



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

ROY COOPER
GOVERNOR

J. ERIC BOYETTE
SECRETARY

September 17, 2021

CONTRACT: DB00513
WBS ELEMENT: 2022CPT.02.05.20161
COUNTY: CARTERET
ROUTE: VARIOUS
DESCRIPTION: MILLING, STRENGTHENING, RESURFACING, AND
SHOULDER RECONSTRUCTION OF VARIOUS ROUTES IN
THE TOWN OF BEAUFORT IN CARTERET COUNTY

ADDENDUM 1

TO: PROSPECTIVE BIDDERS

Please note the following revisions to the proposal.

- Removal of the resurfacing of SR 1170 and associated quantities from the project.
- Revise Intermediate Contract Time Numbers 2 and 3 to reflect revised map numbers.
- Revise Notes to Contractor to reflect revised map numbers.
- Addition of Shoulder Wedge Provision to the proposal.
- Revised Plans to remove the resurfacing of SR 1170 from the project and revise the existing map numbers.
- Please see attached page A1-A15.
- A revised electronic file has been uploaded to bid express named DB00513.001.

Sincerely,

DocuSigned by:
Mary Voelker Moore
714C11DCCEBC4C6...

Mary Voelker Moore, PE
Division Contract Engineer

cc: Ms. Mary Beth Houston, PE
Mr. Jeremy Stroud, PE
Mr. Brad McMannen, PE
Ms. Heather Lane, PE
Mr. Cadmus Capehart, PE
Mr. Jeff Cabaniss, PE

County : ~~Cartersville~~ DB00513

A1

CRAVEN

Line #	Item Number	Sec #	Description	Quantity	Unit Cost	Amount
ROADWAY ITEMS						
0001	0000100000-N	800	MOBILIZATION	Lump Sum	L.S.	
0002	0262000000-N	SP	GENERIC GRADING ITEM HAULING NCDOT SUPPLIED SHOULDER MATERIAL	68 EA		
0003	1220000000-E	545	INCIDENTAL STONE BASE	103 TON		
0004	1245000000-E	SP	SHOULDER RECONSTRUCTION	3.38 SMI		
0005	1297000000-E	607	MILLING ASPHALT PAVEMENT, **** DEPTH 1.75" MILLING	77,000 SY		
0006	1330000000-E	607	INCIDENTAL MILLING	1,600 SY		
0007	1503000000-E	610	ASPHALT CONC INTERMEDIATE COURSE, TYPE I19.0C	800 TON		
0008	1519000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5B	9,700 TON		
0009	1575000000-E	620	ASPHALT BINDER FOR PLANT MIX	689 TON		
0010	1880000000-E	SP	GENERIC PAVING ITEM 4" DEPTH MILL PATCHING EXISTING PAVEMENT - B 25.0 C	150 TON		
0011	2752000000-E	SP	GENERIC PAVING ITEM 2'6" CURB & GUTTER - REMOVE/REPLACE	219 LF		
0012	2759000000-N	SP	GENERIC PAVING ITEM CONCRETE CURB RAMP - REMOVE/REPLACE	14 EA		
0013	2830000000-N	858	ADJUSTMENT OF MANHOLES	20 EA		
0014	2845000000-N	858	ADJUSTMENT OF METER BOXES OR VALVE BOXES	4 EA		
0015	4413000000-E	SP	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	400 SF		
0016	4457000000-N	SP	TEMPORARY TRAFFIC CONTROL	Lump Sum	L.S.	
0017	6000000000-E	1605	TEMPORARY SILT FENCE	660 LF		

County : Carter

A2

CRAVEN

Line #	Item Number	Sec #	Description	Quantity	Unit Cost	Amount
0018	6071010000-E	SP	WATTLE	120	LF	
0019	6084000000-E	1660	SEEDING & MULCHING	4.13	ACR	
0020	6117000000-N	SP	RESPONSE FOR EROSION CONTROL	1	EA	

1216/Sep17/Q91557.51/D56159110000/E20

Total Amount Of Bid For Entire Project :

INTERMEDIATE CONTRACT TIME NUMBER 2 AND LIQUIDATED DAMAGES:

(2-20-07)

108

SP1 G14 D

The Contractor shall complete the required work of installing, maintaining and removing the traffic control devices for road closures and restoring traffic to the existing traffic pattern. The Contractor shall not close **Maps #2, #3, and #4** during the following time restrictions:

DAY AND TIME RESTRICTIONS

No Work Between Memorial Day and Labor Day

The time of availability for this intermediate contract time will be the time the Contractor begins to install traffic control devices required for road closures according to the time restrictions stated herein.

The completion time for this intermediate contract time will be the time the Contractor is required to complete the removal of traffic control devices required for the road closures according to the time restrictions stated herein and restore traffic to the existing traffic pattern.

The liquidated damages are **Eight Hundred Fifty Dollars (\$ 850.00)** per hour.

INTERMEDIATE CONTRACT TIME NUMBER 3 AND LIQUIDATED DAMAGES:

(2-20-07)

108

SP1 G14 C

The Contractor shall complete the required work of installing, maintaining and removing the traffic control devices for lane closures and restoring traffic to the existing traffic pattern. The Contractor shall not close or narrow a lane of traffic on **Map #1** during the following time restrictions:

DAY AND TIME RESTRICTIONS

**One Hour Before School Starts to 30 Minutes After School Starts
One Hour Before School Dismissal to 30 Minutes After School Dismissal
Monday to Friday
School Days Only**

The time of availability for this intermediate contract time will be the time the Contractor begins to install traffic control devices required for the lane closures according to the time restrictions stated herein.

The completion time for this intermediate contract time will be the time the Contractor is required to complete the removal of traffic control devices required for the lane closures according to the time restrictions stated herein and restore traffic to the existing traffic pattern.

The liquidated damages are **Eight Hundred Fifty Dollars (\$ 850.00)** per hour

PERMANENT VEGETATION ESTABLISHMENT:

(2-16-12) (Rev. 10-15-13)

104

SP1 G16

Establish a permanent stand of the vegetation mixture shown in the contract. During the period between initial vegetation planting and final project acceptance, perform all work necessary to establish permanent vegetation on all erodible areas within the project limits, as well as, in borrow and waste pits. This work

PROJECT SPECIAL PROVISIONS

ROADWAY

NOTE TO CONTRACTOR

Map #3 (SR 1493) allows on-street parking. It is the Contractor's responsibility to ensure that these spaces are clear for paving.

Map #4 (SR 1493) includes the mill and fill of NC 101 from SR 1493 (Live Oak Street) north for approximately 300 feet.

All Concrete work, Drainage Structure Adjustments, Manhole Adjustments, and Valve Adjustments shall be performed before the final surface layer of asphalt is placed.

SHOULDER RECONSTRUCTION PER SHOULDER MILE:

(11-16-10) (Rev. 8-21-12)

560

SP1 R07AR (Rev)

Description

This work consists of reconstructing each shoulder (including median shoulders as applicable) in accordance with Standard Drawing No. 560.01 and 560.02 of the *2018 Roadway Standard Drawings* except that the rate of slope and width will be as shown on typical section, or to the existing shoulder point, whichever is nearer, as long as the desired typical is achieved, and when completed, seeding and mulching. This work shall be performed immediately after the resurfacing operations are complete as directed by the Engineer.

This project will require the removal of excess material from the existing pavement by mechanical means prior to paving routes. Excess material generated by clipping excessive shoulder material from the existing pavement will be required to be removed by the Contractor. The Contractor should take care in removing excessive material from the existing pavement to minimize the amount of disturbance to adjacent established vegetation. There will be no direct payment for the removal of any excess material generated from the existing pavement as this work shall be considered incidental to the asphalt pavement line items that correspond with that map.

Quantities for Shoulder Reconstruction are included to address drop off issues identified by the Engineer after paving is completed. Shoulder Reconstruction shall not be performed unless directed by the Engineer.

Materials

The NCDOT will furnish all earth material necessary for the construction of the shoulders at the Carteret County Maintenance Yard located at 139 Masontown Road in Newport.

Hauling

SHOULDER WEDGE:

(9-20-11) (Rev. 8-21-12)

610

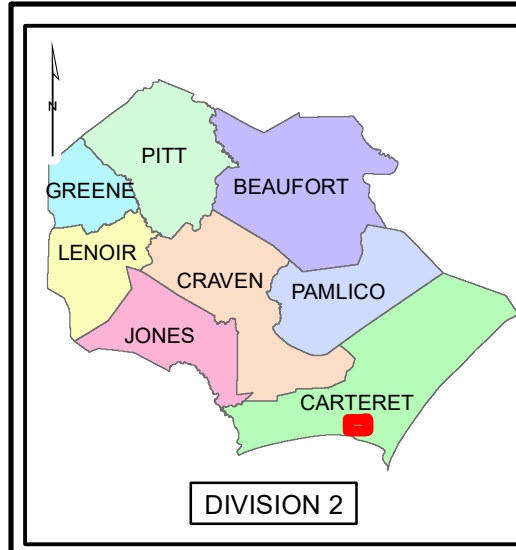
SP6 R03R

Revise the *2018 Standard Specifications* as follows:

Page 6-21, Article 610-8, SPREADING AND FINISHING, add the following after line 39:

Attach a device, mounted on screed of paving equipment, capable of constructing a shoulder wedge with an angle of 30 degrees plus or minus 4 degrees along the outside edge of the roadway, measured from the horizontal plane in place after final compaction on the final surface course. Use an approved mechanical device which will form the asphalt mixture to produce a wedge with uniform texture, shape and density while automatically adjusting to varying heights.

Payment for use of this device will be incidental to the other pay items in the contract.



CARTERET COUNTY

DB00513

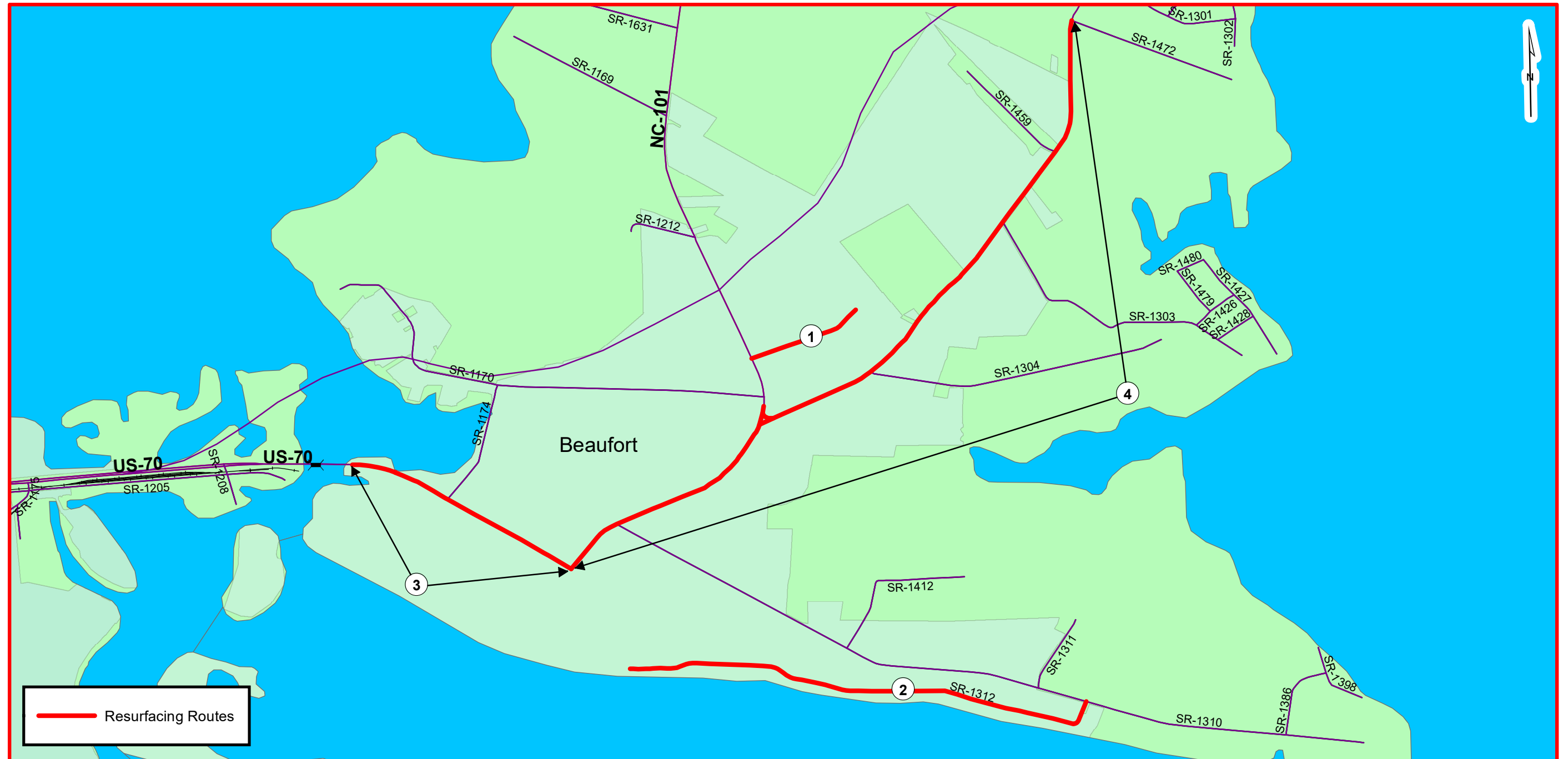
WBS# 2022CPT.02.05.20161

PROJECT REFERENCE NO.	SHEET NO.
DB00513	1

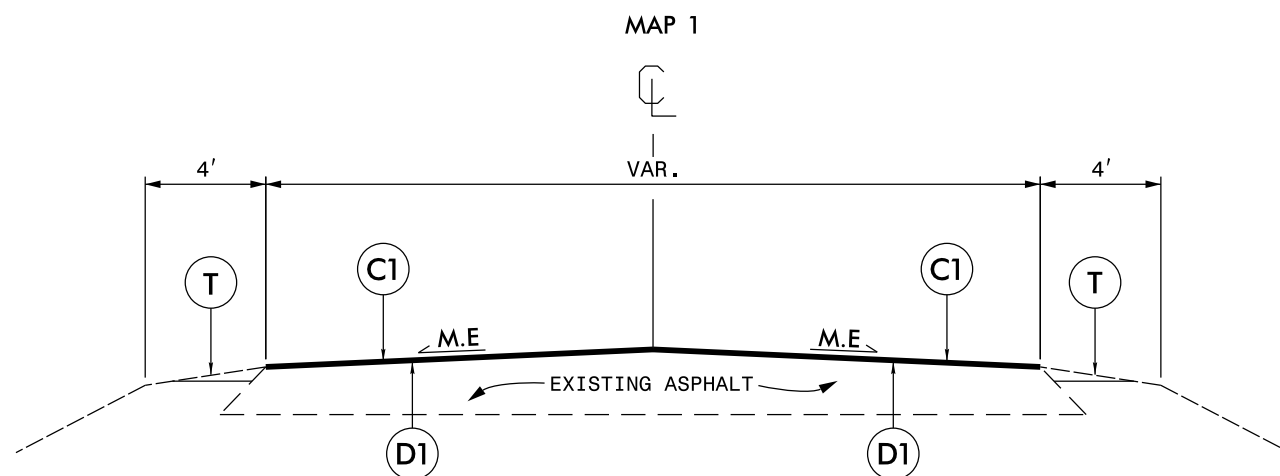


NCDOT
DIVISION 2

TYPE OF WORK: MILLING, STRENGTHENING, RESURFACING, SHOULDER RECONSTRUCTION



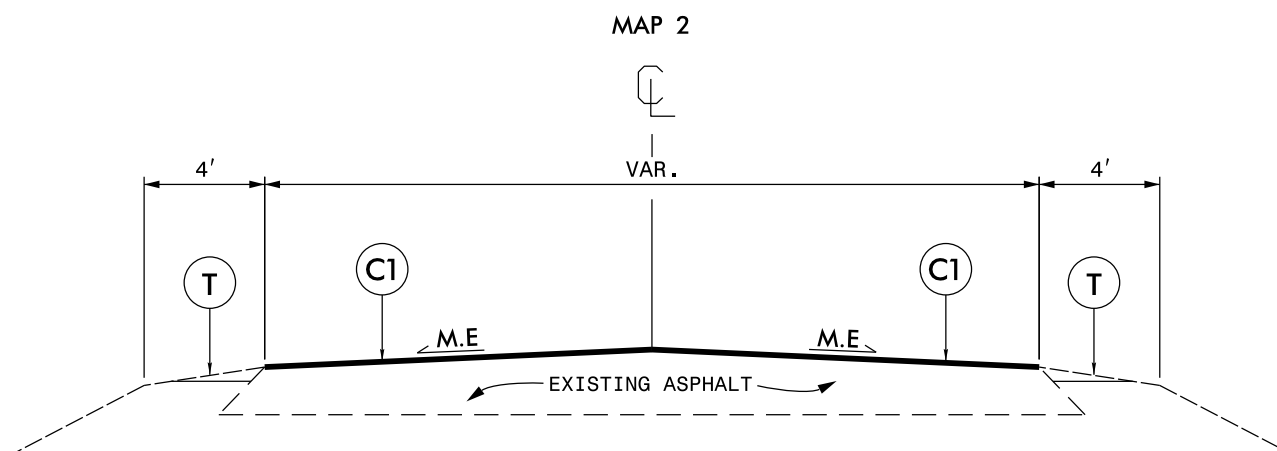
TYPICAL SECTION NO. 1



NOTE:

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
3. PERFORM SHOULDER RECONSTRUCTION AFTER PAVING IS COMPLETED.

TYPICAL SECTION NO. 2



NOTE:

1. PERFORM 4" DEPTH MILL PATCHING AT LOCATIONS AND WIDTHS AS SHOWN ON SHEET 4. PLACE ASPHALT BASE COURSE B25.0C IN ONE LIFT TO BACKFILL.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
3. PERFORM SHOULD RECONSTRUCTION AFTER PAVING IS COMPLETED.

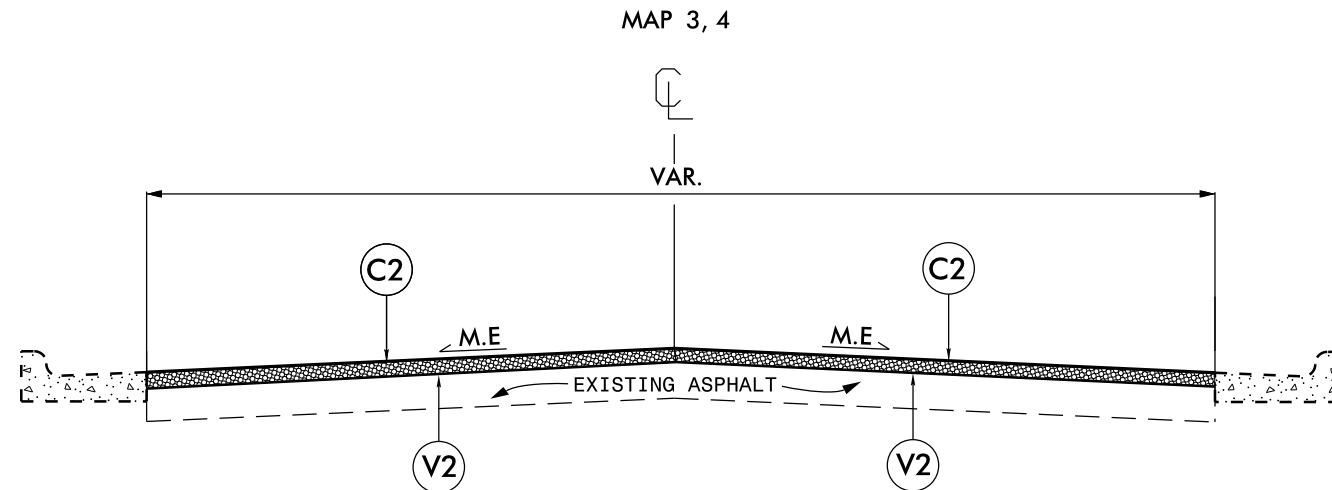
PAVEMENT SCHEDULE

C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.75" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 192.5 LBS. PER SQ. YD.
D1	PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
V1	INCIDENTAL MILLING.
V2	MILLING DEPTH 1.75" FOR THE ENTIRE WIDTH OF ROADWAY.
T	SHOULDER RECONSTRUCTION.

DRAWINGS NOT TO SCALE

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

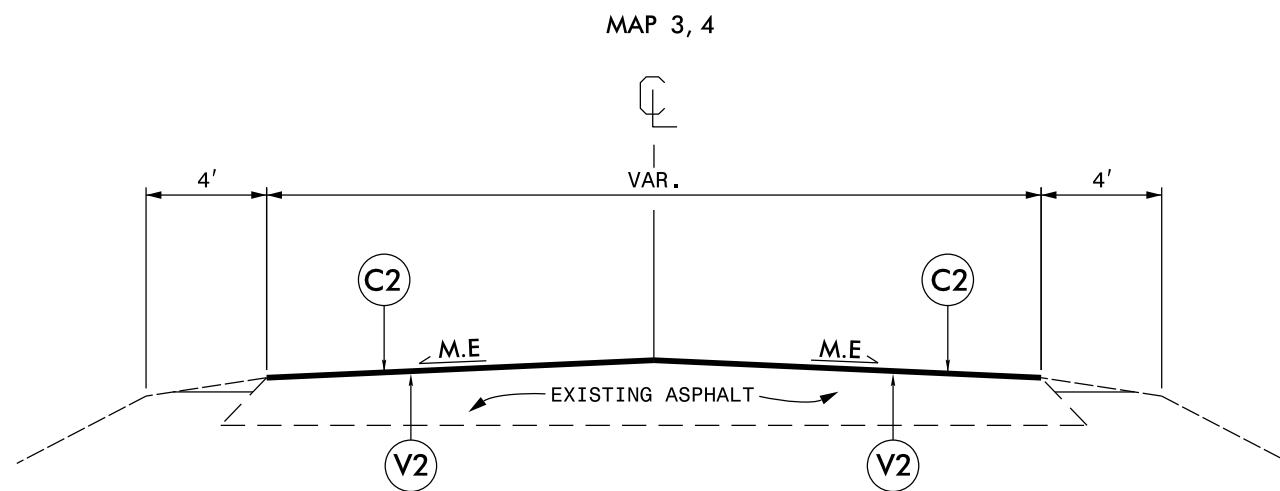
TYPICAL SECTION NO. 3



NOTE:

1. PERFORM 1.75" DEPTH MILLING FROM CURB AND GUTTER TO CURB AND GUTTER, FULL WIDTH.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.

TYPICAL SECTION NO. 4



NOTE:

1. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT.
2. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.75" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 192.5 LBS. PER SQ. YD.
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T	SHOULDER RECONSTRUCTION.

DRAWINGS NOT TO SCALE

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

PROJECT NO.	SHEET NO.	TOTAL NO.
DB00513	4	

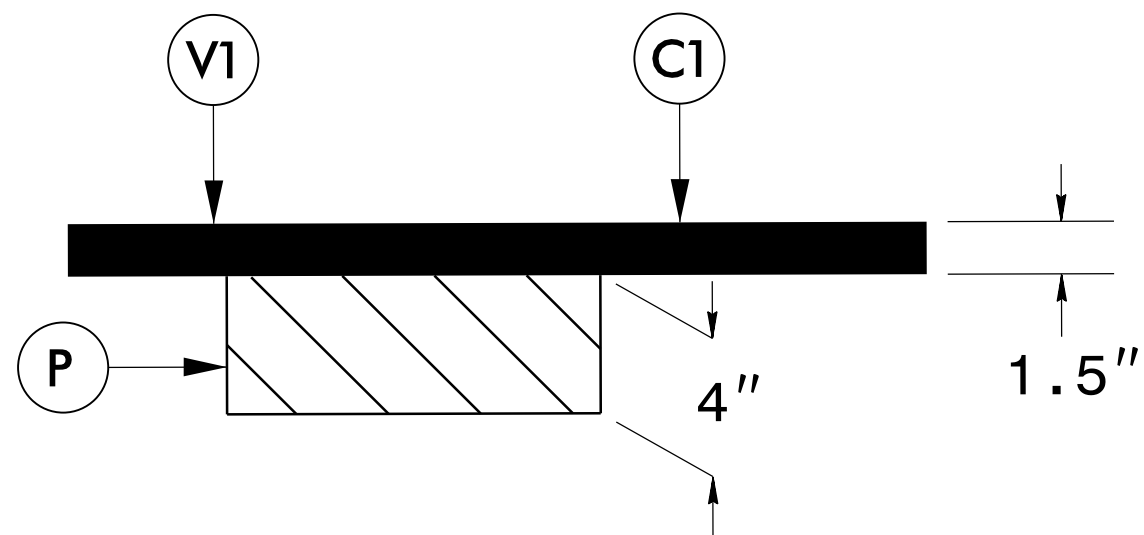
SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0262000000-N	1220000000-E	1245000000-E	1297000000-E	1330000000-E	1503000000-E	1519000000-E	1575000000-E	1880000000-E	2752000000-E	2759000000-N	2830000000-N	2845000000-N	6000000000-E	6071010000-E	6084000000-E	6117000000-N	4413000000-E	4457000000-N		
												HAULING NCDOT SUPPLIED SHOULDER MATERIAL	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1.75" MILLING	INCIDENTAL MILLING	INTERMEDIATE COURSE, 119.0C	SURFACE COURSE, 99.58	ASPHALT BINDER FOR PLANT MIX	4" DEPTH MILL PATCHING EXISTING PAVEMENT - B 25.0 C	2'5" CURB & GUTTER - REMOVE/REPLACE	CONCRETE CURB RAMP - REMOVE/REPLACE	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	TEMPORARY SILT FENCE	WATTLE	SEED & MULCHING	RESPONSE FOR EROSION CONTROL	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL		
												EA	TONS	SMI	SY	SY	TONS	TONS	TONS	TONS	TONS	LF	EA	EA	EA	EA	LF	LF	AC	EA	SF	LS
2022CPT.02.05.20161	Carteret	1	SR 1299 - CARRAWAY RD	FROM NC 101 TO CAMPEN RD	1	2	2WU	NO	NO	0.35	25	14	18	0.70		500	800	500	72						56	40	0.35		40	0.07		
TOTAL FOR MAP NO. 1												0.35	14	18	0.70		500	800	500	72				56	40	0.35		40	0.07			
2022CPT.02.05.20161	Carteret	2	SR 1312 - FRONT ST	FROM HILL ST TO SR 1310 LENNOXVILLE RD	2	2	2WU	NO	NO	1.34	20	54		2.68	300		1,500	101	150				8	4	214	40	1.34		150	0.28		
TOTAL FOR MAP NO. 2												1.34	54		2.68	300		1,500	101	150			8	4	214	40	1.34		150	0.28		
2022CPT.02.05.20161	Carteret	3	SR 1493 - CEDAR ST	FROM DEAD END TO SR 1493 LIVE OAK RD	3,4	2	MU	NO	NO	0.70	40			17,000	300		1,700	114		53	12							80	0.14			
TOTAL FOR MAP NO. 3												0.70				17,000	300		1,700	114		53	12							80	0.14	
2022CPT.02.05.20161	Carteret	4	SR 1493 - LIVE OAK ST	FROM CEDAR ST TO SR 1472 PIVER RD	3,4	2	MU	NO	NO	2.44	35		85	60,000	500		6,000	402		166	2	12			390	40	2.44	1	130	0.51		
TOTAL FOR MAP NO. 4												2.44	85	60,000	500		6,000	402		166	2	12		390	40	2.44	1	130	0.51			
TOTAL FOR PROJ NO. 2022CPT.02.05.20161												4.83	68	103	3	77,000	1,600	800	9,700	689	150	219	14	20	4	660	120	4.13	1	400	1.00	
GRAND TOTAL												4.83	68	103	3.38	77,000	1,600	800	9,700	689	150	219	14	20	4	660	120	4.13	1	400	1	

4" MILL PATCH	STA.	STA.	WIDTH	MAP
	0+10	0+52	8' RT	2
	4+01	6+23	8' RT	2
	20+33	22+96	8' RT	2
CURB & GUTTER REPAIR	STA.	STA.	LENGTH	MAP
	6+13	6+33	20' RT	3
	25+47	25+66	19' RT	3
	25+47	25+61	14' LT	3
	19+10	19+18	8' LT	4
	26+10	27+20	110' LT	4
	26+53	26+84	31 RT	4
	27+06	27+23	17 RT	4

4" DEPTH MILL PATCHING DETAIL

MAP 2

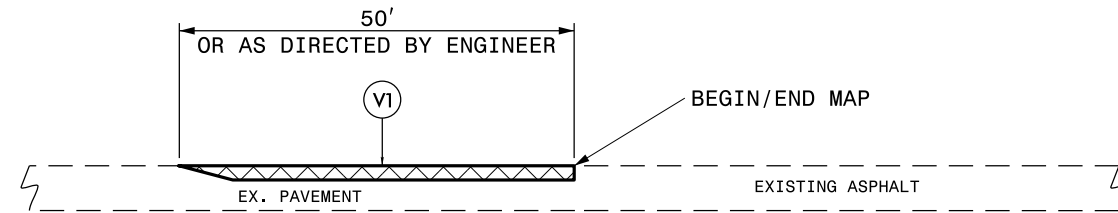


PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 165.0 LBS. PER SQ. YD.
V1	INCIDENTAL MILLING
P	4" DEPTH MILL PATCHING W/ B 25.0C
DRAWINGS NOT TO SCALE	

NOTE:

1. THE CONTRACTOR SHALL PERFORM ANY UNIFORM OR INCIDENTAL MILLING AT TIE-INS BEFORE PERFORMING THE 4" DEPTH MILL PATCHING.
2. THE CONTRACTOR SHALL PERFORM THE MILL PATCHING REMOVAL AND REPLACEMENT IN THE SAME DAY.
3. 4" DEPTH MILL PATCHING SHALL BE PERFORMED AT LOCATIONS AS SHOWN ON SHEET 4, AND AS DIRECTED BY THE ENGINEER.

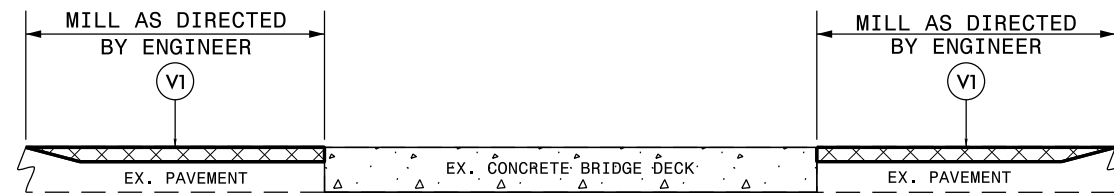
MILLING TYPICALS



DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:

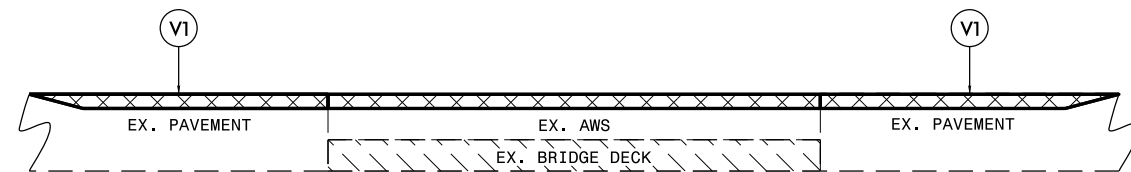
- MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

NOTE:

- MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

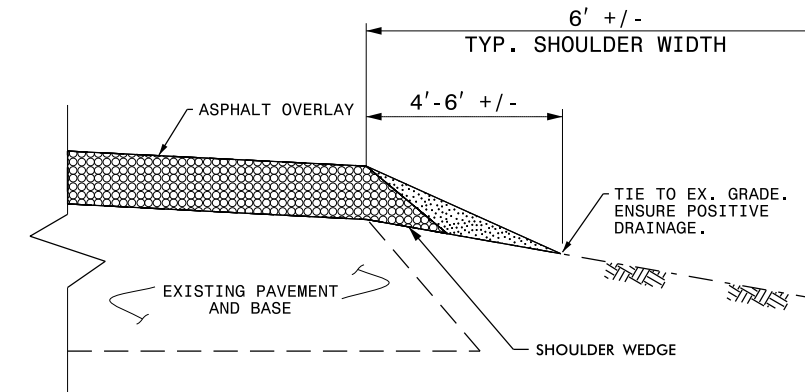


DETAIL 3
BRIDGE MILLING

NOTE:

- INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

SHOULDER RECONSTRUCTION TYPICAL

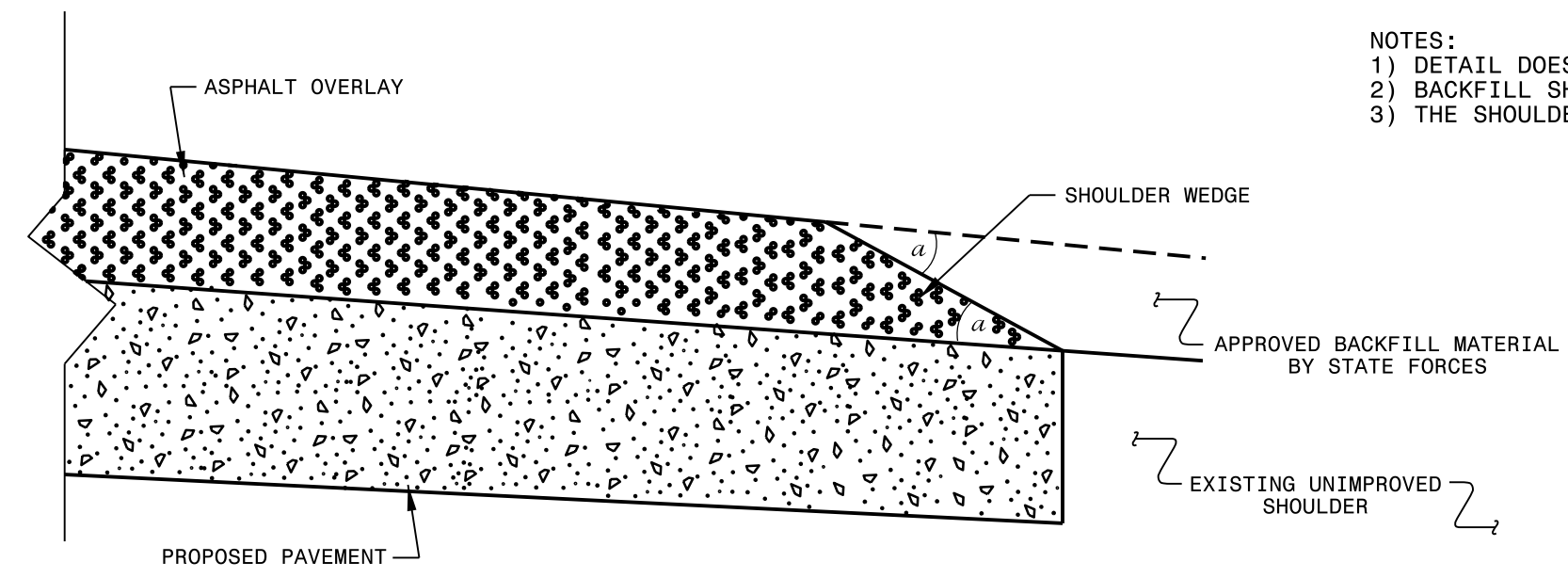


SHOULDER RECONSTRUCTION DETAIL

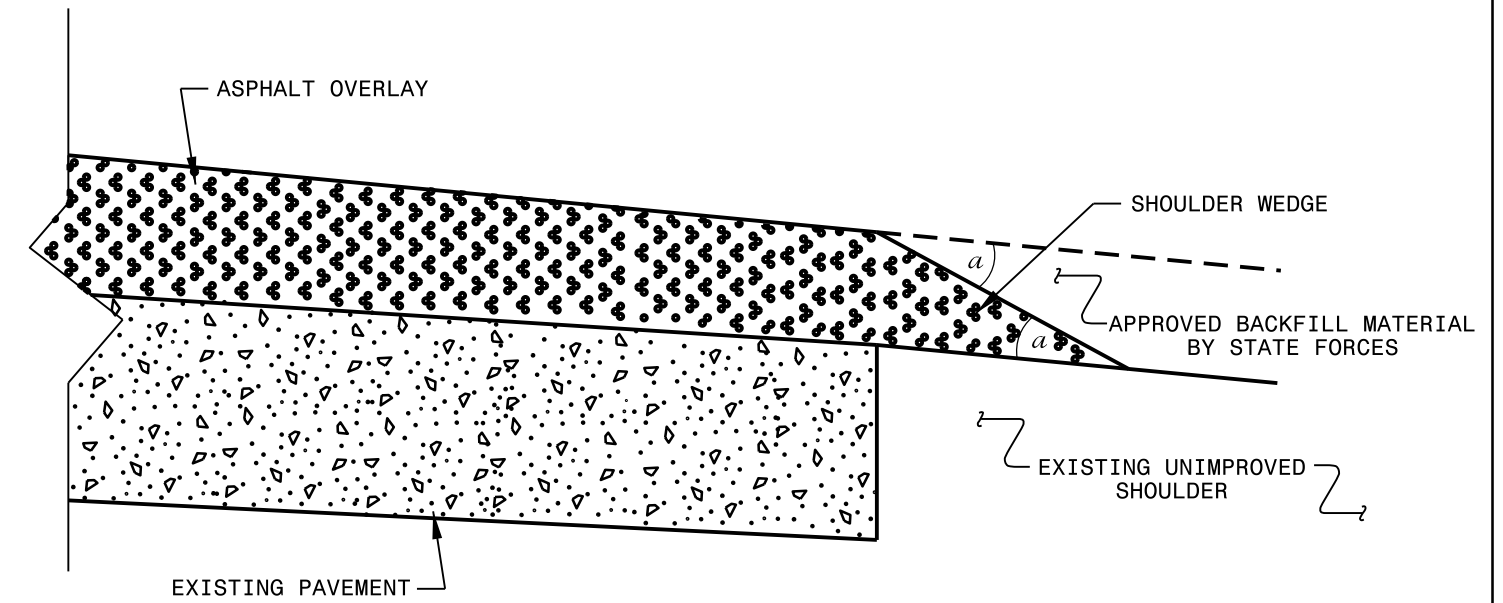
NOTE:

- SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

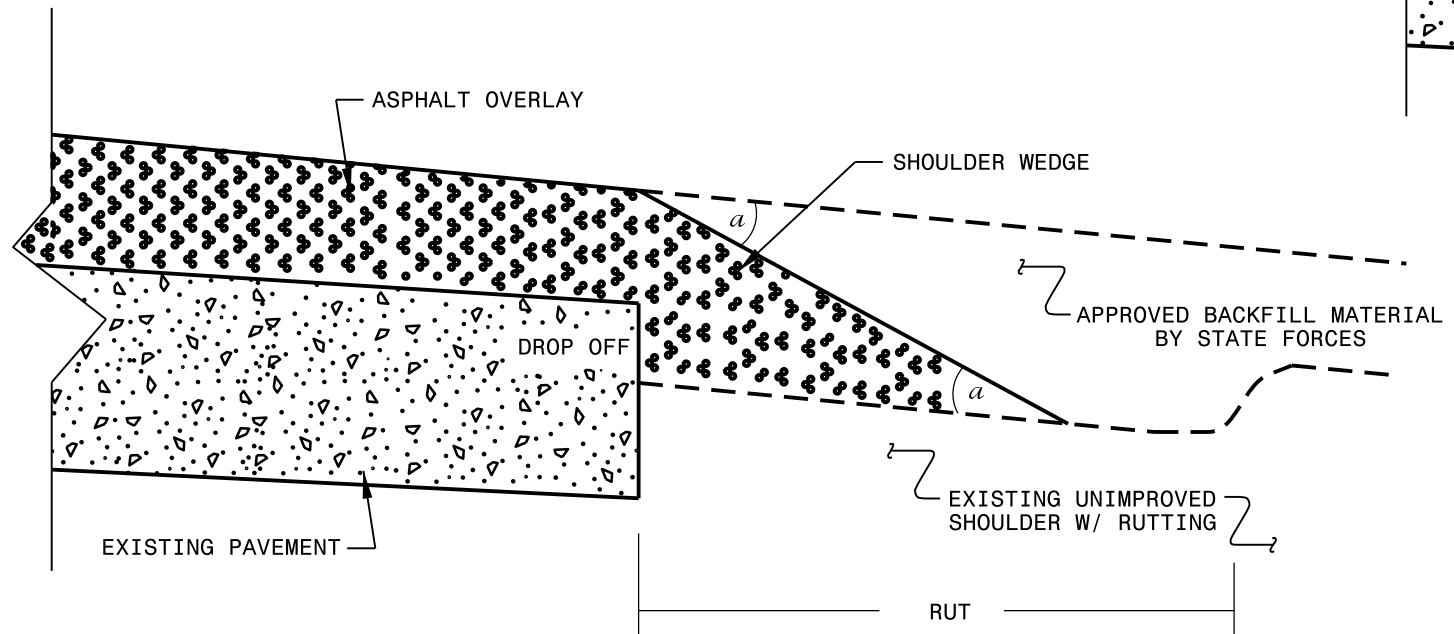
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

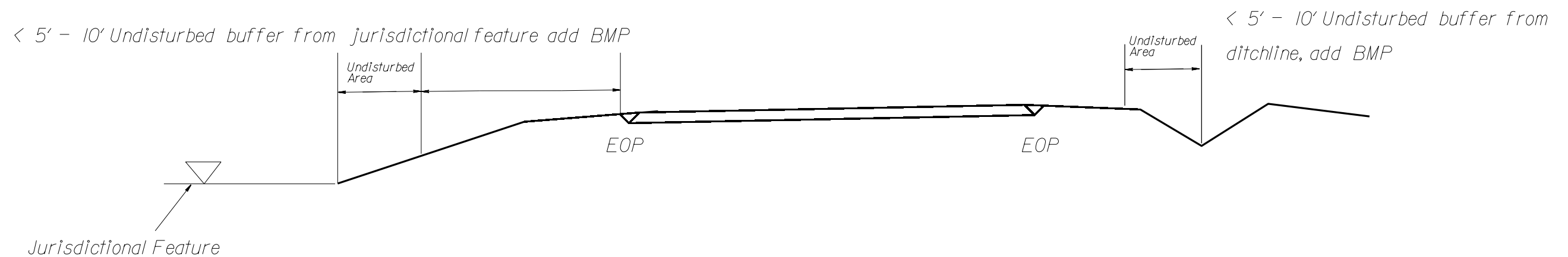
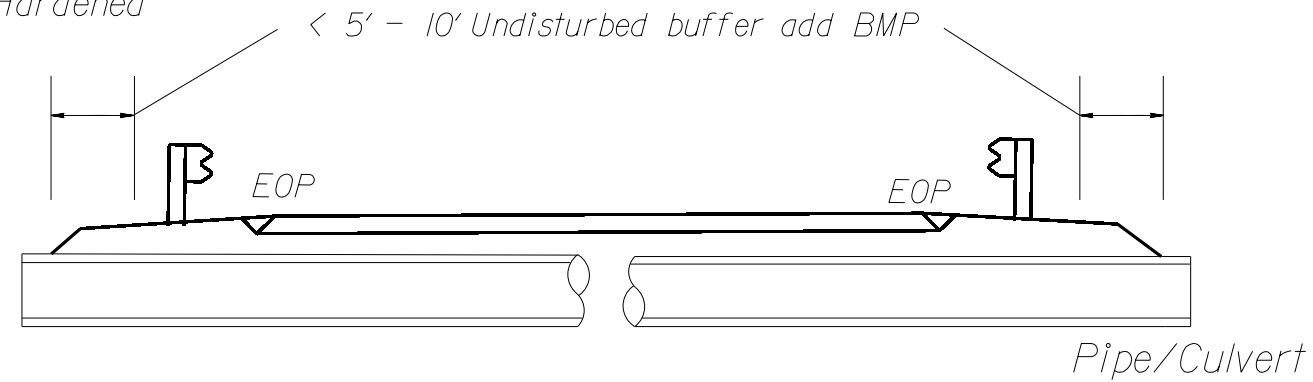
- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T. SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn	

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

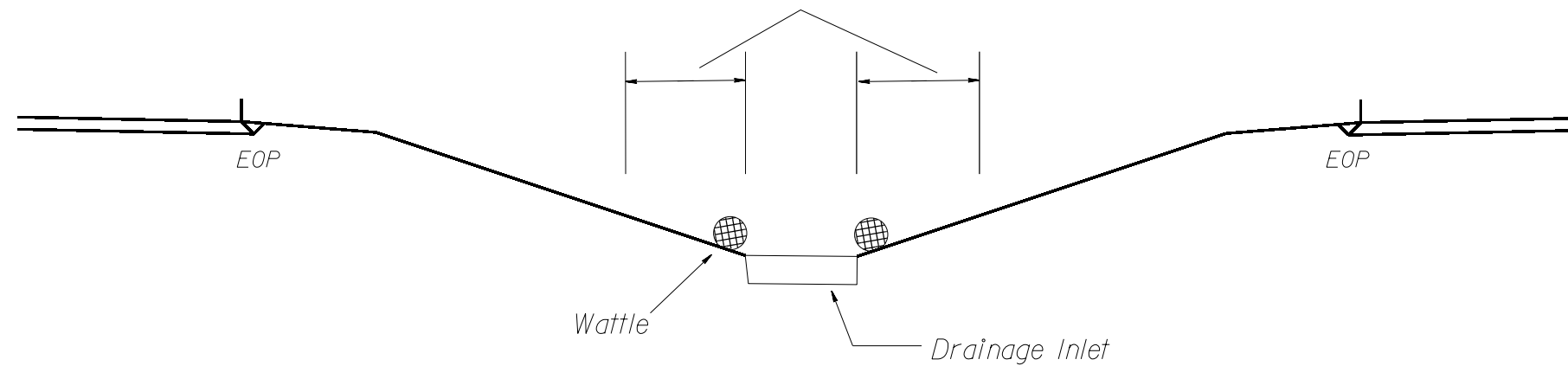
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

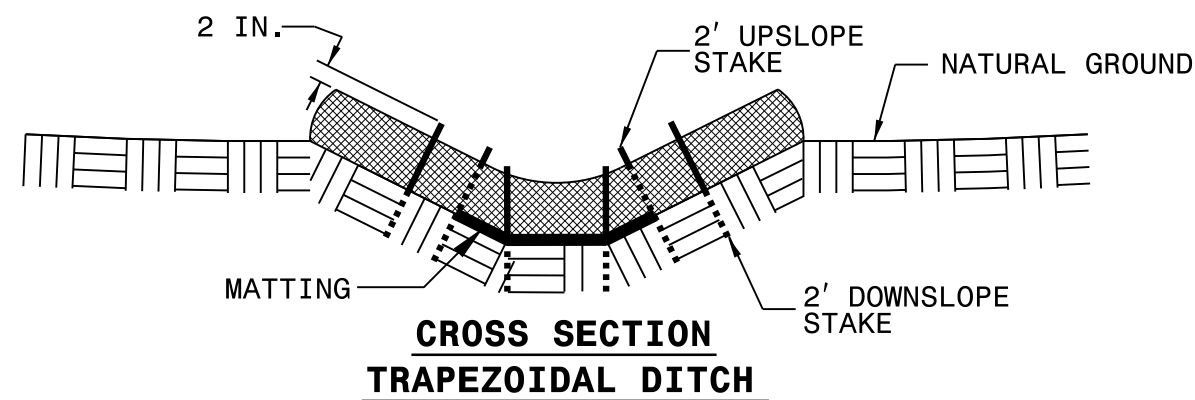
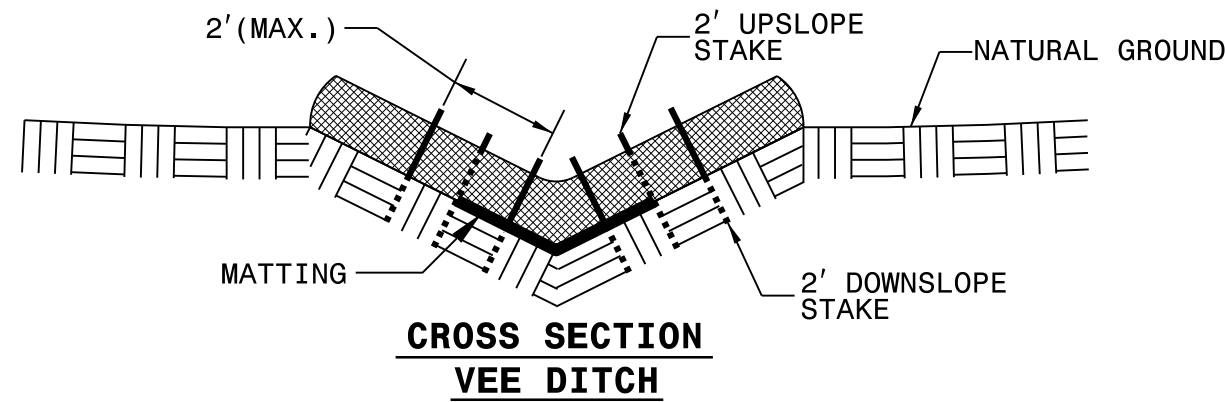
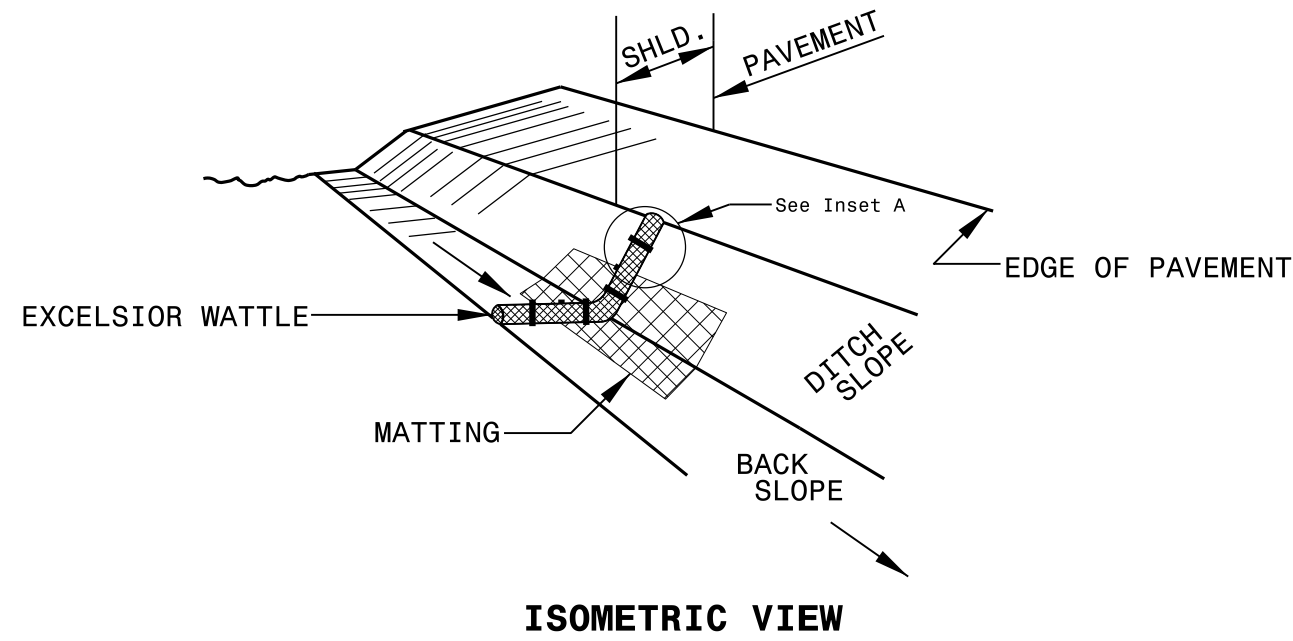


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

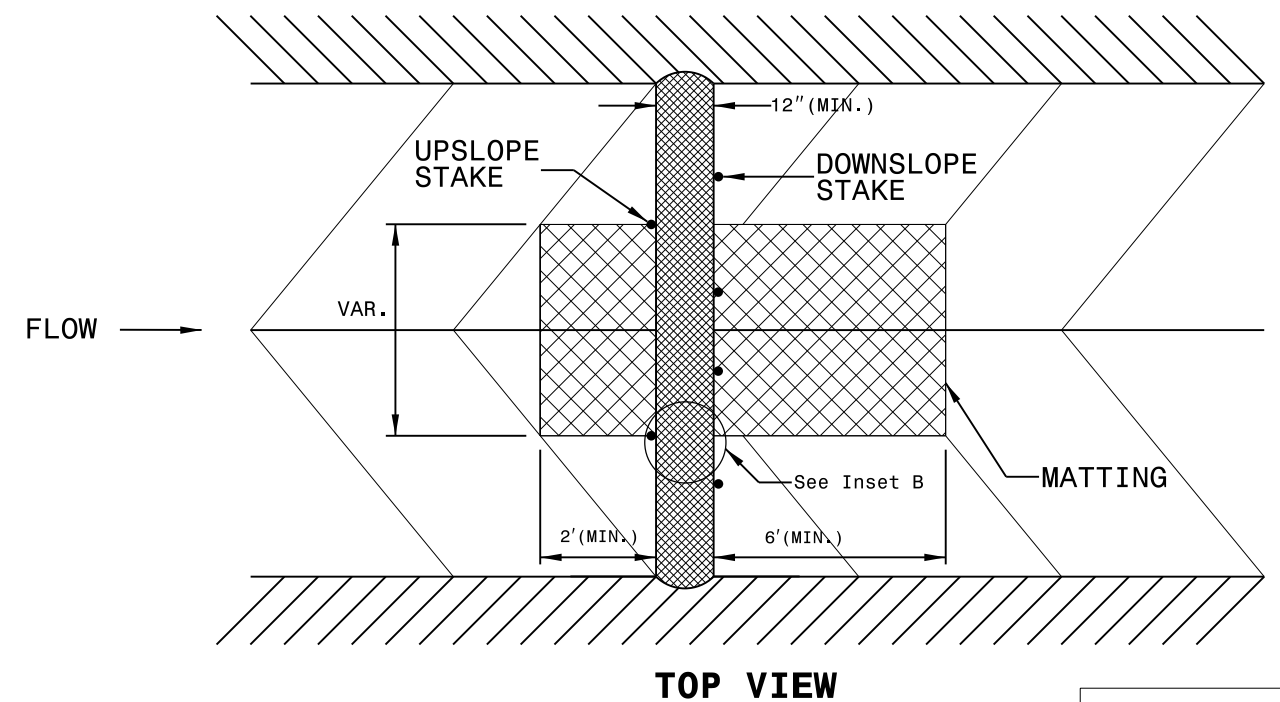
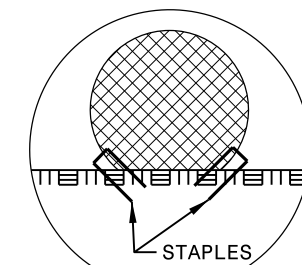
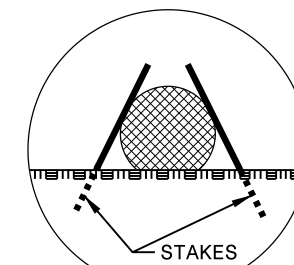
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

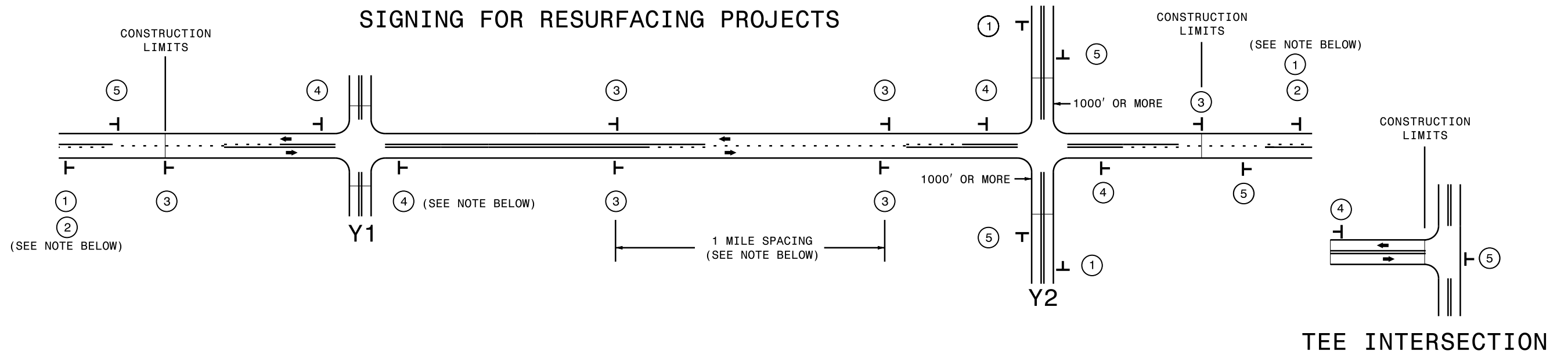
INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE

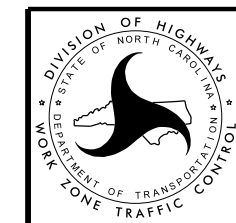
SIGNING FOR RESURFACING PROJECTS



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. #2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> </div> <div style="text-align: center;"> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.		



RESURFACING
 ADVANCE WARNING SIGNS
 FOR
 RURAL AND SUBURBAN
 2 LANE ROADWAYS