

PITT COUNTY

REVISED 5/24/18

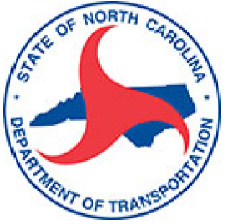
PROJECT REFERENCE NO.	SHEET NO.
DB00381	1

DB00381

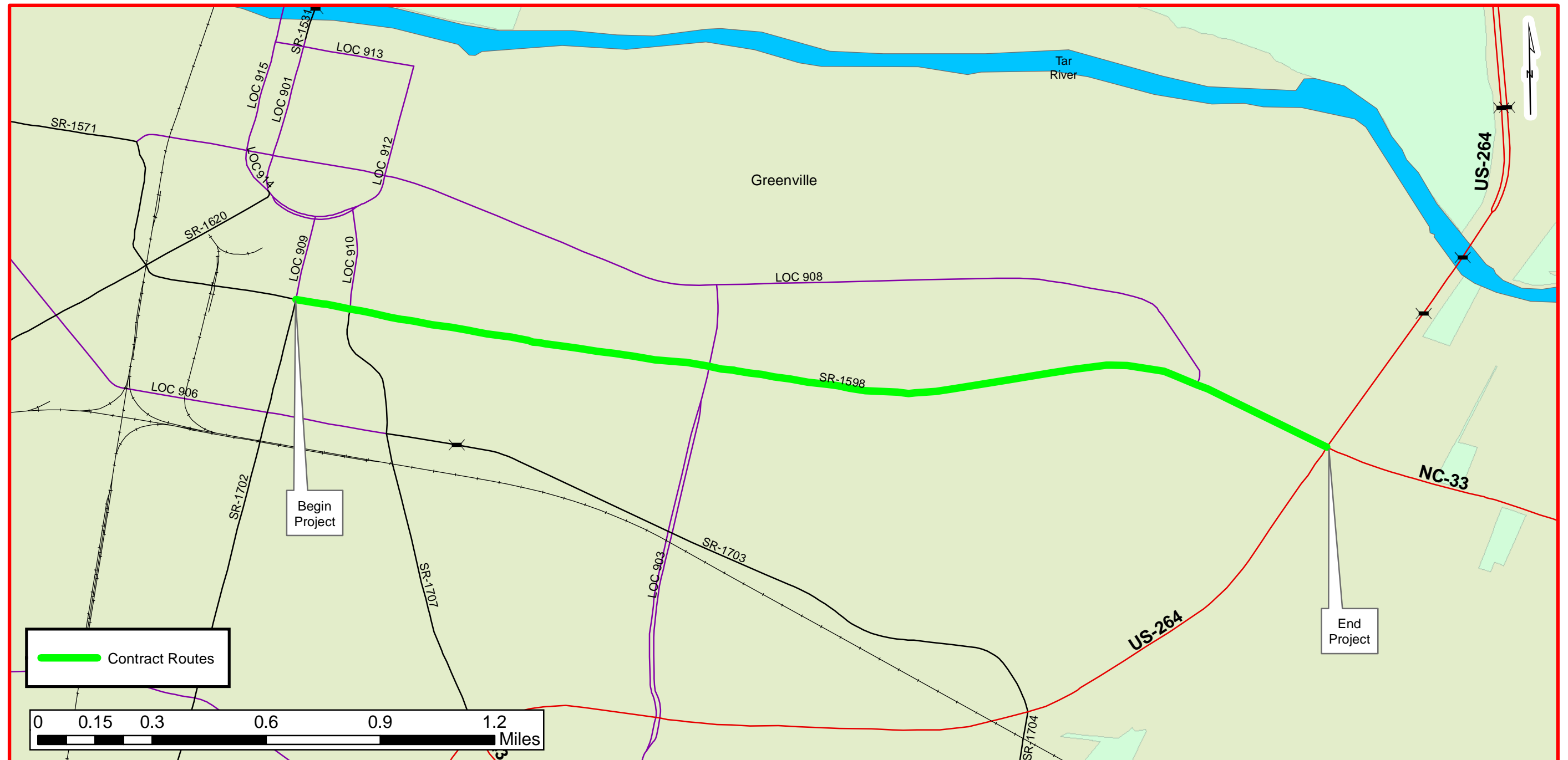
WBS# 2018CPT.02.23.20741

LOCATION:
MAP 01 - SR 1598 (10TH STREET) FROM SR 1702 (EVANS STREET) TO US 264 ALT (GREENVILLE BLVD)

TYPE OF WORK: MILLING, RESURFACING, PAVEMENT MARKINGS, PAVEMENT MARKERS



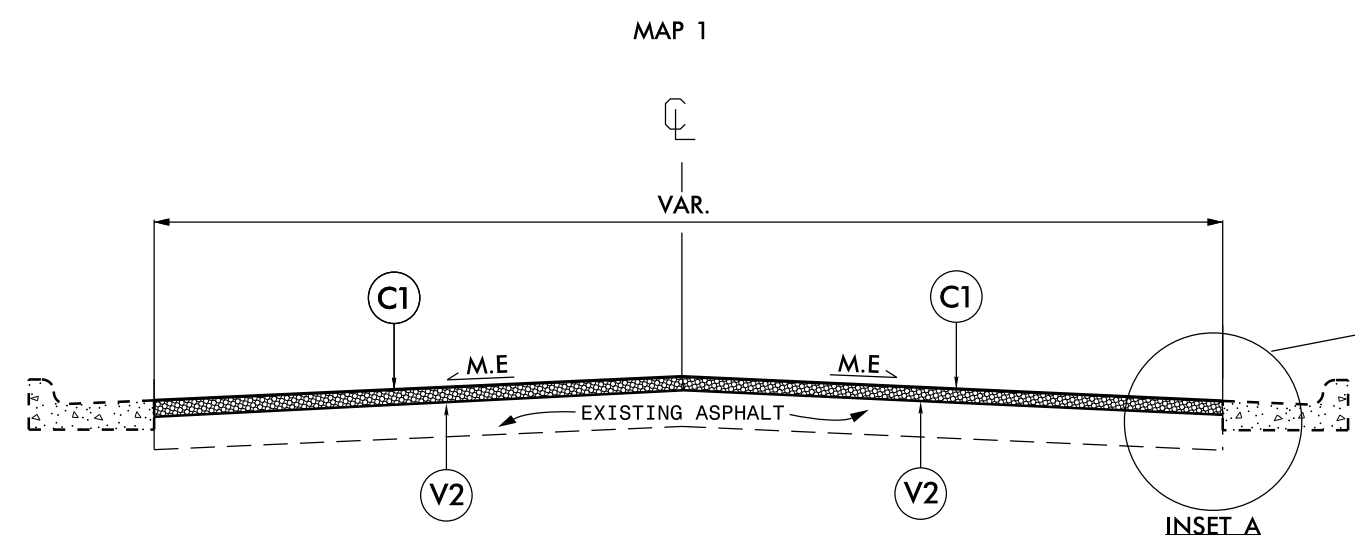
NCDOT
DIVISION 2



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165.0 LBS. PER SQ. YD.
V1	INCIDENTAL MILLING.
V2	2½" DEPTH MILLING FOR THE ENTIRE WIDTH OF ROADWAY
DRAWINGS NOT TO SCALE	

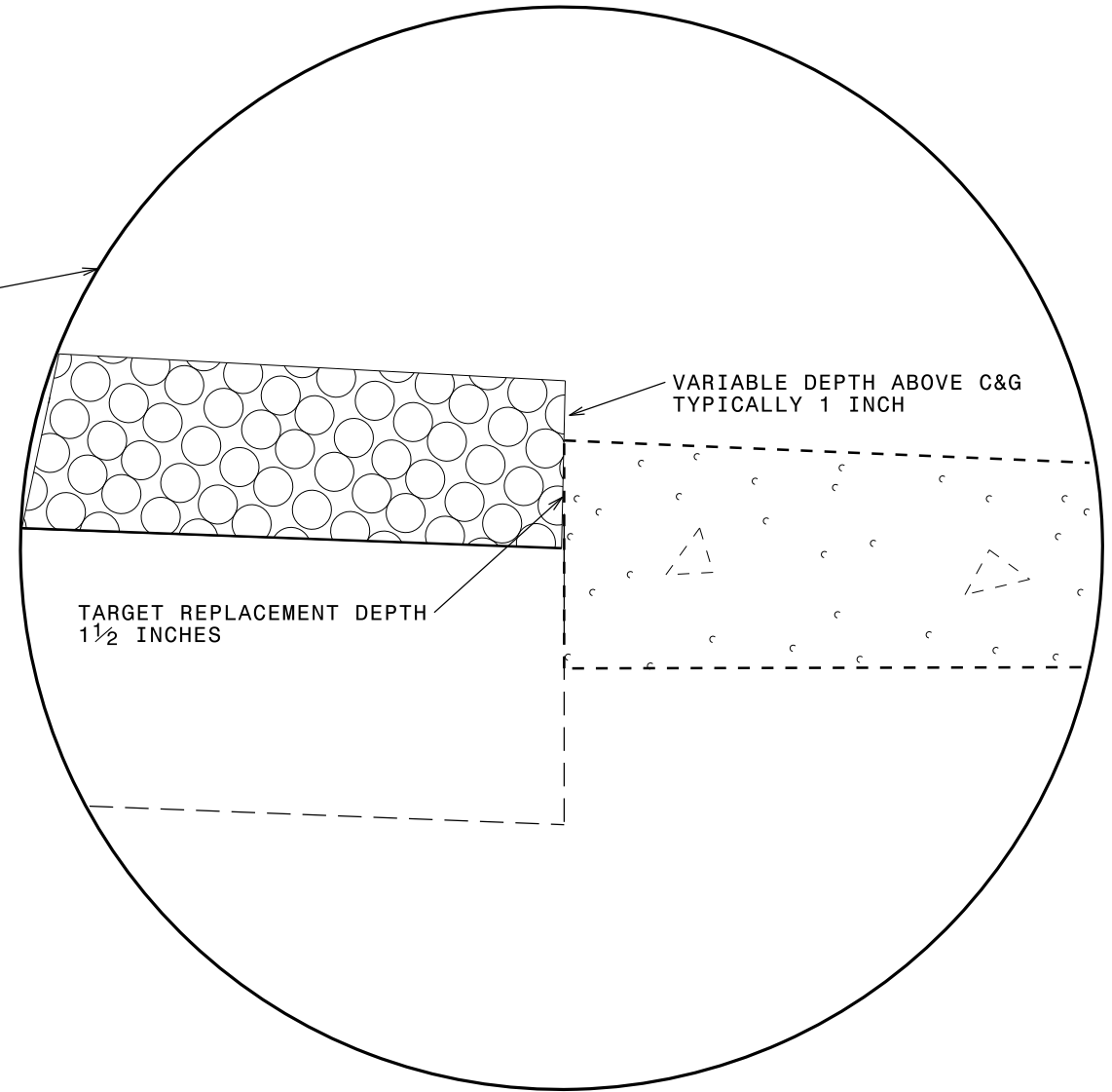
NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

TYPICAL SECTION NO. 1



- NOTE:**
1. MILL FULL WIDTH OF THE ENTIRE ROADWAY.
 2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
 3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
 4. REPAIR VALLEY GUTTER AND CURB AND GUTTER AT LOCATIONS SHOWN ON PAGE 3.

INSET A

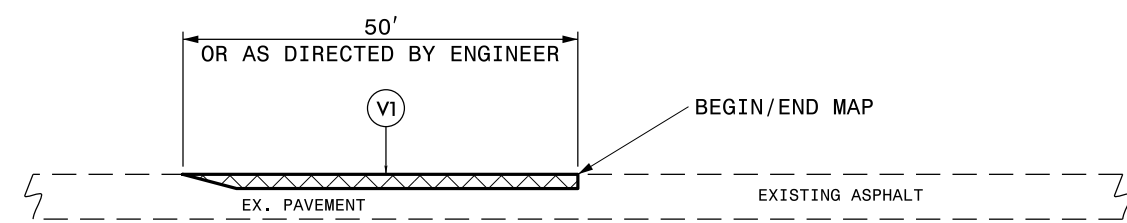


PROJECT NO.	SHEET NO.	TOTAL NO.
DB00381	4	

THERMOPLASTIC AND PAINT QUANTITIES

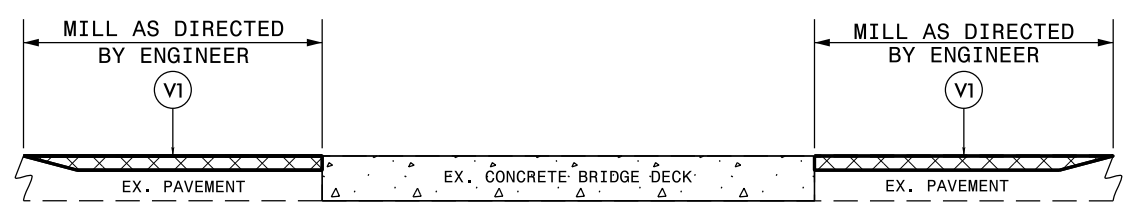
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4690000000-E		4702000000-E	4710000000-E	4721000000-E	4725000000-E					4905000000-N		
										WORK ZONE ADVANCE/GE NERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	6" X 120 M WHITE THERMO	6" X 120 M YELLOW THERMO	12" X 120 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO XING 120(MILS)	THERMO LT ARROW 90 M	THERMO STR ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR & LT ARROW 90 M	SNOW PLOWABLE MARKERS		
										SF	LS	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA		
2018CPT.02.23.20741	Pitt	1	SR 1598	FROM SR 1702 TO US 264 ALT.	1	5	MU	2.29	60	130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750		
TOTAL FOR MAP NO. 1								2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750		
TOTAL FOR PROJ NO. 2018CPT.02.23.20741								2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750		
										39,000														
GRAND TOTAL								2.29		130	1	8,500	30,500	1,200	2,100	4	90	40	10	15	1	750		
										39,000														

MILLING TYPICALS



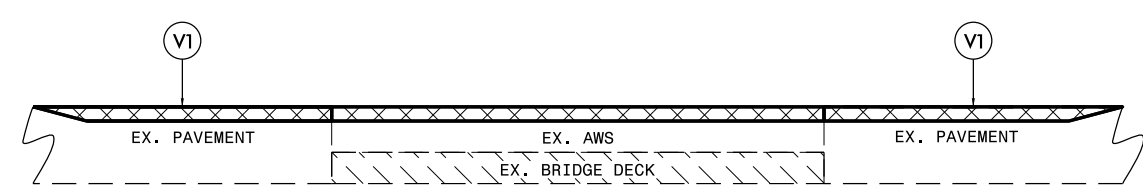
DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:
1. MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

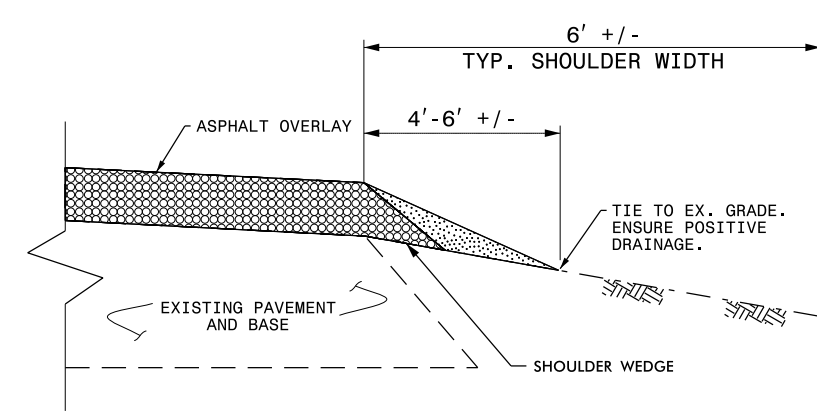
NOTE:
1. MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 3
BRIDGE MILLING

NOTE:
1. INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

SHOULDER RECONSTRUCTION TYPICAL

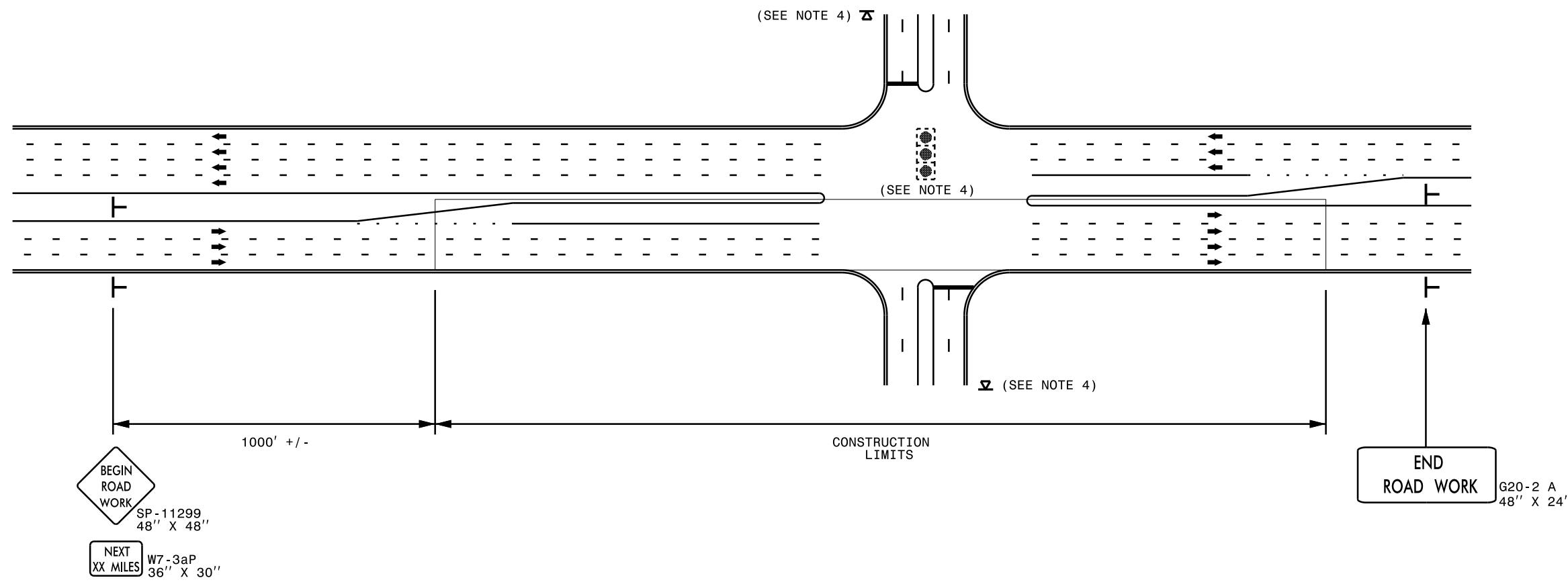


SHOULDER RECONSTRUCTION DETAIL

NOTE:

1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

URBAN / SUBURBAN WORKZONES

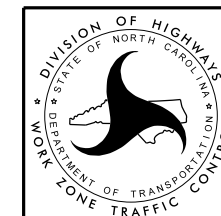


NOTES:

- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW



**RESURFACING ADVANCE
WARNING SIGNS FOR
URBAN / SUBURBAN
FACILITIES**