

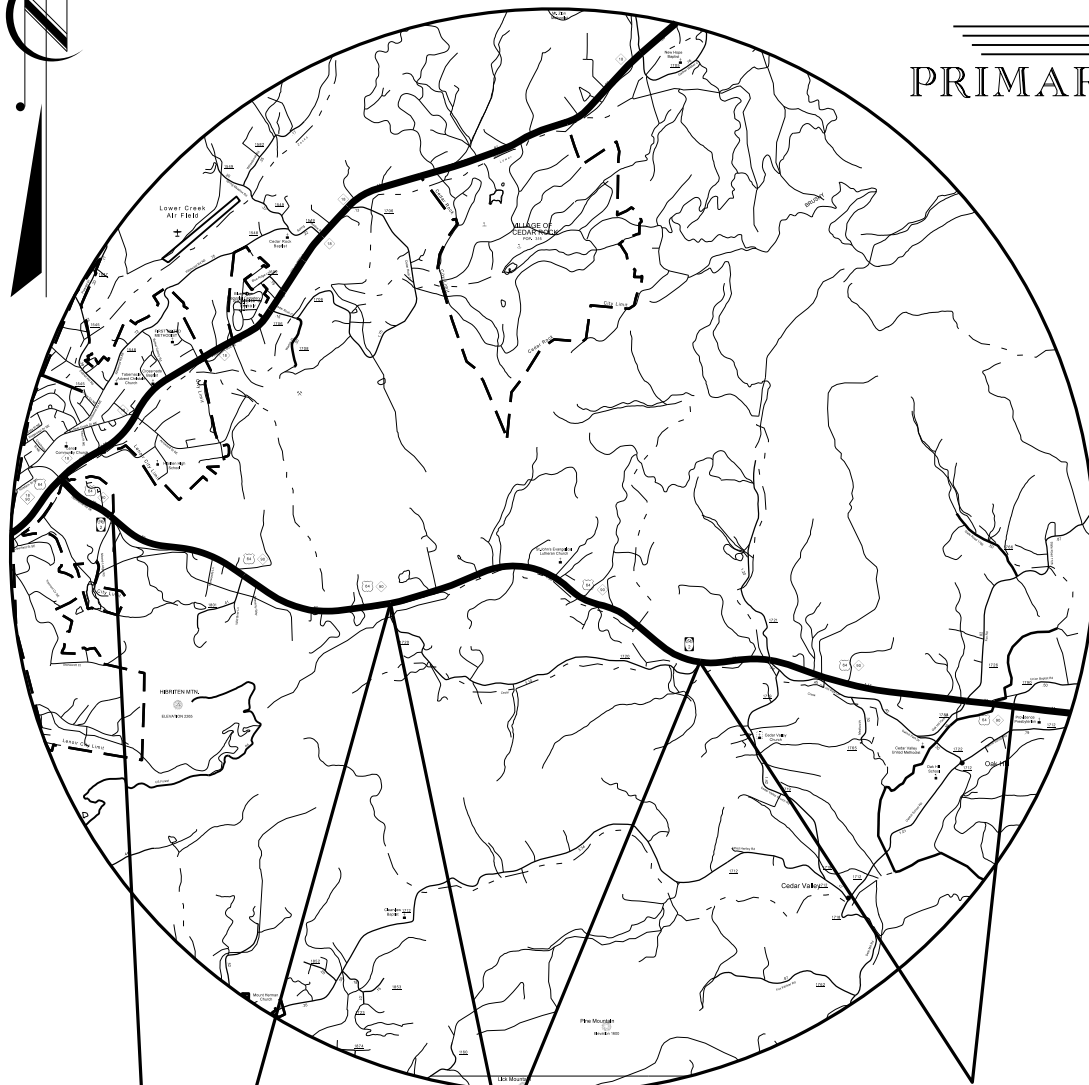
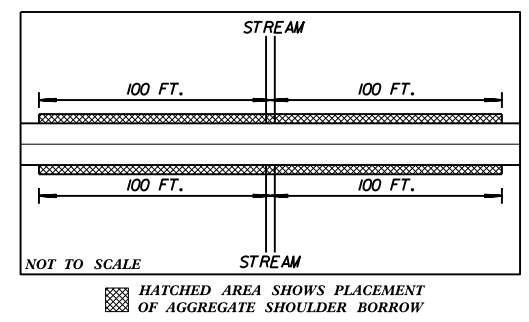
8/17/99
04-MAR-2016 12:43
C:\Users\j\Documents\Project Manager\Contract\Division\DK00185 - 2017 Caldwell Resurfacing\2017 Caldwell Resurfacing Maps.dgn

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

2017

*DETAIL FOR AGGREGATE SHOULDER BORROW
AT STREAM CROSSING

CALDWELL COUNTY
PRIMARY ASPHALT RESURFACING



MAP #1 US 64 (1.55 MILES)
FROM PROJECT LIMITS OF R-5745
TO SR 1720

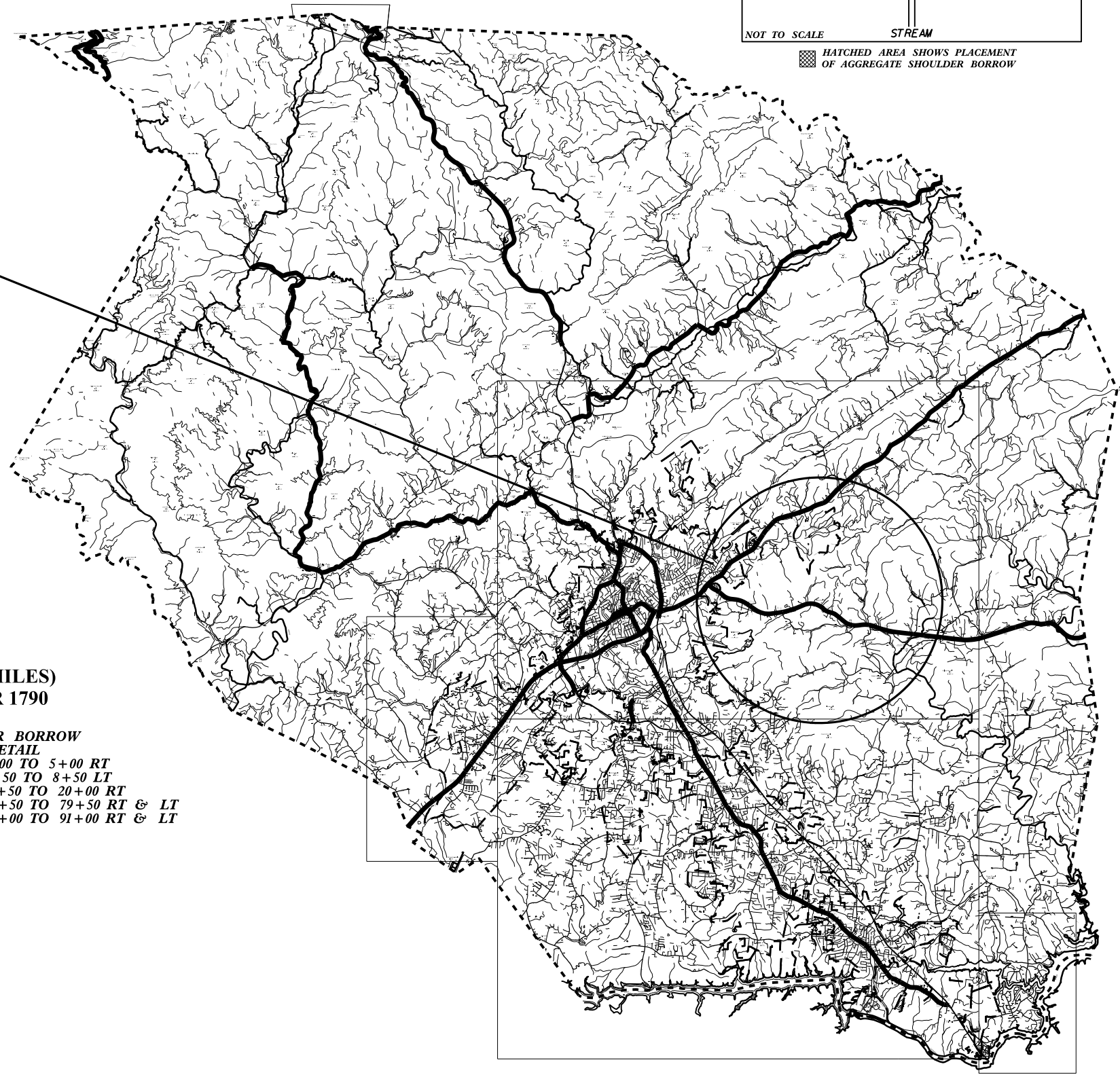
AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 68+00 TO 70+00 RT & LT
* APPROX. STATION 98+00 TO EOP RT

MAP #2 US 64 (1.90 MILES)
FROM SR 1720 TO SR 1720

AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 12+00 TO 13+00 RT & LT
* APPROX. STATION 20+50 TO 24+50 RT
* APPROX. STATION 27+10 TO 28+10 RT & LT
* APPROX. STATION 34+00 TO 35+00 LT
* APPROX. STATION 34+00 TO 38+50 RT
* APPROX. STATION 44+00 TO 53+50 RT
* APPROX. STATION 78+50 TO 79+50 RT & LT

MAP #3 US 64 (1.76 MILES)
FROM SR 1720 TO SR 1790

AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 3+00 TO 5+00 RT
* APPROX. STATION 7+50 TO 8+50 LT
* APPROX. STATION 12+50 TO 20+00 RT
* APPROX. STATION 77+50 TO 79+50 RT & LT
* APPROX. STATION 90+00 TO 91+00 RT & LT



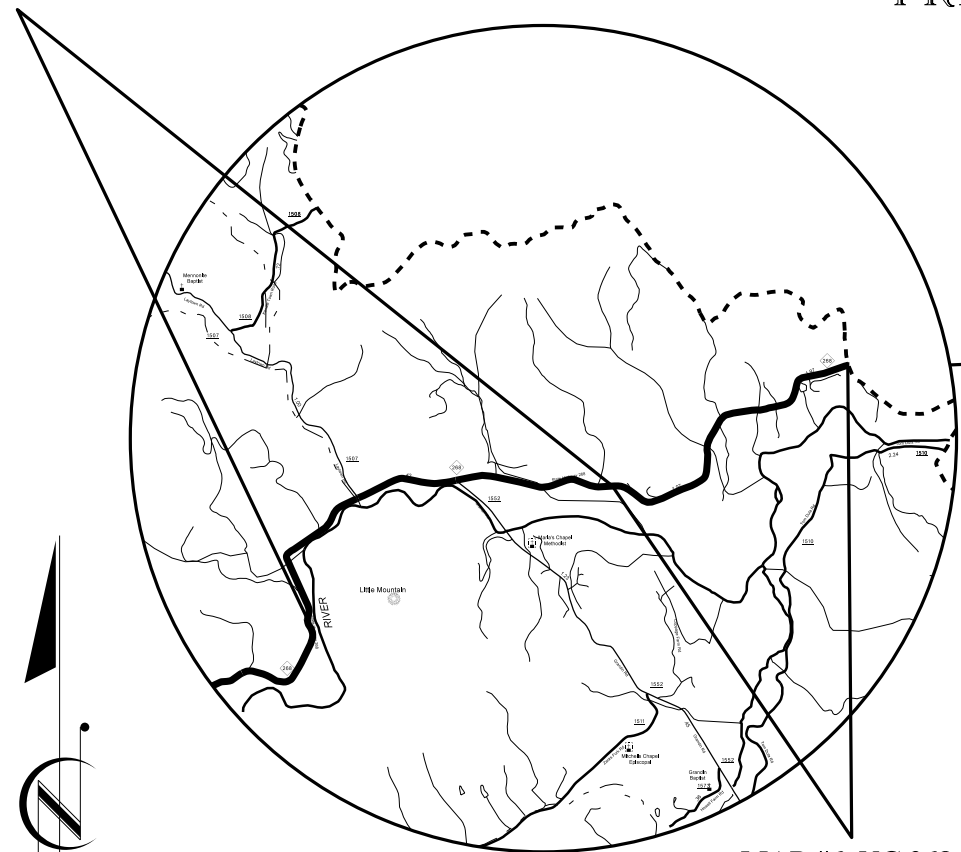
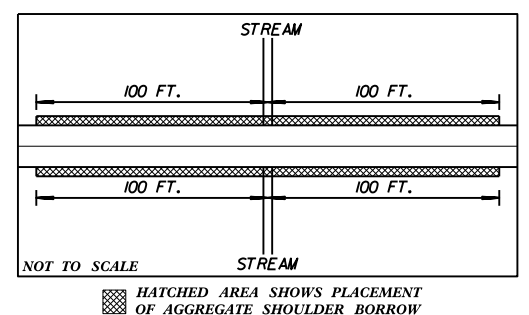
AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 00+00 TO 15+00 RT
* APPROX. STATION 25+75 TO 26+75 LT
* APPROX. STATION 25+75 TO 37+00 RT
* APPROX. STATION 46+75 TO 47+75 RT & LT
* APPROX. STATION 63+50 TO 69+60 RT
* APPROX. STATION 63+50 TO 64+50 LT
* APPROX. STATION 81+00 TO 84+00 LT
* APPROX. STATION 83+00 TO 89+00 RT
* APPROX. STATION 100+00 TO 101+00 RT & LT

MAP #5 NC 268 (2.0 MILES)
FROM SR 1507+1.53 MILES
TO SR 1552 + 0.67 MILES

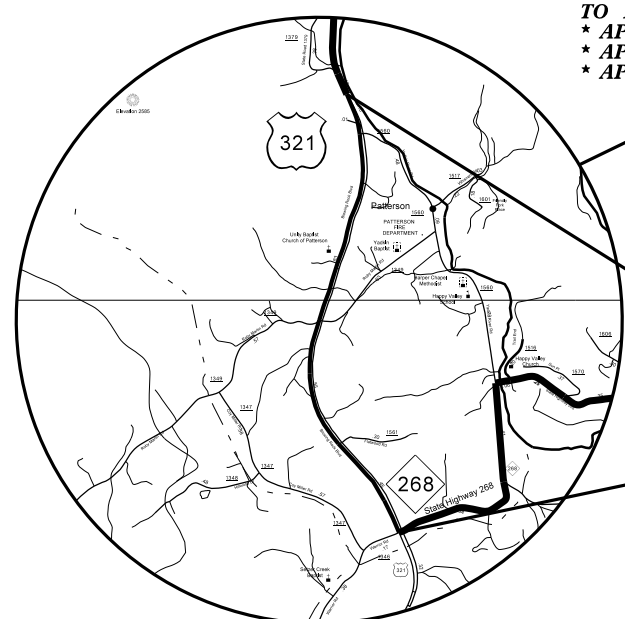
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS
CALDWELL COUNTY
PRIMARY ASPHALT RESURFACING

2017

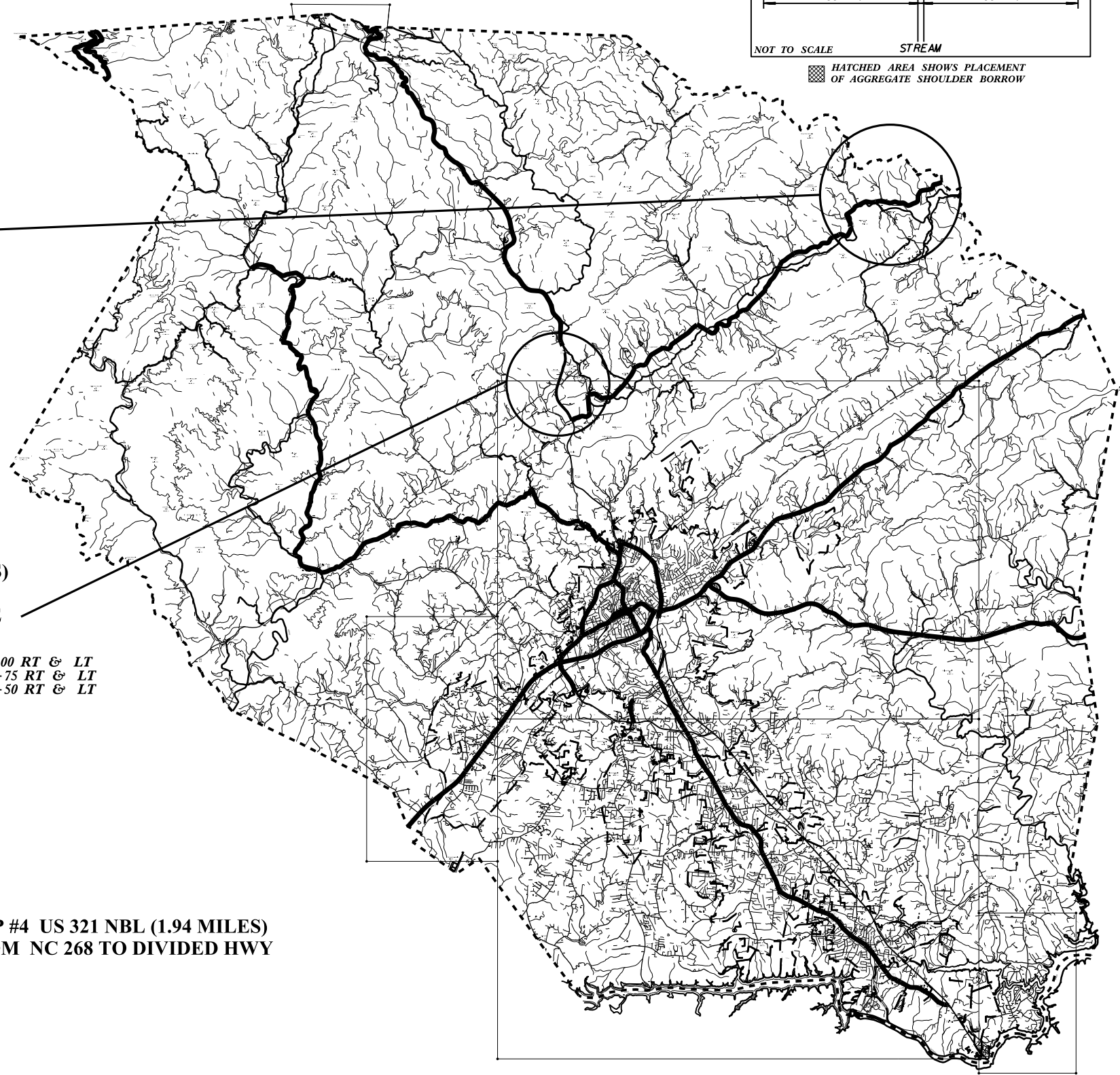
*DETAIL FOR AGGREGATE SHOULDER BORROW
AT STREAM CROSSING



MAP #6 NC 268 (1.30 MILES)
FROM SR 1552+0.67 MILES
TO WILKES COUNTY LINE
AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 28+00 TO 29+00 RT & LT
* APPROX. STATION 44+25 TO 45+75 RT & LT
* APPROX. STATION 56+50 TO 57+50 RT & LT



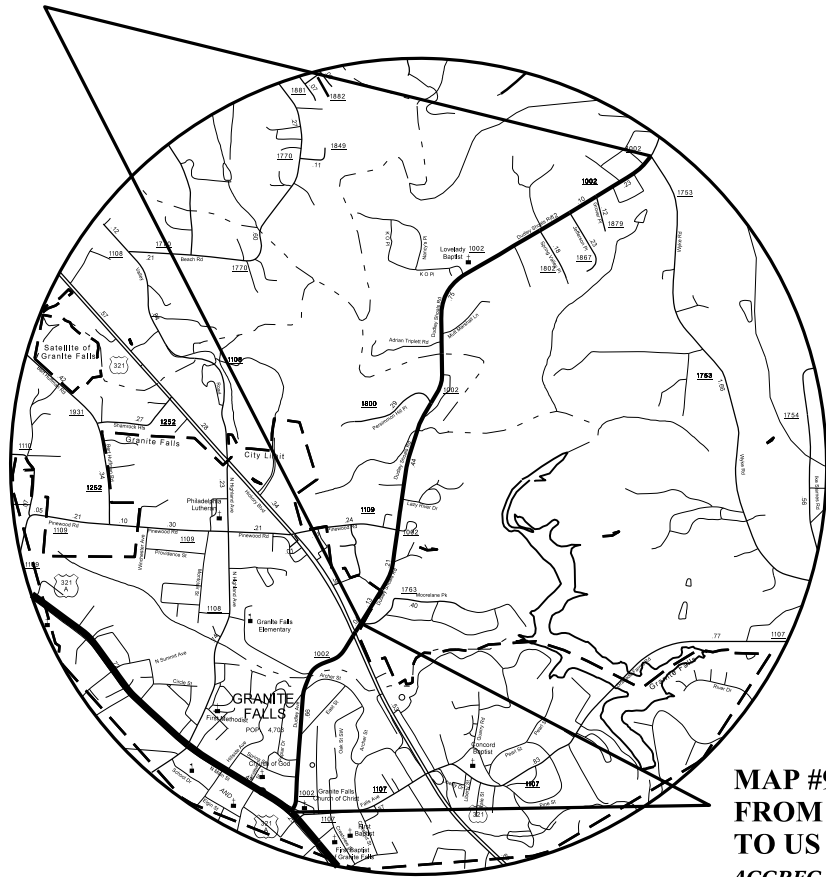
MAP #4 US 321 NBL (1.94 MILES)
FROM NC 268 TO DIVIDED HWY



AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 12+50 TO 17+00 RT & LT
MAP #7 SR 1002 (0.56 MILES)
FROM SR 1734 TO SR 1746



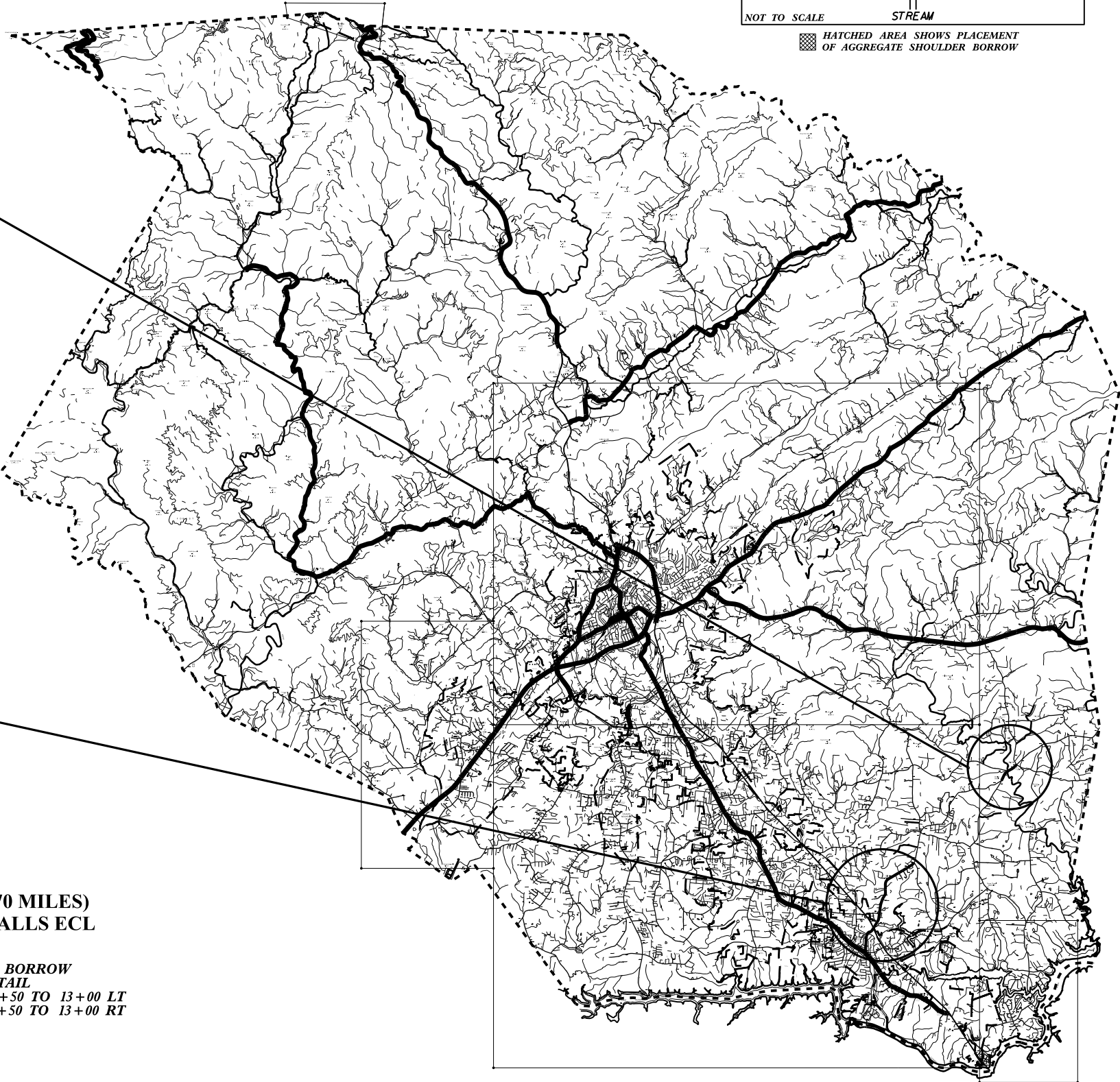
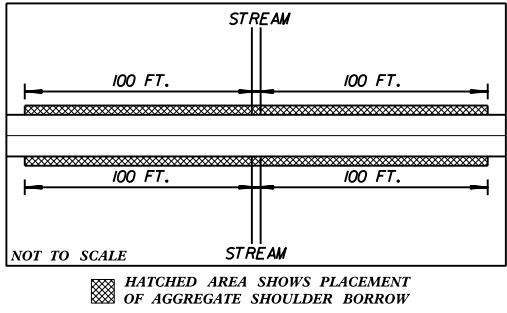
AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 45+65 TO 46+15 RT & LT
* APPROX. STATION 47+50 TO 48+00 RT & LT
* APPROX. STATION 60+50 TO 61+50 RT & LT
MAP #8 SR 1002 (1.86 MILES)
FROM SR 1753 TO GRANITE FALLS ECL



STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS
CALDWELL COUNTY
SECONDARY ASPHALT RESURFACING

2017

*DETAIL FOR AGGREGATE SHOULDER BORROW
AT STREAM CROSSING



MAP #9 SR 1002 (0.70 MILES)
FROM GRANITE FALLS ECL
TO US 321A
AGGREGATE SHOULDER BORROW
TO BE PLACED: SEE DETAIL
* APPROX. STATION 9+50 TO 13+00 LT
* APPROX. STATION 12+50 TO 13+00 RT

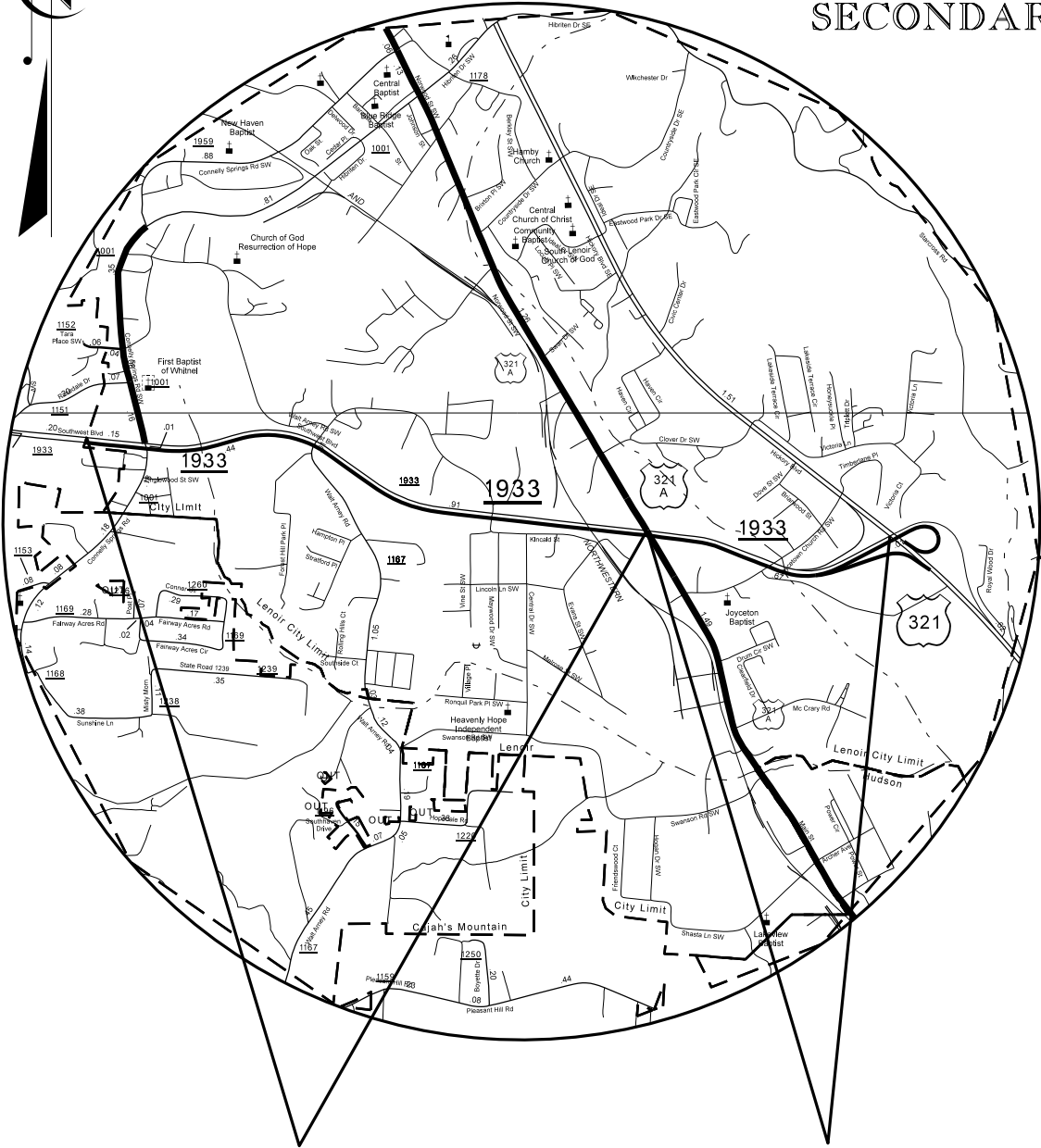


STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

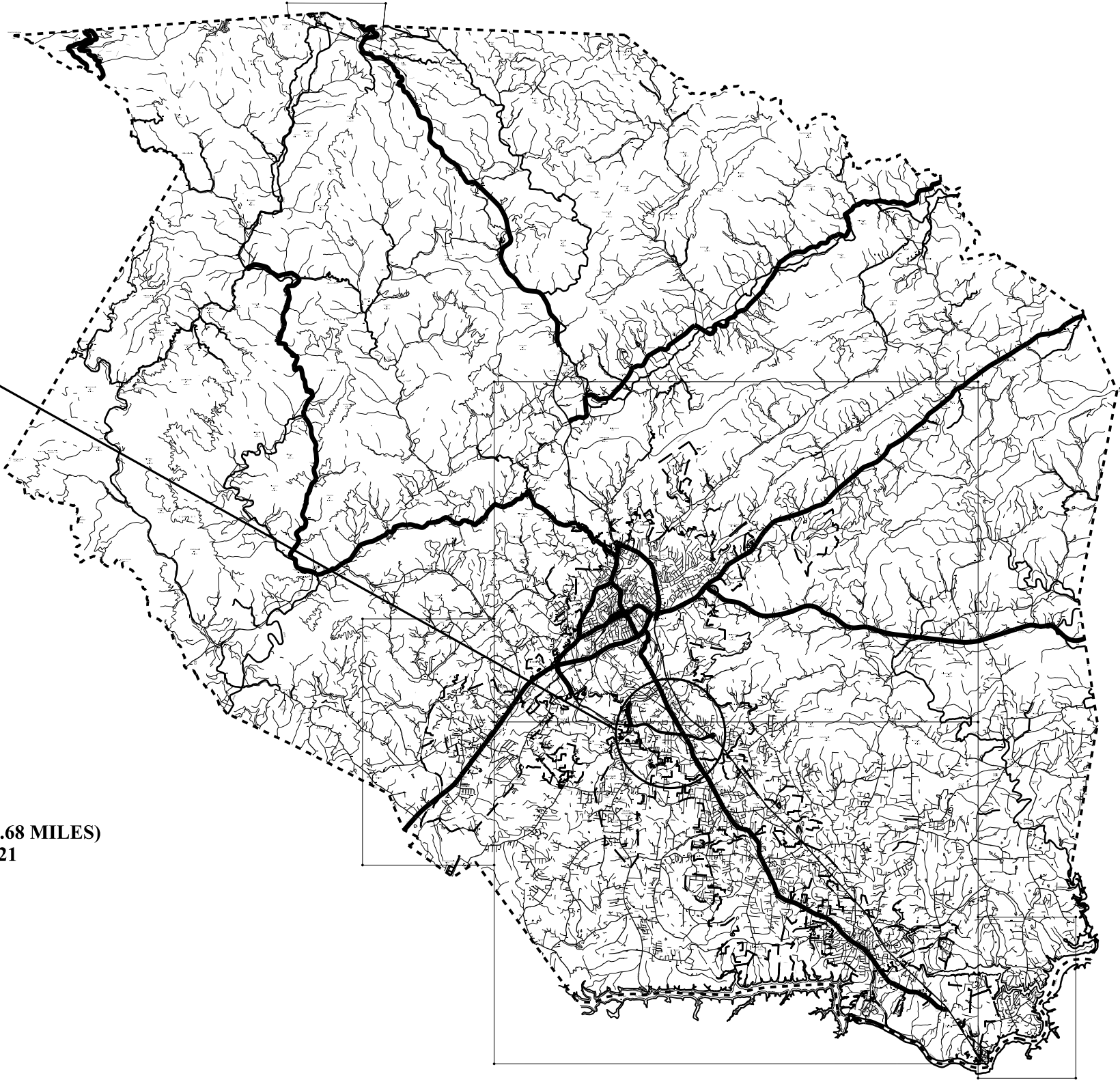
2017

CALDWELL COUNTY

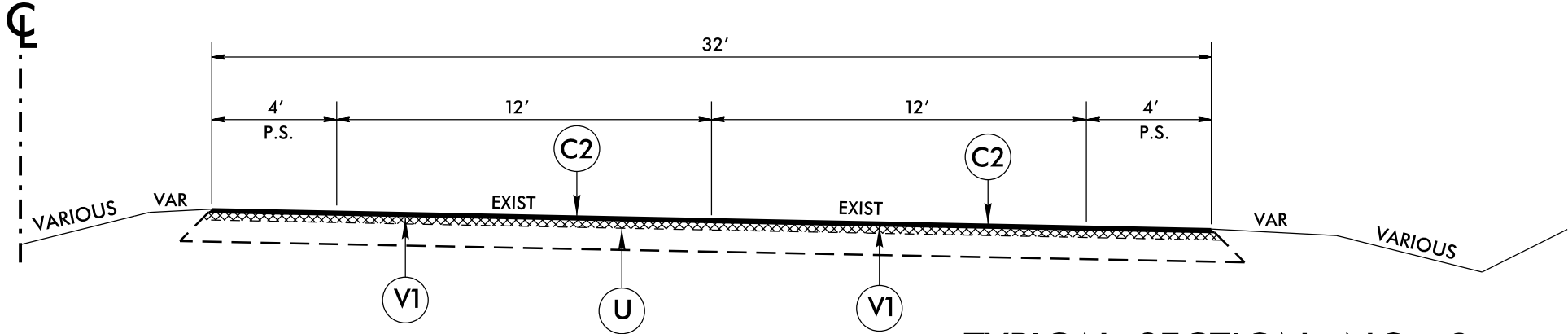
SECONDARY ASPHALT RESURFACING



MAP #10 SR 1933 EBL (1.53 MILES)
FROM LENOIR WCL TO US 321A

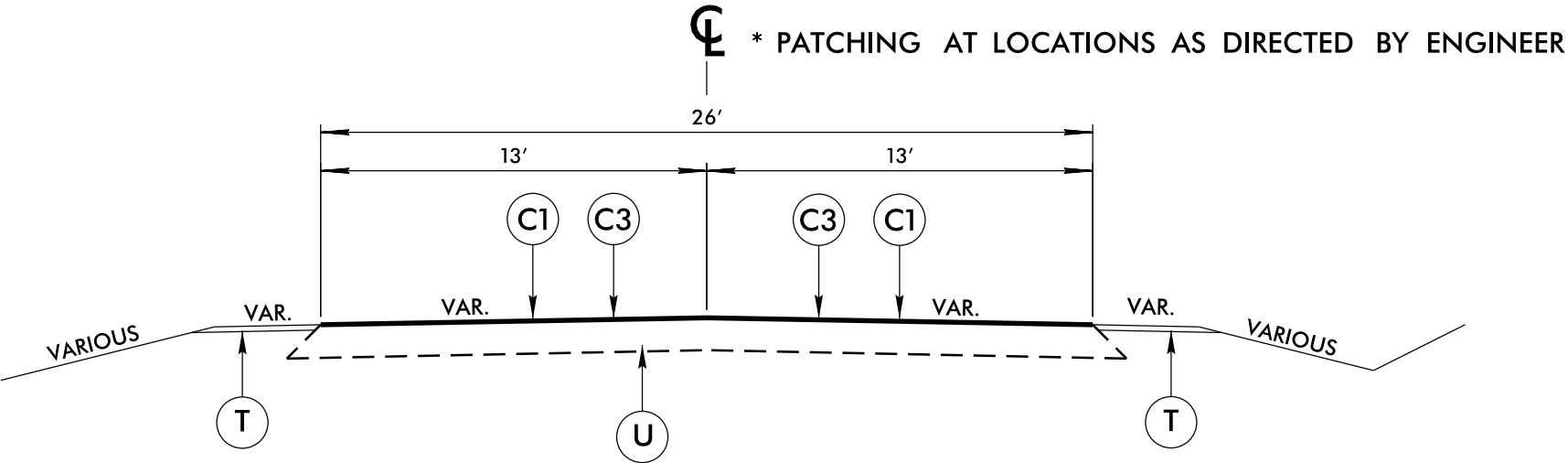


MAP #11 SR 1933 EBL (0.68 MILES)
FROM US 321A TO US 321



TYPICAL SECTION NO. 2

MAP 4 – US 321 NBL FROM NC 268 TO US 321



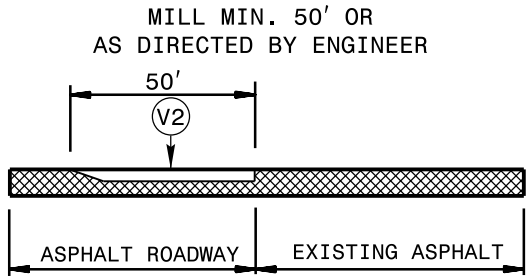
TYPICAL SECTION NO. 1

*MAP 1 – US 64 FROM PROJECT LIMITS FO R-5745 TO SR 1720

*MAP 2 – US 64 FROM SR 1720 TO SR 1720

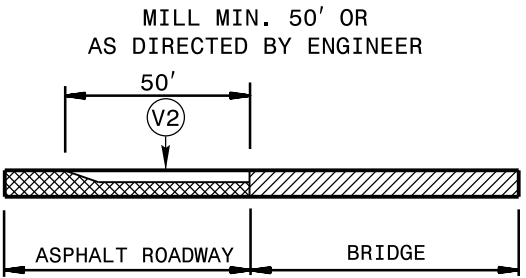
*MAP 3 – US 64 FROM SR 1720 TO SR 1790

	PAVEMENT SCHEDULE
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. ASPHALT SURFACE TREATMENT, MATCOAT #6 STONE
T	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING ASPHALT PAVEMENT AT DEPTH OF 1½"
V2	INCIDENTAL MILLING



BEGIN & END MAP TIE IN DETAIL

TIE ASPHALT OVERLAY TO EXISTING ASPHALT PAVEMENT AT BEGIN & END OF MAPS OR AS DIRECTED BY THE ENGINEER



BRIDGE TIE IN DETAIL

TIE ASPHALT OVERLAY TO BRIDGE APPROACH SLABS OR AS DIRECTED BY THE ENGINEER

CALDWELL COUNTY

PRIMARY AND SECONDARY RESURFACING

DIVISION II

REVISIONS	INIT.	DATE

N.C. DEPARTMENT of TRANSPORTATION

DIVISION of HIGHWAYS

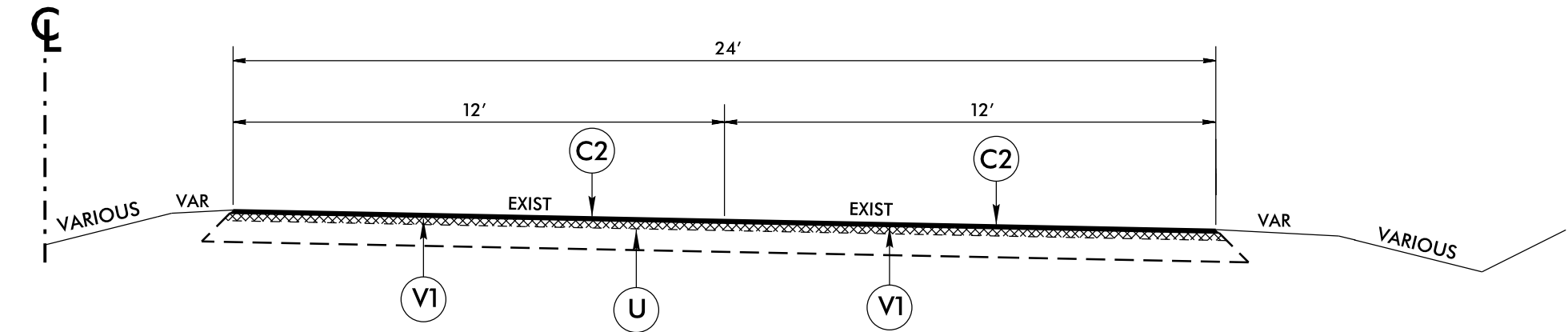
DIVISION ELEVEN

DIVISION OF HIGHWAYS

STATE OF NORTH CAROLINA

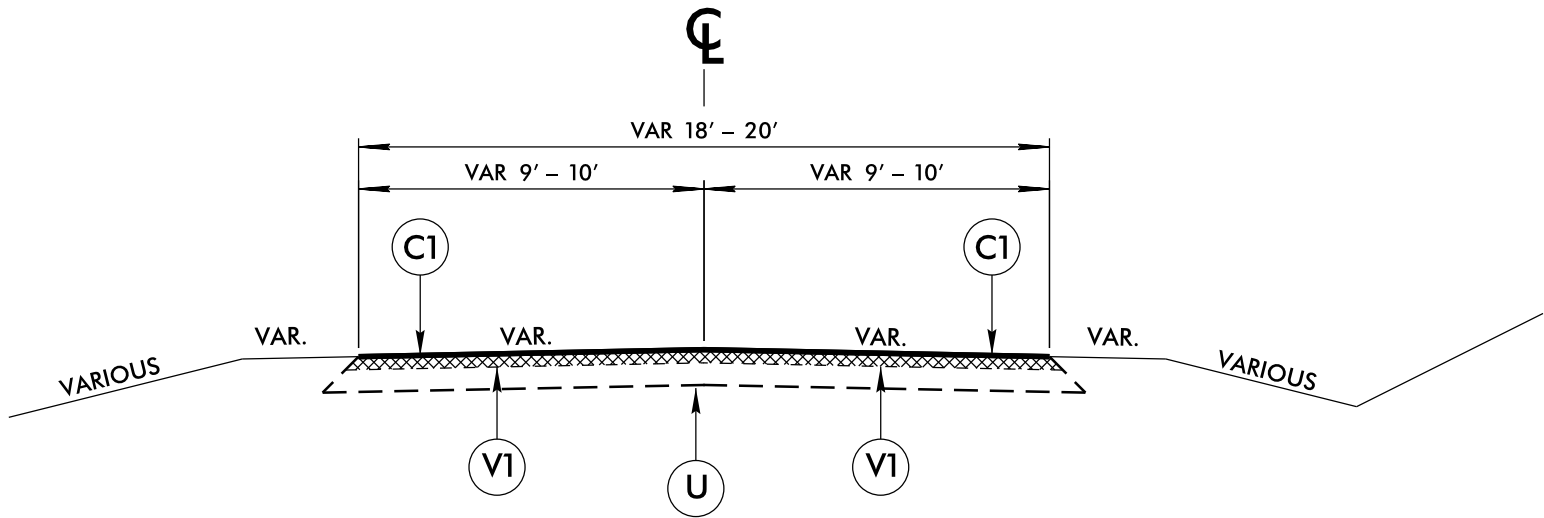
DEPARTMENT OF TRANSPORTATION

SCALE:	N/A	DATE:	02/2016
PREPARED BY:	GREG KIRBY		
REVIEWED BY:			
REVIEWED BY:			



TYPICAL SECTION NO. 4

MAP 10 – SR 1933 EBL FROM LENOIR WCL TO US 321A
MAP 11 – SR 1933 EBL FROM US 321A TO US 321



TYPICAL SECTION NO. 3

MAP 5 – NC 268 FROM SR 1507 +1.53 TO SR 1552 +0.67 MILES
MAP 6 – NC 268 FROM SR 1552 +0.67 MILES TO WILKES COUNTY LINE
MAP 7 – SR 1002 FROM SR 1734 TO SR 1746
MAP 8 – SR 1002 FROM SR 1753 TO GRANITE FALLS ECL
MAP 9 – SR 1002 FROM GRANITE FALLS ECL TO US 321A

	PAVEMENT SCHEDULE
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C3	PROP. ASPHALT SURFACE TREATMENT, MATCOAT #6 STONE
T	SHOULDER RECONSTRUCTION
U	EXISTING PAVEMENT
V1	MILLING OF EXISTING ASPHALT PAVEMENT AT DEPTH OF 1½"
V2	INCIDENTAL MILLING

CALDWELL COUNTY
PRIMARY AND SECONDARY RESURFACING

DIVISION II

REVISIONS	INIT.	DATE

N.C. DEPARTMENT of TRANSPORTATION
DIVISION of HIGHWAYS
DIVISION ELEVEN



SCALE: N/A DATE: 02/2016

PREPARED BY: GREG KIRBY

REVIEWED BY:

REVIEWED BY:

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.11.06.10141, 2017CPT.11.07.20141		

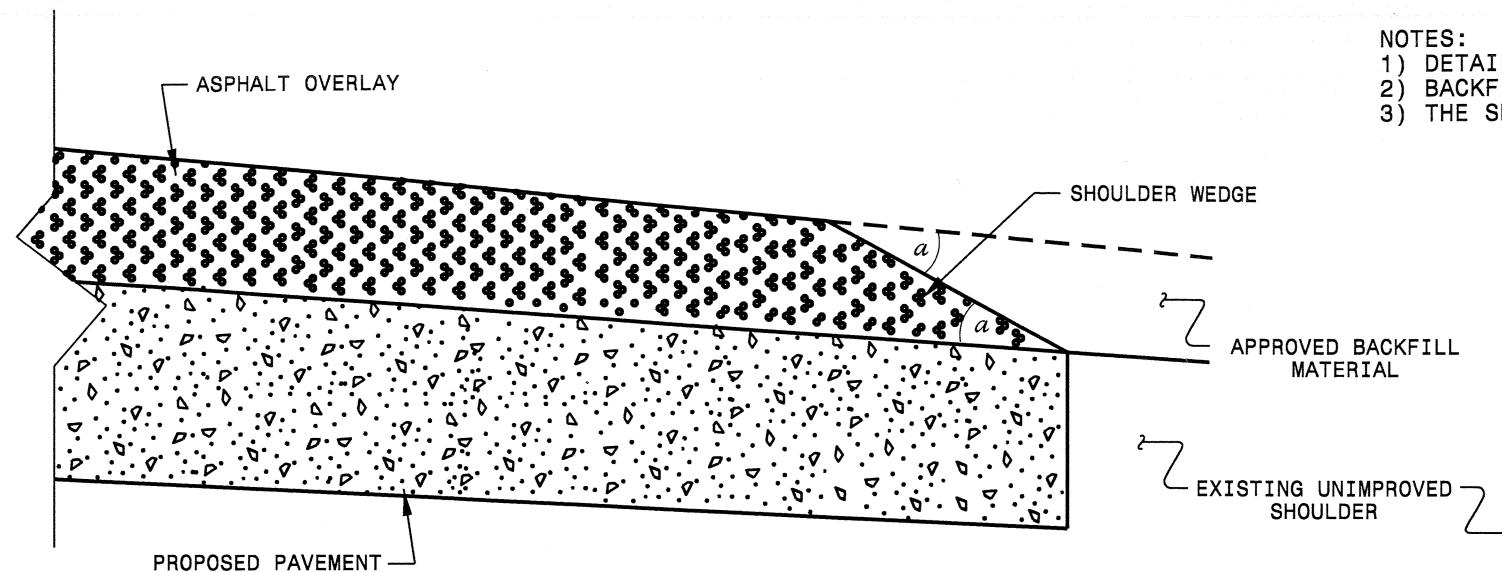
SUMMARY OF QUANTITIES

PROJECT	COUNTY	MAP	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BORROW EXCAVATION	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1½" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, MATCOAT #6 STONE	EMULSION FOR ASPHALT SURFACE TREATMENT GAL	ADJ. OF DROP INLET	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	SEED & MULCHING	PAVED TRENCHING (1, 2")	UNPAVED TRENCHING (1, 2")	JUNCTION BOX (STANDARD SIZE)	INDUCTIVE LOOP	LEAD-IN CABLE	
NO		NO			NO					MI	FT	CY	TONS	SMI	SY	SY	TONS	TONS	TONS	TONS	SY		EA	EA	EA	AC	LF	LF	EA	LF	LF	
2017CPT.11.06.10141	Caldwell	1	US 64	FROM PROJECT LIMITS OF R-5745 TO SR 1720	1	2	2WU	NO	NO	1.55	26	310	50	3.10		150	2,050		123	100	23,643	9,458				1.15						
2017CPT.11.06.10141	Caldwell	2	US 64	FROM SR 1720 TO SR 1720	1	2	2WU	NO	NO	1.9	26	380	75	3.80			2,520		151	100	28,981	11,582				1.35						
2017CPT.11.06.10141	Caldwell	3	US 64	FROM SR 1720 TO SR 1790	1	2	2WU	NO	NO	1.76	26	352	100	3.52		150	2,350		141	100	26,846	10,738				1.20						
2017CPT.11.06.10141	Caldwell	4	US 321 NBL	FROM NC 268 TO US 321	2	2	MD	NO	NO	1.94	32		75		40,000			3,300	195													
2017CPT.11.06.10141	Caldwell	5	NC 268	FROM SR 1507 +1.53 MILES TO SR 1552 +0.67 MILES	3	2	2WU	NO	NO	2	18		100		22,000		2,000		120						1.45							
2017CPT.11.06.10141	Caldwell	6	NC 268	FROM SR 1552 +0.67 MILES TO WILKES COUNTY LINE	3	2	2WU	NO	NO	1.3	18		100		13,800		1,275		77													
TOTAL FOR PROJ NO. 2017CPT.11.06.10141										10.45		1,042	500	10.42	75,800	300	10,195	3,300	807	300	79,470	31,778				5.15						
2017CPT.11.07.20141	Caldwell	7	SR 1002	FROM SR 1734 TO SR 1746	3	2	2WU	NO	NO	0.56	20		25		6,700		630		38								50.00	50.00	1.00	184	50	
2017CPT.11.07.20141	Caldwell	8	SR 1002	FROM SR 1753 TO GRANITE FALLS ECL	3	2	2WU	NO	NO	1.86	18		100		21,000		1,915		115						2							
2017CPT.11.07.20141	Caldwell	9	SR 1002	FROM GRANITE FALLS ECL TO US 321A	3	2	2WU	NO	NO	0.7	18		50		7,500		725		43				1	2	6							
2017CPT.11.07.20141	Caldwell	10	SR 1933 EBL	FROM LENOIR WCL TO US 321A	4	2	MD	NO	NO	1.53	24		20		32,000			2,600	153						100.00	50.00	1.00	372	100			
2017CPT.11.07.20141	Caldwell	11	SR 1933 EBL	FROM US 321A TO US 321	4	2	MD	NO	NO	0.68	24		20		14,000			1,200	71													
TOTAL FOR PROJ NO. 2017CPT.11.07.20141										5.33			215		81,200		3,270	3,800	420				1	2	8		150.00	100.00	2.00	556	150	
GRAND TOTAL										15.78		1,042	715	10.42	157,000	300	13,465	7,100	1,227	300	79,470	31,778	1	2	8	5.15	150.00	100.00	2.00	556	150	

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.11.06.10141, 2017CPT.11.07.20141		

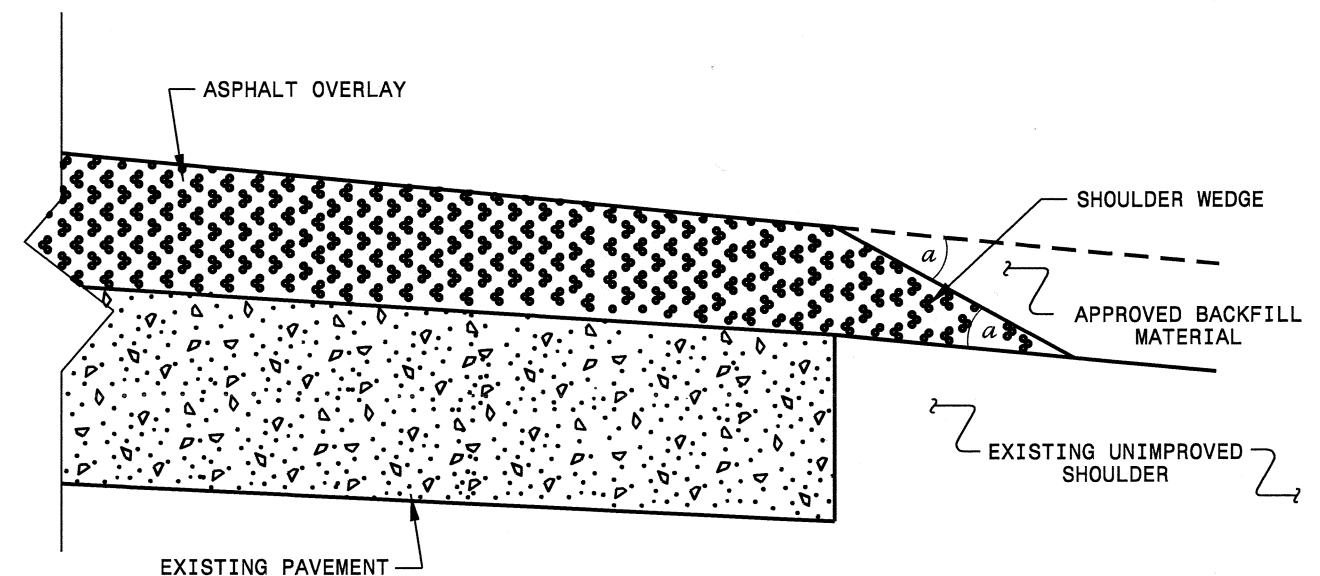
THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4810000000-E		4820000000-E		4835000000-E	4840000000-N	4845000000-N			4905000000-N
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" WHITE PAINT	4" YELLOW PAINT	8" YELLOW PAINT	8" WHITE PAINT	24" WHITE PAINT	PAINT MSG SCHOOL	PAINT LT ARROW	PAINT RT ARROW	PAINT STR ARROW	SNOW PLOWABLE MARKERS
										SF	LS	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA
2017CPT.11.06.10141	Caldwell	1	US 64	FROM PROJECT LIMITS OF R-5745 TO SR 1720	1	2	2WU	1.55	26	84	1.00	32,736	32,736			100	12				102
2017CPT.11.06.10141	Caldwell	2	US 64	FROM SR 1720 TO SR 1720	1	2	2WU	1.9	26	84	*	40,128	40,128								125
2017CPT.11.06.10141	Caldwell	3	US 64	FROM SR 1720 TO SR 1790	1	2	2WU	1.76	26	84	*	37,171	37,171								116
2017CPT.11.06.10141	Caldwell	4	US 321 NBL	FROM NC 268 TO US 321	2	2	MD	1.94	32	164	*	28,200	21,500	50		108		18	8		180
2017CPT.11.06.10141	Caldwell	5	NC 268	FROM SR 1507 +1.53 MILES TO SR 1552 +0.67 MILES	3	2	2WU	2	18	116	*	42,240	42,240								135
2017CPT.11.06.10141	Caldwell	6	NC 268	FROM SR 1552 +0.67 MILES TO WILKES COUNTY LINE	3	2	2WU	1.3	18	84	*	27,456	27,456								90
TOTAL FOR PROJ NO. 2017CPT.11.06.10141								10.45		616		207,931	201,231	50		208	12	18	8		748
												409,162		50				26			
2017CPT.11.07.20141	Caldwell	7	SR 1002	FROM SR 1734 TO SR 1746	3	2	2WU	0.56	20	52	*	12,400	12,000		160	170	12				37
2017CPT.11.07.20141	Caldwell	8	SR 1002	FROM SR 1753 TO GRANITE FALLS ECL	3	2	2WU	1.86	18	136	*	39,283	39,283	100							124
2017CPT.11.07.20141	Caldwell	9	SR 1002	FROM GRANITE FALLS ECL TO US 321A	3	2	2WU	0.7	18	52	*	14,784	14,784		220	100		2			48
2017CPT.11.07.20141	Caldwell	10	SR 1933 EBL	FROM LENOIR WCL TO US 321A	4	2	MD	1.53	24	154	*	32,000	17,000		100	160		20	16	20	260
2017CPT.11.07.20141	Caldwell	11	SR 1933 EBL	FROM US 321A TO US 321	4	2	MD	0.68	24	84	*	9,000	7,200		850						85
TOTAL FOR PROJ NO. 2017CPT.11.07.20141								5.33		478		107,467	90,267	100	1,330	430	12	22	16	20	554
												197,734		1,430				58			
GRAND TOTAL								15.78		1,094	1	315,398	291,498	150	1,330	638	24	40	24	20	1,302
												606,896		1,480				84			

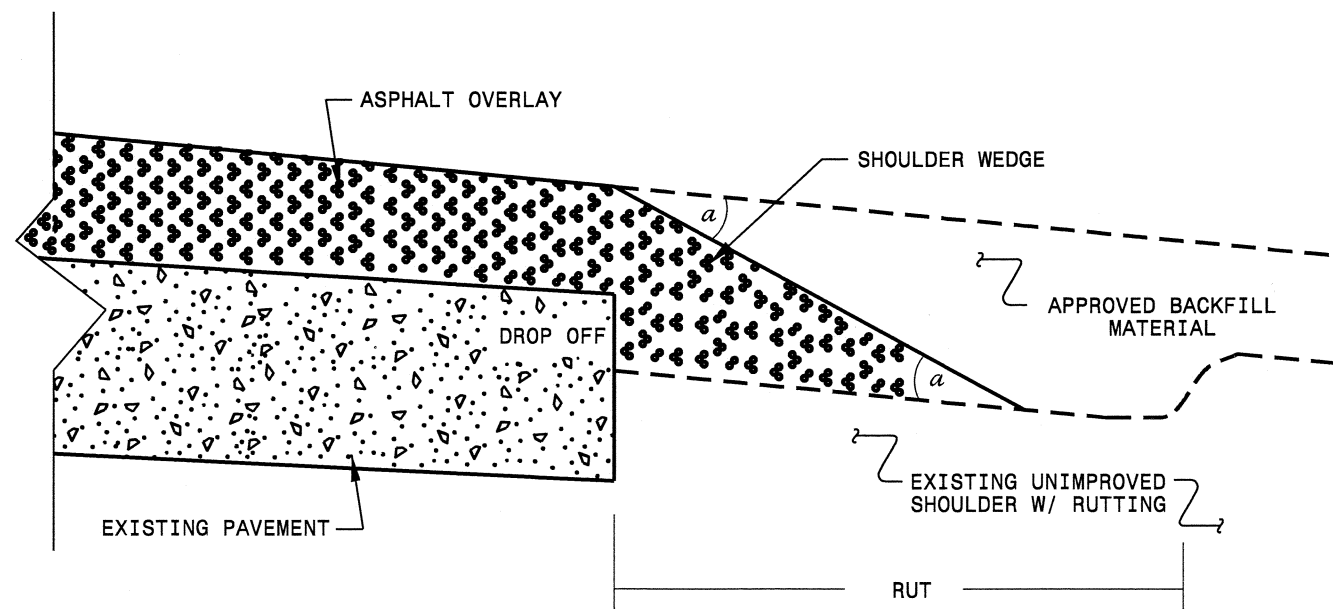


- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.

SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or
with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)



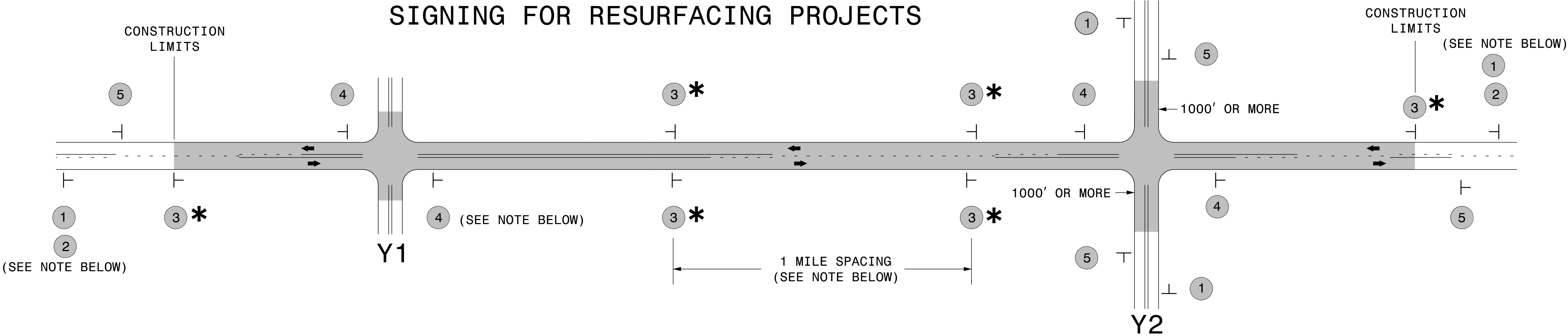
SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to
Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS
AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

**SHOULDER WEDGE
DETAILS**

ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: s:\user\details\stand\shoulderwedgedetail.dgn	



LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

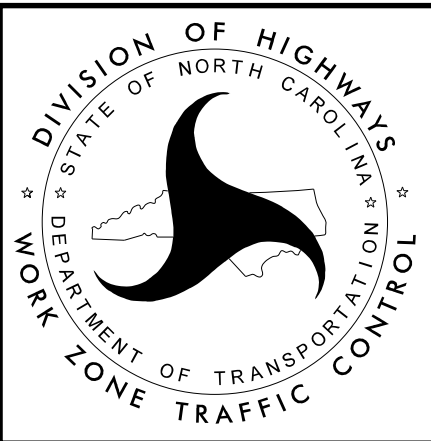
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div><div>1</div><div>ROAD WORK AHEAD W20-1 48" X 48"</div></div> <div><div>2</div><div>NEXT XX MILES W7-3aP 24" X 18"</div></div>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.
	<div><div>3</div><div>LOW/SOFT SHOULDER SP 13107 48" X 48"</div></div>	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET ½ MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.
	<div><div>4</div><div>ROAD UNDER CONST SP 13106 48" X 48"</div></div>	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.
	<div><div>5</div><div>END ROAD WORK G20-2 A 48" X 24"</div></div>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.
	<div>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS</div> <div>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</div> <div><div>ROAD WORK AHEAD W20-1 48" X 48"</div><div>FLAGGER W20-7 A 48" X 48"</div></div> <div>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</div>	

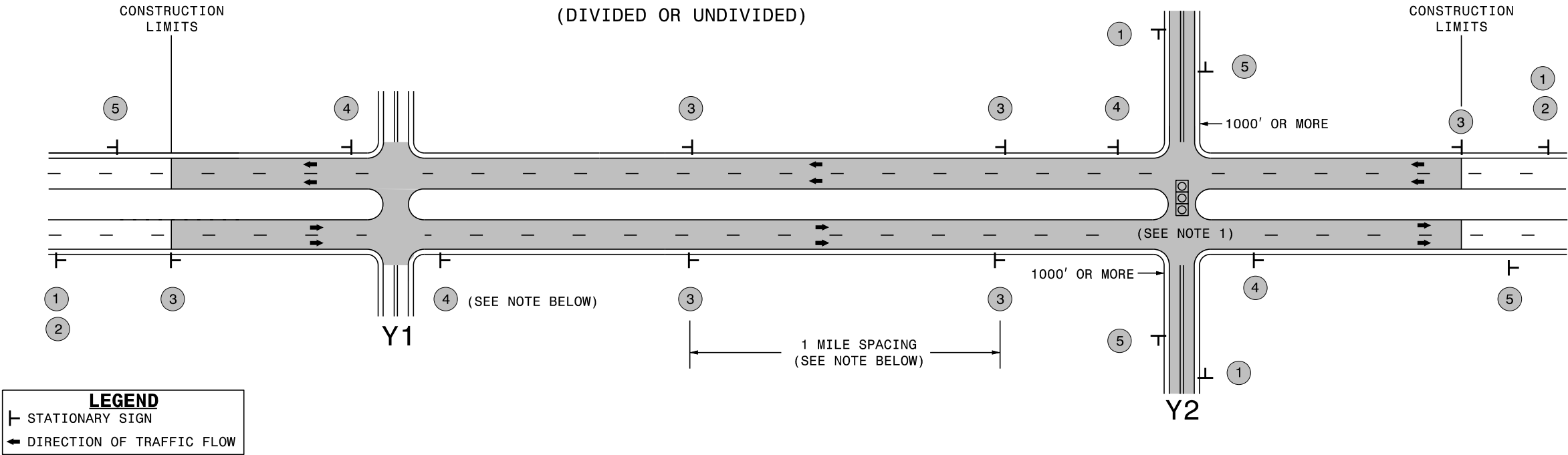
* SIGNING FOR ASPHALT SURFACE TREATMENTS (ONLY)

SUBSTITUTE LOW/SOFT SHOULDER SIGNS BY ALTERNATING THE FOLLOWING TWO SIGNS:
STARTING WITH "UNMARKED PAVEMENT AHEAD" (SP 06026) FOLLOWED BY "LOOSE GRAVEL" (W8-7).



RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS

SIGNING FOR RURAL AND SUBURBAN MULTI-LANE
ROADWAYS WITH SHOULDER SECTIONS
(DIVIDED OR UNDIVIDED)



MAINLINE (-L-) SIGNING

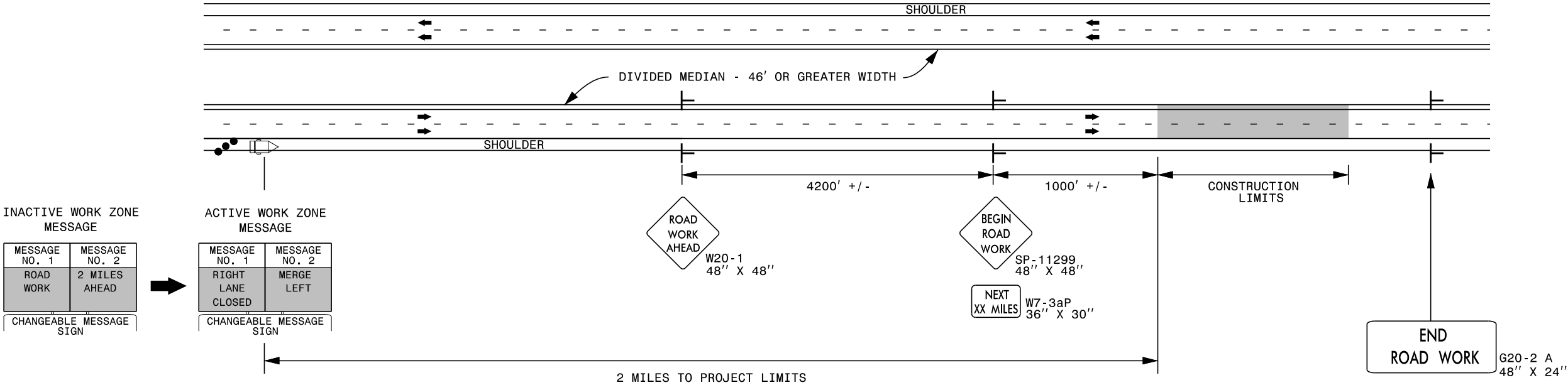
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<div>1</div> <div>2</div> <div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div><div>NEXT XX MILES</div><div>W7-3aP 24" X 18"</div></div> <div>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</div> <div>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</div>	<div>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</div> <div>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS</div> <div>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</div> <div><div><div>ROAD WORK AHEAD</div><div>W20-1 48" X 48"</div></div><div><div><div>ROAD WORK AHEAD</div><div>W20-7 A 48" X 48"</div></div></div><div>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</div></div>
	<div>3</div> <div><div>LOW/SOFT SHOULDER</div><div>SP 13107 48" X 48"</div></div> <div>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET ½ MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</div>	
	<div>4</div> <div><div>ROAD UNDER CONST</div><div>SP 13106 48" X 48"</div></div> <div>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</div>	
	<div>5</div> <div><div>END ROAD WORK</div><div>G20-2 A 48" X 24"</div></div> <div>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</div>	
		<div>NOTES:</div> <div>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</div>

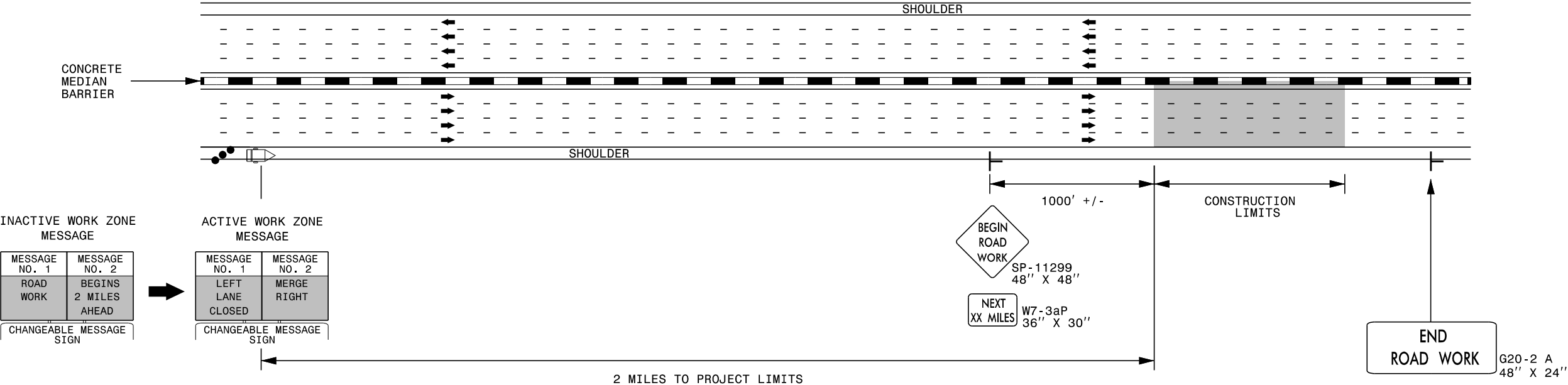


RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS
(DIVIDED OR UNDIVIDED)

DIVIDED MEDIANS WITH WIDTHS 46' OR GREATER



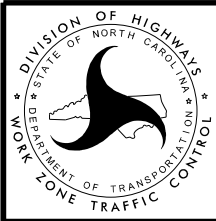
DIVIDED MEDIANS WITH WIDTHS LESS THAN 46' OR WITH PERMANENT MEDIAN BARRIER



- NOTES:
- 1) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 6' AS MEASURED FROM THE EDGE OF PAVEMENT.
 - 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
 - 3) FOR MEDIAN WIDTHS LESS THAN 46' (MEASURED EDGELINE TO EDGELINE) USE THE BOTTOM DRAWING.
 - 4) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
 - 5) INSTALL "ROAD WORK AHEAD" (W20-1) ALONG ENTRANCE RAMP 500' PRIOR TO RAMP TERMINAL, AND "END ROAD WORK" (G20-2a) AT THE END OF EXIT RAMP WITHIN THE WORK ZONE.
 - 6) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER AND WITH DIVIDED MEDIANS OF 46' OR GREATER. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- CHANGEABLE MESSAGE SIGN (CMS)
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW
- TRAFFIC DRUM



RESURFACING ADVANCE
WARNING SIGNS FOR
HIGH SPEED FACILITIES
≥ 60 MPH