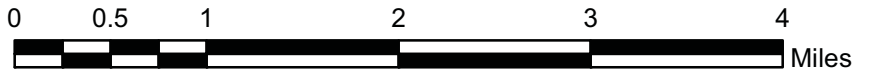
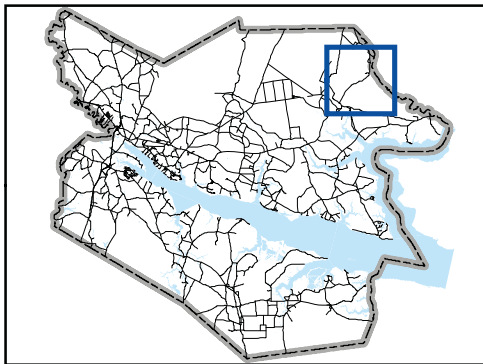
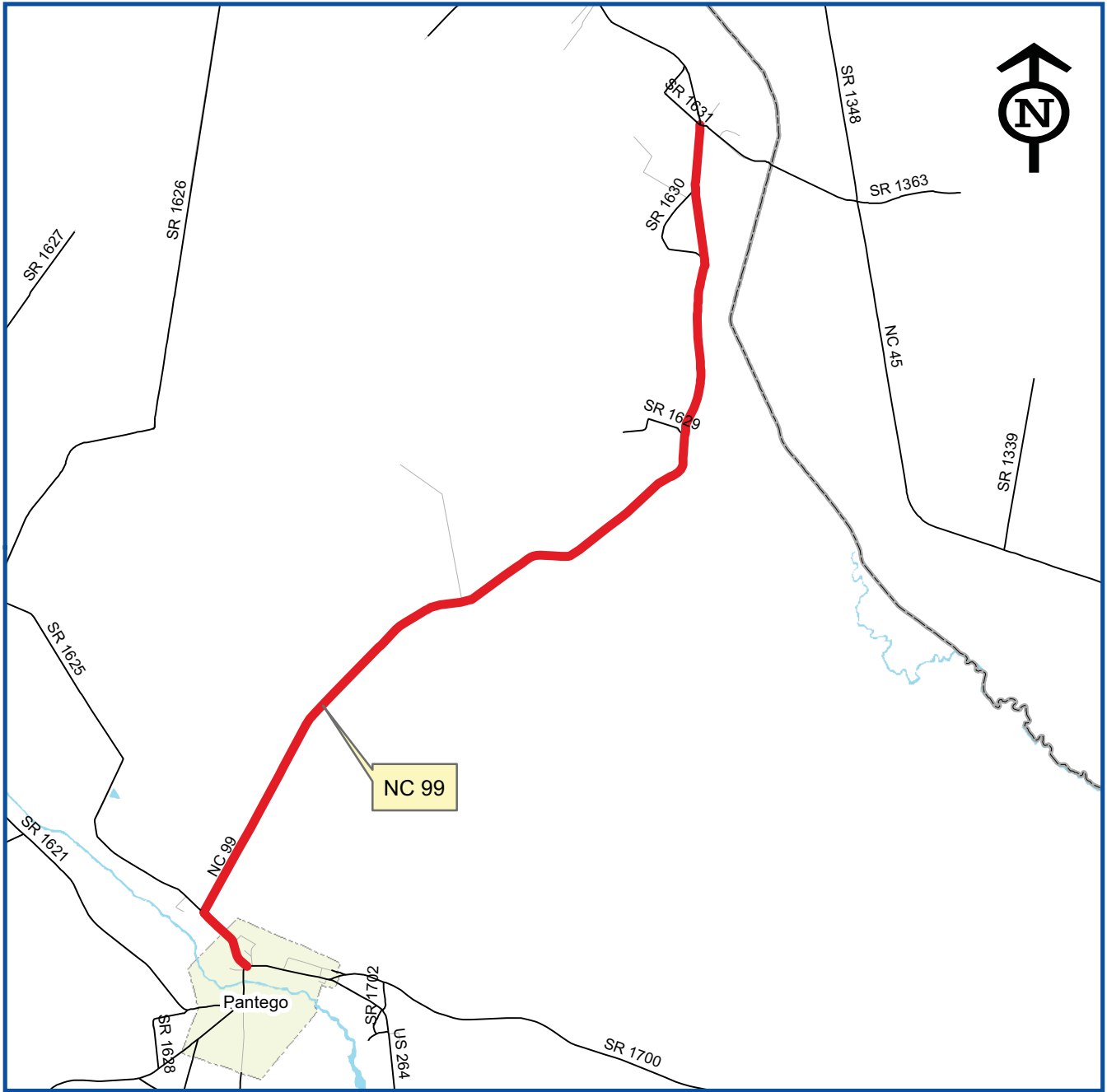
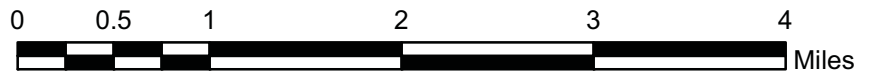
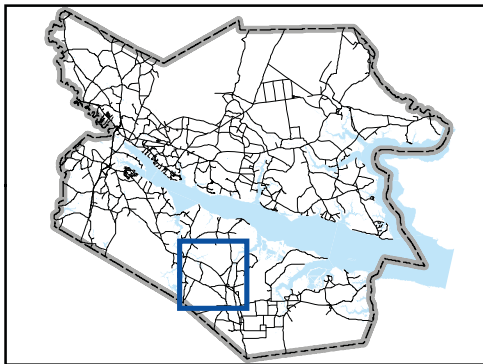
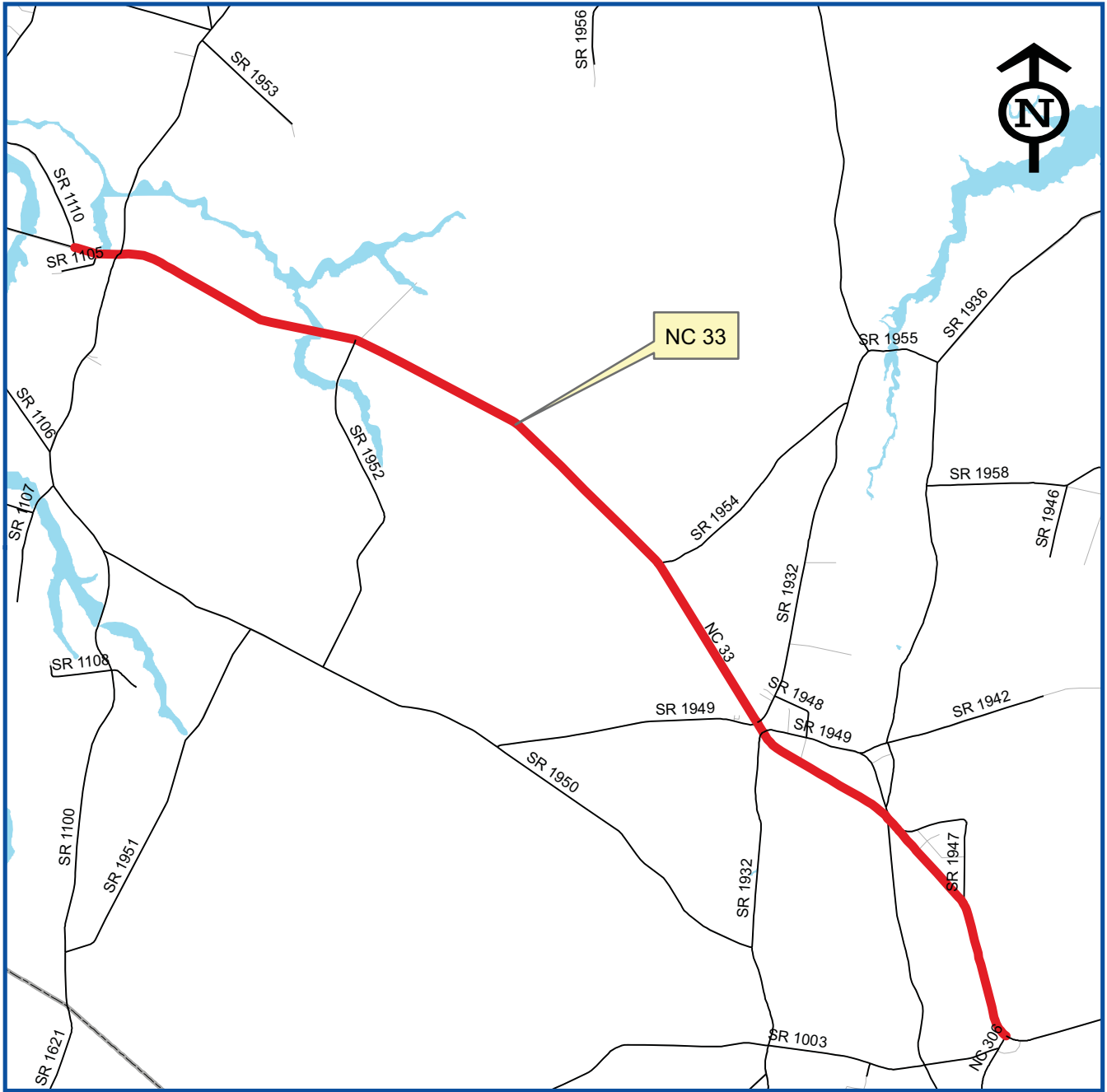


2CR.10071.9



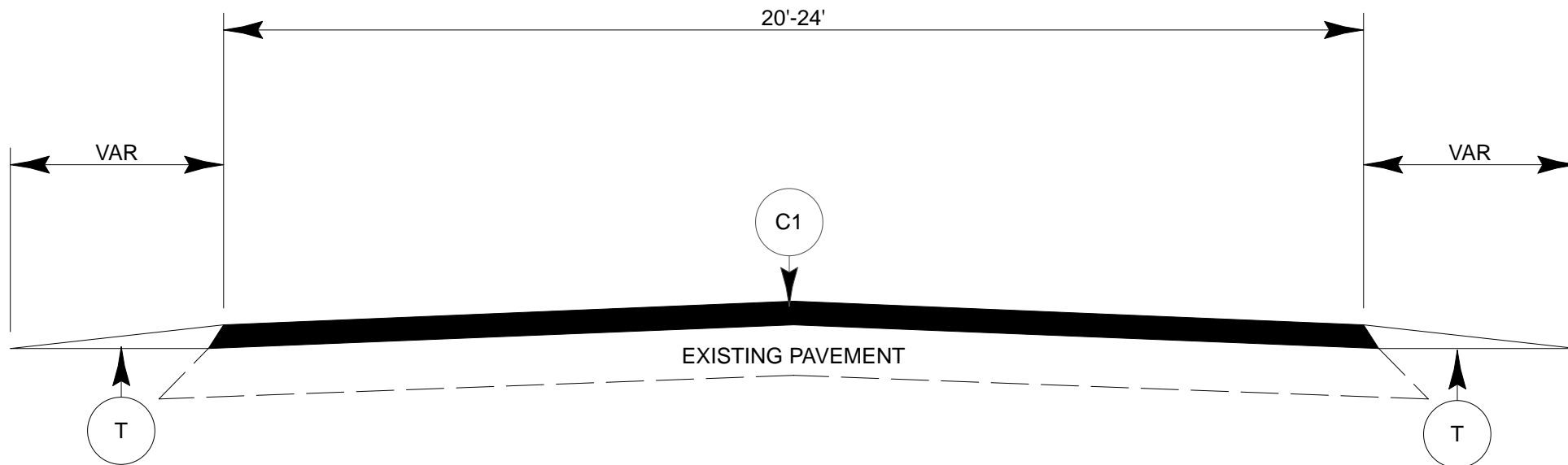
Beaufort County
Resurfacing
NC 99 from US 264 to NC 45

2CR.10071.11



Beaufort County
Resurfacing
NC 33 from SR 1110 to NC 306

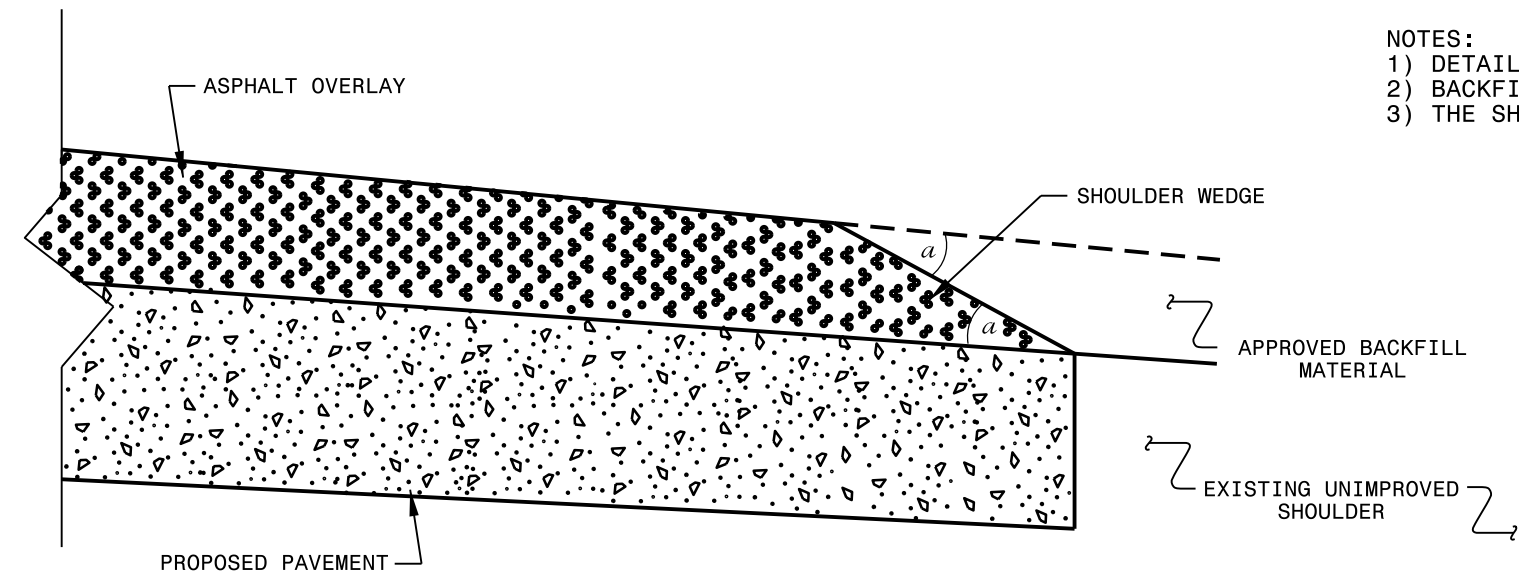
PROJECT NO.	SHEET NO.	TOTAL SHEETS
2CR.10071.9, 2CR.10071.11		



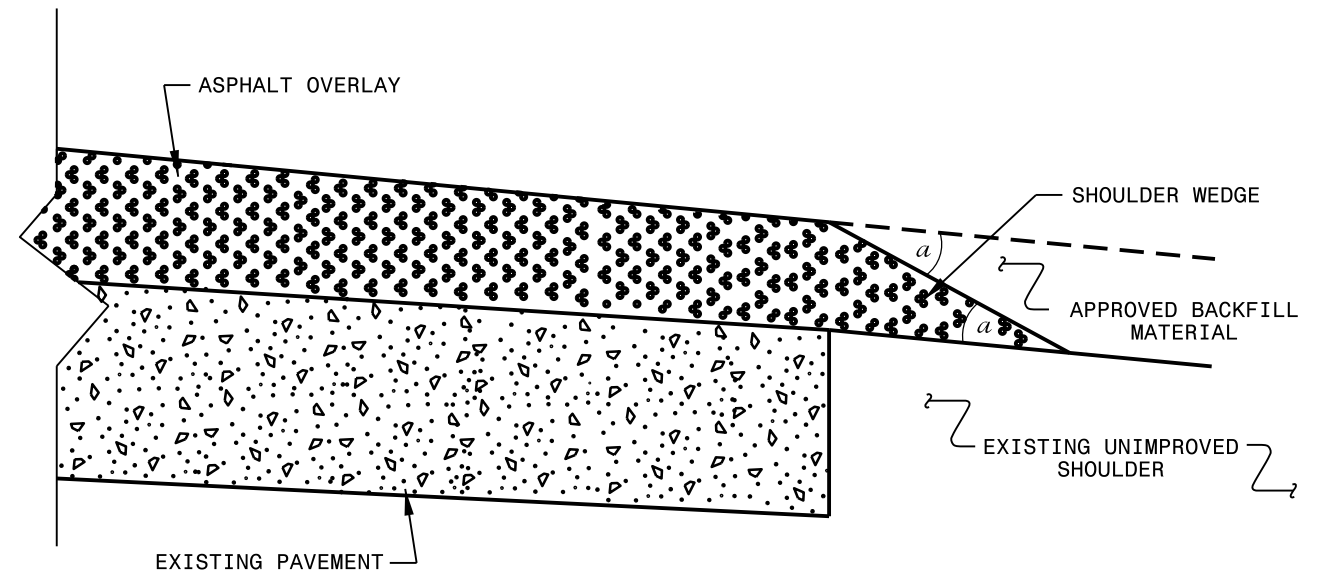
TYPICAL SECTION NO. 1

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 2.0" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION (AGGREGATE SHOULDER BORROW)

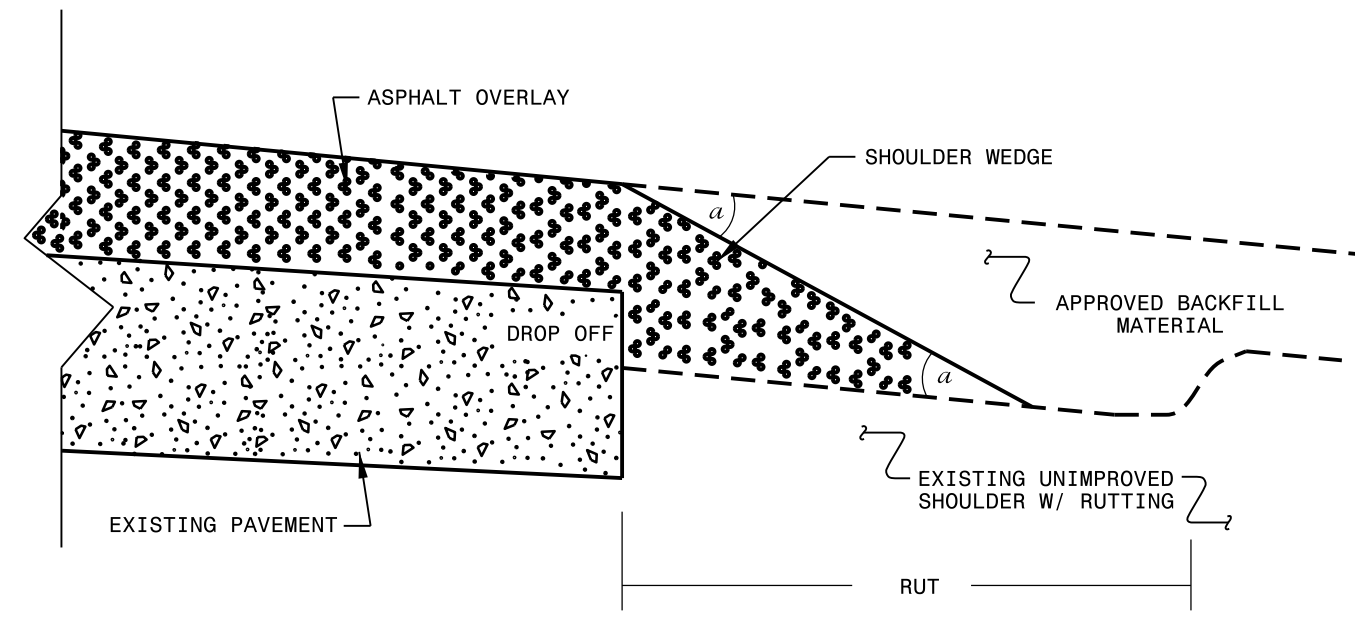
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

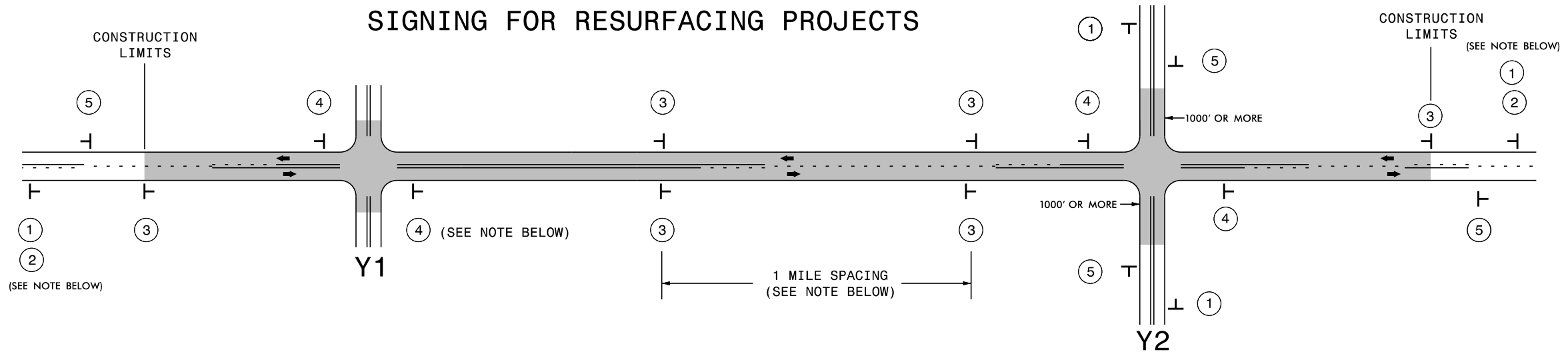
CONTRACT STANDARDS
 AND DEVELOPMENT UNIT
 Office 919-707-6950 FAX 919-250-4119

**SHOULDER WEDGE
 DETAILS**

ORIGINAL BY: T.SPELL DATE: 7-19-11
 MODIFIED BY: DATE: 10/16/12
 CHECKED BY: DATE:
 FILE SPEC.: susr/details/stand/shoulderwedgedetail.dgn

SYSTEMS DESIGN
 USER NAME

SIGNING FOR RESURFACING PROJECTS



LEGEND	
┃	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION		
<div style="display: flex; flex-direction: column; align-items: center;"> <div style="border: 1px solid black; padding: 2px; margin-bottom: 5px;">①</div> <div style="border: 1px solid black; padding: 2px; margin-bottom: 5px;">②</div> </div>	<div style="display: flex; align-items: center; margin-bottom: 10px;"> <div> <p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> </div> </div> <div style="display: flex; align-items: center; margin-bottom: 10px;"> <div> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p> </div> </div>	<p style="text-align: center;">NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p style="text-align: center; font-size: small;">WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center; margin: 10px 0;"> </div> <p style="text-align: center; font-size: x-small;">PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
<div style="border: 1px solid black; padding: 2px; margin-bottom: 5px;">③</div>	<div style="display: flex; align-items: center; margin-bottom: 10px;"> <div> <p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p> </div> </div>	
<div style="border: 1px solid black; padding: 2px; margin-bottom: 5px;">④</div>	<div style="display: flex; align-items: center; margin-bottom: 10px;"> <div> <p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> </div> </div>	
<div style="border: 1px solid black; padding: 2px; margin-bottom: 5px;">⑤</div>	<div style="display: flex; align-items: center; margin-bottom: 10px;"> <div> <p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p> </div> </div>	

8/8/2013 S:\TMU\WZTC\Resurfacing\2013Documents\New_Procedures_05_09_2013\Resurfacing_AdvWarn_2Ln.dgn User:rmgarratt

**RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS**

PROJECT NO.	SHEET NO.	TOTAL NO.
2CR.10071.9, 2CR.10071.11		

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	AGGREGATE SHOULDER BORROW TON	SHOULDER RECONSTRUCTION SMI	INCIDENTAL MILLING SY	ASPHALT CONC SURFACE COURSE, TYPE S9.5B TON	ASPHALT BINDER FOR PLANT MIX TON
2CR.10071.9	Beaufort	1	NC 99	FROM US 264 TO NC 45	1	NO	NO	7	20	1,400	14.00	192	9,461	568
TOTAL FOR MAP NO. 1								7		1,400	14.00	192	9,461	568
TOTAL FOR PROJ NO. 2CR.10071.9								7		1,400	14.00	192	9,461	568
2CR.10071.11	Beaufort	2	NC 33	FROM NC 306 TO SR 1110 (HERRING RUN RD)	1	NO	NO	7.9	24	1,580	15.80		12,795	768
TOTAL FOR MAP NO. 2								7.9		1,580	15.80		12,795	768
TOTAL FOR PROJ NO. 2CR.10071.11								7.9		1,580	15.80		12,795	768
GRAND TOTAL								14.9		2,980	29.80	192	22,256	1,336