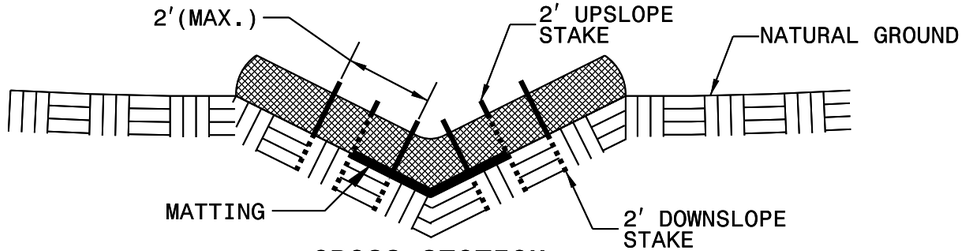


NOT TO SCALE

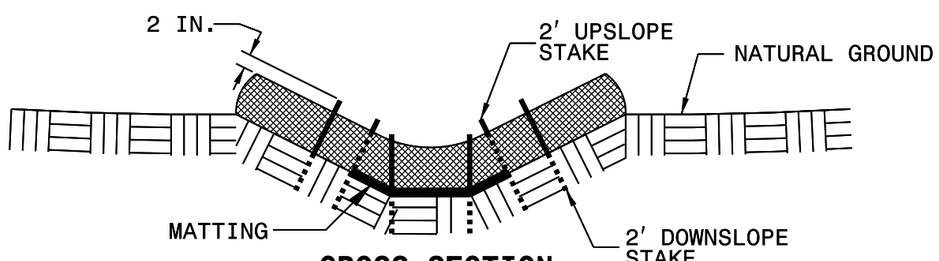
COIR FIBER WATTLE DETAIL



ISOMETRIC VIEW



CROSS SECTION VEE DITCH



CROSS SECTION TRAPEZOIDAL DITCH

NOTES:

USE MINIMUM 12 IN. DIAMETER COIR FIBER (COCONUT FIBER) WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

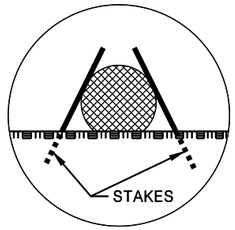
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

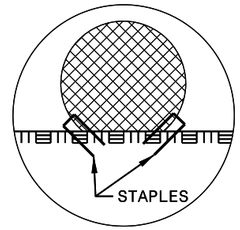
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12″ IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

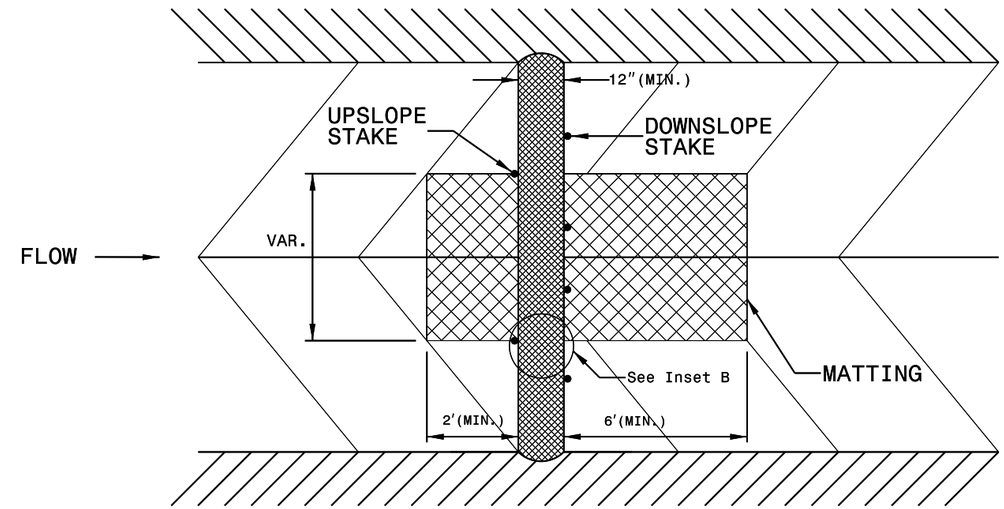
INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



INSET A



INSET B



TOP VIEW

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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

SOIL STABILIZATION TIMEFRAMES

SITE DESCRIPTION	STABILIZATION TIME	TIMEFRAME EXCEPTIONS
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

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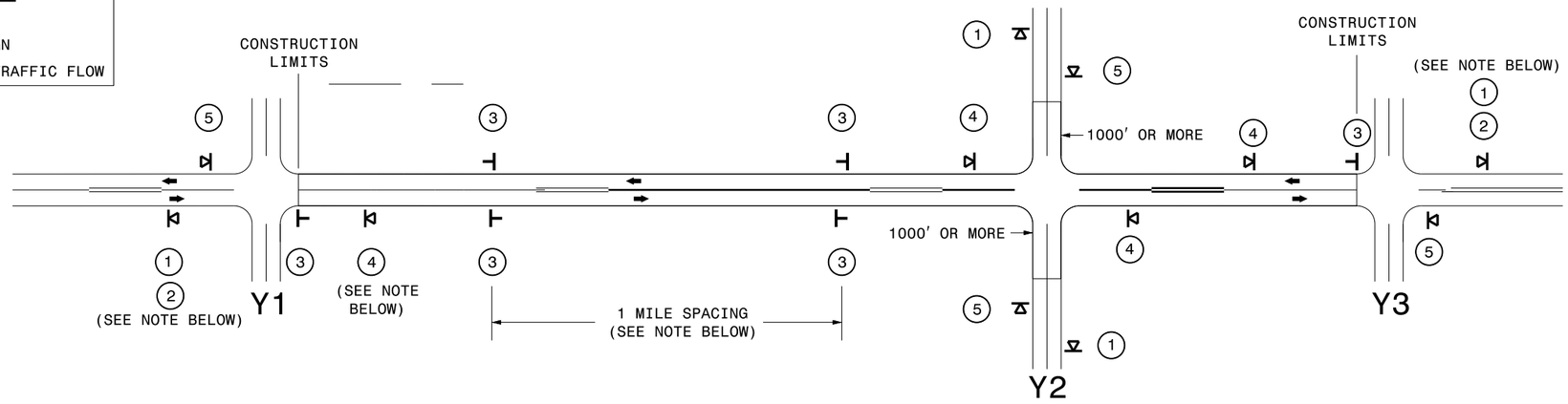
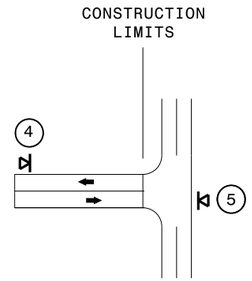
SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND
- PORTABLE SIGN
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW





TEE INTERSECTION

MAINLINE (-L-) SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION

1 ROAD WORK AHEAD W20-1 48" X 48"
2 NEXT XX MILES W7-3aP 24" X 18"
- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.
- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.

3 LOOSE GRAVEL W8-7 48" X 48"; UNMARKED PAVEMENT SP 48" X 48"
- ALTERNATE THE FOLLOWING TWO SIGNS:
- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".
- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.
- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.

4 ROAD UNDER CONST SP 13106 48" X 48"
- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.
- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.
- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.
- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.

5 END ROAD WORK G20-2 A 48" X 24"

PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.

-Y- LINE SIGNING

STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:

1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
2) SUBDIVISION ROADS
3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALLE BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

ROAD WORK AHEAD W20-1 48" X 48"
PLACED 500' IN ADVANCE OF FLAGGER.

W20-7 A 48" X 48"
PLACED 250' IN ADVANCE OF FLAGGER.

DIVISION OF HIGHWAYS • STATE OF NORTH CAROLINA • DEPARTMENT OF TRANSPORTATION • WORK ZONE TRAFFIC CONTROL

ADVANCE WARNING SIGNS
FOR
2-LANE ROADWAY
ASPHALT SURFACE TREATMENT

PROJ. REFERENCE NO.	SHEET NO.
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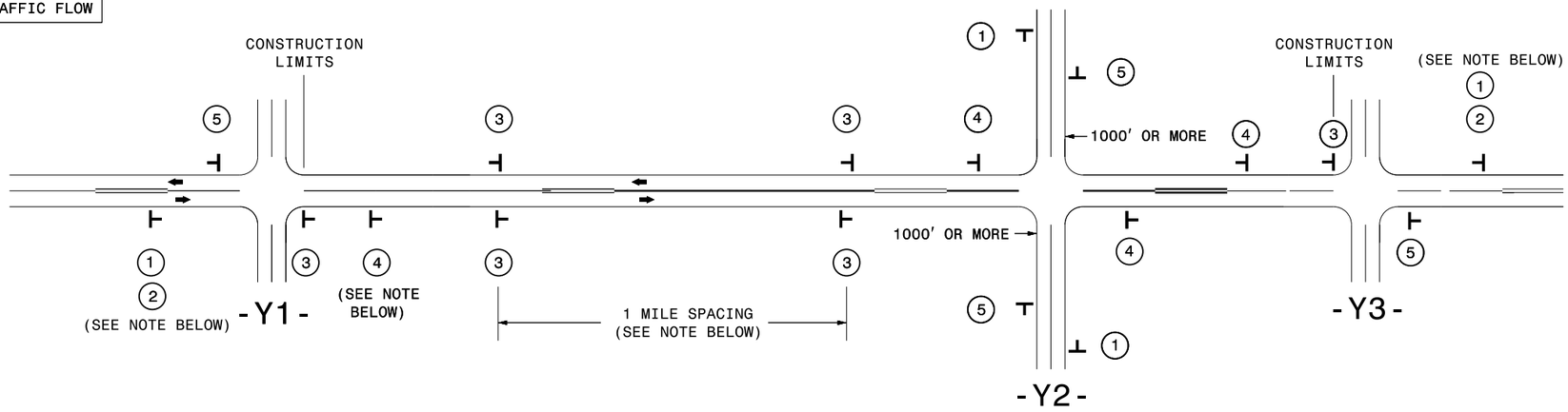
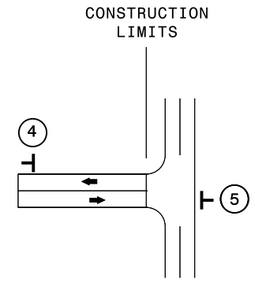
SIGNING FOR RESURFACING PROJECTS

LEGEND
- ⊢ STATIONARY SIGN
- ← DIRECTION OF TRAFFIC FLOW





TEE INTERSECTION

MAINLINE (-L-) SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION

1	ROAD WORK AHEAD W20–1 48″ X 48″	PLACE 1000′ PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000′ ALONG -Y- LINE.
2	NEXT XX MILES W7–3aP 24″ X 18″	#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)
3	LOW/SOFT SHOULDER SP 13107 48″ X 48″ / UNMARKED PAVEMENT SP 48″ X 48″	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY ½ MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
4	ROAD UNDER CONST SP 13106 48″ X 48″	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500′ +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500′ OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500′ +/- OF THE INTERSECTION ALONG -L- LINE.
5	END ROAD WORK G20–2 A 48″ X 24″	PLACE 500′ FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.

-Y- LINE SIGNING

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

1) LESS THAN 1000′ OF RESURFACING ALONG -Y- LINE
2) SUBDIVISION ROADS
3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

ROAD WORK AHEAD W20–1 48″ X 48″

W20–7 A 48″ X 48″

PLACED 500′ IN ADVANCE OF FLAGGER.

DIVISION OF HIGHWAYS • STATE OF NORTH CAROLINA • DEPARTMENT OF TRANSPORTATION • WORK ZONE TRAFFIC CONTROL

ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

08-SEP-2020 11:16
S:\Shared\Division One Resurfacing & Retreatment Plans\2020-202!Resurfacing\DA00479-Northampton NC 186 & Secondary\Design Files\DA00479_Dldd_sh3-5, 7-12.dgn
$$$$USERNAME$$$$