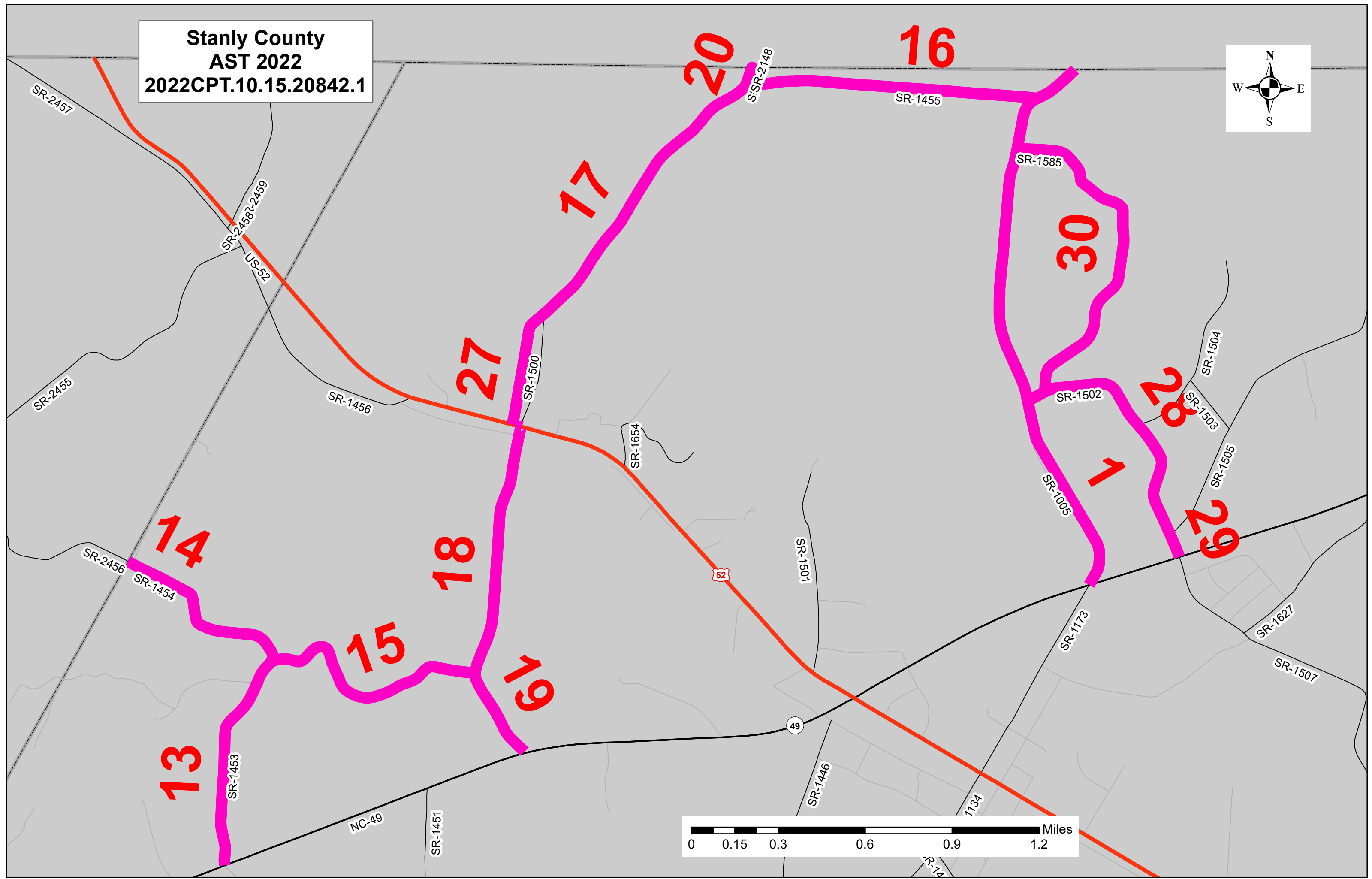
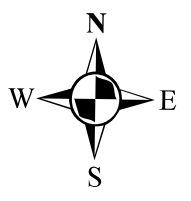


PROJECT NO.	SHEET NO.	TOTAL NO.
2022CPT.10.15.20842.1	1	9

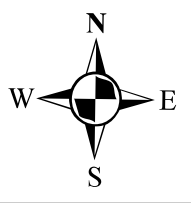
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	1803500000-E	1838000000-E	1838500000-N	2143000000-E	4413000000-E	4457000000-N
												ASPHALT SURFACE TREATMENT, DOUBLE SEAL	EMULSION FOR ASPHALT SURFACE TREATMENT	VACUUM TRUCK	BLOTTING SAND	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL
										MI	FT	SY	GAL	WK	TON	SF	LS
2022CPT.10.15.20842.1	Stanly	1	SR 1005 - HIGH ROCK RD	FROM COUNTY LINE (MP 0.00) TO NC 49 (MP 1.96)		2		NO	NO	1.96	20	23,457	12,650	0.25	0.50	126.0	0.07
TOTAL FOR MAP NO. 1										1.96		23,457	12,650	0.25	0.50	126.0	0.07
2022CPT.10.15.20842.1	Stanly	2	SR 1422 OLD HERLOCKER ROAD	FROM SR 1400 (OLD SALISBURY ROAD) (MP 0.00) TO DEAD END (MP 1.30)		2		NO	NO	1.3	18	14,003	7,555	0.15	0.50	84.0	0.04
TOTAL FOR MAP NO. 2										1.3		14,003	7,555	0.15	0.50	84.0	0.04
2022CPT.10.15.20842.1	Stanly	3	SR 1423 PICKLER ROAD	FROM SR 1214 (AUSTIN ROAD) (MP 0.00) TO SR 1422 (OLD HERLOCKER ROAD) (MP 1.15)		2		NO	NO	1.15	19	13,075	7,055	0.15	0.50	74.0	0.04
TOTAL FOR MAP NO. 3										1.15		13,075	7,055	0.15	0.50	74.0	0.04
2022CPT.10.15.20842.1	Stanly	4	SR 1424 CLETE ROAD	FROM NC 73 (MP 0.00) TO SR 1462 CLETE ROAD (MP 0.26)		2		NO	NO	0.26	17	2,645	1,430	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 4										0.26		2,645	1,430	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	5	SR 1427 ESTHER ROAD	FROM SR 1134 (MILLINGPORT ROAD) (MP 0.00) TO SR 1426 (HARWOOD ROAD) (MP 1.43)		2		NO	NO	1.43	19	16,259	8,770	0.15	0.50	92.0	0.05
TOTAL FOR MAP NO. 5										1.43		16,259	8,770	0.15	0.50	92.0	0.05
2022CPT.10.15.20842.1	Stanly	6	SR 1432 GOLD HILL ROAD	FROM SR 1428 (BEAR CREEK ROAD) (MP 0.00) TO COUNTY LINE (MP 1.92)		2		NO	NO	1.92	20	22,979	12,395	0.25	0.50	124.0	0.07
TOTAL FOR MAP NO. 6										1.92		22,979	12,395	0.25	0.50	124.0	0.07
2022CPT.10.15.20842.1	Stanly	7	SR 1435 POPLIN ROAD	FROM SR 1426 (HARWOOD ROAD) (MP 0.70) TO SR 1435 (KENDALLS CHURCH ROAD) (MP 2.42)		2		NO	NO	1.72	19	19,556	10,545	0.20	0.50	110.0	0.06
TOTAL FOR MAP NO. 7										1.72		19,556	10,545	0.20	0.50	110.0	0.06
2022CPT.10.15.20842.1	Stanly	8	SR 1439 CAUDLE ROAD	FROM SR 1400 (OLD SALISBURY ROAD) (MP 0.00) TO DIRT ROAD (MP 0.55)		2		NO	NO	0.55	20	6,582	3,550	0.15	0.40	40.0	0.02
TOTAL FOR MAP NO. 8										0.55		6,582	3,550	0.15	0.40	40.0	0.02
2022CPT.10.15.20842.1	Stanly	9	SR 1440 MISENHEIMER ROAD	FROM SR 1400 (OLD SALISBURY ROAD) (MP 1.52) TO SR 1134 (MILLINGPORT ROAD) (MP 3.00)		2		NO	NO	1.48	18	15,941	8,600	0.15	0.50	95.0	0.06
TOTAL FOR MAP NO. 9										1.48		15,941	8,600	0.15	0.50	95.0	0.06
2022CPT.10.15.20842.1	Stanly	10	SR 1443 DANVILLE ROAD	FROM SR 1400 (OLD SALISBURY ROAD) (MP 0.00) TO US 52 (MP 1.29)		2		NO	NO	1.29	18	13,895	7,495	0.15	0.50	82.0	0.05
TOTAL FOR MAP NO. 10										1.29		13,895	7,495	0.15	0.50	82.0	0.05
2022CPT.10.15.20842.1	Stanly	11	SR 1447 FRICK ROAD	FROM SR 1451 (PAULS CROSSING ROAD) (MP 0.00) TO SR 1134 (MILLINGPORT ROAD) (MP 1.87)		2		NO	NO	1.87	19	21,261	11,465	0.25	0.50	120.0	0.06
TOTAL FOR MAP NO. 11										1.87		21,261	11,465	0.25	0.50	120.0	0.06
2022CPT.10.15.20842.1	Stanly	12	SR 1450 RYAN ROAD	FROM SR 1134 (MILLINGPORT ROAD) (MP 0.00) TO DEAD END (MP 0.69)		2		NO	NO	0.69	19	7,845	4,235	0.15	0.40	44.0	0.02
TOTAL FOR MAP NO. 12										0.69		7,845	4,235	0.15	0.40	44.0	0.02
2022CPT.10.15.20842.1	Stanly	13	SR 1453 MATTON GROVE CHURCH ROAD	FROM NC 49 (MP 0.00) TO SR 1454 (COLLIE ROAD) (MP 0.77)		2		NO	NO	0.77	18	8,294	4,475	0.15	0.40	49.0	0.03
TOTAL FOR MAP NO. 13										0.77		8,294	4,475	0.15	0.40	49.0	0.03
2022CPT.10.15.20842.1	Stanly	14	SR 1454 COLLIE ROAD	FROM SR 1453 (MATTON GROVE CHURCH ROAD) (MP 0.85) TO COUNTY LINE (MP 1.51)		2		NO	NO	0.66	20	7,899	4,260	0.15	0.40	42.0	0.02
TOTAL FOR MAP NO. 14										0.66		7,899	4,260	0.15	0.40	42.0	0.02
2022CPT.10.15.20842.1	Stanly	15	SR 1454 MATTON GROVE CHURCH ROAD	FROM SR 1455 (WESLEY CHAPEL ROAD) (MP 0.00) TO SR 1454 (COLLIE ROAD) (MP 0.85)		2		NO	NO	0.85	19	9,664	5,215	0.15	0.50	54.0	0.03
TOTAL FOR MAP NO. 15										0.85		9,664	5,215	0.15	0.50	54.0	0.03
2022CPT.10.15.20842.1	Stanly	16	SR 1455 GOODMAN ROAD	FROM SR 1455 (REEVES ISLAND ROAD) (MP 2.66) TO SR 1005 (HIGH ROCK ROAD) (MP 3.67)		2		NO	NO	1.01	19	11,483	6,195	0.15	0.50	65.0	0.04
TOTAL FOR MAP NO. 16										1.01		11,483	6,195	0.15	0.50	65.0	0.04
2022CPT.10.15.20842.1	Stanly	17	SR 1455 REEVES ISLAND ROAD	FROM SR 1500 (REEVES ISLAND ROAD) (MP 1.61) TO SR 1455 (GOODMAN ROAD) (MP 2.66)		2		NO	NO	1.05	19	11,938	6,440	0.15	0.50	67.0	0.04
TOTAL FOR MAP NO. 17										1.05		11,938	6,440	0.15	0.50	67.0	0.04
2022CPT.10.15.20842.1	Stanly	18	SR 1455 WESLEY CHAPEL ROAD	FROM SR 1453 (MATTON GROVE CHURCH ROAD) (MP 0.33) TO US 52 (MP 1.20)		2		NO	NO	0.87	16	8,330	4,495	0.15	0.50	56.0	0.03
TOTAL FOR MAP NO. 18										0.87		8,330	4,495	0.15	0.50	56.0	0.03
2022CPT.10.15.20842.1	Stanly	19	SR 1455 WESLEY CHAPEL ROAD	FROM NC 49 (MP 0.00) TO SR 1453 (MATTON GROVE CHURCH ROAD) (MP 0.33)		2		NO	NO	0.33	20	3,949	2,130	0.15	0.40	0.0	0.01
TOTAL FOR MAP NO. 19										0.33		3,949	2,130	0.15	0.40	0.0	0.01
2022CPT.10.15.20842.1	Stanly	20	SR 1461 REEVES ISLAND ROAD	FROM SR 1455 (GOODMAN ROAD) (MP 0.00) TO COUNTY LINE (MP 0.07)		2		NO	NO	0.07	19	796	430	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 20										0.07		796	430	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	21	SR 1463 HARKEY ROAD	FROM EOP -DIRT ROAD (MP 0.21) TO SR 1425 (NELSON MOUNTAIN ROAD) (MP 0.81)		2		NO	NO	0.6	20	7,181	3,875	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 21										0.6		7,181	3,875	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	22	SR 1488 PINWOOD DRIVE	FROM SR 1443 (DANVILLE ROAD) (MP 0.00) TO DEAD END (MP 0.24)		2		NO	NO	0.24	16	2,298	1,240	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 22										0.24		2,298	1,240	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	23	SR 1491 TERCHER ROAD	FROM SR 1214 (AUSTIN ROAD) (MP 0.00) TO DEAD END (MP 0.52)		2		NO	NO	0.52	20	6,223	3,360	0.15	0.50	40.0	0.02
TOTAL FOR MAP NO. 23										0.52		6,223	3,360	0.15	0.50	40.0	0.02
2022CPT.10.15.20842.1	Stanly	24	SR 1495 FIRE LINE ROAD	FROM SR 1400 (OLD SALISBURY ROAD) (MP 0.00) TO DEAD END (MP 0.25)		2		NO	NO	0.25	18	2,693	1,455	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 24										0.25		2,693	1,455	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	25	SR 1499 CHIP ROAD	FROM SR 1424 (CLETE ROAD) (MP 0.00) TO DEAD END (MP 0.08)		2		NO	NO	0.08	17	814	440	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 25										0.08		814	440	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	26	SR 1424 CHIP ROAD	FROM SR 1462 (CLETE ROAD) (MP 0.26) TO SR 1425 (NELSON MOUNTAIN ROAD) (MP 0.34)		2		NO	NO	0.08	17	814	440	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 26										0.08		814	440	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	27	SR 1500 REEVES ISLAND ROAD	FROM US 52 (MP 0.00) TO SR 1455 (WESLEY CHAPEL ROAD) (MP 0.41)		2		NO	NO	0.41	16	3,926	2,120	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 27										0.41		3,926	2,120	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	28	SR 1502 GOLD BRANCH ROAD	FROM SR 1005 (HIGH ROCK ROAD) (MP 0.08) TO SR 1505 (WILLIE ROAD) (MP 0.89)		2		NO	NO	0.81	18	8,725	4,705	0.15	0.50	52.0	0.02
TOTAL FOR MAP NO. 28										0.81		8,725	4,705	0.15	0.50	52.0	0.02
2022CPT.10.15.20842.1	Stanly	29	SR 1502 GOLD BRANCH ROAD	FROM NC 49 (MP 0.00) TO SR 1505 (WILLIE ROAD) (MP 0.08)		2		NO	NO	0.08	18	862	465	0.15	0.50	40.0	0.01
TOTAL FOR MAP NO. 29										0.08		862	465	0.15	0.50	40.0	0.01
2022CPT.10.15.20842.1	Stanly	30	SR 1585 RIDER ROAD	FROM SR 1005 (HIGH ROCK ROAD) (MP 0.00) TO SR 1502 (GOLD BRANCH ROAD) (MP 1.19)		2		NO	NO	1.19	20	14,242	7,680	0.15	0.50	76.0	0.03
TOTAL FOR MAP NO. 30										1.19		14,242	7,680	0.15	0.50	76.0	0.03
2022CPT.10.15.20842.1	Stanly	31	SR 1462 CLETE ROAD	FROM SR 1424 CHIP ROAD (MP 0.00) TO DEAD END (MP 0.24)		2		NO	NO	0.24	17	2,441	1,320	0.15	0.50	40	0.10
TOTAL FOR MAP NO. 31										0.24		2,441	1,320	0.15	0.50	40	0
TOTAL FOR PROJ NO. 2022CPT.10.15.20842.1										25.73		290,070	156,485	5	15	1,892	1
GRAND TOTAL										25.73		290,070	156,485	5	15	1,892	1

Stanly County
AST 2022
2022CPT.10.15.20842.1



Stanly County
AST 2022
2022CPT.10.15.20842.1



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SR-1451

SR-1447

SR-1446

SR-1134

SR-1400

SR-2101

SR-1439

US-52

SR-1444

SR-1638

SR-1442

SR-1507

SR-1488

SR-1443

SR-1489

SR-1490

SR-1440

SR-1441

SR-1448

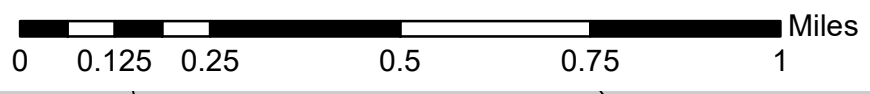
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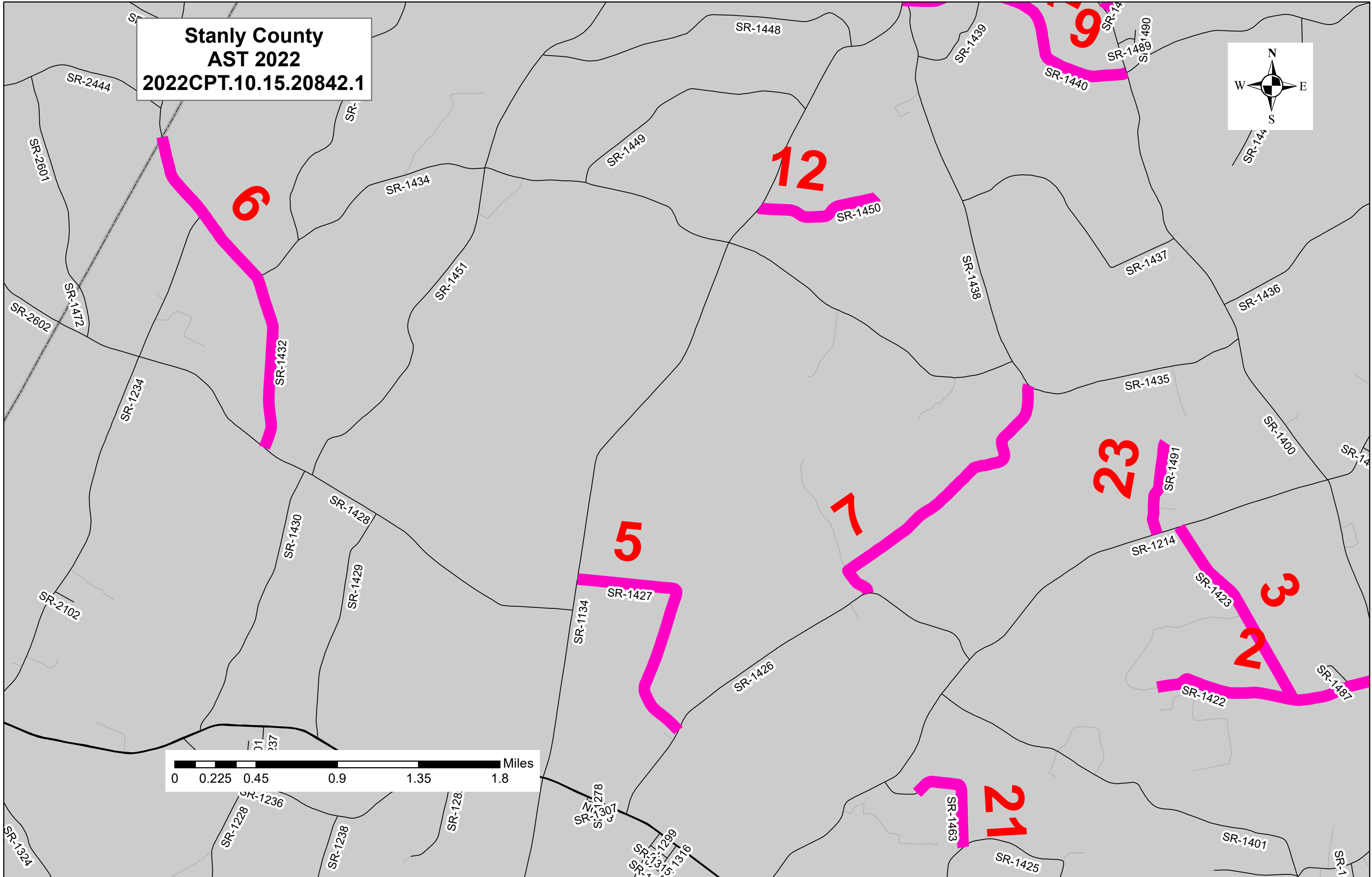
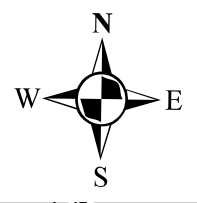
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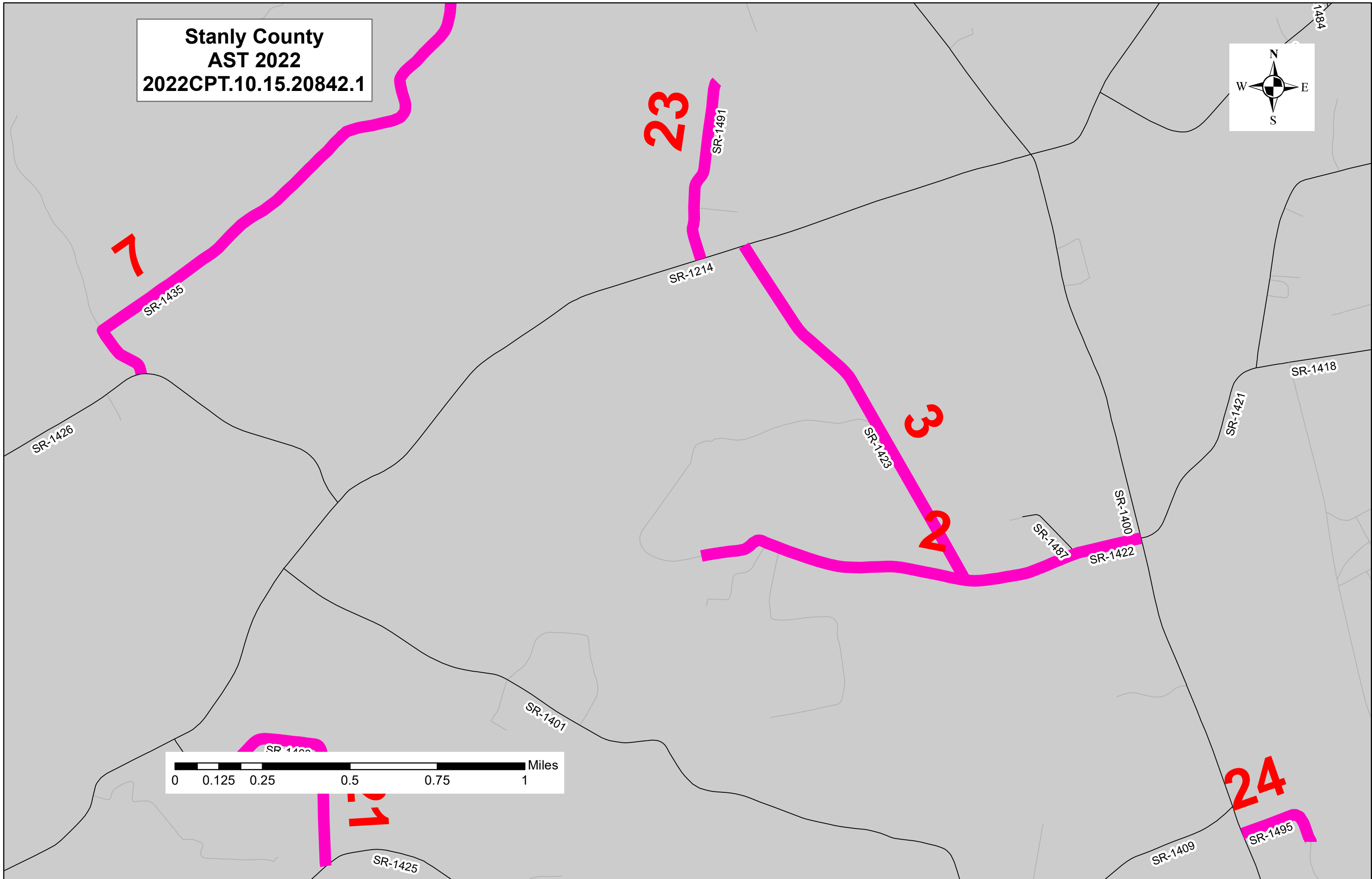
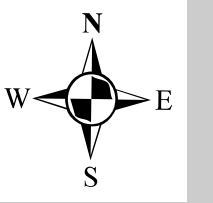
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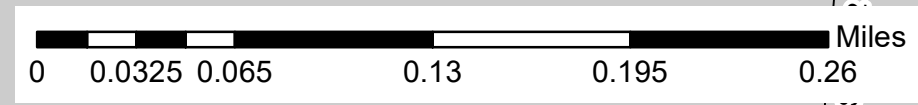
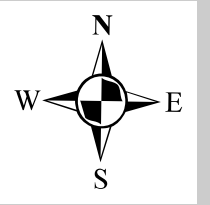
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Stanly County
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NC-73

SR-1243

SR-1424

SR-1499

SR-1462

SR-1425

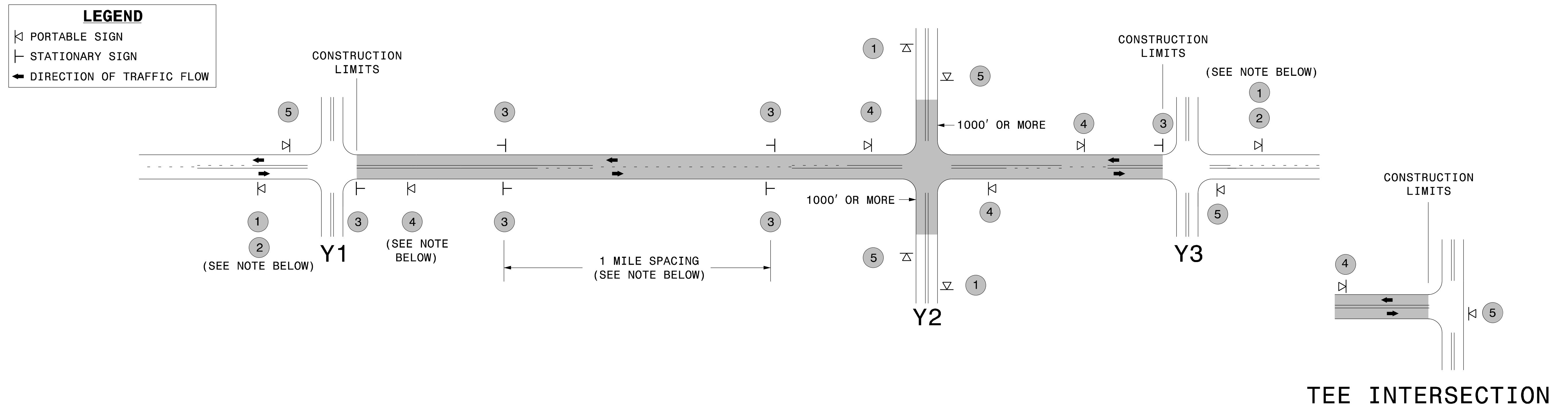
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SIGNING FOR ASPHALT SURFACE TREATMENT



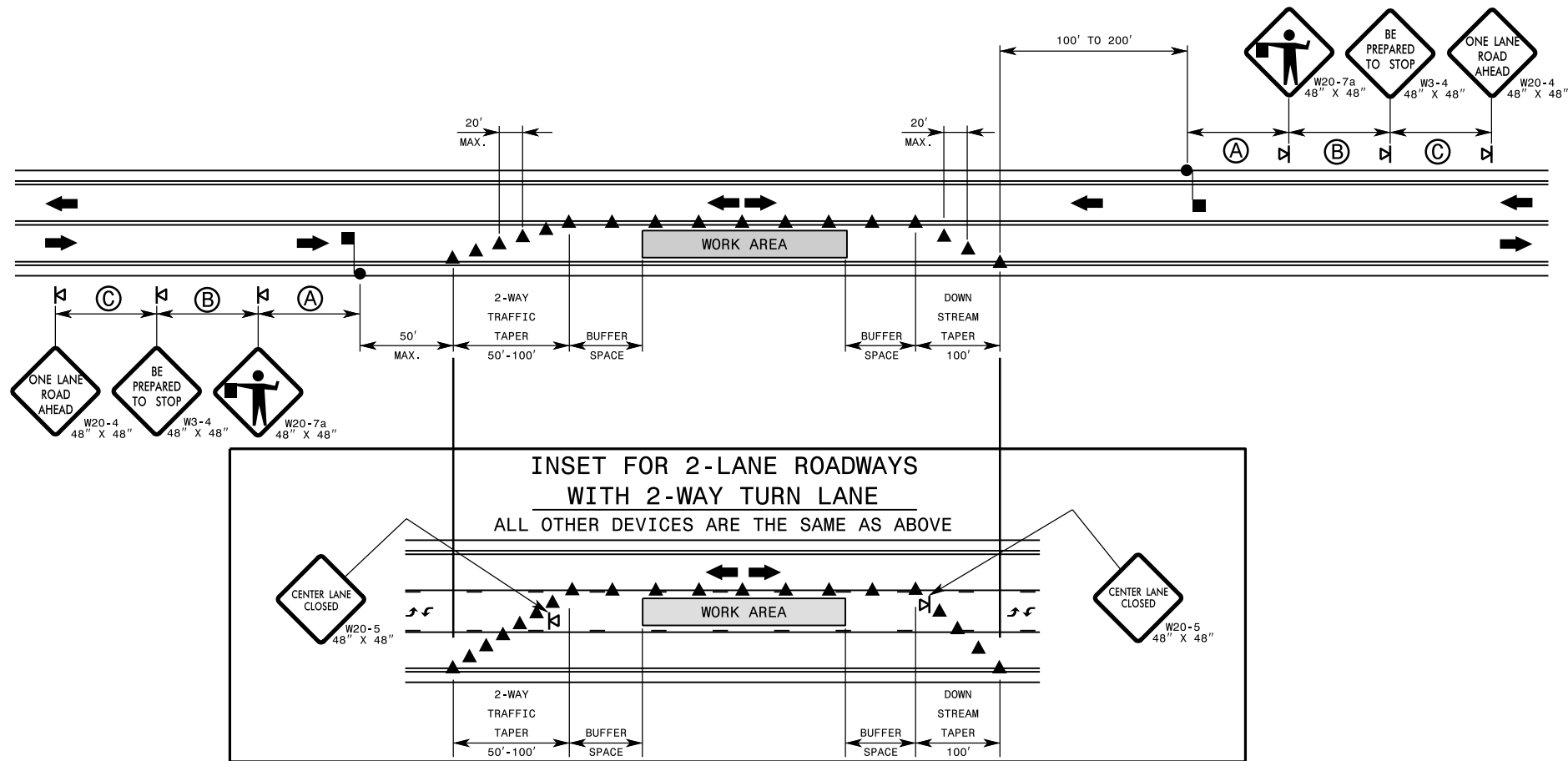
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div><div>1</div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div><div><div>2</div><div><div>NEXT XX MILES</div><div>W7-3aP 24" X 18"</div></div></div><div><div>- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div><div>- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.</div></div></div>
	<div><div>3</div><div><div>LOOSE GRAVEL</div><div>W8-7 48" X 48"</div></div><div><div>UNMARKED PAVEMENT</div><div>SP 48" X 48"</div></div><div><div>- ALTERNATE THE FOLLOWING TWO SIGNS:</div><div>- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</div><div>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.</div><div>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</div></div></div>
	<div><div>4</div><div><div>ROAD UNDER CONST</div><div>SP 13106 48" X 48"</div></div><div><div>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</div><div>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</div><div>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</div><div>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</div></div></div>
	<div><div>5</div><div><div>END ROAD WORK</div><div>G20-2 A 48" X 24"</div></div><div><div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</div></div></div>
	<div><div>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</div></div>
MAPS LESS THAN 2 MILES	<div><div>FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.</div></div>

STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:	
1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS	
WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALLE BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.	
<div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div><div>PLACED 500' IN ADVANCE OF FLAGGER.</div></div>	<div><div><div><div></div></div><div>W20-7 A 48" X 48"</div></div><div>PLACED 250' IN ADVANCE OF FLAGGER.</div></div>

<div><div><div>DIVISION OF HIGHWAYS STATE OF NORTH CAROLINA DEPARTMENT OF TRANSPORTATION WORK ZONE TRAFFIC CONTROL</div><div></div></div></div>	ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT
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GENERAL NOTES FOR FLAGGING OPERATIONS

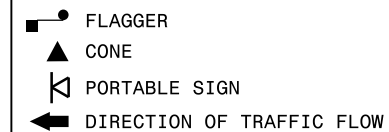
- 1- REFER TO RSD. 1101.11, SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 2- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- 3- REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- PLACE CONES THRU THE WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 5- EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE FLAGGER (REFER TO RSD. 1101.11, SHEET 2).
- 6- DO NOT STOP TRAFFIC IN ANY ONE DIRECTION FOR MORE THAN 5 MINUTES AT A TIME.
- 7- DRUMS OR SKINNY DRUMS MAY BE USED IN LIEU OF CONES. REFER TO RSD. 1180.01 FOR SKINNY DRUM REQUIREMENTS.
- 8- USE FLAGGERS TO CONTROL TRAFFIC AT INTERSECTIONS AFFECTED BY THE LANE CLOSURE. SUPPLEMENT FLAGGERS LOCATED AT INTERSECTIONS WITH FLAGGER AHEAD SIGNS (W20-7a) PLACED APPROXIMATELY 250 FT. IN ADVANCE OF THE FLAGGER. FOR SIGNALIZED INTERSECTIONS PLACE SIGNALS IN THE FLASH MODE AND USE LAW ENFORCEMENT.
- 9- REFER TO THE CURRENT MUTCD FOR FLAGGER CONTROL, REQUIREMENTS, AND PROCEDURES.
- 10- DO NOT EXCEED A 1 MILE LANE CLOSURE LENGTH UNLESS OTHERWISE SHOWN IN THE TMP OR AS DIRECTED BY THE ENGINEER.

- 11- IF VEHICLE QUEUES WILL REACH WITHIN 15' OF EITHER SIDE OF ACTIVE RAILROAD TRACKS, PROVIDE A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER TO PREVENT VEHICLES FROM STOPPING WITHIN THE GRADE CROSSING. PROVIDE OFFICER OR FLAGGER EVEN IF AUTOMATIC WARNING MEASURES ALREADY EXIST.

GENERAL NOTES FOR PILOT CAR OPERATIONS

- 1- USE PILOT CARS WHEN DIRECTED BY THE ENGINEER.
- 2- IF ROADWAY WIDTH IS LESS THAN 22 FEET (EOP TO EOP), CONES MAY NOT BE REQUIRED ALONG WORK AREA, AND AT THE DISCRETION OF THE ENGINEER, CONES MAY BE OMITTED ALONG THE WORK AREA IF USING A PILOT CAR.
- 3- CONES ARE ALWAYS REQUIRED IN THE UPSTREAM AND DOWNSTREAM TAPERS.
- 4- MOUNT SIGN G20-4 "PILOT CAR FOLLOW ME" AT A CONSPICUOUS POSITION ON THE REAR OF THE PILOT VEHICLE.
- 5- DO NOT INSTALL MORE THAN ONE (1) MILE OF LANE CLOSURE, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- 6- ADVISE RESIDENTS AND BUSINESSES WITHIN THE LANE CLOSURE LIMITS ABOUT METHODS OF SAFE EGRESS AND INGRESS FROM DRIVEWAYS DURING FLAGGING AND PILOT CAR OPERATIONS.

LEGEND



ROADWAY STANDARD DRAWING FOR

TEMPORARY LANE CLOSURES

2-LANE, 2-WAY ROADWAY-1 LANE CLOSED

1-18

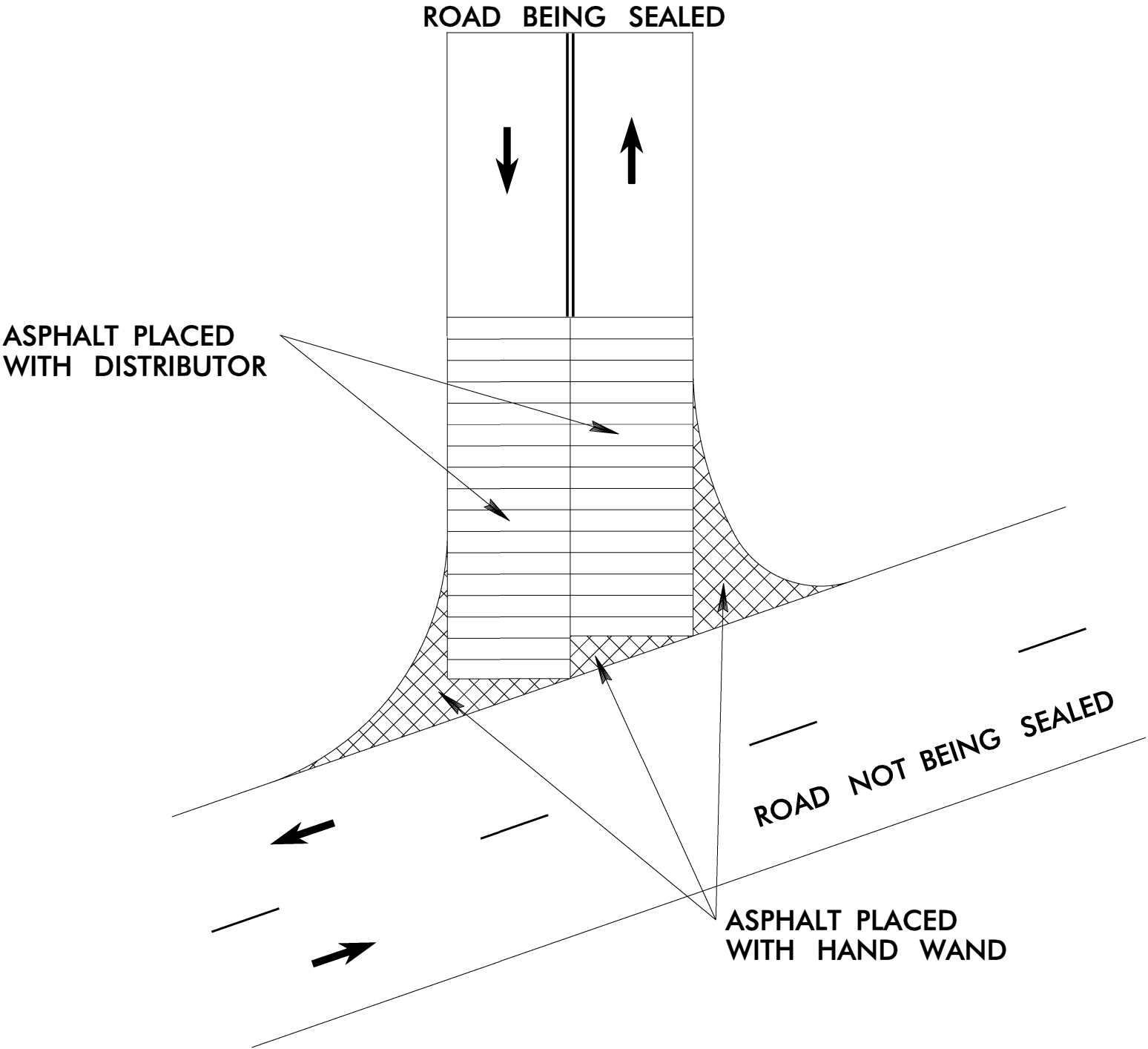
STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.


SHEET 1 OF 14

1101.02

INTERSECTION EMULSION PLACEMENT TWO LANE TWO WAY ROADWAY

PROJECT NO.	SHEET NO.
2022CPTJ015.20142	9
F.A. PROJECT NO.	



INTERSECTION EMULSION PLACEMENT			REVISIONS	
SCALE	N/A			
DATE	9-2019			
DWG. BY	TBL			
DESIGN BY	TBL			
APPROVED	TWB			