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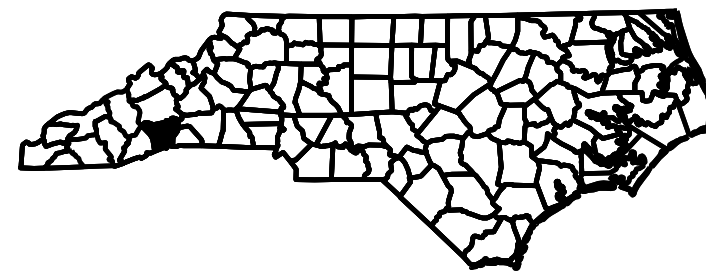
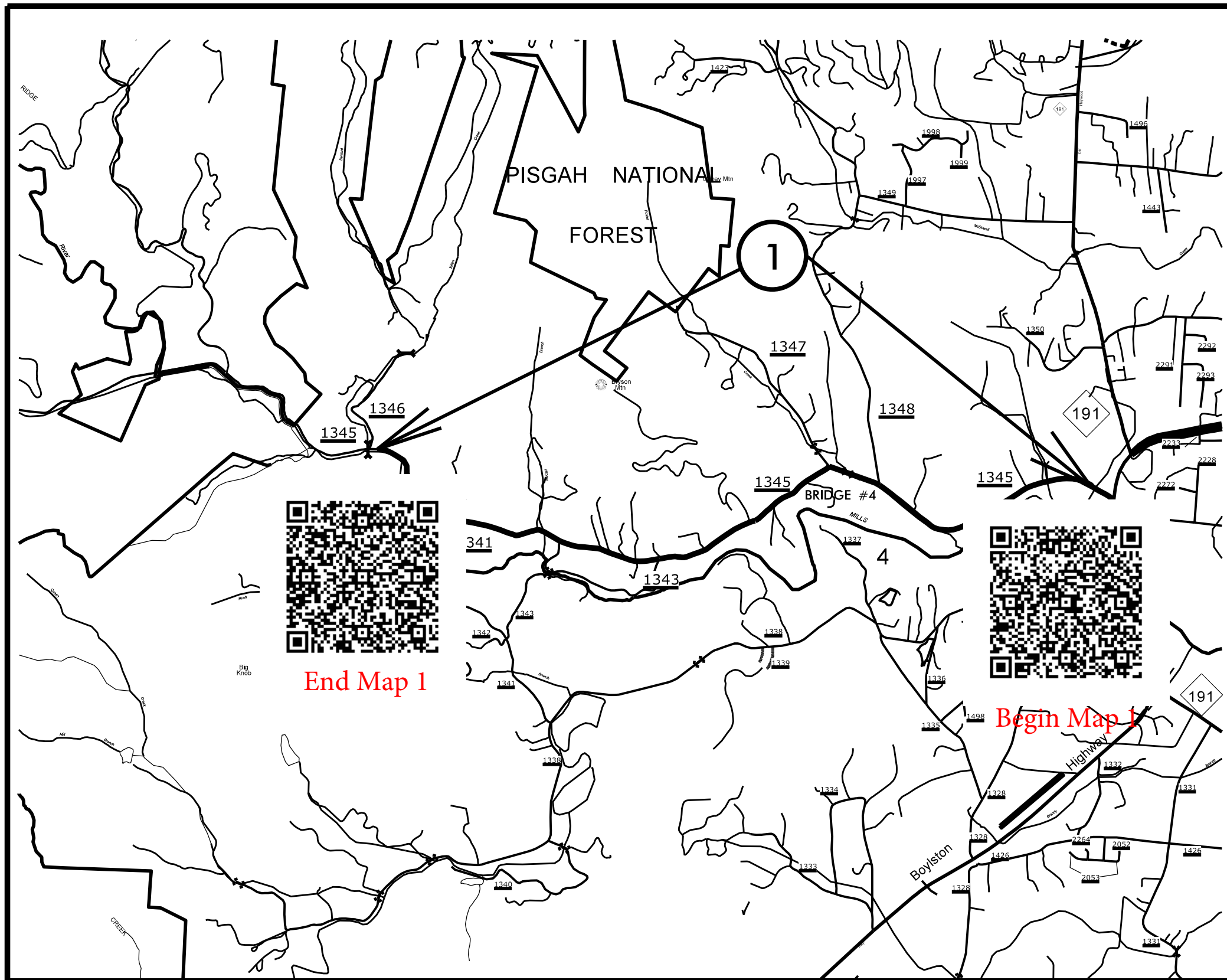
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TIP PROJECT: R-5780

CONTRACT: DN00554

# HENDERSON COUNTY

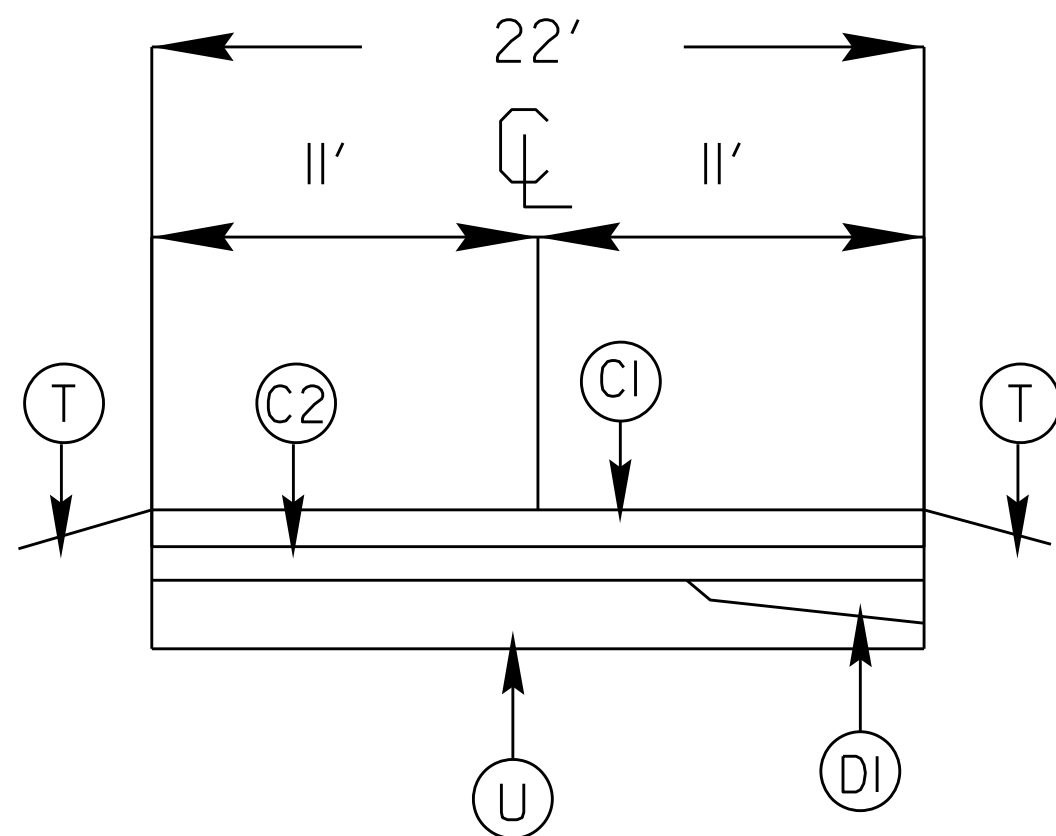
PROJECT REFERENCE NO.		SHEET NO.
44909.3.1		
STATE PROJECT	F.A. PROJECT NO.	DESCRIPTION
	R-5780	DN00554



PROJECT REFERENCE NO.		SHEET NO.
44909.3.1		2
STATE PROJECT	F.A. PROJECT NO.	DESCRIPTION
	R-5780	DN00554

## SURFACING SCHEDULE

# TYPICAL 1



ITEM NO	DESCRIPTION
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD
C2	PROP. APPROX. 3/4" ASPHALT CONCRETE SURFACE COURSE, (LEVELING) TYPE SF9.5A, AT AN AVERAGE RATE OF 70 LBS PER SQ. YD. AT LOCATIONS AS DIRECTED BY PROJECT ENGINEER
D1	PROP. VAR DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, WEDGING TYPE I19.0B, SQ.YARD. AT LOCATIONS AS DIRECTED BY PROJECT ENGINEER
T	SHOULDER RECONSTRUCTION
U	EXISTING ASPHALT

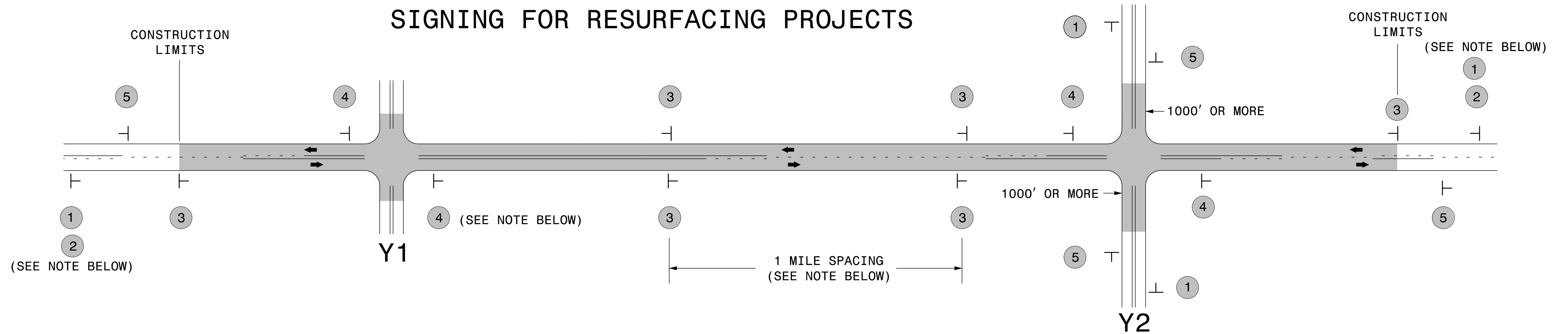
\*NOTE\* BRIDGE #4 MILL 2" IS FOR BRIDGE DECK  
AS DIRECTED BY PROJECT ENGINEER  
AND REPLACE WITH 2" 59.5B

PROJECT NO.	SHEET NO.	TOTAL NO.
44909.3.1	4	

**SUMMARY OF QUANTITIES**

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	SHOULDER RECONSTRUCTION SMI	2" MILLING SY	INTERMEDIATE COURSE (WEDGING), 119.0B TONS	SURFACE COURSE, S9.5B TONS	LEVELING COURSE, SF9.5A TON	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS
44909.3.1	Henderson	1	SR-1345 NORTH MILLS RIVER ROAD	FROM .02 MILES NORTH OF NC-280 TO END DIVIED LANES .03 MILES SOUTH OF USFS CAMPGROUND	1	2	2WU	NO	NO	4.6	22	9.90	600	455	5,363	685	385	850
<b>TOTAL FOR MAP NO. 1</b>										4.6		9.90	600	455	5,363	685	385	850
<b>TOTAL FOR PROJ NO. 44909.3.1</b>										4.6		9.90	600	455	5,363	685	385	850
<b>GRAND TOTAL</b>										4.6		9.90	600	455	5,363	685	385	850

# SIGNING FOR RESURFACING PROJECTS



LEGEND	
	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p style="text-align: center;"><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small> </div> </div> <p style="text-align: center;">PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
		<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
		<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	

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**RESURFACING  
ADVANCE WARNING SIGNS  
FOR  
RURAL AND SUBURBAN  
2 LANE ROADWAYS**