# PITT BEAUFORT GREENE LENOIR DIVISION 2 RAVEN

## **PITT COUNTY**

**DB00381** 

**REVISED 5/24/18** 

PROJECT REFERENCE NO.

DB00381

SHEET NO.

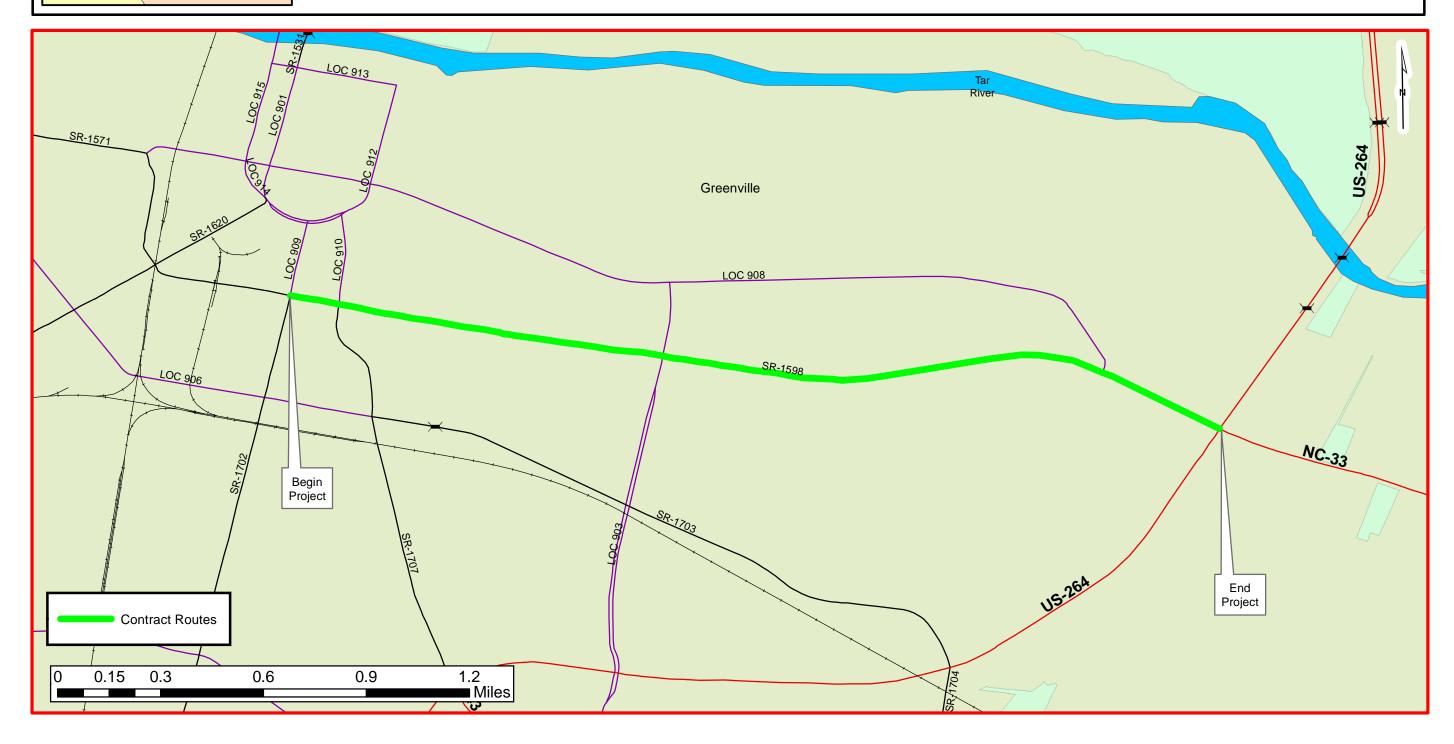
### WBS# 2018CPT.02.23.20741

LOCATION:

MAP 01 - SR 1598 (10TH STREET) FROM SR 1702 (EVANS STREET) TO US 264 ALT (GREENVILLE BLVD)

TYPE OF WORK: MILLING, RESURFACING, PAVEMENT MARKINGS, PAVEMENT MARKERS

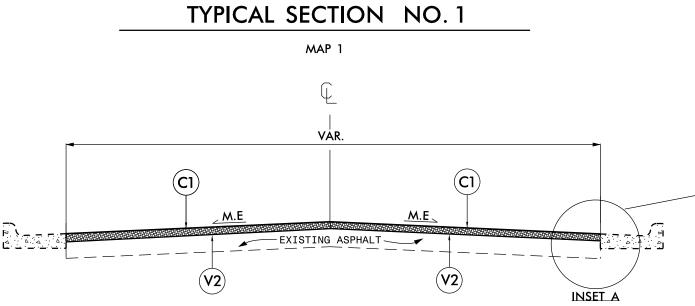




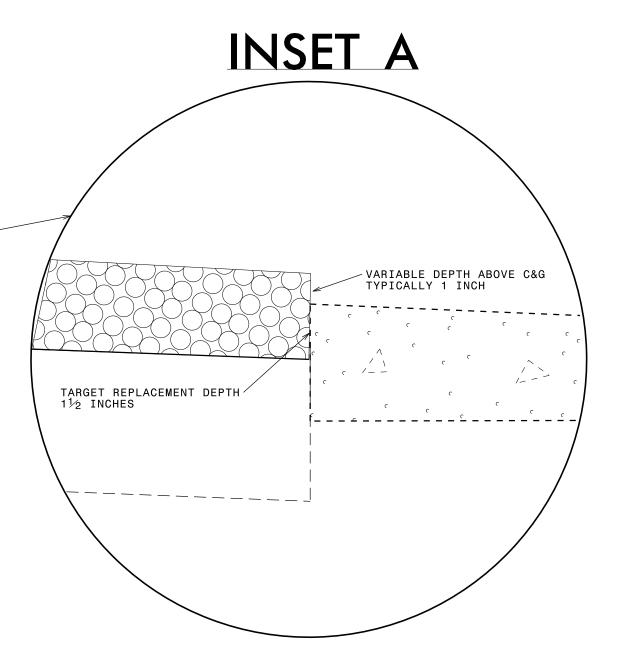
ROJECT REFERENCE NO.	SHEET NO.
DB0038I	2 - REV

PAVEMENT SCHEDULE						
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165.0 LBS. PER SQ. YD.					
V1	INCIDENTAL MILLING.					
V2	$2lac{1}{2}$ " DEPTH MILLING FOR THE ENTIRE WIDTH OF ROADWAY					
DRAWINGS NOT TO SCALE						

NOTE: PAVEMENT EDGE SLOPES ARE I: IUNLESS SHOWN OTHERWISE.



- NOTE:
- 1. MILL FULL WIDTH OF THE ENTIRE ROADWAY.
- 2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
- 3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
- 4. REPAIR VALLEY GUTTER AND CURB AND GUTTER AT LOCATIONS SHOWN ON PAGE 3.



PROJECT NO.	SHEET NO.	TOTAL NO.
DB00381 - REV	3	

### SUMMARY OF QUANTITIES

												000093	0000-Е	1308000000-E	133000000-E	1519000000-E	1575000000-E	260000000-N	2605000000-N	2830000000-N	2845000000-N	4898000000-N
PROJE	CT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES LANE	FINAL	WARM MIX	LENGTH	WIDTH	2'6" CURB &	CONCRETE	2 1/2" MILLING	INCIDENTAL	SURFACE	ASPHALT	RETROFIT	WHEELCHAIR	ADJ. OF	ADJ. OF METER	THERMO BIKE
							TYPE	SURFACE	ASPHALT			GUTTER -	VALLEY		MILLING	COURSE, S9.5B	BINDER FOR	EXISTING CURB	RAMPS	MANHOLES	OR VALVE BOX	SYMBOL
								TESTING	REQUIRED			REMOVE/RE	GUTTER -				PLANT MIX	RAMP				
								REQUIRED				PLACE	REMOVE/RE									
													PLACE									
										MI	FT	LF	LF	SY	SY	TONS	TONS	EA	EA	EA	EA	EA
2018CPT.0	2.23.20741	Pitt	1	SR 1598	FROM SR 1702 TO US 264 ALT.	1	5 MU	NO	NO	2.29	60	650	100	90,000	800	8,250	555	75	10	1	1	2
	TOTAL F	OR MAP N	0.1							2.29		650	100	90,000	800	8,250	555	75	10	1	1	2
TOTAL	. FOR PROJ N	IO 2019C	T 02 22 2	07/1						2.29		650	100	90,000	800	8,250	555	75	10	1	1	2
TOTAL	- FOR PROJ N	10. 2016CF	1.02.23.2	0/41								75	0									
	GRAND TOTAL			·					2.29		650	100	90,000	800	8,250	555	75	10	1	1	2	
	GRA	IND IOIAL	•		·							75	0									

					1	1	-
MAP			STATION	STATION	LT	RT	
	VALLEY GUTTER REPAIR						
1			49+56	49+92		36'	
1			68+10	68+50	40'		
	CURB AND GUTTER REPAIR						
1			18+92	19+22	30'		
1			26+00	26+16		16'	
1			29+98	30+50	52'		1
1			33+88	34+06	18'		
1			43+22	43+70	48'		1
1			45+38	45+90	52'		
1			58+64	59+12		48'	
1			58+76	59+04	28'		
1			59+42	60+00		58'	
1			59+42	59+74	32'		
1			61+26	61+82	56'		
1			62+20	63+12	92'		1
1			73+32	73+44	12'		1
1			101+30	101+82	52'		1
1		FRO	M 10TH ST. to	o 5TH ST.		30'	OFF RA
1			M 5TH ST. to			12'	RT. TUR

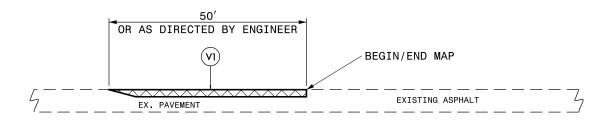
PROJECT NO.	SHEET NO.	TOTAL NO.
DB00381	4	

## THERMOPLASTIC AND PAINT QUANTITIES

										4413000000-E	4457000000-N	469000	0000-E	4702000000-E	4710000000-E	4721000000-E			4725000000-E			4905000000-N
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE	LENGTH	WIDTH	WORK ZONE	TEMPORARY	6" X 120 M	6" X 120 M	12" X 120 M	24" X 120 M	THERMO XING	THERMO L	T THERMO STR	THERMO RT	THERMO STR	THERMO STR	SNOW
							TYPE			ADVANCE/GE	TRAFFIC	WHITE	YELLOW	WHITE	WHITE	120(MILS)	ARROW 9	90 ARROW 90 M	ARROW 90	& RT ARROW	& LT ARROW	PLOWABLE
										NERAL	CONTROL	THERMO	THERMO	THERMO	THERMO		M		M	90 M	90 M	MARKERS
										WARNING												
										SIGNING												
								MI	FT	SF	LS	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA
2018CPT.02.23.20741	l Pitt	1	SR 1598	FROM SR 1702 TO US 264 ALT.	1	5	MU	2.29	60	130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750
TOTAL	FOR MAP I	NO. 1						2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750
TOTAL FOR PROJ	NO 2019C	DT 02 22 2	00741					2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750
TOTAL FOR PROJ	NO. 2016C	F1.U2.23.2	20741									39,000			156							
GRAND TOTAL							2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750	
GR	AIND IOIA	L										39,	39,000						156			

PROJECT REFERENCE NO.	SHEET NO.
DB0038I	DIV2-I

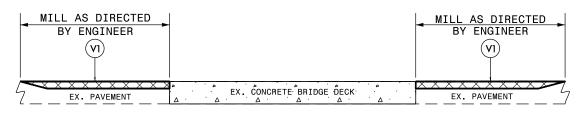
## MILLING TYPICALS



## DETAIL 1 BEGIN/END MAP TIE-IN

#### NOTE:

1. MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

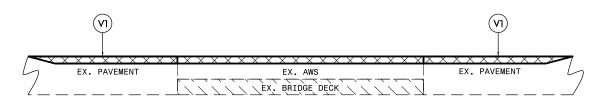


#### DETAIL 2

#### BRIDGE MILLING

#### NOTE:

1. MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



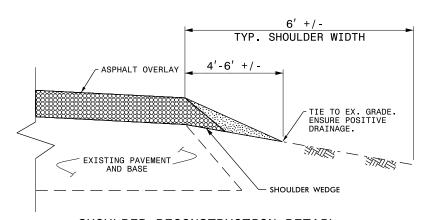
#### DETAIL 3

#### BRIDGE MILLING

#### NOTE:

1. INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

## SHOULDER RECONSTRUCTION TYPICAL



SHOULDER RECONSTRUCTION DETAIL

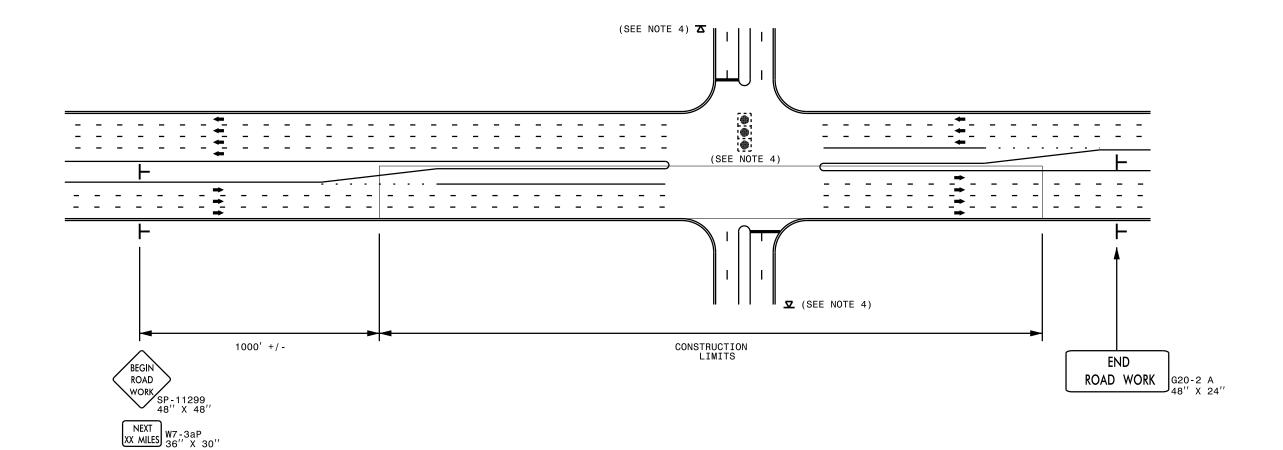
#### NOTE:

- 1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- 2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- 3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

PROJ. REFERENCE NO. SHEET NO

DB00381 TCP-1

#### URBAN / SUBURBAN WORKZONES



#### NOTES:

- 1)  $48" \times 48"$  SIZED SIGNS (SP- 11299) MAY BE REDUCED TO  $36" \times 36"$  ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS.THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.



RESURFACING ADVANCE WARNING SIGNS FOR URBAN / SUBURBAN FACILITIES

