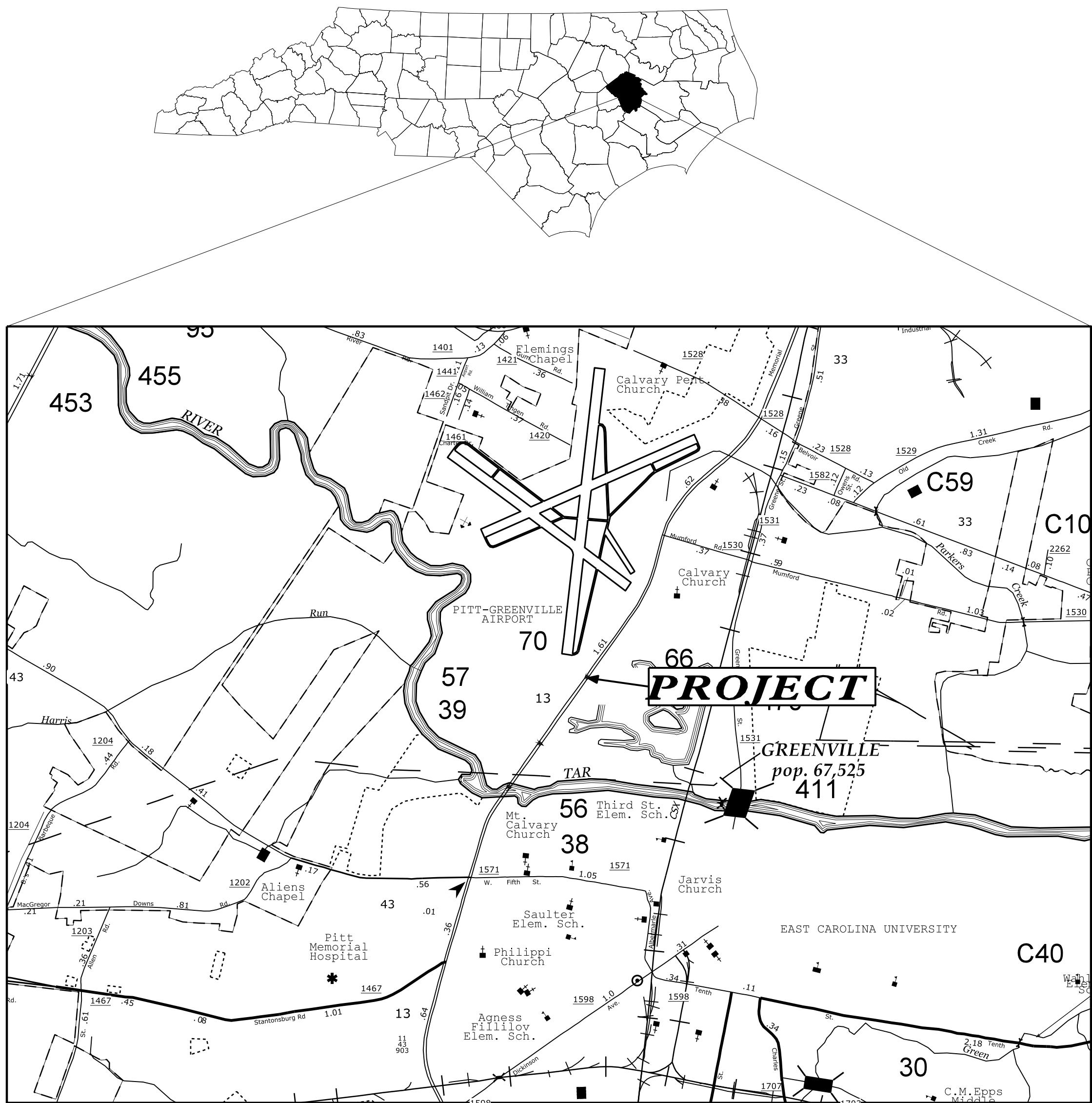


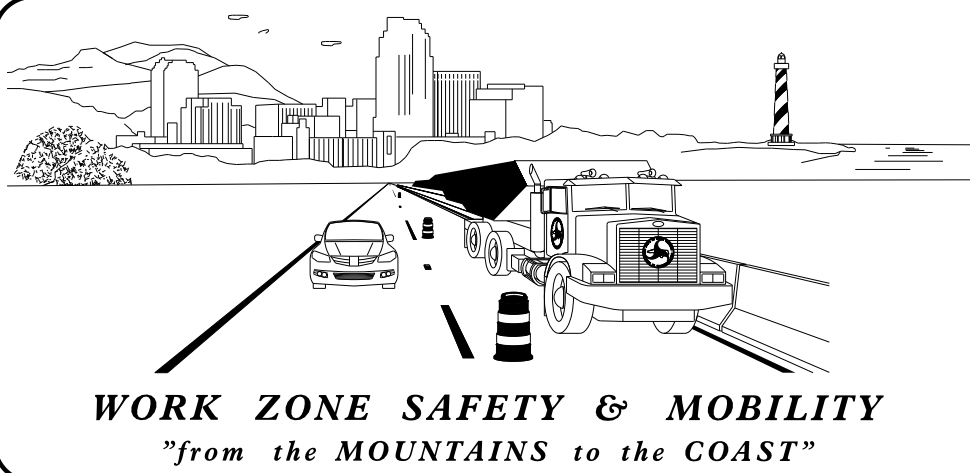
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

TRANSPORTATION MANAGEMENT PLAN

PITT COUNTY



VICINITY MAP



N.C.D.O.T. DIVISION TWO DDC P.O. BOX 1587 GREENVILLE, NC 27835	
J. S. BOURNE, P.E.	STATE TRAFFIC MANAGEMENT ENGINEER
DWAYNE ALLIGOOD, P.E	TRAFFIC CONTROL PROJECT ENGINEER
LANG JONES	TRAFFIC CONTROL PROJECT DESIGN ENGINEER
LANG JONES	TRAFFIC CONTROL DESIGN ENGINEER



APPROVED: <i>Dwayne H. Allgood</i> DATE: <i>06/02/2014</i>	
SEAL	

INDEX OF SHEETS

SHEET NO.	TITLE
TMP-1	TITLE SHEET, VICINITY MAP AND INDEX OF SHEETS
TMP-1A	LIST OF APPLICABLE ROADWAY STANDARD DRAWINGS, AND LEGEND
TMP-2	TEMPORARY TRAFFIC CONTROL PLANS
TMP-3	TEMPORARY TRAFFIC CONTROL PLANS
TMP-4	TEMPORARY TRAFFIC CONTROL PLANS

SHEET NO.
TMP-1

17BP.2.P.20

PROJECT:

02-JUN-2014 14:06
G:\PROJECTS\PI\T\N\PI\T 66\PI\T+66.TMP\A.dgn
\$\$\$USERNAME\$\$\$

ROADWAY STANDARD DRAWINGS






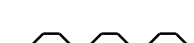
THE FOLLOWING ROADWAY STANDARDS AS SHOWN IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2012 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:


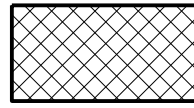


STD. NO.	TITLE
1101.01	WORK ZONE ADVANCE WARNING SIGNS
1101.02	TEMPORARY LANE CLOSURES
1101.04	TEMPORARY SHOULDER CLOSURES
1101.05	WORK ZONE VEHICLE ACCESSES
1101.11	TRAFFIC CONTROL DESIGN TABLES
1110.01	STATIONARY WORK ZONE SIGNS
1110.02	PORTABLE WORK ZONE SIGNS
1115.01	FLASHING ARROW BOARDS
1130.01	DRUM
1135.01	CONES
1145.01	BARRICADES
1150.01	FLAGGING DEVICES
1160.01	TEMPORARY CRASH CUSHION
1165.01	WORK VEHICLE LIGHTING SYSTEMS AND TMA DELINEATION
1170.01	POSITIVE PROTECTION
1180.01	SKINNY-DRUM

PROJ. REFERENCE NO.	SHEET NO.
17BP.2.P.20	TMP-1A

LEGEND

GENERAL







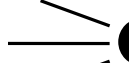


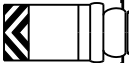

-  DIRECTION OF TRAFFIC FLOW
-  DIRECTION OF PEDESTRIAN TRAFFIC FLOW
-  EXIST. PVMT.
-  NORTH ARROW
-  PROPOSED PVMT.
-  TEMP. SHORING (LOCATION PURPOSES ONLY)

-  WORK AREA
-  REMOVAL
-  USER DEFINED (IF NEEDED)
-  USER DEFINED (IF NEEDED)




PAVEMENT MARKINGS


-  EXISTING LINES


TRAFFIC CONTROL DEVICES

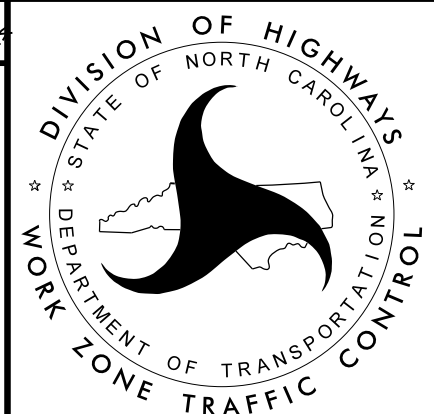
-  BARRICADE (TYPE III)
-  CONE
-  DRUM  SKINNY DRUM  TUBULAR MARKER
-  TEMPORARY CRASH CUSHION
-  FLASHING ARROW BOARD
-  FLAGGER
-  LAW ENFORCEMENT
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  CHANGEABLE MESSAGE SIGN (PROVIDED BY NCDOT)
(CONTACT JIM EVANS AT 252-830-3493 FOR COORDINATION)

TEMPORARY SIGNING

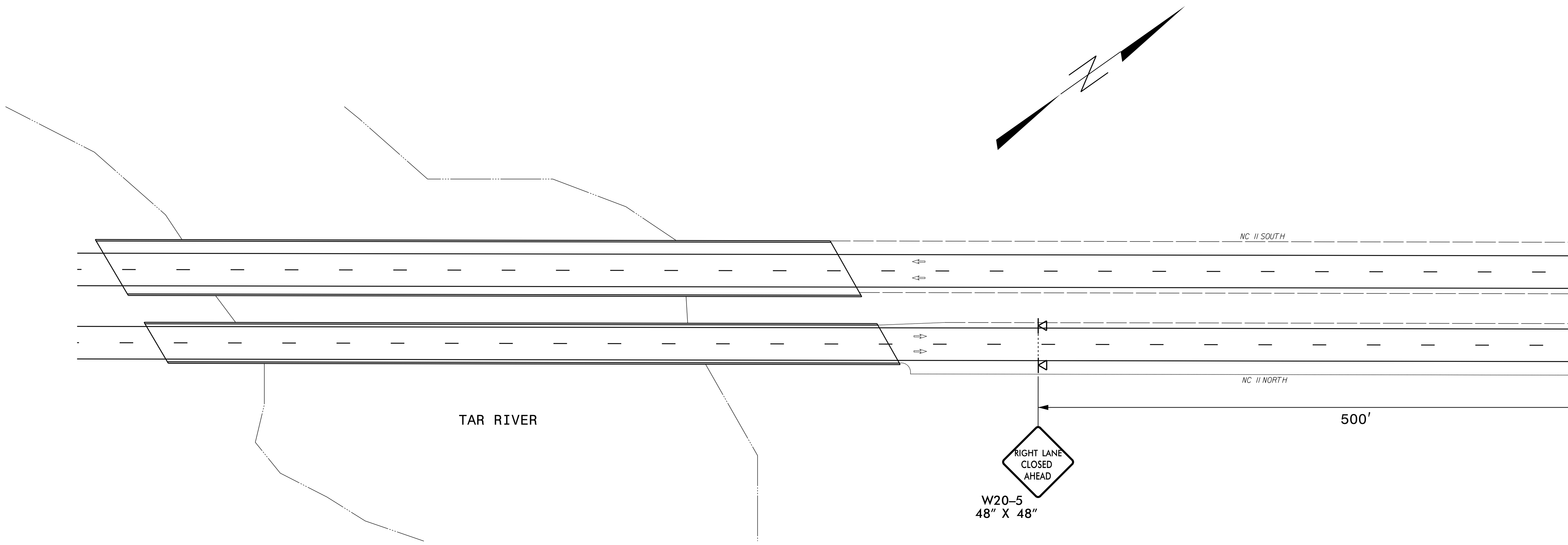
-  PORTABLE SIGN
-  STATIONARY SIGN
-  STATIONARY OR PORTABLE SIGN

APPROVED:  DATE: 06/02/2014





ROADWAY STANDARD
DRAWINGS & LEGEND



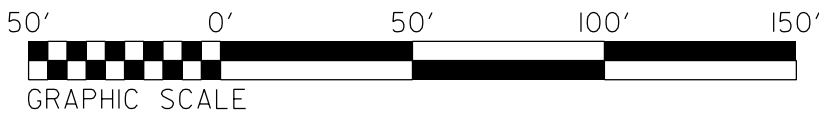
GENERAL NOTES

- 1- IF NECESSARY USE THIS STD. FOR ONE-WAY CITY TYPE STREETS WHERE SIGNS MAY BE MOUNTED ON BOTH SIDES OF THE ROADWAY.
- 2- PLACE ARROW PANELS ON THE SHOULDER (PAVED OR UNPAVED). PLACE ARROW PANELS WITHIN THE TAPER IF SHOULDERS DO NOT EXIST. MEET THE REQUIREMENTS FOR STOPPING SIGHT DISTANCE AT THE ARROW PANEL LOCATION. IF NEEDED, EXTEND LANE CLOSURES AT THE BUFFER SPACE, SUCH THAT STOPPING SIGHT DISTANCE TO THE ARROW PANEL IS MET. (SEE STD. 1101.11 SHEET 2)
- 3- PLACE DRUMS IN TAPERS AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. PLACE DRUMS ALONG THE BUFFER SPACE AND WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 4- REFER TO STD. 1101.11 SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 5- REFER TO STD. 1101.02 SHEETS 6 AND 7 FOR TREATMENT OF LANE CLOSURES THRU INTERCHANGES.
- 6- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC. REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 7- TMIA'S ARE REQUIRED ONLY WHEN A BUFFER SPACE CANNOT BE ATTAINED, OR WHEN DIRECTED BY THE ENGINEER OR THE PLANS. WHEN USED, POSITION THE TMIA TO MAINTAIN A ROLL-AHEAD DISTANCE AS RECOMMENDED BY THE MANUFACTURER.
- 8- PLACE CHANGEABLE MESSAGE SIGN (CMS) ON THE OUTSIDE OF THE TRAVELWAY AS DIRECTED BY THE ENGINEER. PLACE CMS APPROXIMATELY 1/2 MILE IN ADVANCE OF THE W20-5 SIGNS. IF TRAFFIC BACKS UP TO WHERE THE CMS IS INITIALLY PLACED, RELOCATE CMS 1/2 MILE FROM ANTICIPATED BACKUP. CONTINUE TO MONITOR TRAFFIC, MOVE CMS APPROXIMATELY 1/2 MILE IN CONJUNCTION WITH ANTICIPATED BACKUP.
- 9- MESSAGES MAY READ "LEFT LANE CLOSED" OR "RIGHT LANE CLOSED", "MERGE RIGHT" OR "MERGE LEFT" DEPENDING ON LANE BEING CLOSED.

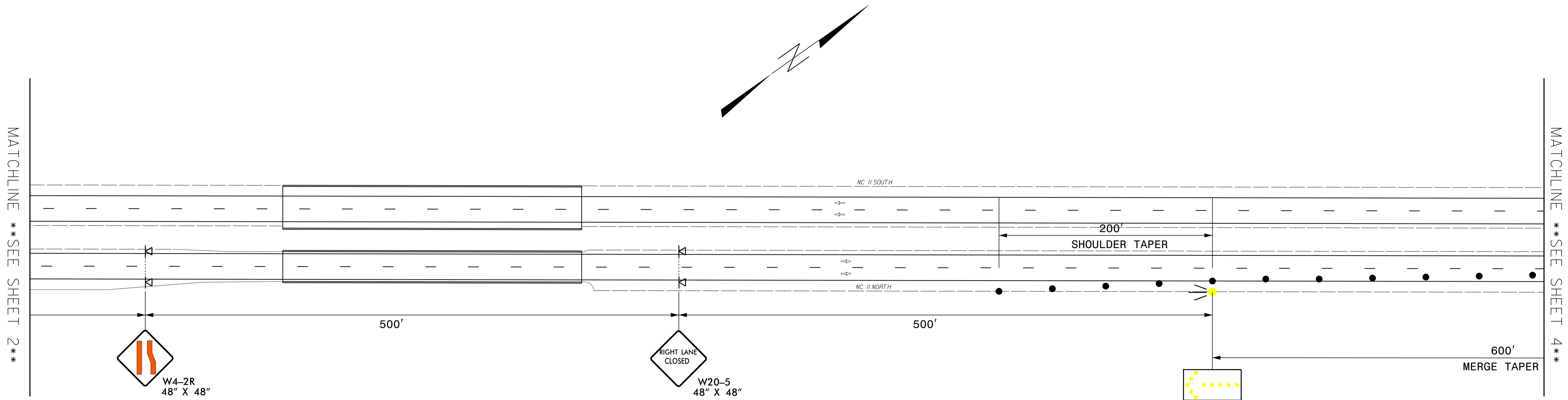
LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- SKINNY DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

CHANGEABLE MESSAGE SIGN (PROVIDED BY NCDOT)
(CONTACT JIM EVANS AT 252-830-3493 FOR COORDINATION)



APPROVED <i>Design & Approval</i> DATE <i>6/22/2019</i>		



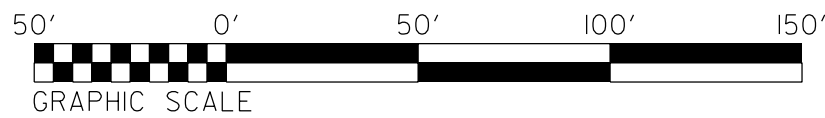
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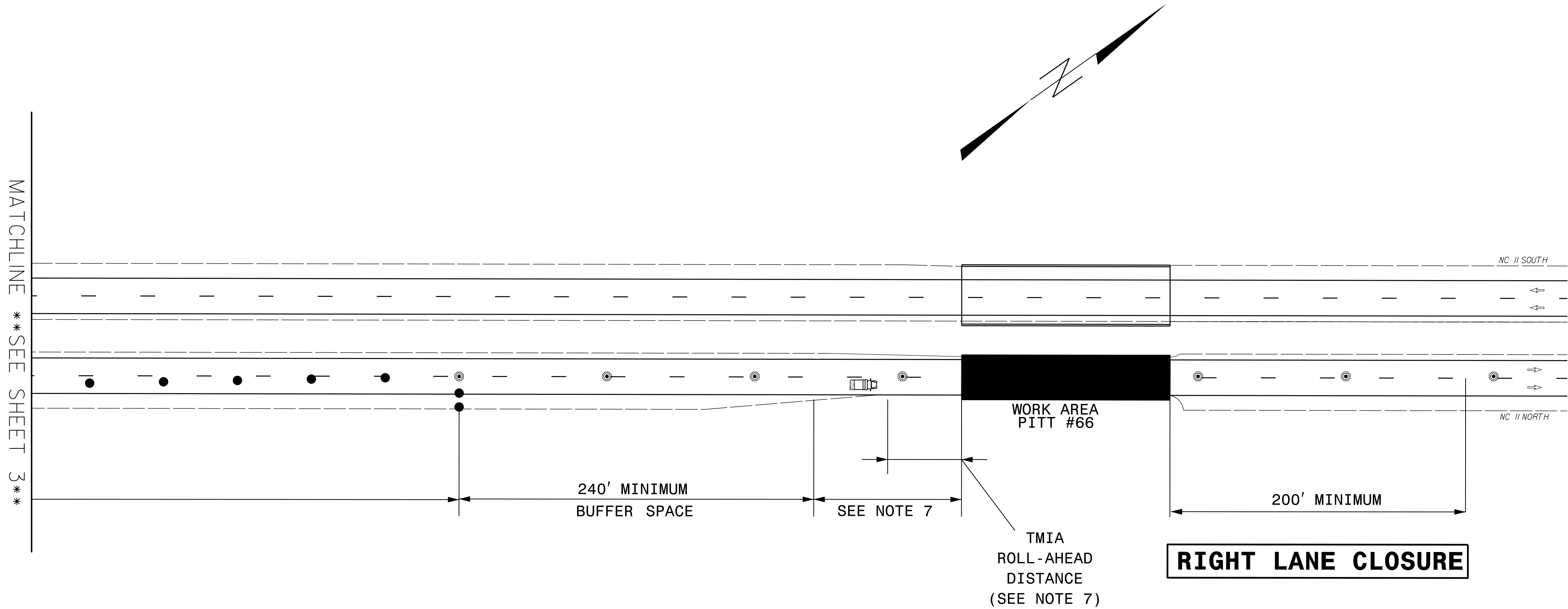
LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR(TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- SKINNY DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

CHANGEABLE MESSAGE SIGN (PROVIDED BY NCDOT)
(CONTACT JIM EVANS AT 252-830-3493 FOR COORDINATION)



APPROVED <i>Dayne H. Allgood</i> DATE <i>6/28/2019</i>		
SEAL		



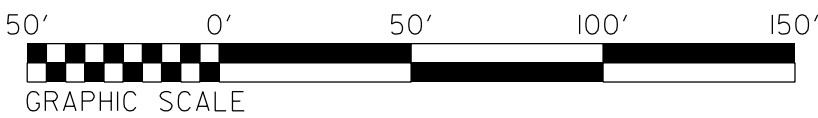
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LEGEND

- FLASHING ARROW PANEL (TYPE C)
- TRUCK MOUNTED IMPACT ATTENUATOR (TMIA)
- CHANGEABLE MESSAGE SIGN (CMS)
- DRUM
- SKINNY DRUM
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

CHANGEABLE MESSAGE SIGN (PROVIDED BY NCDOT)
(CONTACT JIM EVANS AT 252-830-3493 FOR COORDINATION)



APPROVED <i>Dianna H. Allgood</i> DATE <i>6/28/2019</i>		
SEAL		