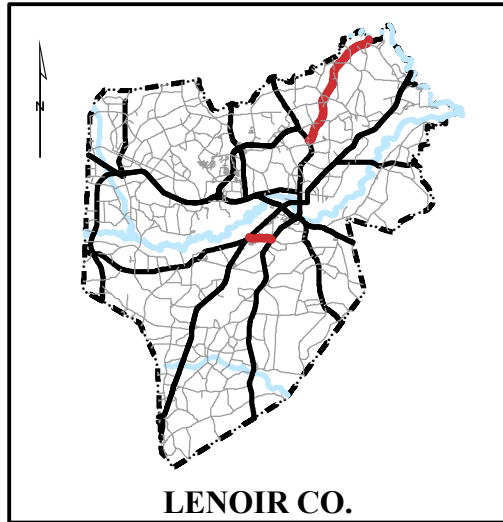


PROJECT REFERENCE NO.	SHEET NO.
2017CPT.02.07.20541.1	1

LENOIR COUNTY

WBS: 2017CPT.02.07.20541.1
DB00308



LENOIR CO.

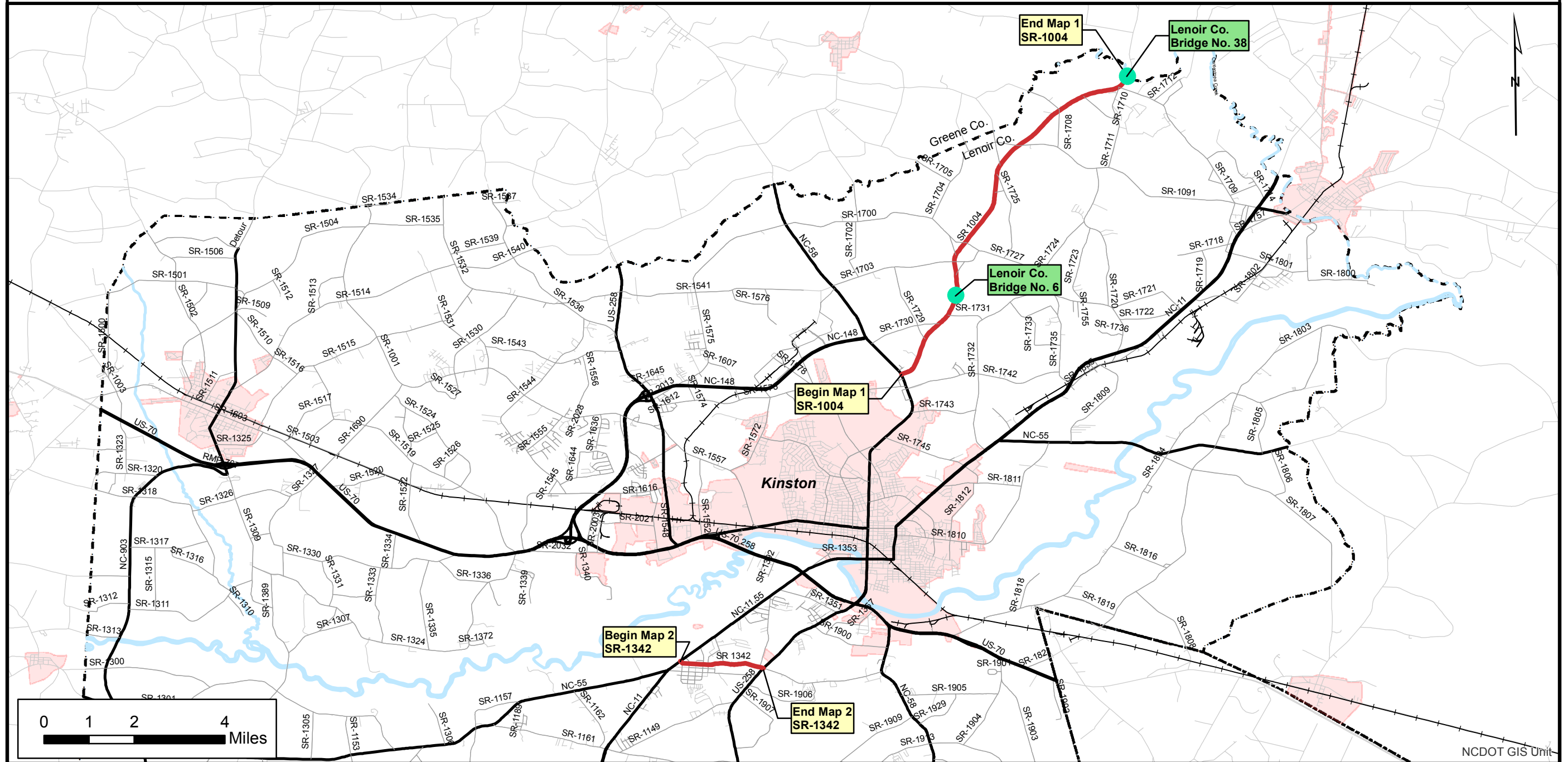
LOCATION:

MAP 1 - SR-1004 - FROM NC-58 TO THE GREENE CO. LINE.
MAP 2 - SR-1342 - FROM NC-11 TO US-258

TYPE OF WORK: RESURFACING & SHOULDER RECONSTRUCTION.

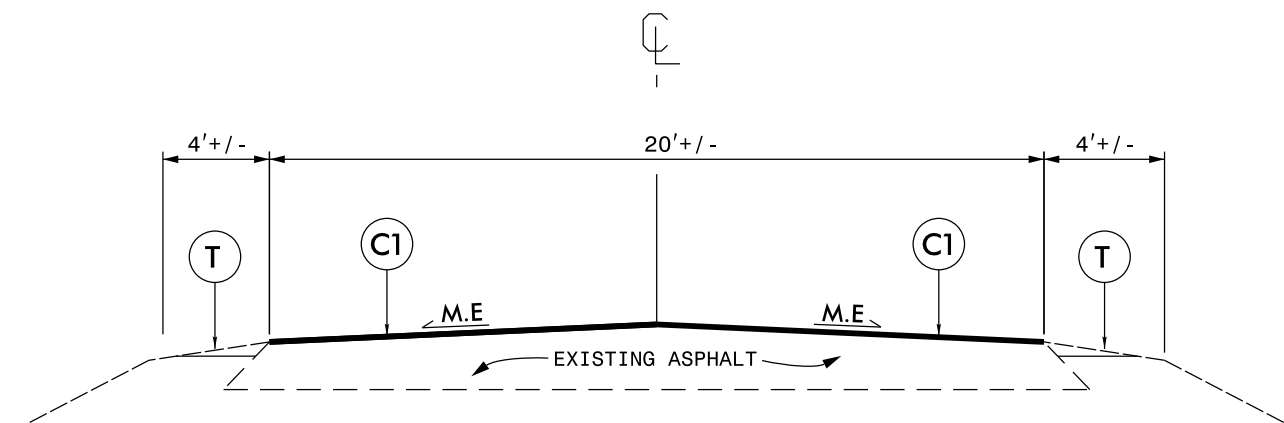


NCDOT
DIVISION 2



TYPICAL SECTION NO. 1

MAP 1 - SR-1004 FROM NC58 (0+00) TO THE GREENE CO. LINE(432+82).

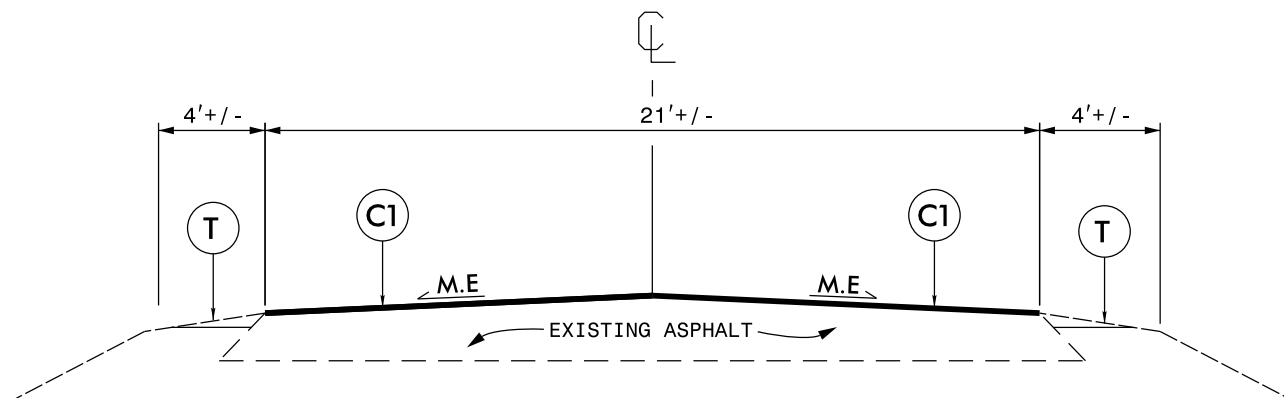


NOTE:

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER, SEE DETAIL 1.
3. INCLUDES INCIDENTAL MILLING AT BRIDGE APPROACH TIE-INS AS DIRECTED BY THE ENGINEER. SEE DETAIL 2.

TYPICAL SECTION NO. 2

MAP 2 - SR-1342 FROM NC-11(0+00) TO US-258(78+40).



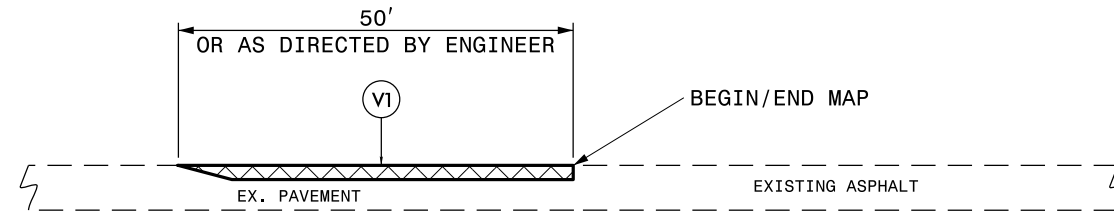
NOTE:

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5B, AT AN AVERAGE RATE OF 196.0 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	INCIDENTAL MILLING.
DRAWINGS NOT TO SCALE	

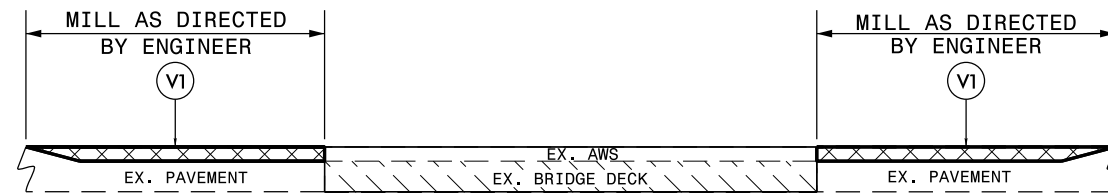
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:

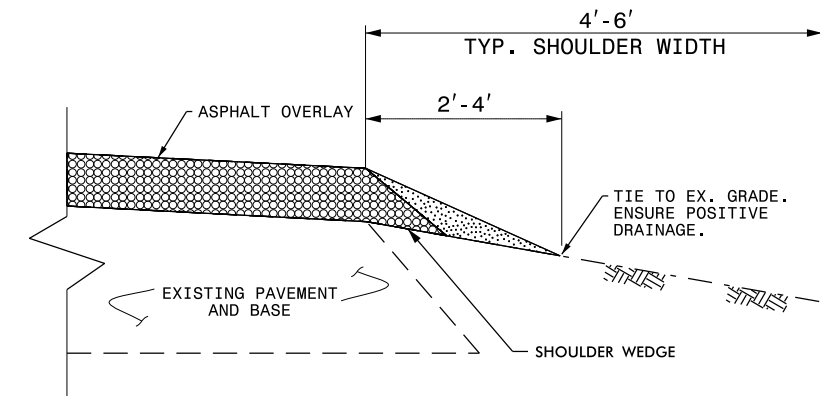
1. MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

NOTE:

1. MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

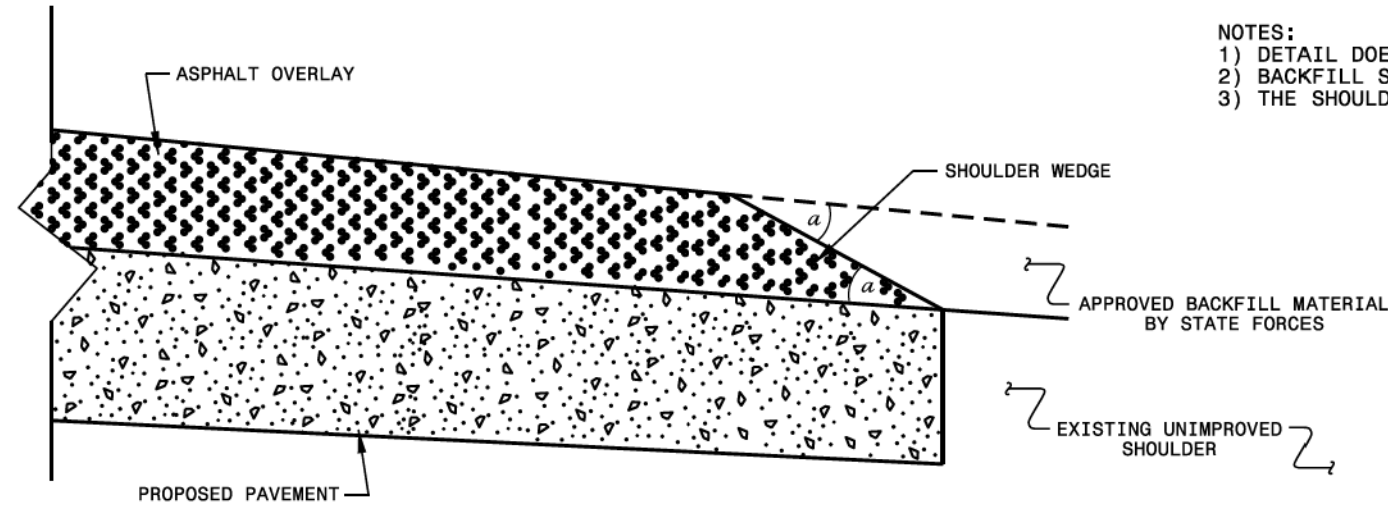


SHOULDER RECONSTRUCTION DETAIL

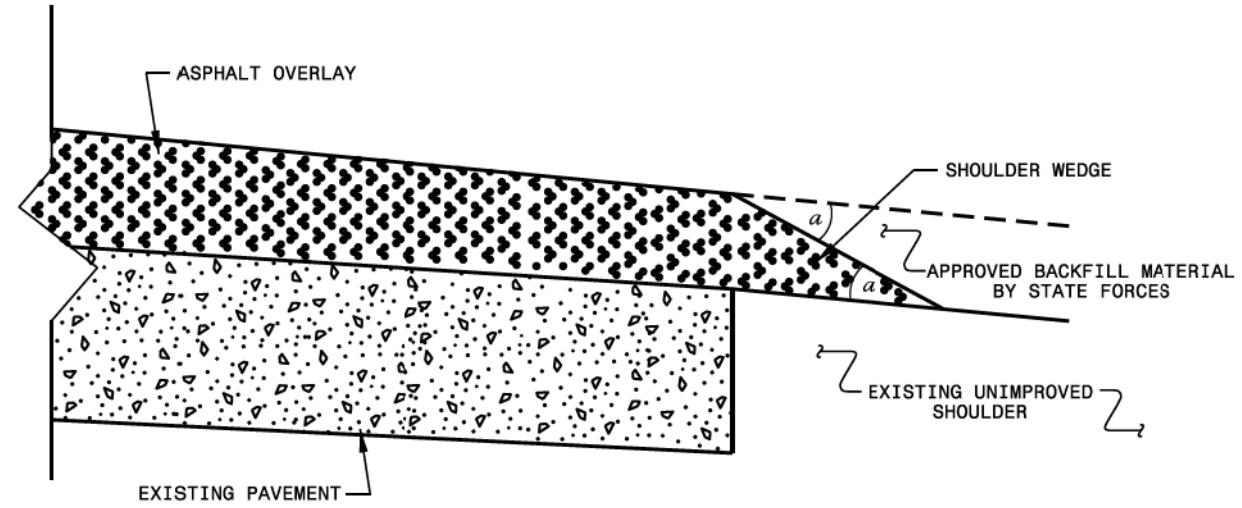
NOTE:

1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

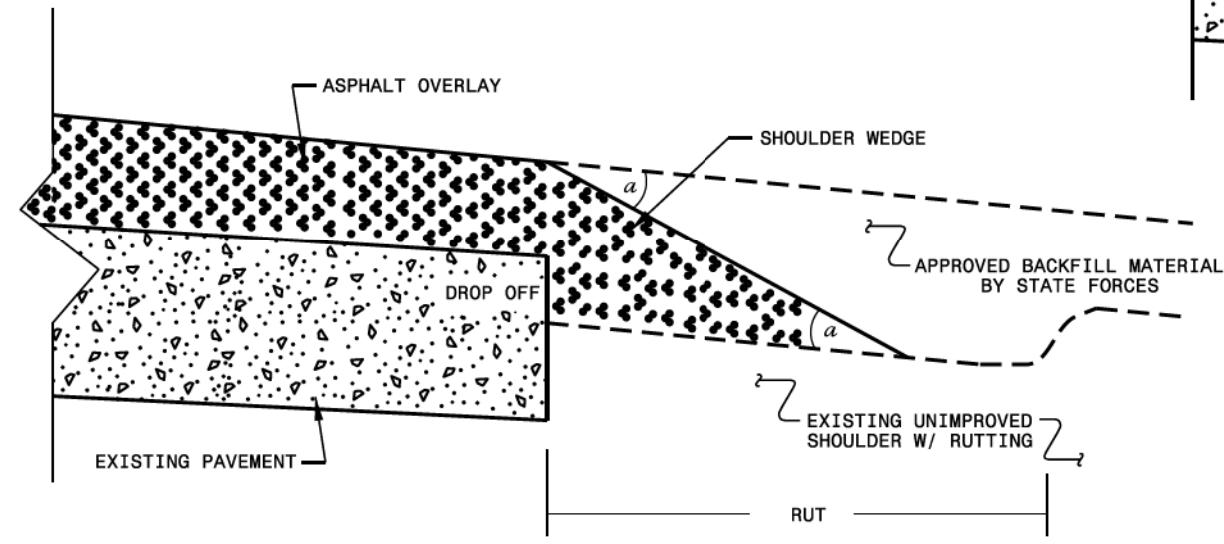
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY:	T.SPELL	DATE:	7-18-11
MODIFIED BY:		DATE:	10/16/12
CHECKED BY:		DATE:	
FILE SPEC:	s:\usr\details\stand\shoulder-wedge\detail.dwg		

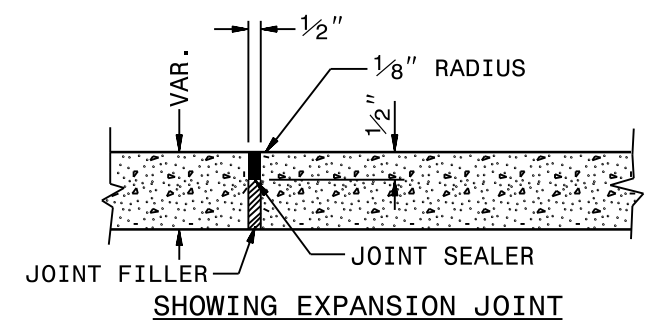
24-MAR-2016 14:16
 SA LOUAFACIA\Contract\Resurfacing Projects\Shoulder Wedge Details\Revised Shoulder Wedge Detail.dgn
 USER:RHE

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

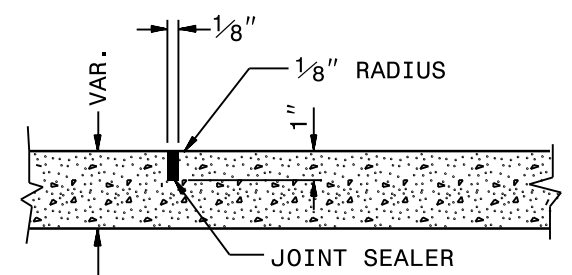
1-12

ENGLISH STANDARD DRAWING FOR
CONCRETE ISLANDS

SHEET 1 OF 1
852.01



SHOWING EXPANSION JOINT



SHOWING GROOVED JOINT

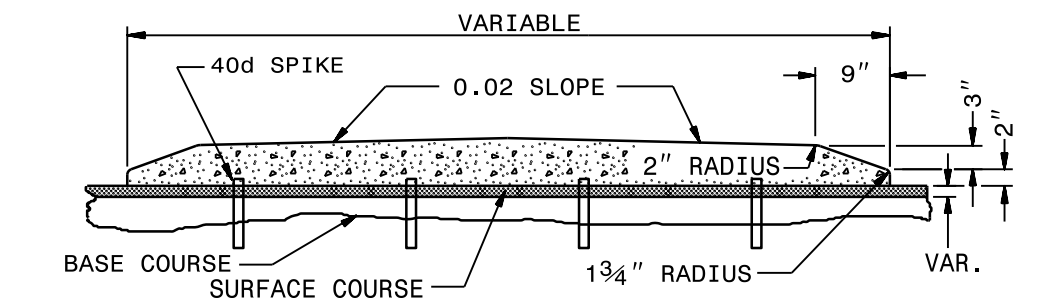
PARTIAL LONGITUDINAL SECTIONS OF PAVED ISLANDS

NOTE:
WHEN MONOLITHIC CONCRETE ISLAND IS ON TOP OF SURFACE COURSE, DRIVE 40d SPIKES INTO SURFACE UNDER MONOLITHIC CONCRETE ISLAND. STAGGER SPIKES ON 2' CENTERS EACH WAY.
IN THE CONCRETE PAVEMENT (ISLAND) AND CONCRETE ISLAND (MONOLITHIC) PLACE 1/2" EXPANSION JOINTS AT 30' INTERVALS AND GROOVED JOINTS 1" DEEP AT 10' INTERVALS BETWEEN EXPANSION JOINTS.
LINE UP THE JOINTS IN THE CONCRETE PAVEMENT (ISLAND) WITH THE JOINTS IN THE CURB OR CURB AND GUTTER.
FILL AND SEAL THE TOP 1/2" OF THE EXPANSION JOINTS AND THE ENTIRE DEPTH OF GROOVED JOINTS WITH JOINT SEALER.
FOR JOINTS IN THE CURB AND/OR CURB AND GUTTER, SEE STANDARD NO. 846.01

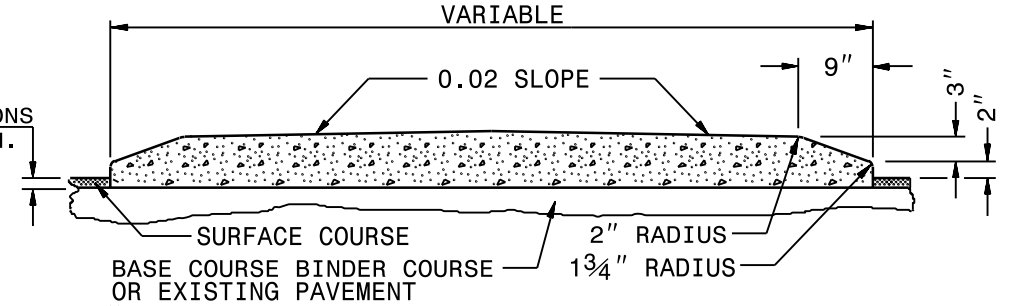
SEE TYPICAL SECTIONS FOR PAVEMENT DEPTH. KEY IN ON THE LAST LAYER OF PAVEMENT SURFACE COURSE

CURB AND GUTTER AS CALLED FOR ON PLANS.

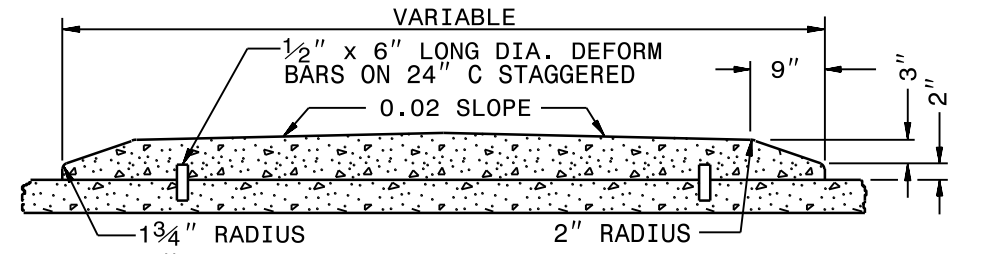
SURFACING



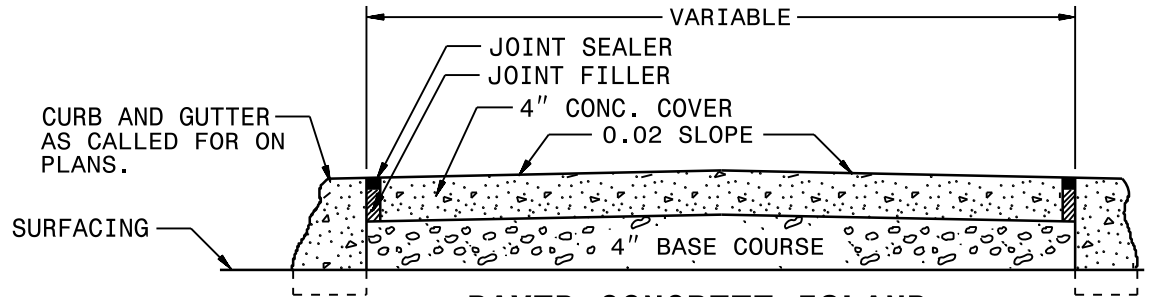
5" MONOLITHIC CONCRETE ISLAND (SURFACE MOUNTED) ON ASPHALT CONCRETE PAVEMENT
(USE ON ISLAND 4' WIDE OR GREATER)



5" MONOLITHIC CONCRETE ISLAND (KEYED IN) ON ASPHALT CONCRETE PAVEMENT
(USE ON ISLAND LESS THAN 4' WIDE)



5" MONOLITHIC CONCRETE ISLAND (SURFACE MOUNTED) ON CONCRETE PAVEMENT



PAVED CONCRETE ISLAND

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

1-12

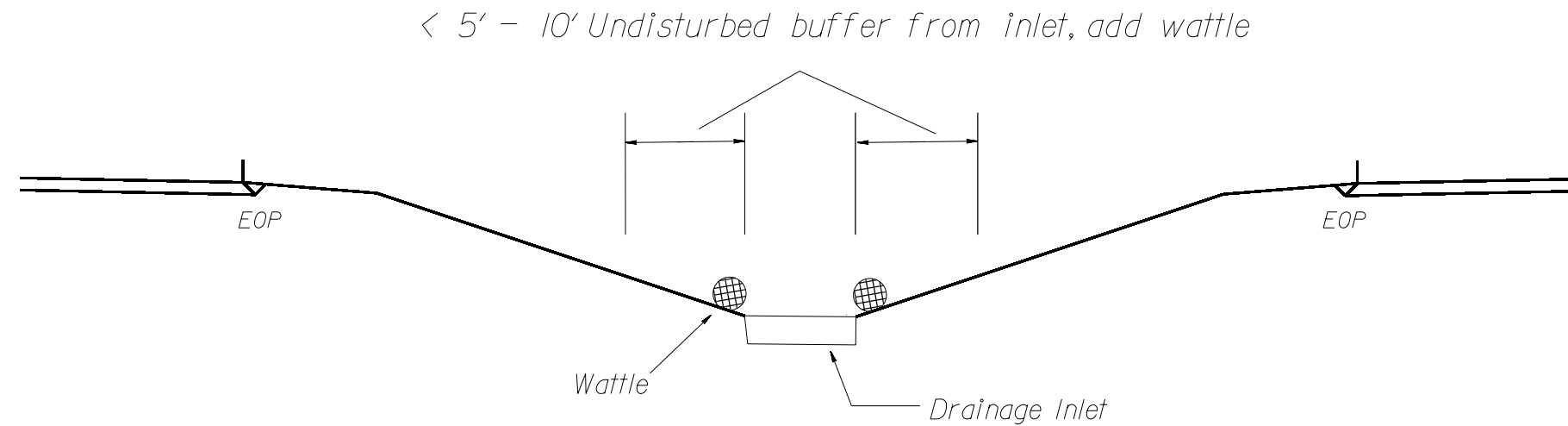
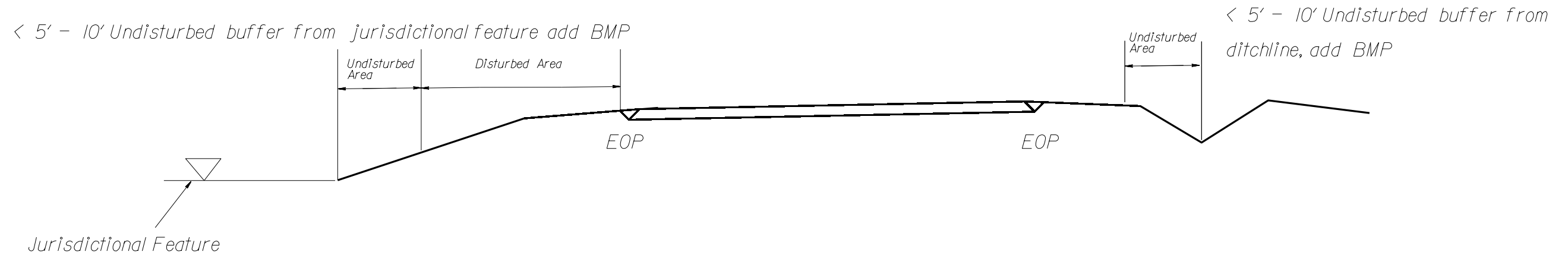
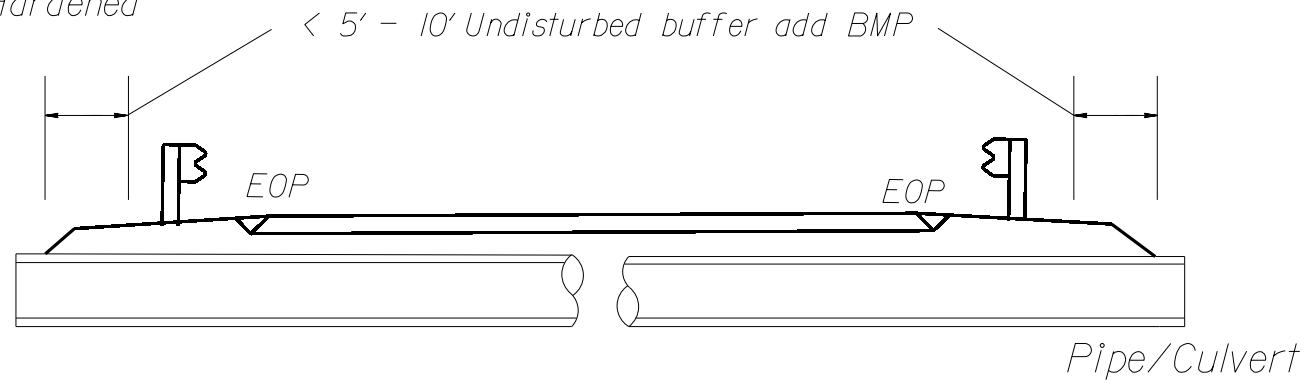
ENGLISH STANDARD DRAWING FOR
CONCRETE ISLANDS

SHEET 1 OF 1
852.01

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

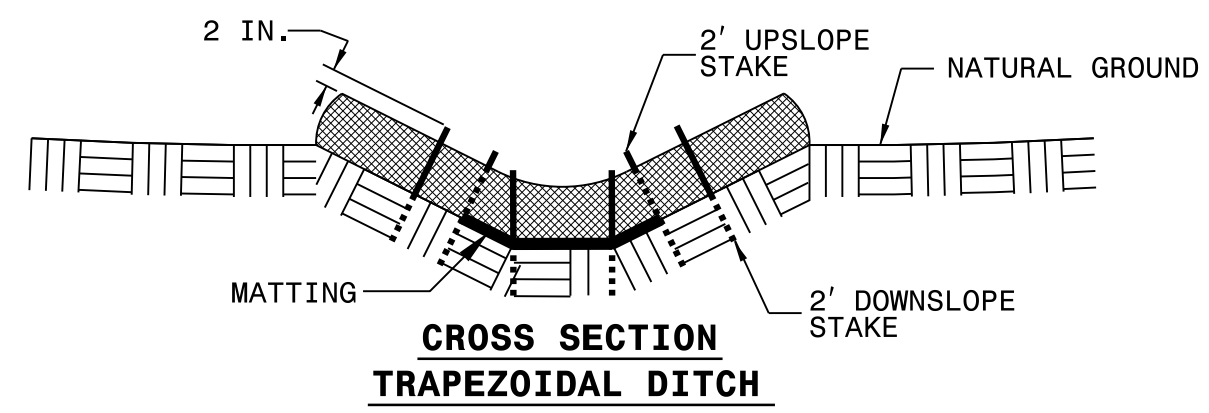
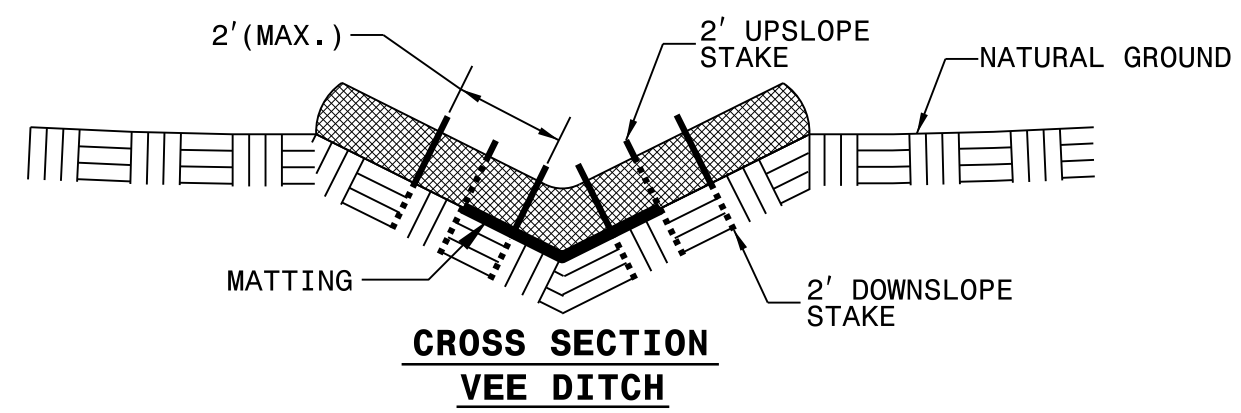
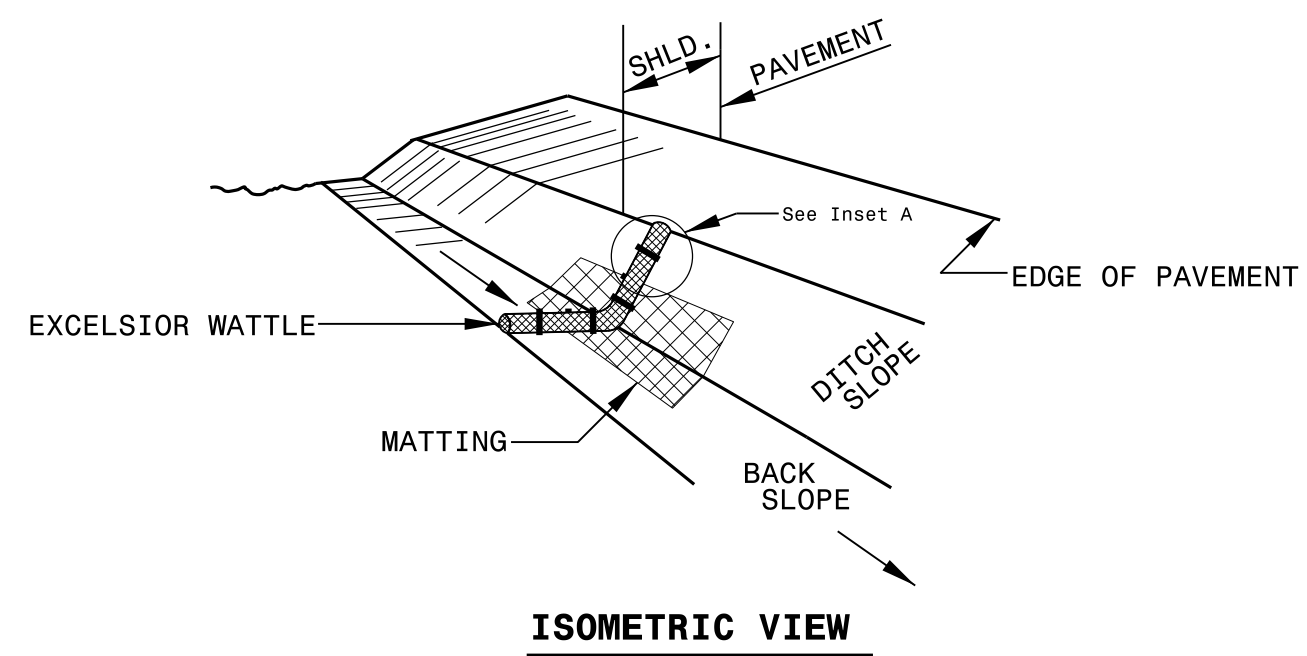
BMP Options: Wattle, Silt Fence or Hardened Aggregate.

EROSION CONTROL DETAIL



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

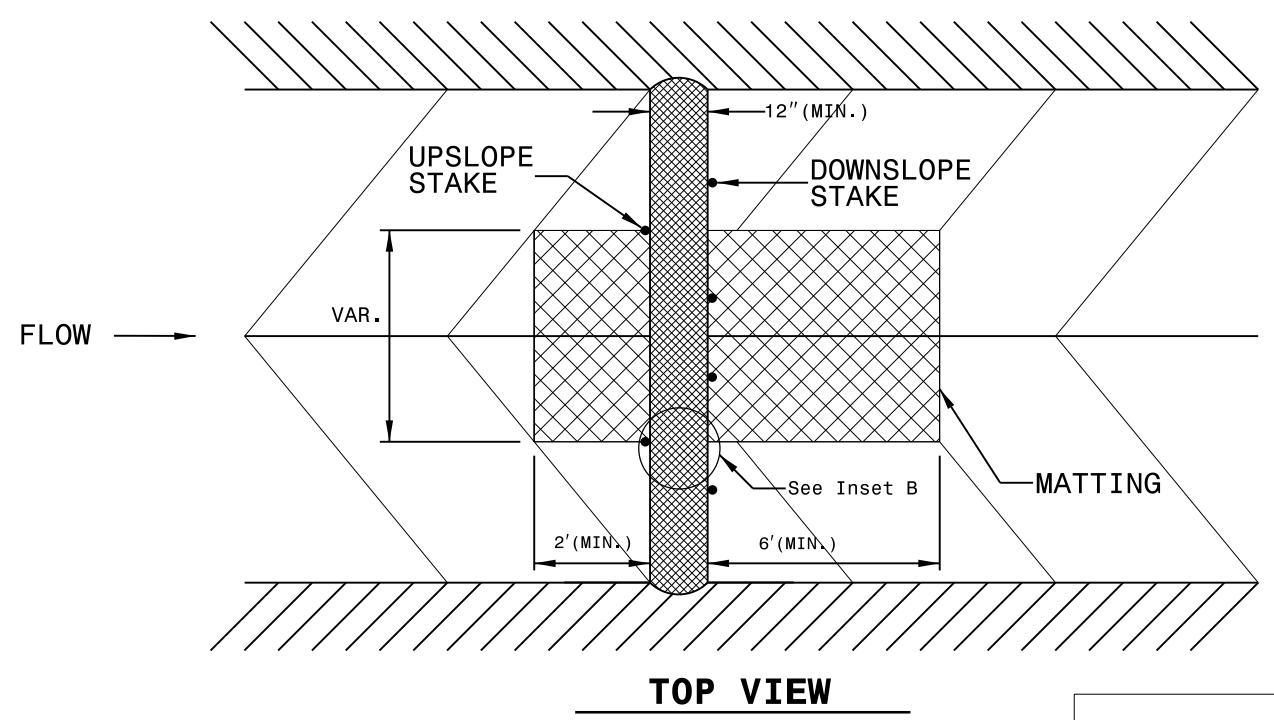
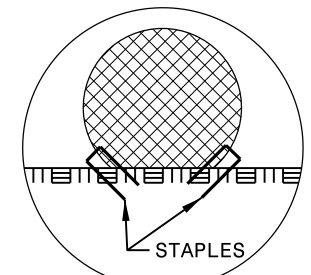
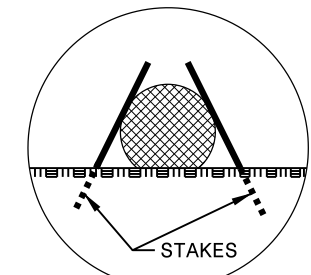
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE

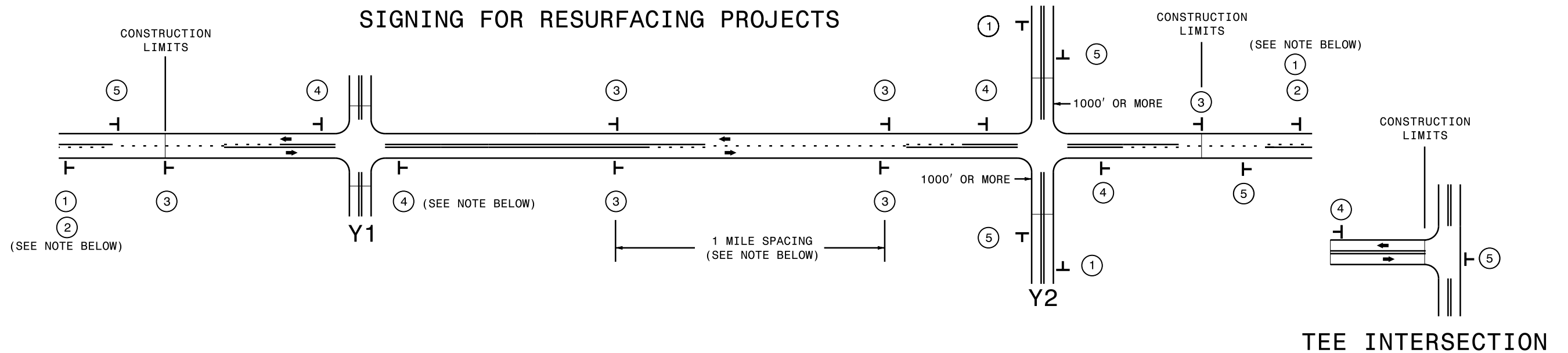
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	REMOVAL OF EXISTING ASPHALT ISLAND SY	HAULING NCDOT SUPPLIED SHOULDER LOAD	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	INCIDENTAL MILLING SY	SURFACE COURSE, S9.5B TONS	ASPHALT BINDER FOR PLANT MIX TONS	5" MONOLITHIC CONCRETE ISLANDS (KEYED IN) SY	TEMPORARY SILT FENCE LF	WATTLE LF	SEED & MULCHING AC	RESPONSE FOR EROSION CONTROL EA
2017.CPT.02.07.20541.1	Lenoir	1	SR-1004	FROM NC-58 TO THE GREENE CO. LINE	1	2	2WU	NO	NO	8.20	20	8.00	230	160	17	2,750	10,950	657	8.00	850	130.0	8.00	4
TOTAL FOR MAP NO. 1										8.20		8.00	230	160	17	2,750	10,950	657	8.00	850	130.0	8.00	4
2017.CPT.02.07.20541.1	Lenoir	2	SR-1342	FROM NC-11 TO US-258	2	2	2WU	NO	NO	1.49	21		45	50	3	850	2,104	126		150	30.0	1.50	2
TOTAL FOR MAP NO. 2										1.49			45	50	3	850	2,104	126		150	30.0	1.50	2
TOTAL FOR PROJ NO. 2017.CPT.02.07.20541.1										9.68		8.00	275	210	20	3,600	13,054	783	8.00	1,000	160.0	9.50	6
GRAND TOTAL										9.68		8.00	275	210	20	3,600	13,054	783	8.00	1,000	160.00	9.50	6

WORK ZONE TRAFFIC CONTROL

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	441300000-E	445700000-N
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL
										SF	LS
2017.CPT.02.07.20541.1	Lenoir	1	SR-1004	FROM NC-58 TO THE GREENE CO. LINE	1	2	2WU	8.20	20	918	0.85
TOTAL FOR MAP NO. 1							8.20			918	0.85
2017.CPT.02.07.20541.1	Lenoir	2	SR-1342	FROM NC-11 TO US-258	2	2	2WU	1.49	21	168	0.15
TOTAL FOR MAP NO. 2							1.49			168	0.15
TOTAL FOR PROJ NO. 2017.CPT.02.07.20541.1							9.68			1,086	1.00
GRAND TOTAL							9.68			1,086	1.00

SIGNING FOR RESURFACING PROJECTS



LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	①		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	②		#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	③		<ul style="list-style-type: none"> - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. 	
	④		<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE. 	
⑤		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.		



RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS