

ADDENDUM No. 3

TO: Prospective Bidders

FROM: Donna Johnson, Contracts Administrator

DATE: July 29, 2022

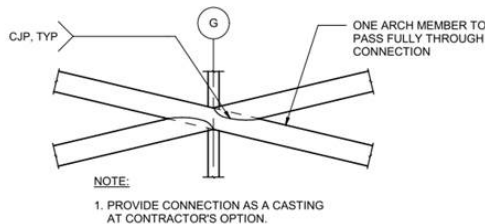
PROJECT: I-85 North Bridge
 Project No.: 512-15-003
 Bid Number: HC2020-2040

The following items are being issued herein for modification and clarification to the Bid Requirements for the project referenced above. All Bidders shall acknowledge this Addendum within their submittal.

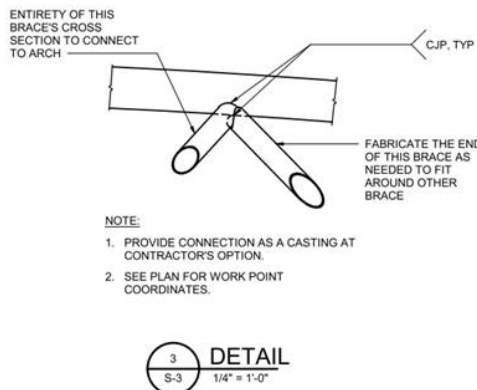
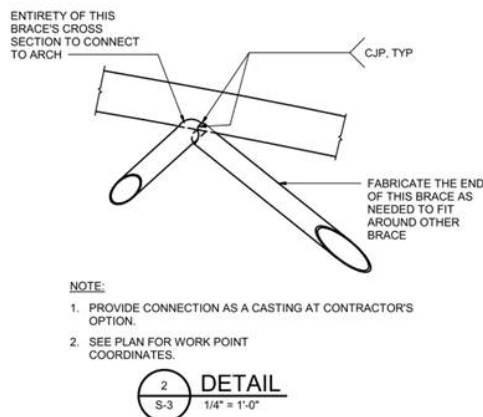
CLARIFICATIONS

QUESTIONS & ANSWERS

1. Please confirm what details can be provided for the following. The key issues with the details below are :
 - a) There is no joints and segment length specified for 79.2m span arch girders, therefore no detail connections for required joints
 - b) The connection between Bracings to main arch girder is shop welding, this makes the parts too hard to be shipped
 - c) No specific corrosion protective coating specified.
 Connection joint of one main arch to the other main arch:



Connection joints for pipe bracings to main arches:



Answer: See respective answers below:

- a) Correct splice locations are not specified. The number and locations of splices are deferred to the fabricator and erector. Per NOTE: B and C/S-2 "Splices are to develop full axial, shear, and moment capacities of the section."
- b) Our thought was that those connections would be shop welded and splices made away from the connection members due to the geometry of the connections. Those welds can be field constructed if preferred by the fabricator and erector.
- c) Painting shall be in accordance with Section 442 Painting Steel Structures of the NCDOT 2018 Standard Specifications for Roads and Structures.

2. The only specifications for the fabrication and painting of the Aesthetic Arches, is that they are did as Structural Steel (Item 59) and Painting of Structural Steel (Item #60), appears on Plan Sheets S-1 through S-6. Is this all there is, or will a Special Provision be issued to cover these bid items in more detail?

Answer: Plans Sheets S-1 through S-6 (Aesthetic Structure Plans), Pay Item 59 (Approximately 166,500 Lbs. Structural Steel), and Sections 440 and 442 of the 2018 NCDOT Standard Specifications cover all payment and specifications needed to construct the aesthetic arches on the bridge.

3. It was stated today in the pre-bid that early grading had taken place in order to facilitate duct bank installation related to utility relocations and that duct bank installation would be complete in September. When will the relocations into the new duct bank be complete?

Answer: The duct bank construction is anticipated to be complete in September 2022. The relocations into the duct bank will be sequentially established over the next 8 to 12 months.

4. Standard Special Provisions, Section 2.3 Existing Utilities, states effected utility owners have been provided a construction schedule. It also states the Contractor shall include the cost of any coordination and cooperation with utilities. Can a list of the utilities actually affected be provided? Can the schedule that was provided to the utilities be provided? Can the anticipated schedule for each of the utility relocations be provided?

Answer: Fall 2022 was the anticipated construction start date shared with utility companies. Utility relocations along Doby Creek are complete. Utility relocations on the project's east side of I-85 should be complete this fall. Utility relocations into the duct bank will be sequentially scheduled over the next 8 to 12 months. Please note that City staff will provide the coordination with the utility companies regarding scheduling the utility relocations into the duct bank.

The owners of utilities in the project include:

1. Alltel / Windstream Communications
2. Duke Energy Company
3. Piedmont Natural Gas Company
4. Charlotte-Mecklenburg Utility Department
5. American Telephone & Telegraph
6. Charter/Spectrum
7. MCI Communications
8. Charlotte Department of Transportation
9. Xspedius Communications
10. Crown Castle
11. Lumen
12. Segra
13. Zayo
14. MCNC
15. Duke Fiber

16. Verizon

5. Item 7, Borrow Excavation, refers to SP-05 which appears to describe Borrow Excavation as it pertains to pipe and drainage structure installation. Based on the earthwork summary, it appears this item is intended to be used and paid for as the Borrow Excavation required to construct the Embankments for the project. Please clarify.

Answer: The intent of SP-05 is to clarify how the removal and replacement of soil that has been found to be unsuitable material would be paid for when encountered during the construction of drainage pipe and drainage structures. The excavation and replacement of the unsuitable material will be paid for as Borrow Excavation, which is the same pay item used for roadway embankment. Section 230 of the Standard Specifications is unchanged and would still apply to Borrow Excavation needed for embankment on the project. Pay Item 7, Borrow Excavation, should be used to pay for both the embankment for the project as well as the replacement of unsuitable material determined needed for drainage pipe and drainage structure installation, as defined in SP-05.

6. The plans call out concrete drainage structures (i.e., 840.02, 840.14, 840.31, etc.). Would brick drainage structures be allowed (i.e., 840.01, 840.15, 840.32, etc.)?

Answer: Concrete Drainage Structures may be substituted with equivalent NCDOT Standard structures meeting the dimensions of the structure called out on the plans. The structures will meet NCDOT Standard Specification.

7. There are various drainage structures called out on the plans as 840.52, but on the profiles they are listed as 840.31. Can these drainage structures called out as 840.52 (precast manholes), but constructed in place as a junction box with manhole, 840.31?

Answer: Drainage structures called out as MHs on the plans sheets have constraints such as structure depth or pipe angle of intersection that does not allow for a JB. During construction the contractor can provide NC PE seal shop drawings for consideration to substitute for these structures.

8. There are two Modified TB DI, Double, called out on the plans. Will these be paid as 4 EA Masonry Drainage Structures, and will the height exceeding 5' be paid twice?

Answer: The Modified TB DI, Double are counted as individual structures in the itemized bid table and should be paid for as two structures and any height above 5' should be paid for each structure at each installation location.

9. Are solid wall structures required if the Contractor elects to use precast drainage structures?

Answer: Precast structures will be solid wall with precast openings. The use of precast waffle boxes is excluded by the Charlotte Land Development Standards Manual (CLDSM).

10. What gauge wire is required for the 6" x 6" epoxy coated welded wire fabric required in the Truck Aprons and WCRs @ the Truck Aprons?

Answer: W1.4 x W1.4 gauge shall be used for the 6" x 6" epoxy coated welded wire fabric in the Truck Aprons.



11. The Traffic Control plans call for Temporary Pavement Markings with Paint and Removable Tape, and the removal of conflicting markings. How will this be paid? Please consider adding line items for this work.

Answer: Traffic Control will remain a lump sum item. As noted in the Payment section of SP-86 (Traffic Control), temporary pavement markings are included in the cost of this pay item (pay item 311).

12. Will the temporary pavement and asphalt wedging required to construct the detour be paid by the appropriate line items for the asphalt types per the typical sections?

Answer: Payment for the appropriate asphalt items and wedging required for the detour route will be paid for with pay items 83 thru 88. There will not be any payment for the removal of asphalt pavement, which will be incidental to the lump sum Traffic Control pay item (pay item 311), as noted in SP-86.

END OF ADDENDUM NO. 3