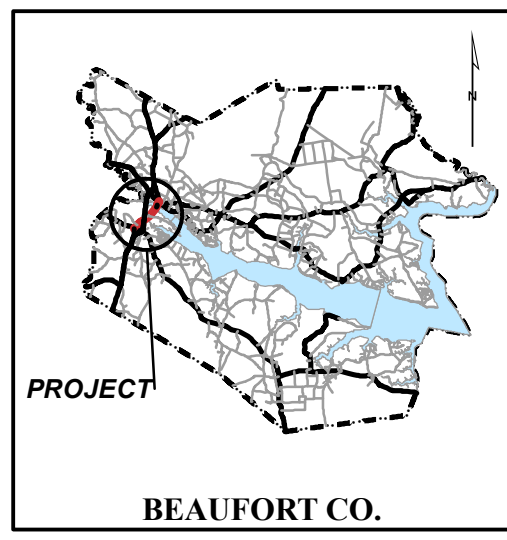


**BEAUFORT COUNTY**  
**RESURFACING CONTRACT:**  
**DB00285**  
**WBS: 2016CPT.02.45.10071.2**

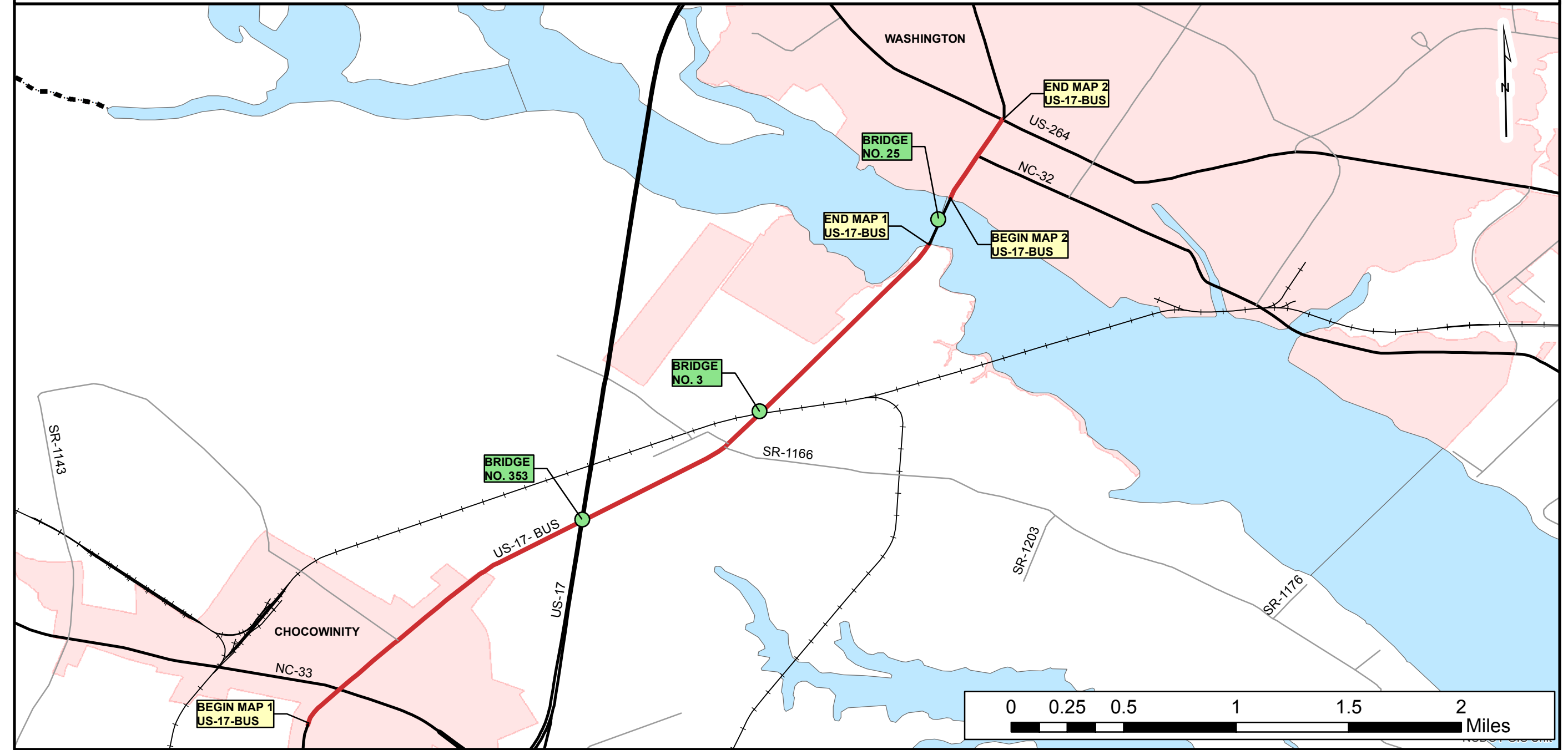


**NCDOT**  
 DIVISION 2



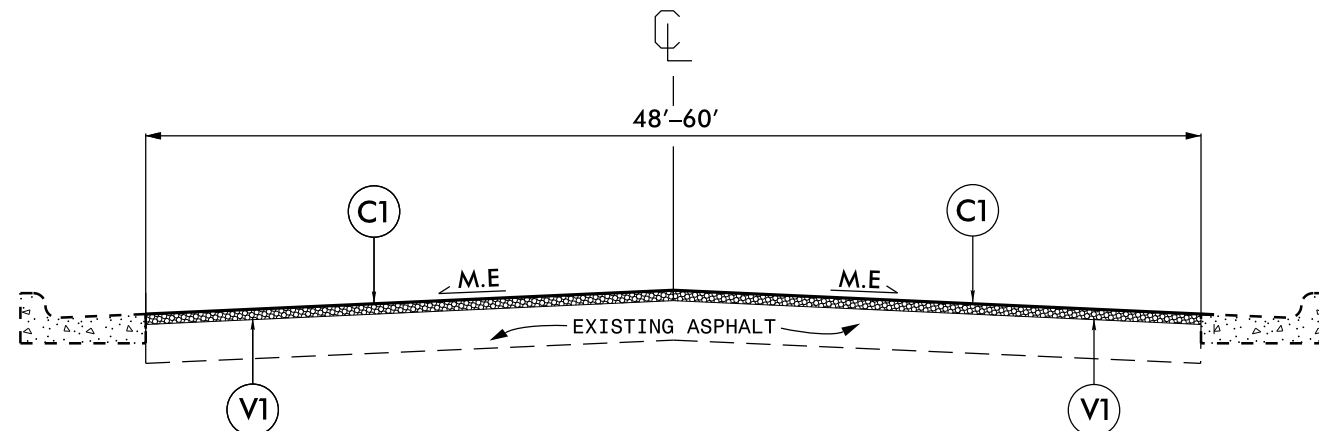
**LOCATION:**  
**MAP 1 - US-17-BUS FROM BEGIN C&G S. OF CHOCWINITY TO BEGIN BRIDGE NO. 25.**  
**MAP 2 - US-17-BUS FROM END BRIDGE NO. 25 TO US-264/5TH ST. IN WASHINGTON.**

**TYPE OF WORK: MILLING, RESURFACING & PAVEMENT MARKINGS.**



## TYPICAL SECTION NO. 1

MAP 1 - US-17 BUS 0+00 TO 43+45+/-.  
 MAP 1 - US-17 BUS 104+35+/- TO 112+80+/- (BEGIN BRIDGE NO. 3).  
 MAP 1 - US-17 BUS 114+50+/- (END BRIDGE NO. 3) TO 124+70+/-.



**NOTE:**

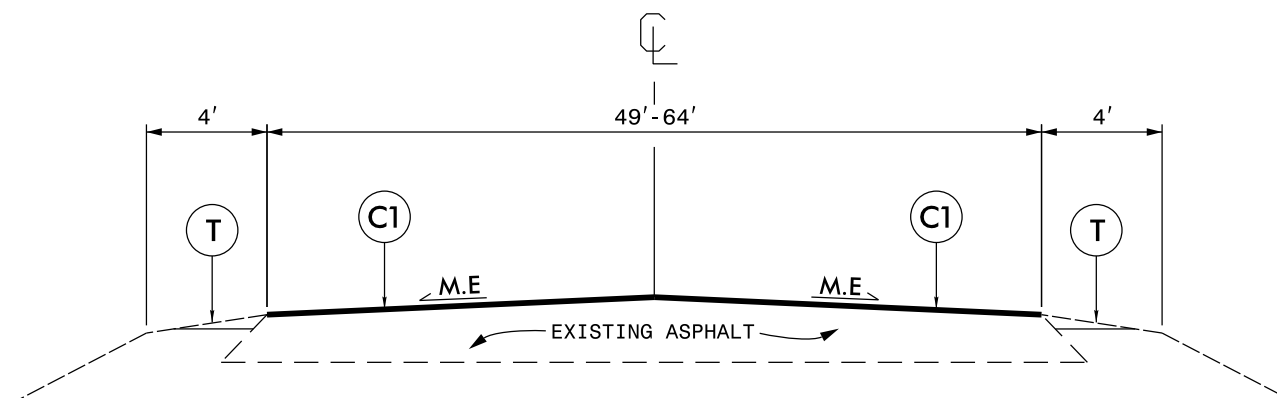
1. INCLUDED MILLING 1 3/4" FOR THE ENTIRE WIDTH OF THE ROADWAY, AS DIRECTED BY THE ENGINEER.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE, Y-LINE AND BRIDGE APPROACHES OR AS DIRECTED BY THE ENGINEER. (SEE DETAIL 1 & 2, SHEET 5).
4. INCLUDES INSTALLATION OF CURB RAMPS W/ LANDING IN VARIOUS LOCATIONS, AS DIRECTED BY THE ENGINEER. SEE STD. DWG 848.05
5. INCLUDES RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING CURB RAMPS, AT VARIOUS LOCATIONS, AS DIRECTED BY THE ENGINEER

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 196 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	MILLING DEPTH 1 3/4", FOR THE ENTIRE WIDTH OF THE ROADWAY.
V4	INCIDENTAL MILLING, (SEE DETAIL 1, 2 & 3, SHEET 5).
DRAWINGS NOT TO SCALE	

*NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.*

## TYPICAL SECTION NO. 2

MAP 1 - US 17 BUS 43+45+/- TO 104+35+/-.  
 MAP 1 - US 17 BUS 124+70+/- TO 158+69+/-.

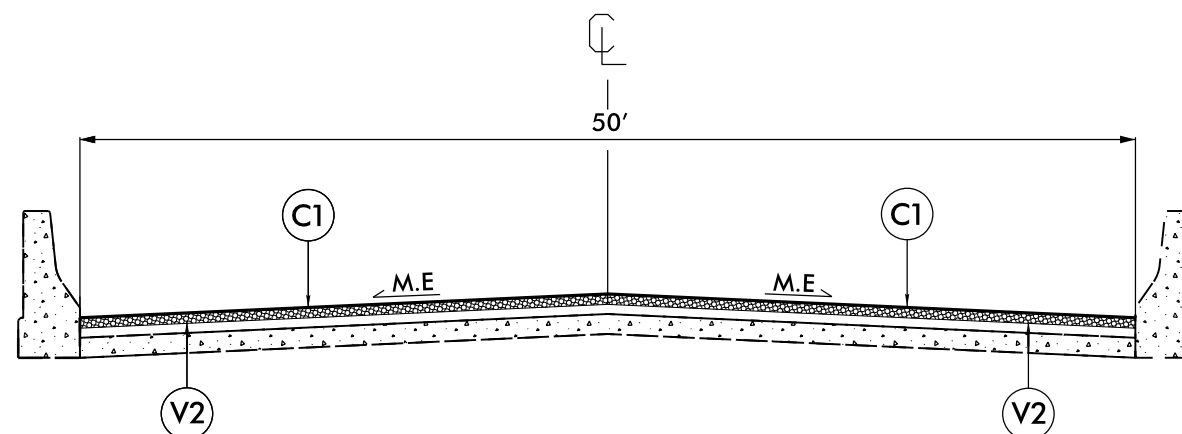


**NOTE:**

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER. (SEE DETAIL 1, SHEET 5).

### TYPICAL SECTION NO. 3

MAP 1 - US-17 BUS 112+80+/- TO 114+50+/- (BRIDGE NO. 3).

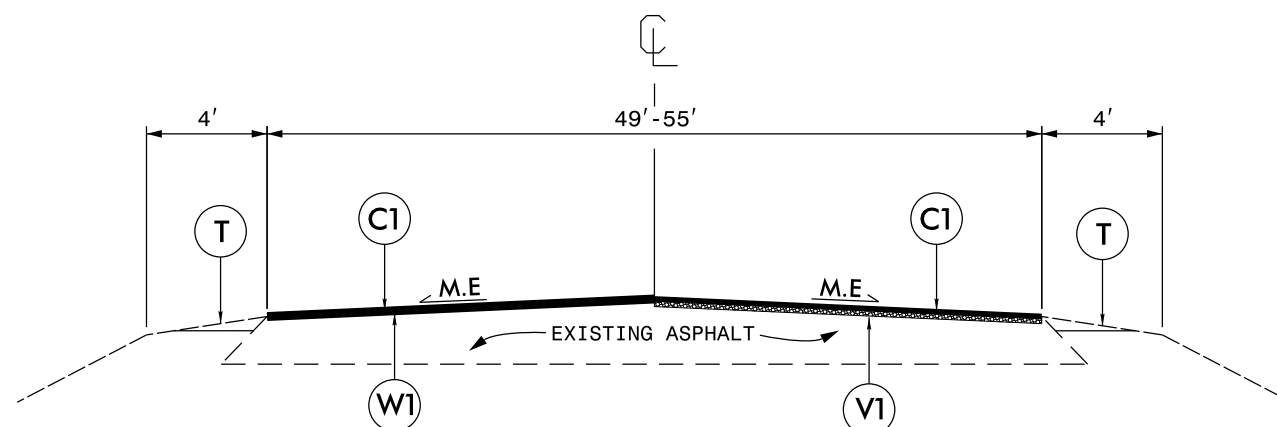


**NOTE:**

1. MILL 2½" FOR THE ENTIRE WIDTH OF THE ROADWAY AT BRIDGE NO. 3, AS DIRECTED BY THE ENGINEER.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
3. INCLUDES INCIDENTAL MILLING AT THE BRIDGE APPROACHES OR AS DIRECTED BY THE ENGINEER. (SEE DETAIL 3, SHEET 5).

### TYPICAL SECTION NO. 4

MAP 1 - US 17 BUS 158+69+/- TO 164+65+/- (BEGIN BRIDGE NO. 25).



**NOTE:**

1. INCLUDES MILLING NBL OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE SECTIONS AND BRIDGE APPROACHES OR AS DIRECTED BY THE ENGINEER. (SEE DETAIL 1, 2 & 3, SHEET 5).
4. INCLUDES WEDGING COURSE ON SBL, AS DIRECTED BY THE ENGINEER.

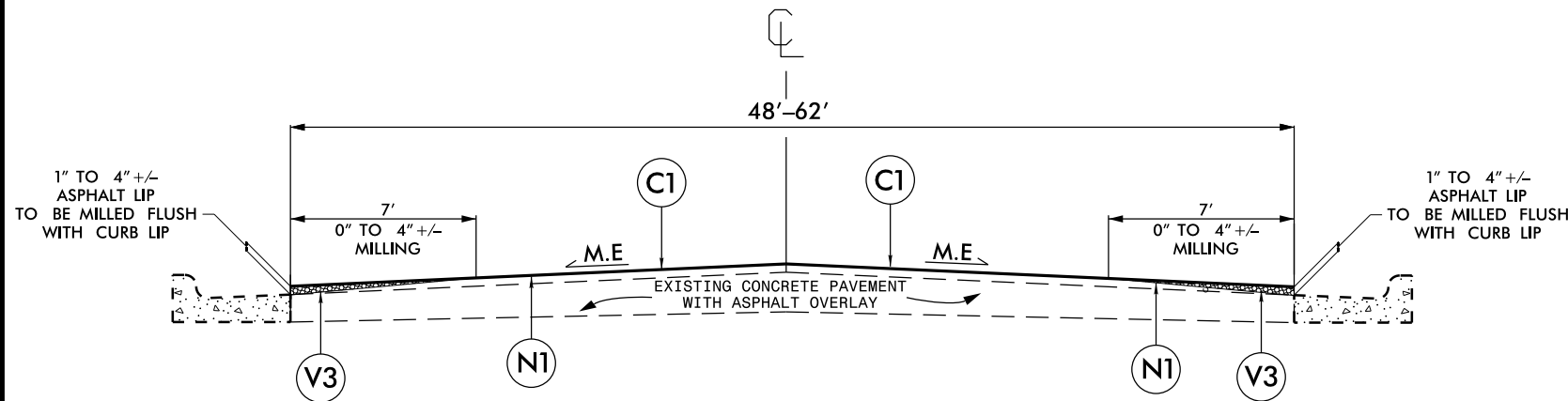
### PAVEMENT SCHEDULE

C1	PROP. APPROX. 1¾" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 196 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	MILLING DEPTH 1¾", FOR THE ENTIRE WIDTH OF THE ROADWAY.
V2	MILLING DEPTH 2½", FOR THE ENTIRE WIDTH OF THE ROADWAY.
V4	INCIDENTAL MILLING (SEE DETAIL 1, 2 & 3, SHEET 5).
W1	VARIABLE DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE TYPE I19.0B, AT AN AVERAGE RATE OF 114 LBS./SQ. YD./IN. (SEE WEDGING DETAIL, SHEET 5).
<b>DRAWINGS NOT TO SCALE</b>	

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

## TYPICAL SECTION NO. 5

MAP 2 - US-17 BUS 0+00 +/- (END BRIDGE NO. 25) TO 20+05 +/- (US-264/5TH ST).



**NOTE:**

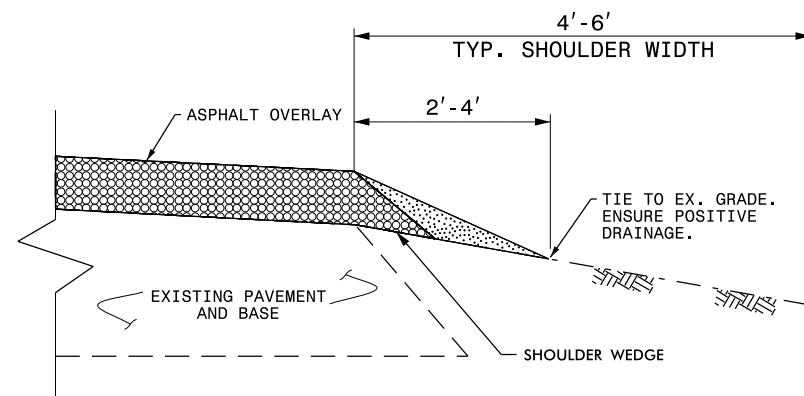
1. MILL 0" TO 4" +/- FOR 7' WIDE SECTION, TO OBTAIN A MINIMUM OF 1 3/4" MILLED DEPTH AT THE LIP OF CURB AND GUTTER, AS DIRECTED BY THE ENGINEER.
2. PLACE COMPOSITE PAVEMENT INTERLAYER MATTING FOR THE ENTIRE WIDTH OF THE ROADWAY.
3. PLACE ASPHALT SURFACE COURSE FOR THE ENTIRE WIDTH OF THE ROADWAY, AS DIRECTED BY THE ENGINEER.
4. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE, Y-LINE SECTIONS AND BRIDGE APPROACHES, AS DIRECTED BY THE ENGINEER. (SEE DETAIL 1 & 2, SHEET 5).
5. INCLUDES INSTALLATION OF CURB RAMPS W/ LANDING IN VARIOUS LOCATIONS, AS DIRECTED BY THE ENGINEER. SEE STD. DWG 848.05
6. INCLUDES RETROFITTING DETECTABLE WARNING DOMES ONTO EXISTING CURB RAMPS, AT VARIOUS LOCATIONS, AS DIRECTED BY THE ENGINEER. SEE STD. DWG 848.06.

### PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 196 LBS. PER SQ. YD.
N1	PROP. COMPOSITE PAVEMENT INTERLAYER. HIGH STRENGTH MATTING FOR THE ENTIRE WIDTH OF THE ROADWAY.
V3	MILLING DEPTH 0" TO 4" +/-, FOR A WIDTH AS SHOWN IN THE TYPICAL, FROM THE LIP OF THE CURB & GUTTER, OR AS DIRECTED BY THE ENGINEER.
V4	INCIDENTAL MILLING (SEE DETAIL 1, 2 & 3, SHEET 5).

**DRAWINGS NOT TO SCALE**

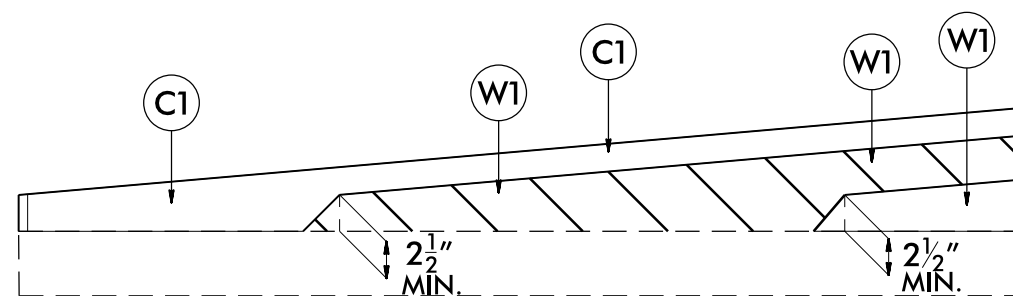
*NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.*



**SHOULDER RECONSTRUCTION DETAIL**

**NOTE:**

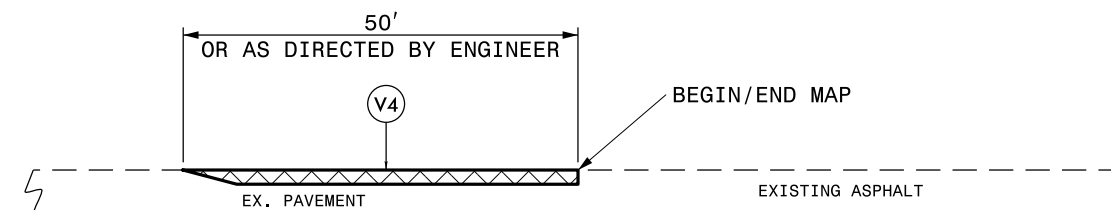
1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.



**Wedging Detail For Resurfacing**

**NOTE:**

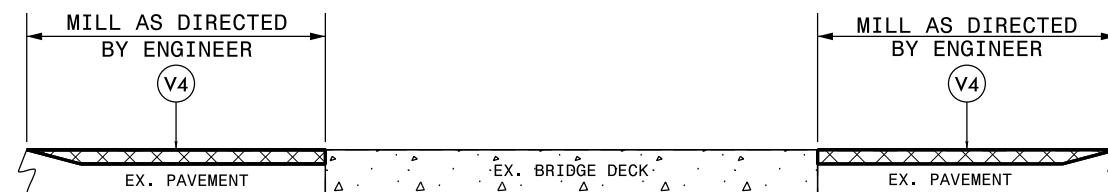
1. SEE TYPICAL SECTION NO. 4 FOR LOCATION OF WEDGING COURSE.



**DETAIL 1  
BEGIN/END MAP TIE-IN**

**NOTE:**

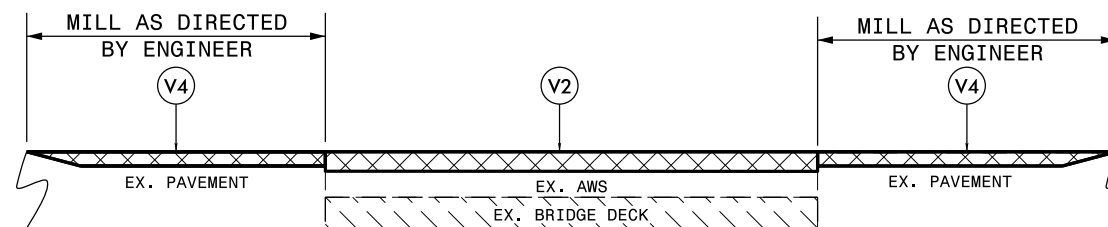
1. MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS, AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



**DETAIL 2  
BRIDGE MILLING**

**NOTE:**

1. MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHS FOR BEAUFORT CO. BRIDGE NUMBER 25, AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

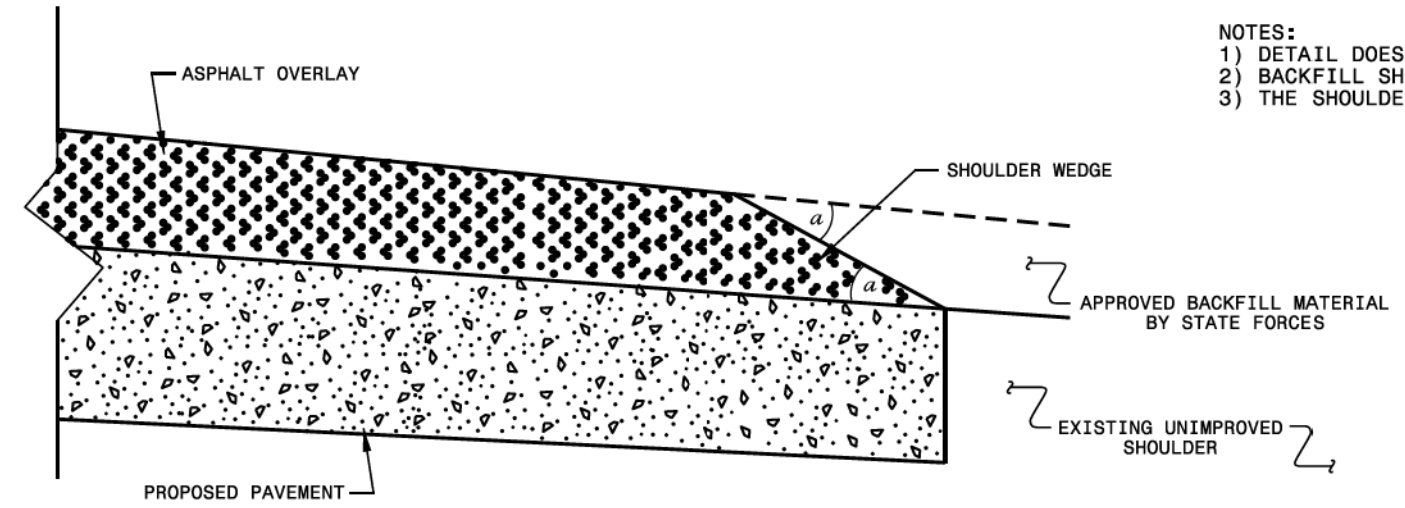


**DETAIL 3  
BRIDGE MILLING**

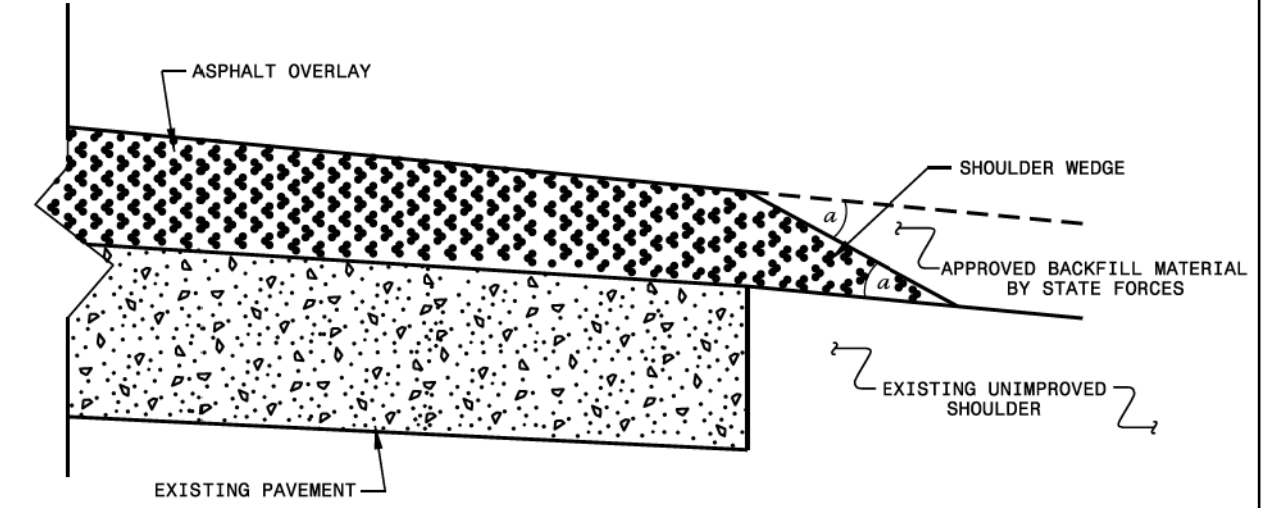
**NOTE:**

1. MILLING SHALL BE PERFORMED AT BEAUFORT CO. BRIDGE BRIDGE NUMBER 3, AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

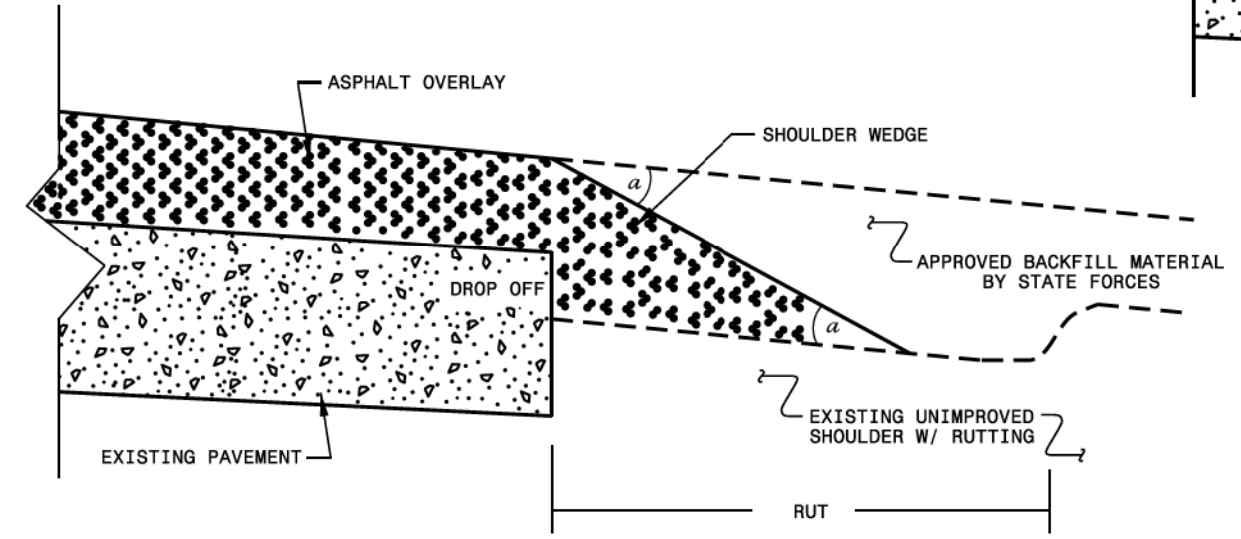
- NOTES:  
 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.  
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.  
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

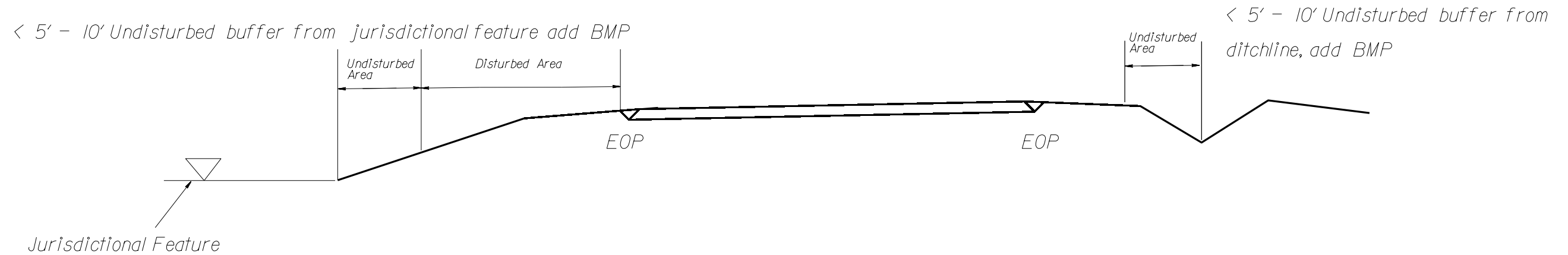
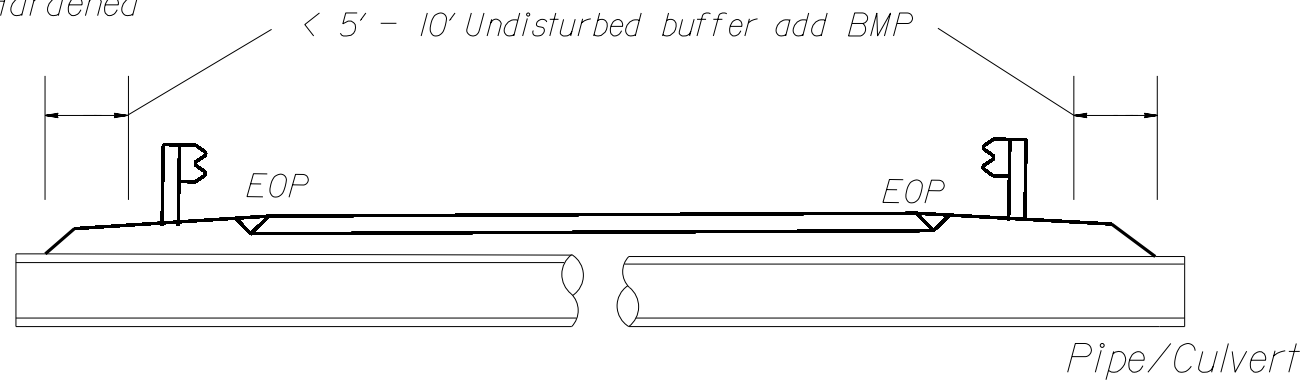
CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-8950	FAX 919-250-4119
<b>SHOULDER WEDGE DETAILS</b>	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC: s:\usr\details\stand\shouderwedgedetail.dgn	

S:\MAR-2016\1146\Projects\Resurfacing\Projects\Shoulder Wedge Details\Revised Shoulder Wedge Detail.dgn  
 T.SPELL  
 10/16/12

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

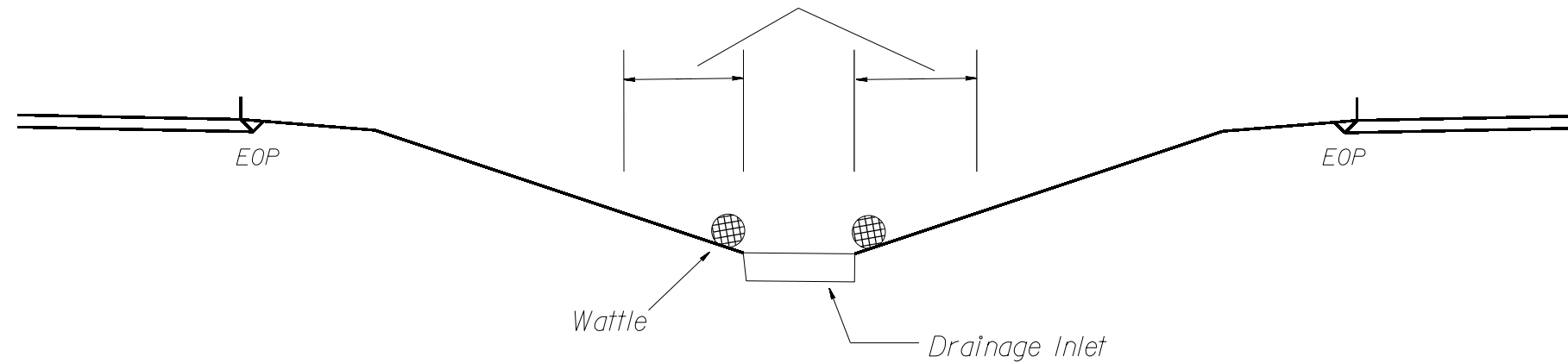
# EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

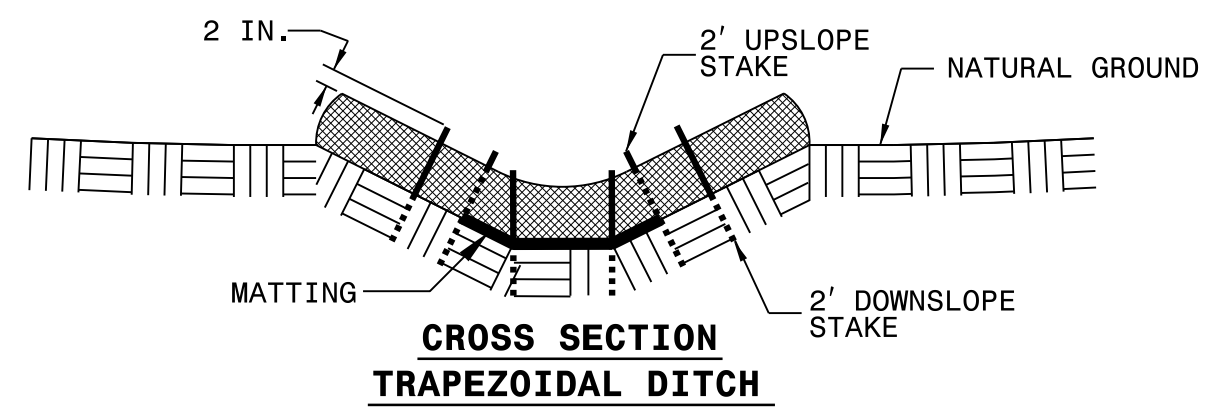
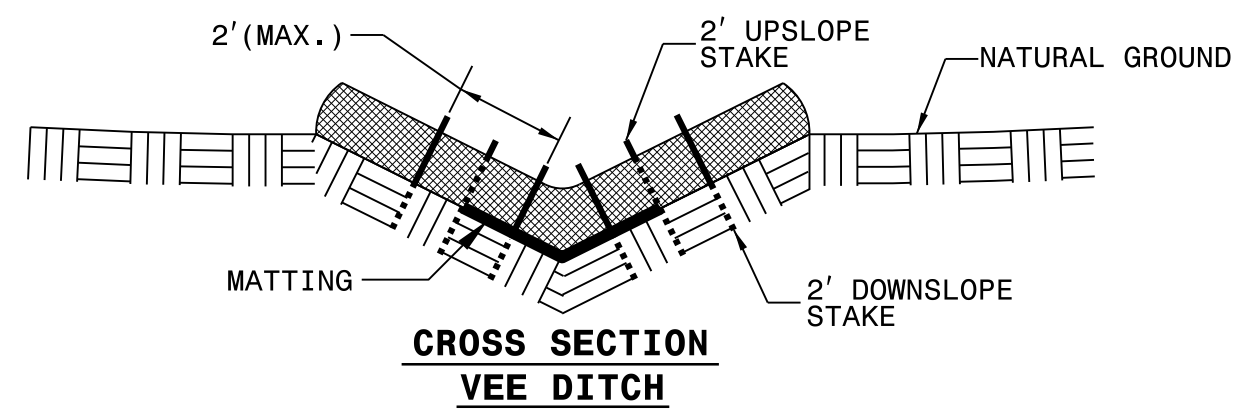
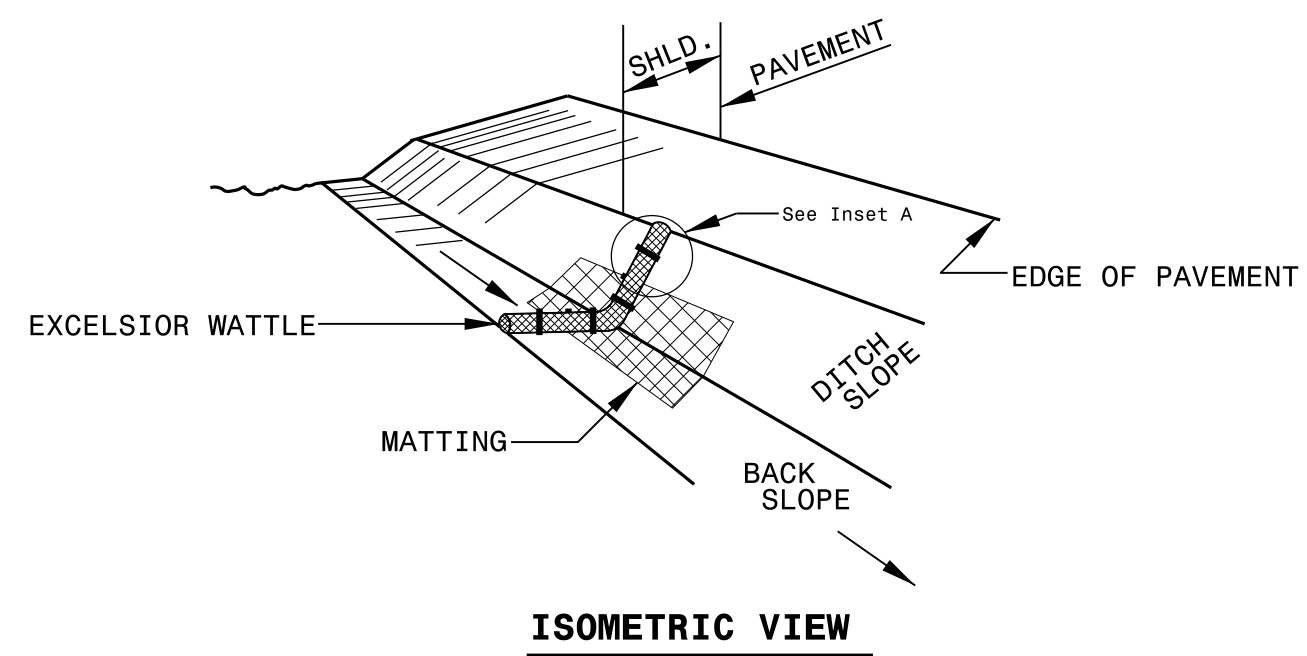


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

# WATTLE DETAIL



**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

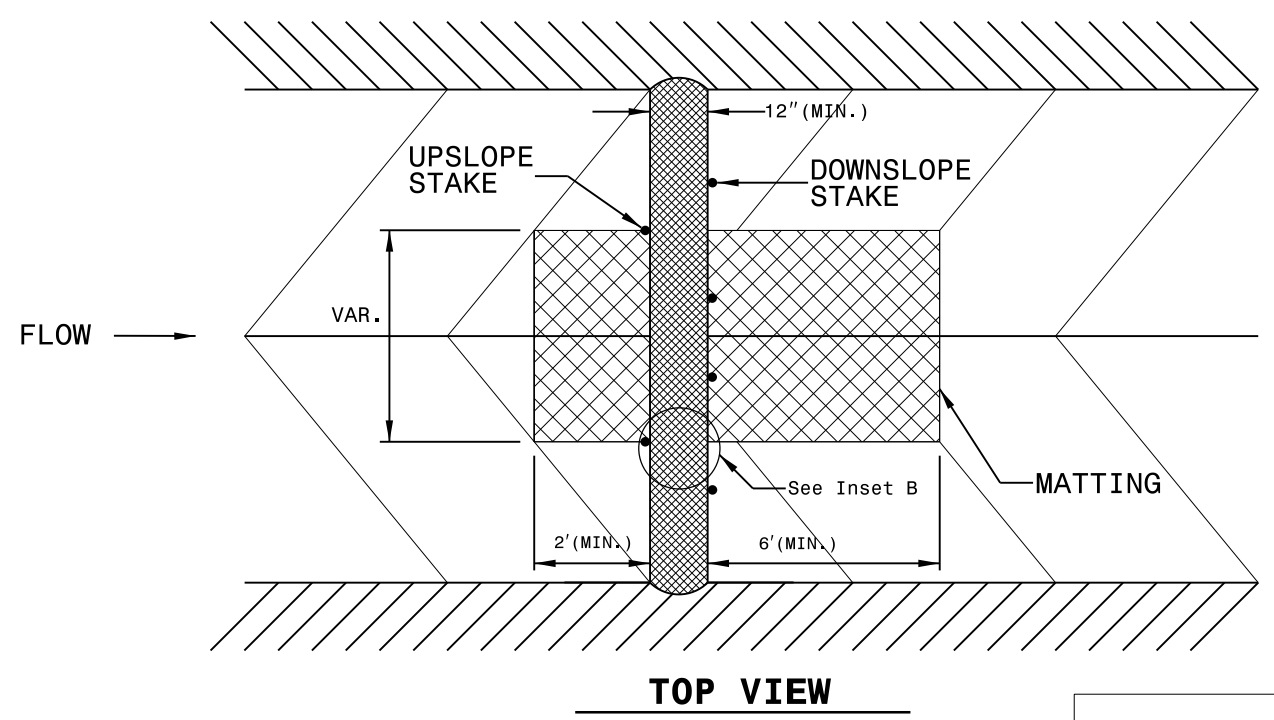
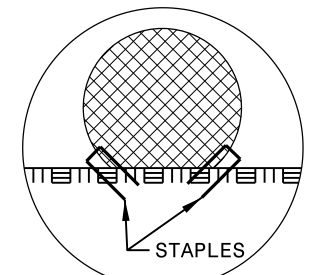
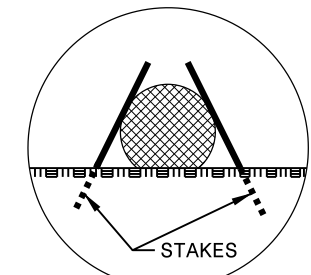
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE



## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	2.5" MILLING SY	1.75" MILLING SY	0"-4" MILLING SY	INCIDENTAL MILLING SY	INTERMEDIATE COURSE, 119.0B TONS	SURFACE COURSE, 9.5B TONS	ASPHALT BINDER FOR PLANT MIX TONS	COMPOSITE PVMT INTERLAYER - HIGH STRENGTH SY
2016CPT.02.45.10071.2	Beaufort	1	US-17 BUS	FROM BEGIN C&G S. OF CHOCOWINITY TO US-264/5TH ST. IN WASHINGTON	1-4	4	MU	NO	NO	3.12	48-64	75	4	1,000	44,000		500	496	11,182	695	
<b>TOTAL FOR MAP NO. 1</b>										<b>3.12</b>		<b>75</b>	<b>4</b>	<b>1,000</b>	<b>44,000</b>		<b>500</b>	<b>496</b>	<b>11,182</b>	<b>695</b>	
2016CPT.02.45.10071.2	Beaufort	2	US-17 BUS	FROM END BRIDGE NO. 25 TO US-264/5TH ST. IN WASHINGTON	5	4	MU	NO	NO	0.38	48-62					3,450	2,600		1,565	94	12,061
<b>TOTAL FOR MAP NO. 2</b>										<b>0.38</b>						<b>3,450</b>	<b>2,600</b>		<b>1,565</b>	<b>94</b>	<b>12,061</b>
<b>TOTAL FOR PROJ NO. 2016CPT.02.45.10071.2</b>										<b>3.50</b>		<b>75</b>	<b>4</b>	<b>1,000</b>	<b>44,000</b>	<b>3,450</b>	<b>3,100</b>	<b>496</b>	<b>12,747</b>	<b>789</b>	<b>12,061</b>
<b>GRAND TOTAL</b>										<b>3.50</b>		<b>75</b>	<b>4</b>	<b>1,000</b>	<b>44,000</b>	<b>3,450</b>	<b>3,100</b>	<b>496</b>	<b>12,747</b>	<b>789</b>	<b>12,061</b>

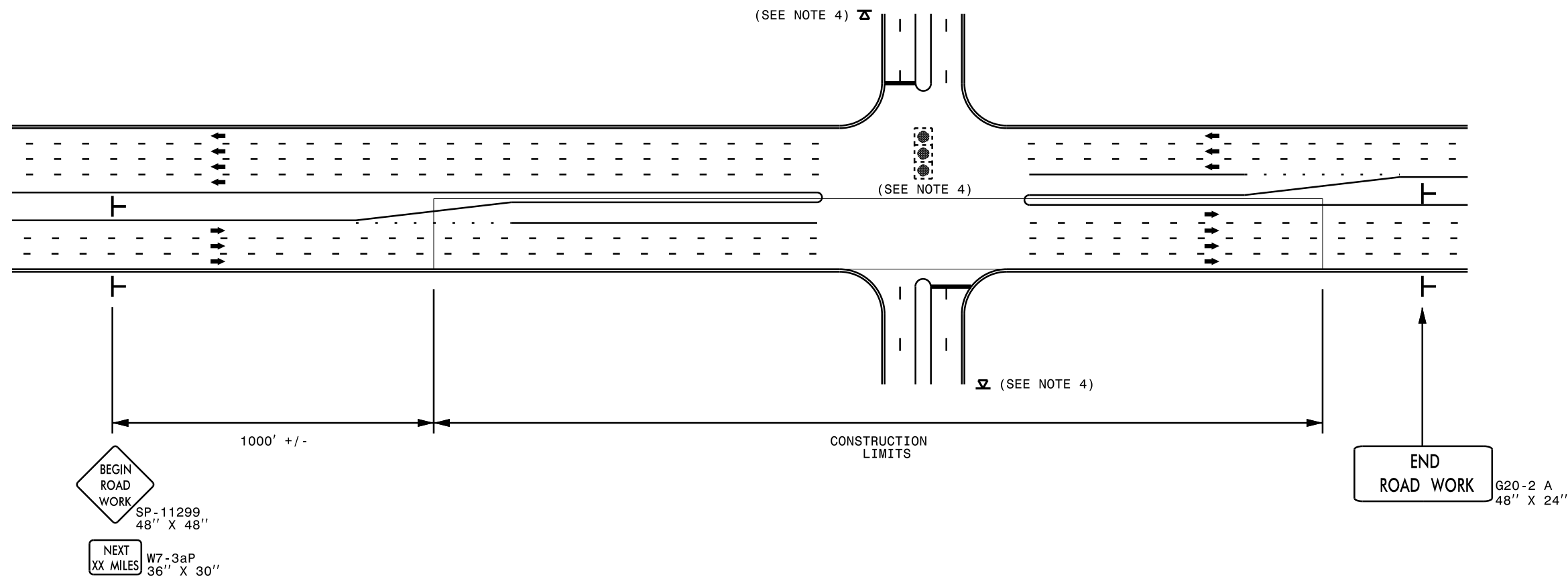
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	CONCRETE VALLEY GUTTER LF	RETROFIT EXT CRB RMP EA	CONC CURB RAMP EA	REM & REPL 2' 6" C&G LF	ADJ. OF MANHOL ES EA	ADJ. OF METER OR VALVE BOX EA	TEMPORARY SILT FENCE LF	WATTLE LF	SEED & MULCHING AC	RESPONSE FOR EROSION CONTROL EA
2016CPT.02.45.10071.2	Beaufort	1	US-17 BUS	FROM BEGIN C&G S. OF CHOCOWINITY TO US-264/5TH ST. IN WASHINGTON	1-4	4	MU	NO	NO	3.12	48-64							200.00	40.0	2.5	2
<b>TOTAL FOR MAP NO. 1</b>										<b>3.12</b>								<b>200.00</b>	<b>40.0</b>	<b>2.5</b>	<b>2</b>
2016CPT.02.45.10071.2	Beaufort	2	US-17 BUS	FROM END BRIDGE NO. 25 TO US-264/5TH ST. IN WASHINGTON	5	4	MU	NO	NO	0.38	48-62	60.0	4	4	180.0	29	5				
<b>TOTAL FOR MAP NO. 2</b>										<b>0.38</b>		<b>60.0</b>	<b>4</b>	<b>4</b>	<b>180.0</b>	<b>29</b>	<b>5</b>				
<b>TOTAL FOR PROJ NO. 2016CPT.02.45.10071.2</b>										<b>3.50</b>		<b>60.0</b>	<b>4</b>	<b>4</b>	<b>180.0</b>	<b>29</b>	<b>5</b>	<b>200.00</b>	<b>40.0</b>	<b>2.5</b>	<b>2</b>
<b>GRAND TOTAL</b>										<b>3.50</b>		<b>60.0</b>	<b>4</b>	<b>4</b>	<b>180.0</b>	<b>29</b>	<b>5</b>	<b>200.00</b>	<b>40.0</b>	<b>2.5</b>	<b>2</b>

### THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4685000000-E	4686000000-E		4695000000-E		4700000000-E	4702000000-E	4710000000-E	4721000000-E		4725000000-E			
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING SF	TEMPORARY TRAFFIC CONTROL LS	4" X 90 M WHITE THERMO LF	4" X 120 M YELLOW THERMO LF	4" X 120 M WHITE THERMO LF	8" X 90 M WHITE THERMO LF	8" X 90 M YELLOW THERMO LF	12" X 90 M WHITE THERMO LF	12" X 120 M WHITE THERMO LF	24" X 120 M WHITE THERMO LF	THERMO MSG SCHOOL 120 M EA	THERMO MSG ONLY 120 M EA	THERMO LT ARROW 90 M EA	THERMO STR ARROW 90 M EA	THERMO RT ARROW 90 M EA	THERMO STR & RT ARROW 90 M EA
2016CPT.02.45.10071.2	Beaufort	1	US-17 BUS	FROM BEGIN C&G S. OF CHOCOWINITY TO US-264/5TH ST. IN WASHINGTON	1-4	4	MU	3.12	48-64	692	0.907	24,800	34,298	10,800	750	350	6,300	620	600	6	4	23	26	9	11
TOTAL FOR MAP NO. 1										692	0.907	24,800	34,298	10,800	750	350	6,300	620	600	6	4	23	26	9	11
2016CPT.02.45.10071.2	Beaufort	2	US-17 BUS	FROM END BRIDGE NO. 25 TO US-264/5TH ST. IN WASHINGTON	5	4	MU	0.38	48-62	126	0.093		4,100	2,250	150			1,900	450			15	21		13
TOTAL FOR MAP NO. 2										126	0.093		4,100	2,250	150			1,900	450			15	21		13
TOTAL FOR PROJ NO. 2016CPT.02.45.10071.2										818	1.000	24,800	38,398	13,050	900	350	6,300	2,520	1,050	6	4	38	47	9	24
GRAND TOTAL										818	1.000	24,800	38,398	13,050	900	350	6,300	2,520	1,050	6	4	38	47	9	24
										51,448			51,448	13,050	1,250				10						118

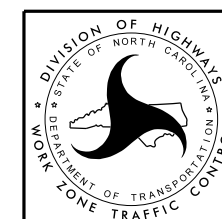
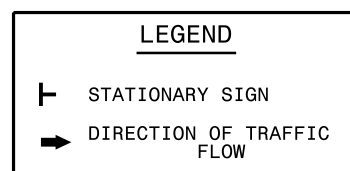
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4810000000-E		4820000000-E		4825000000-E	4835000000-E	4840000000-N		4845000000-N				4905000000-N
										4" WHITE PAINT LF	4" YELLOW PAINT LF	8" YELLOW PAINT LF	8" WHITE PAINT LF	12" WHITE PAINT LF	24" WHITE PAINT LF	PAINT MSG SCHOOL EA	PAINT MSG ONLY EA	PAINT LT ARROW EA	PAINT STR ARROW EA	PAINT RT ARROW EA	PAINT STR & RT ARROW EA	SNOW PLOWABLE MARKERS EA
2016CPT.02.45.10071.2	Beaufort	1	US-17 BUS	FROM BEGIN C&G S. OF CHOCOWINITY TO US-264/5TH ST. IN WASHINGTON	1-4	4	MU	3.12	48-64	15,100	14,550	350	750	7,000	650	6	4	23	26	9	11	748
TOTAL FOR MAP NO. 1										15,100	14,550	350	750	7,000	650	6	4	23	26	9	11	748
2016CPT.02.45.10071.2	Beaufort	2	US-17 BUS	FROM END BRIDGE NO. 25 TO US-264/5TH ST. IN WASHINGTON	5	4	MU	0.38	48-62					350	100				4		13	95
TOTAL FOR MAP NO. 2														350	100				4		13	95
TOTAL FOR PROJ NO. 2016CPT.02.45.10071.2										15,100	14,550	350	750	7,350	750	6	4	23	30	9	24	843
GRAND TOTAL										29,650		1,100				10			86			843
										29,650		1,100				10			86			843

## URBAN / SUBURBAN WORKZONES



### NOTES:

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.



**RESURFACING ADVANCE  
WARNING SIGNS FOR  
URBAN / SUBURBAN  
FACILITIES**