

CONTRACT: DB00085 PROJECT: WBS# 2B.100711

STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS

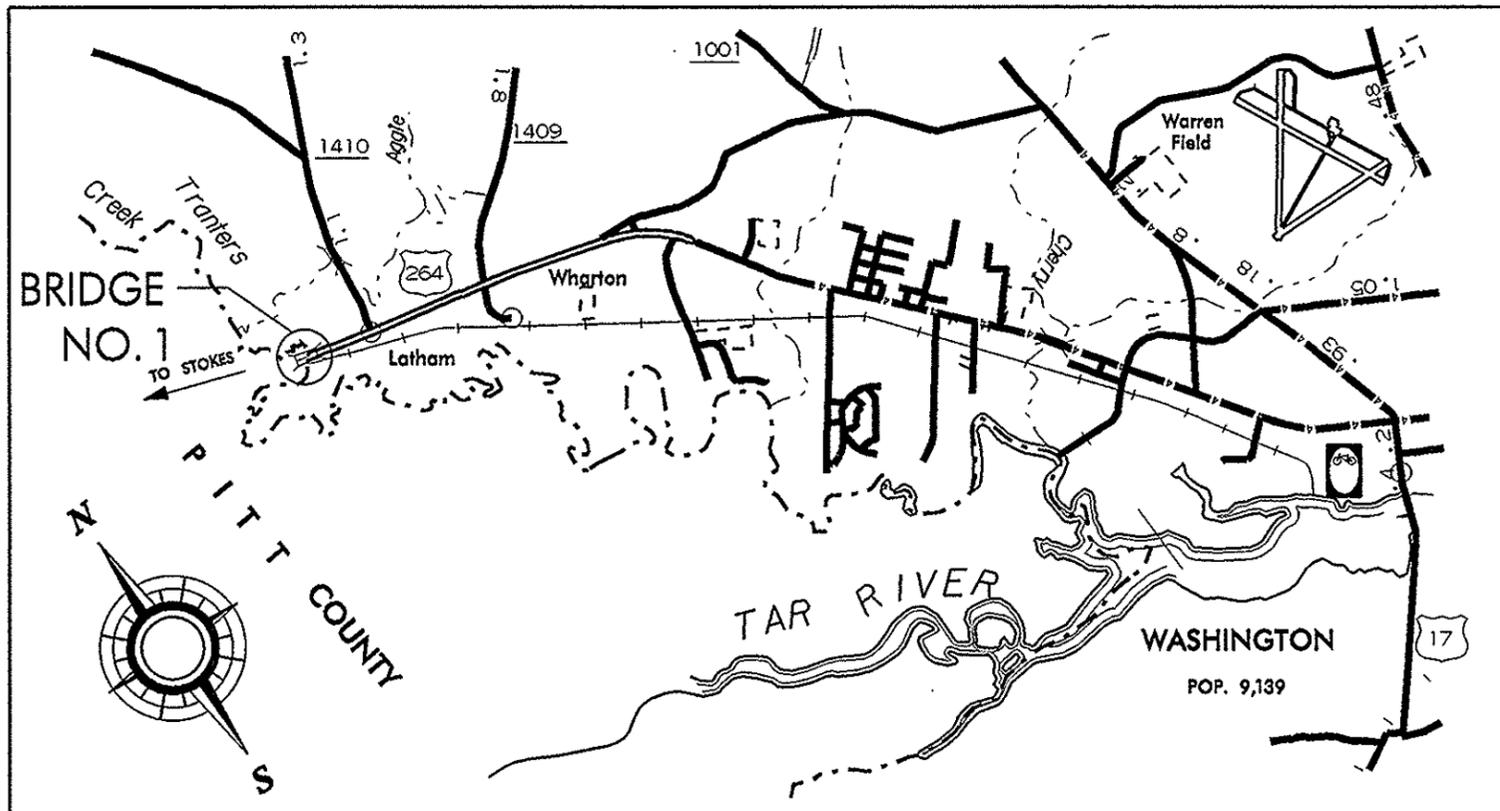


BEAUFORT COUNTY

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.	TOTAL SHEETS
N.C.	WBS 2B.1007111	1	30
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION	

LOCATION: BRIDGE #1, BEAUFORT CO., ON US264 WB ACROSS TRANTER'S CREEK
TYPE OF WORK: REPAIR FOR BENT NO.2

INDEX OF SHEETS:
 1-4: STRUCTURE SHEETS
 SN: STANDARD NOTES
 S1-S24: EXISTING BRIDGE SHEETS

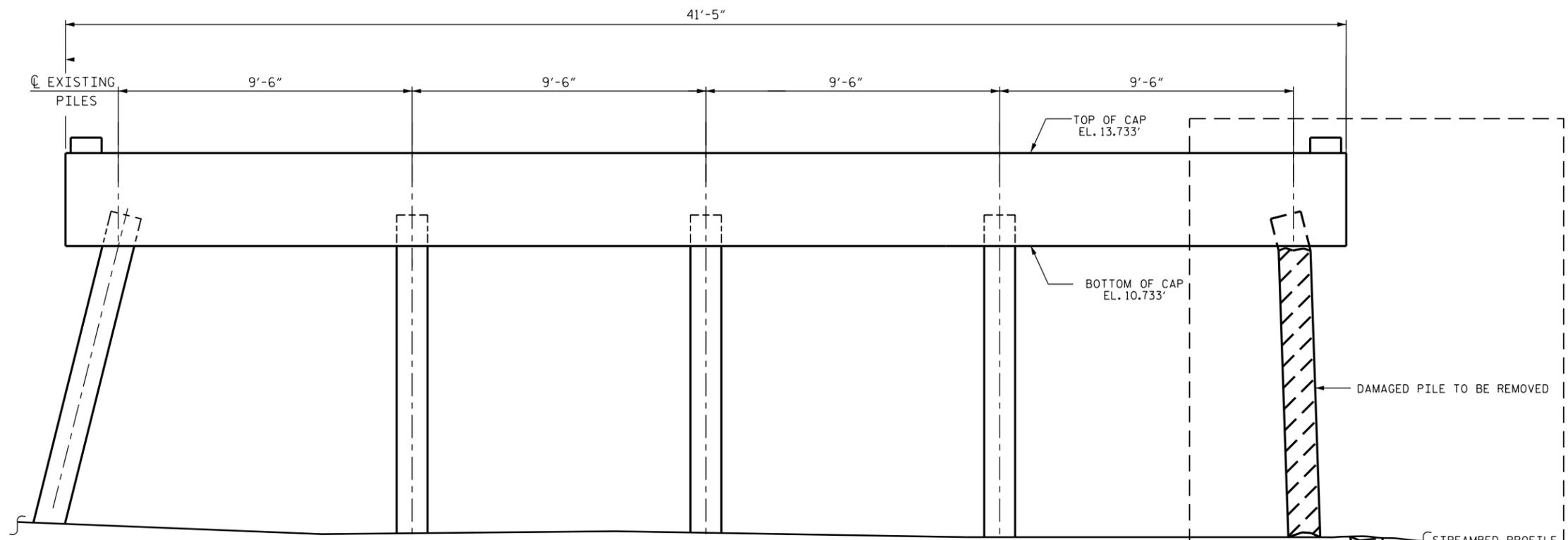


DESIGN DATA
 BEAUFORT
 #1 ADT 2008 = 8000

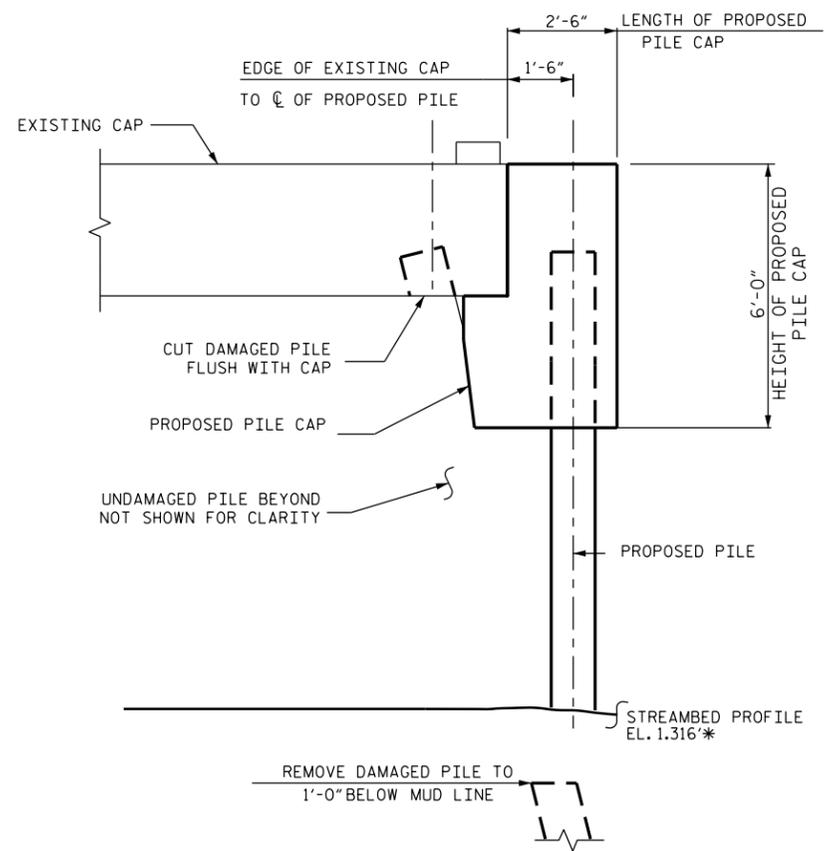
PROJECT LENGTH
 BRIDGE BEAUFORT #1 = .062 MILE

Prepared in the Office of:
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
 STRUCTURES MANAGEMENT UNIT - PRESERVATION & REPAIR GROUP
 1000 BIRCH RIDGE DR. RALEIGH, N.C. 27610
RICK NELSON, P.E.
 2012 STANDARD SPECIFICATIONS
LETTING DATE:
 AUGUST 2012

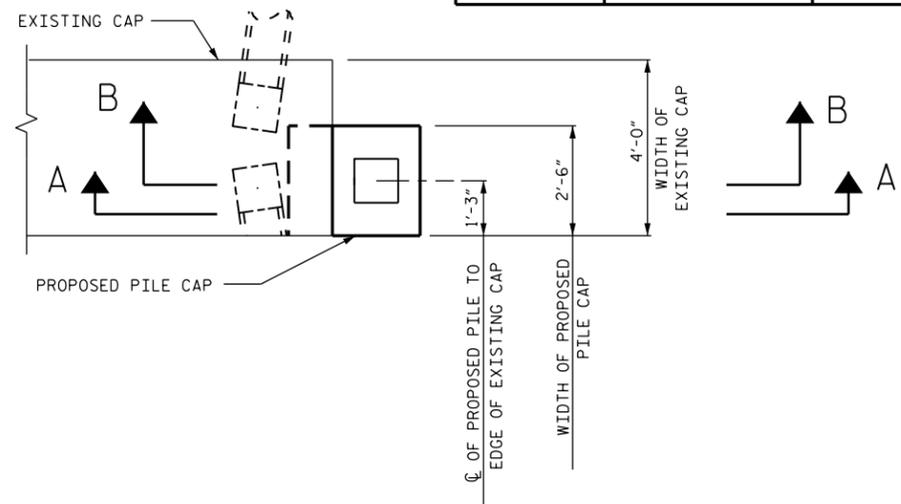
DANIEL MULLER, P.E.
 PROJECT DESIGN ENGINEER



EXISTING ELEVATION



PROPOSED PARTIAL ELEVATION



PROPOSED PARTIAL PLAN

TOTAL BILL OF MATERIAL				
MOBILIZATION	CLASS A CONCRETE	REINFORCING STEEL	DRIVING OF PRESTRESSED CONCRETE PILE	PARTIAL REMOVAL OF EXISTING STRUCTURE
LUMP SUM	1.38 CY	322 LBS	LUMP SUM	LUMP SUM

GENERAL NOTES:

EXISTING BRIDGE AND REPAIR DETAILS INDICATED ON PLANS ARE FROM THE BEST INFORMATION AVAILABLE. SINCE THIS INFORMATION IS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL HAVE NO CLAIM WHATSOEVER AGAINST THE DEPARTMENT OF TRANSPORTATION FOR ANY DELAYS OR ADDITIONAL COST INCURRED BASED ON DIFFERENCES SHOWN ON THE PLANS AND THE ACTUAL CONDITIONS AT THE PROJECT SITE.

FOR SUBMITTAL OF WORKING DRAWINGS, SEE SPECIAL PROVISIONS.

FOR FALSEWORK AND FORMWORK, SEE SPECIAL PROVISIONS.

FOR CRANE SAFETY, SEE SPECIAL PROVISIONS.

THE CONCRETE IN THE PROPOSED PILE CAP SHALL CONTAIN SILICA FUME. SILICA FUME SHALL BE SUBSTITUTED FOR 5% OF THE PORTLAND CEMENT BY WEIGHT. IF THE OPTION OF ARTICLE 1024-1 OF THE STANDARD SPECIFICATIONS TO PARTIALLY SUBSTITUTE CLASS F FLY ASH FOR PORTLAND CEMENT IS EXERCISED, THEN THE RATE OF FLY ASH SUBSTITUTION SHALL BE REDUCED TO 1.0 LB OF FLY ASH PER 1.0 LB OF CEMENT. NO PAYMENT WILL BE MADE FOR THIS SUBSTITUTION AS IT IS CONSIDERED INCIDENTAL TO THE VARIOUS PAY ITEMS.

DRIVE PROPOSED PILE TO A REQUIRED BEARING CAPACITY OF 50 TONS PER PILE. THE REQUIRED BEARING CAPACITY IS EQUAL TO THE ALLOWABLE BEARING CAPACITY WITH A MINIMUM FACTOR OF SAFETY OF TWO.

SEE SHEET 3 FOR 16" PRESTRESSED PILE WHICH IS INCLUDED FOR INFORMATION ONLY.

NEW 16" PRESTRESSED PILE TO BE PROVIDED BY DEPARTMENT. SEE SPECIAL PROVISIONS.

FOR OTHER DESIGN DATA AND GENERAL NOTES, SEE SHEET SN.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL STATE AND FEDERAL SAFETY REQUIREMENTS.

SEE SHEETS S-1 THRU S-24 FOR EXISTING BRIDGE PLANS WHICH ARE ATTACHED FOR THE CONVENIENCE OF THE CONTRACTOR.

FOR PARTIAL REMOVAL OF EXISTING STRUCTURE, SEE SPECIAL PROVISIONS.

PROJECT NO. 2B.100711
BEAUFORT COUNTY
 BRIDGE NO: 1

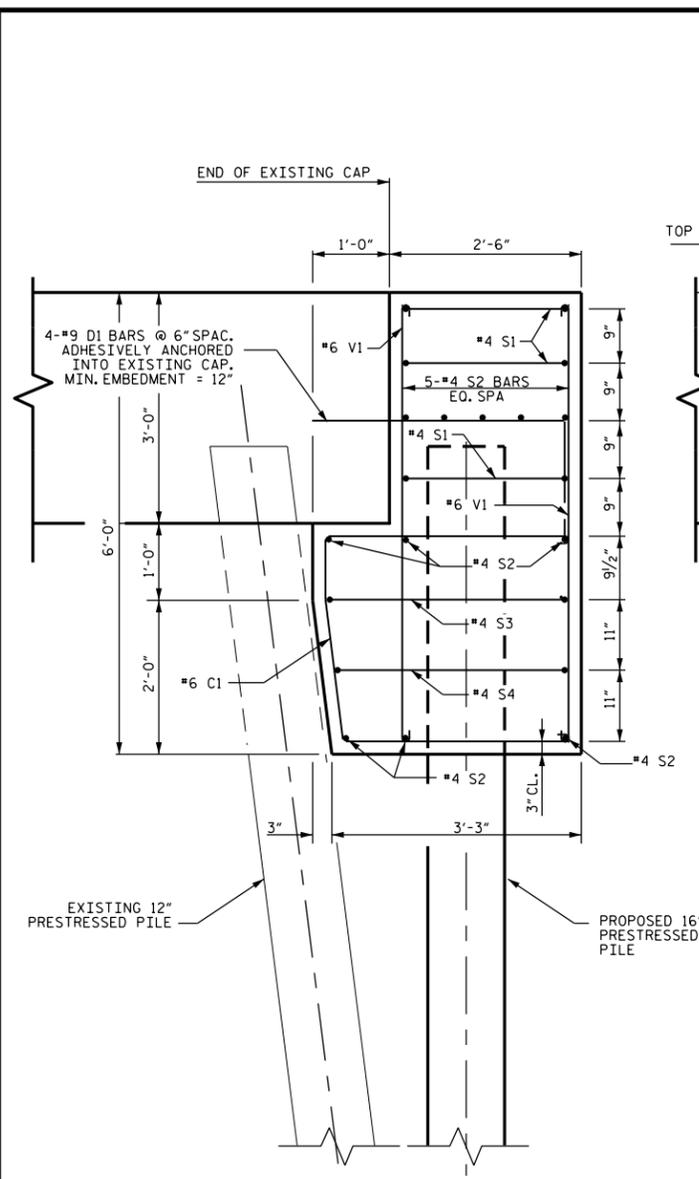
SHEET OF

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

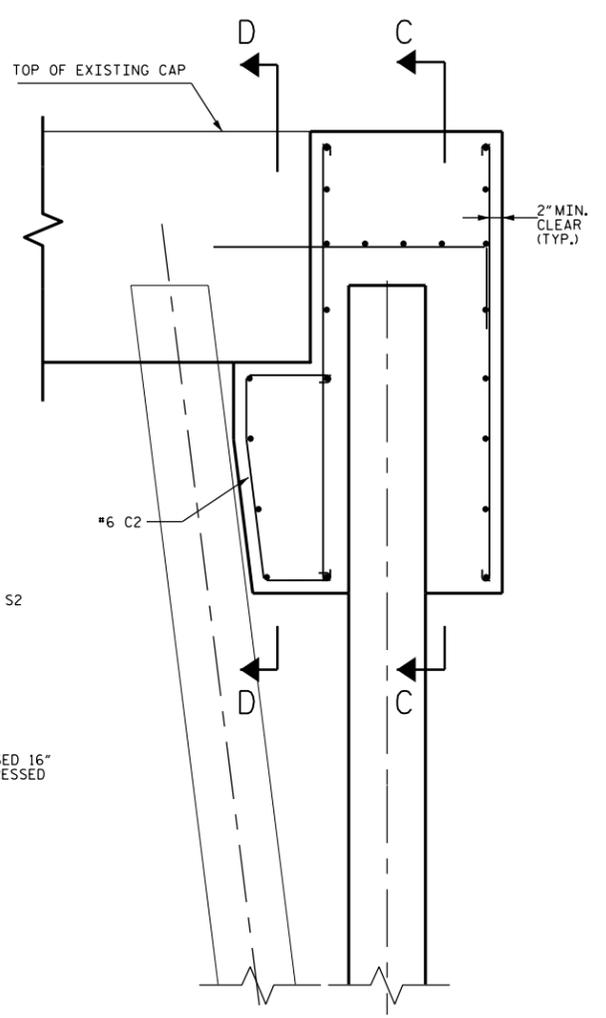
**BENT 2 REPAIR
 AND GENERAL NOTES**

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	1
1			3			TOTAL SHEETS
2			4			4

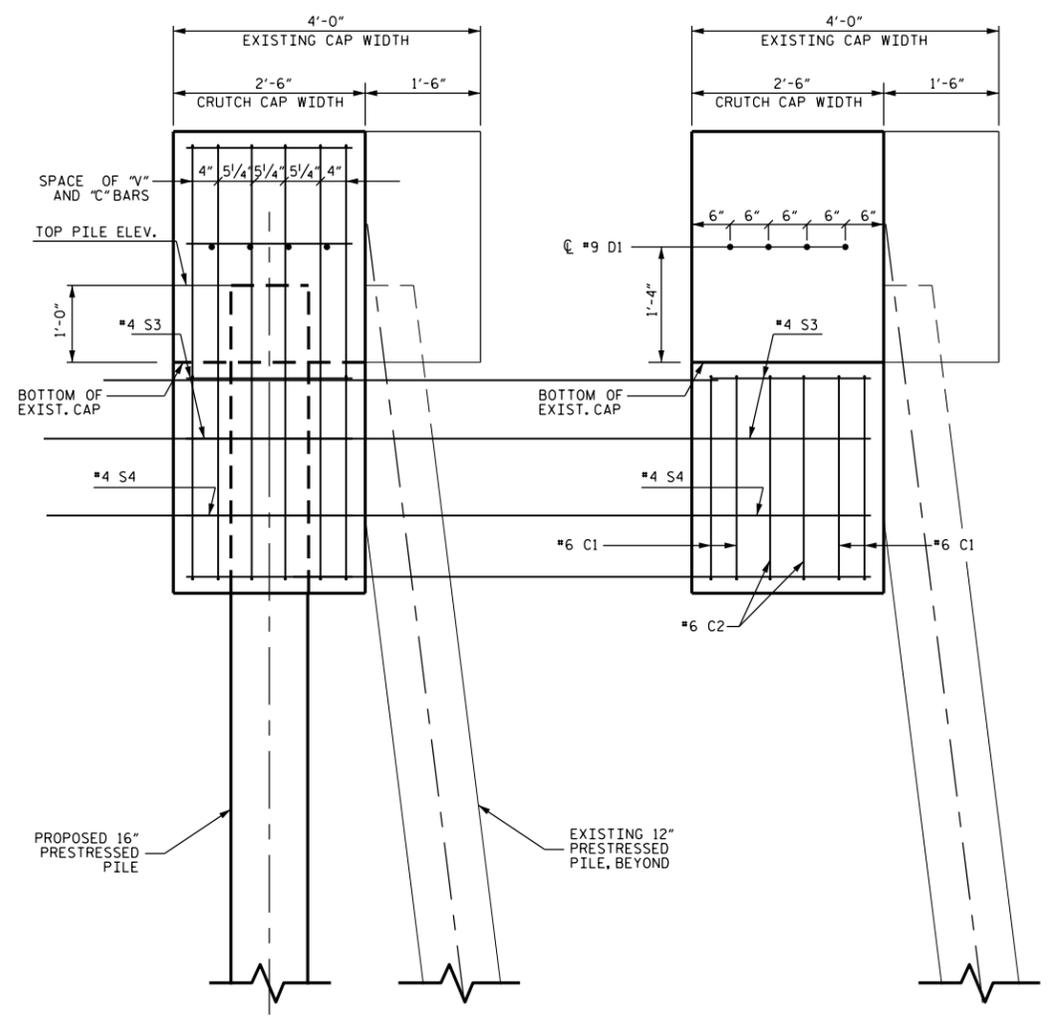
DRAWN BY : S. T. SANDOR DATE : JULY 2012
 CHECKED BY : D. MULLER DATE : JULY 2012



SECTION A-A
(OUTSIDE OF PILE)



SECTION B-B
(THROUGH PILE)



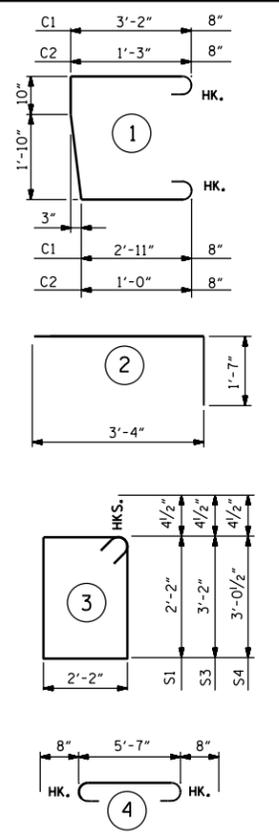
SECTION C-C
(OUTSIDE ELEVATION)

SECTION D-D
(INSIDE ELEVATION)

BILL OF MATERIAL

BAR	NO.	SIZE	TYPE	LENGTH	WEIGHT
C1	4	#6	1	10'-1"	61.0
C2	2	#6	1	6'-3"	19.0
D1	4	#9	2	4'-11"▲	67.0
S1	3	#4	3	9'-5"	19.0
S2	11	#4	STR	2'-2"	16.0
S3	1	#4	3	11'-5"	8.0
S4	1	#4	3	11'-2"	7.0
V1	12	#6	4	6'-11"	125.0

REINFORCING STEEL = 322.0LBS
 CLASS A CONCRETE = * 1.38 CY
 16" SQ. PRESTRESSED CONCRETE PILE
 LINEAR FEET 35



PROJECT NO. 2B.100711
BEAUFORT COUNTY
 BRIDGE NO. : 1
 SHEET OF

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

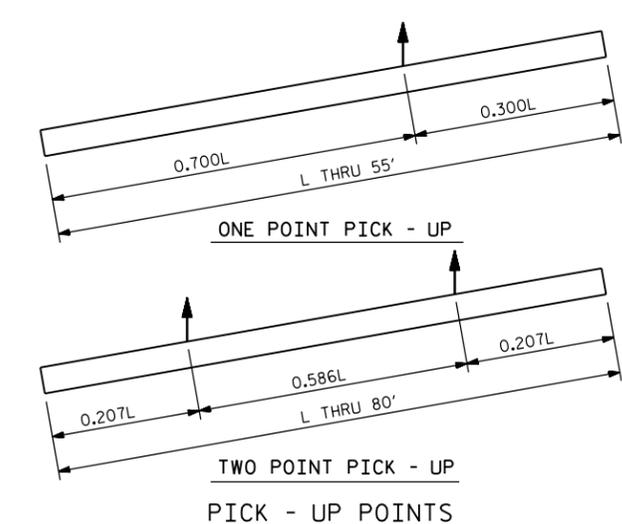
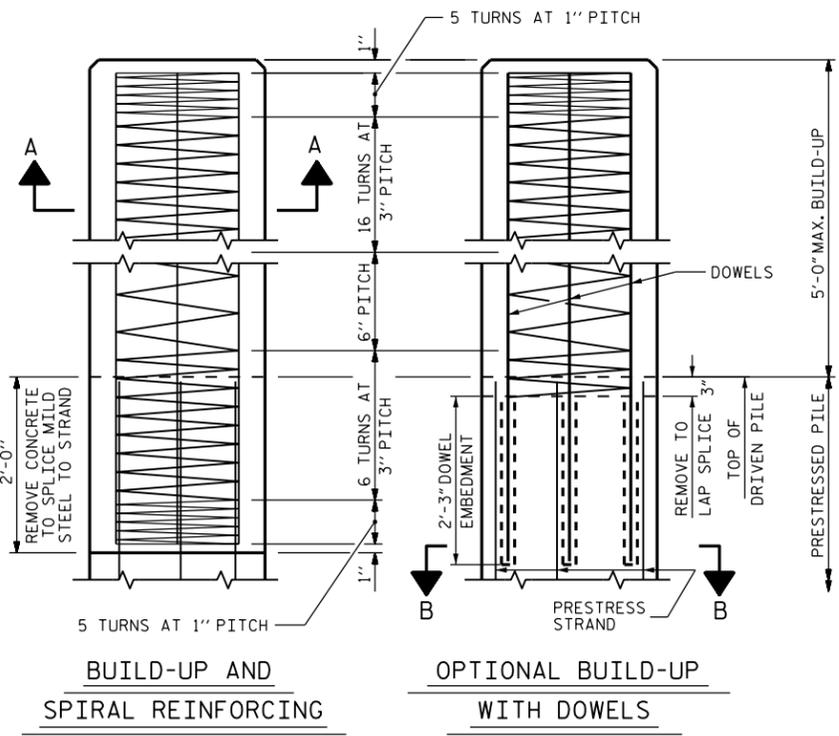
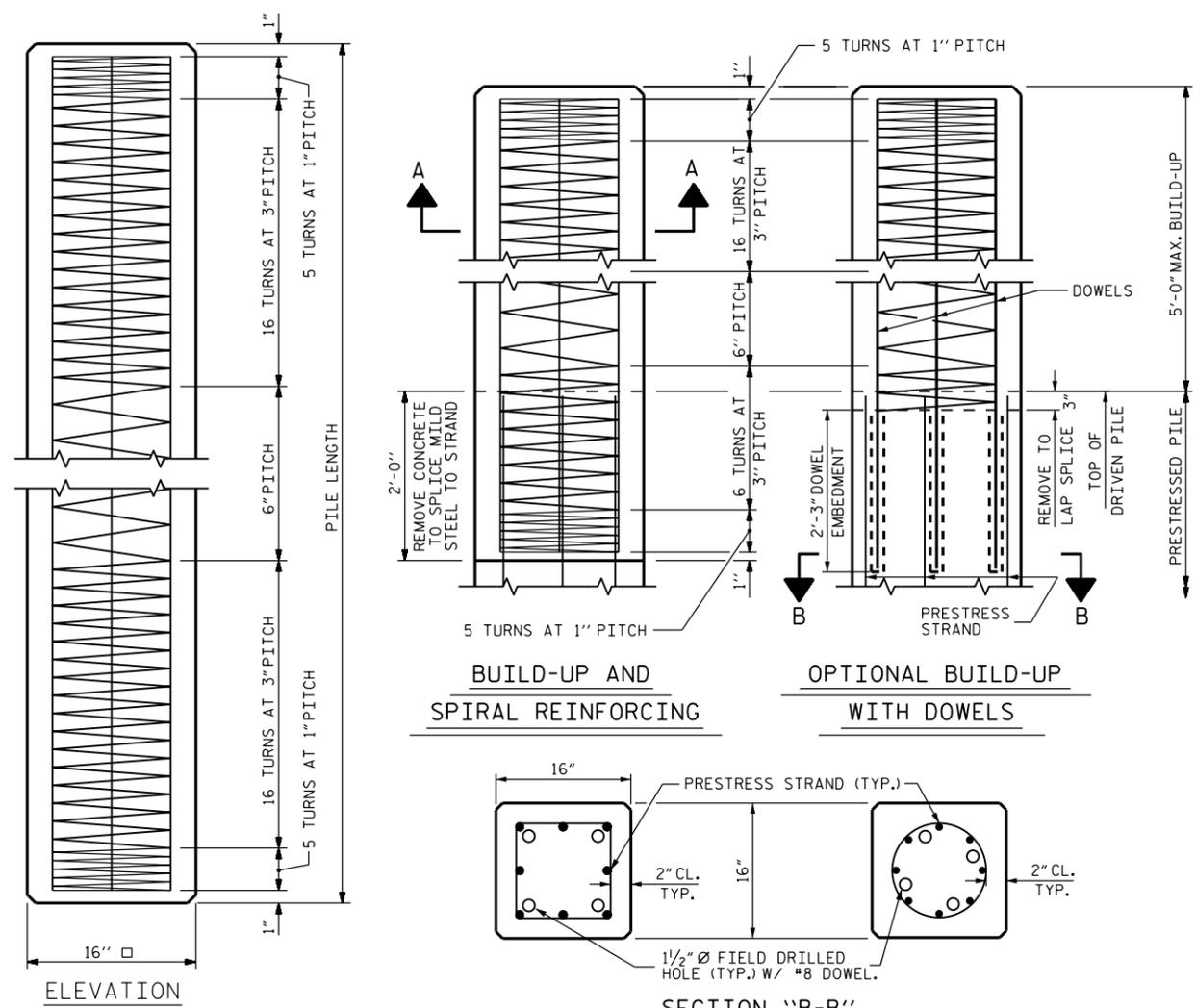
CRUTCH CAP DETAILS

▲ LENGTHS OF #9 D1 BARS HAVE BEEN ASSUMED ON THE BASIS OF A 12" MINIMUM EMBEDMENT INTO EXISTING CONCRETE CAPS. THIS EMBEDMENT REQUIREMENT MAY BE INCREASED TO BE IN COMPLIANCE WITH MINIMUM EMBEDMENT SPECIFIED BY THE MANUFACTURER OF THE EPOXY ADHESIVE BONDING SYSTEM.

* CONCRETE DISPLACED BY PILE HAS BEEN DEDUCTED FROM THE CAP.

DRAWN BY : S. T. SANDOR DATE : 07/2012
 CHECKED BY : P. SPROUSE DATE : 07/2012

REVISIONS						SHEET NO.	
NO.	BY:	DATE:	NO.	BY:	DATE:	2	
1			3			TOTAL SHEETS	
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QUANTITIES FOR ONE 16" PRESTRESSED PILE						
LENGTH	CONCRETE CU. YDS.	PILE WT. TONS	ONE POINT PICK-UP		TWO POINT PICK-UP	
			0.300L	0.700L	0.207L	0.586L
25'-0"	1.63	3.31	7'-6"	17'-6"	5'-2"	14'-8"
30'-0"	1.96	3.97	9'-0"	21'-0"	6'-2 1/2"	17'-7"
35'-0"	2.29	4.63	10'-6"	24'-6"	7'-3"	20'-6"
40'-0"	2.61	5.29	12'-0"	28'-0"	8'-3 1/2"	23'-5"
45'-0"	2.94	5.95	13'-6"	31'-6"	9'-4"	26'-4"
50'-0"	3.27	6.61	15'-0"	35'-0"	10'-4"	29'-4"
55'-0"	3.59	7.28	16'-6"	38'-6"	11'-4 1/2"	32'-3"
60'-0"	3.92	7.94			12'-5"	35'-2"
65'-0"	4.25	8.60			13'-5 1/2"	38'-1"
70'-0"	4.57	9.26			14'-6"	41'-0"
75'-0"	4.90	9.92			15'-6 1/2"	43'-11"
80'-0"	5.23	10.58			16'-7"	46'-10"

NOTES

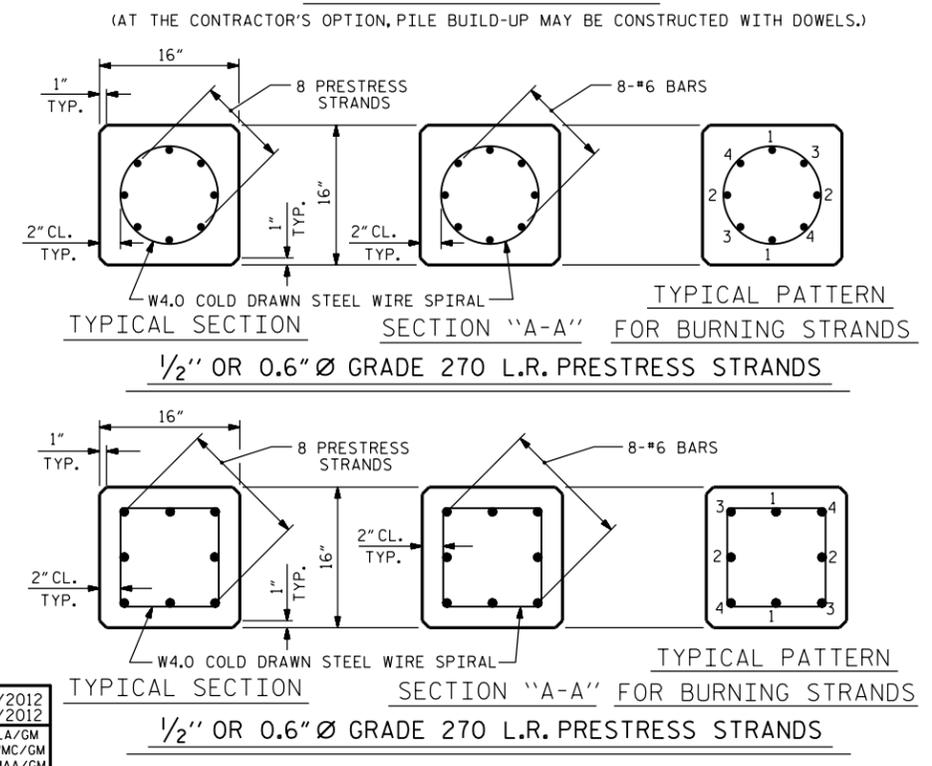
PRESTRESSED CONCRETE STRENGTH : $f'_c = 7,500$ PSI
 BUILD-UP CONCRETE STRENGTH : $f'_c = 7,500$ PSI
 STRAND DATA:

SIZE	GRADE	AREA	ULTIMATE STRENGTH	APPLIED PRESTRESS FORCE
1/2"	270 L.R.	0.153	41,300* PER STRAND	30,980* PER STRAND
0.6"	270 L.R.	0.217	58,600* PER STRAND	43,940* PER STRAND

ALL PRESTRESSING STRANDS SHALL BE 7-WIRE LOW-RELAXATION GRADE 270 STRANDS CONFORMING TO AASHTO M203. STRAND SAMPLING REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
 AT THE CONTRACTOR'S OPTION, 1/2" OR 0.6" STRANDS MAY BE USED IN EITHER STRAND CONFIGURATION SHOWN IN THE TYPICAL SECTION DETAIL. MIXING OF STRAND SIZE IS NOT ALLOWED.
 THE SLIP-FORM METHOD OF CASTING PILES WILL NOT BE PERMITTED.
 TRANSFER THE LOAD FROM THE ANCHORAGES TO THE PILE AFTER THE CONCRETE HAS ATTAINED A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI.
 IF STRAND STRESS IS RELIEVED BY BURNING, THE STRANDS SHALL BE BURNED IN OPPOSITE PAIRS AS INDICATED IN THE TYPICAL PATTERN SHOWN. FOR ANY NUMBER OF STRANDS, BURN IN OPPOSITE PAIRS AND SYMMETRICALLY ABOUT BOTH THE VERTICAL AND HORIZONTAL AXES. STRANDS 1-1 SHALL BE BURNED BEFORE 2-2, ETC. NOT MORE THAN 4 STRANDS, SAY 3-3 AND 4-4, MAY BE BURNED AT ANY ONE SECTION BEFORE THESE SAME PAIRS OF STRANDS ARE BURNED AT BOTH ENDS OF THE BED AND BETWEEN EACH PAIR OF PILES IN THE BED.
 PROPOSED DEVICES FOR LIFTING PILES, RECESS DETAILS, AND PATCHING MATERIAL SHALL BE DETAILED IN SHOP DRAWINGS. AFTER ATTACHMENTS HAVE BEEN REMOVED, OPENINGS SHALL BE REPAIRED SUCH THAT THE APPEARANCE OF THE PILE IS UNIFORM.
 WHERE CAST-IN-PLACE LIFTING DEVICES ARE NOT USED, PICK-UP POINTS ARE TO BE INDICATED WITH A 2" WIDE BLACK MARK.
 DRIVE PILES USING A METHOD APPROVED BY THE ENGINEER, WHEREBY THE HEAD OF THE PILE IS NOT DAMAGED.
 DRIVING OF THE BUILT-UP PILE WILL NOT BE PERMITTED UNTIL THE CONCRETE HAS REACHED A COMPRESSIVE STRENGTH OF 5,000 PSI AND UNTIL A PERIOD OF SEVEN DAYS HAS ELAPSED SINCE CASTING OF THE BUILD-UP.

DOWEL INSTALLATION FOR OPTIONAL BUILD-UP

GROUT COMPRESSIVE STRENGTH: $f'_c = 5,000$ PSI
 BEFORE DRILLING DOWEL HOLES, REMOVE THE UPPER 3" OF CONCRETE FROM THE TOP OF THE PILE WITHOUT DAMAGE TO THE REINFORCING STEEL. THE REMOVAL PLANE SHOULD BE NORMAL TO THE EDGE OF THE PILE.
 DOWEL HOLES SHALL BE POSITIONED TO MAINTAIN 1/2" CLEAR TO ALL EXISTING PRESTRESSING STRANDS IN THE CONCRETE PILE.
 FIELD DRILLED HOLES SHALL BE CLEAN AND FREE OF ANY OBSTRUCTIONS BEFORE GROUTING OF DOWELS. DOWEL BARS SHALL BE INSTALLED AND GROUTED WITH AN APPROVED NON-SHRINK GROUT.
 THE SPIRAL REINFORCING IN ALL BUILD-UPS SHALL BE W4.0 COLD DRAWN WIRE WHICH SHALL BE SECURED TO THE LONGITUDINAL REINFORCEMENT TO MAINTAIN PITCH.
 THE SPIRAL REINFORCING IN THE BUILD-UP AND THE PRESTRESSED CONCRETE PILE SHALL BE SPLICED BY OVERLAPPING A MIN. OF ONE TURN.



PROJECT NO. 2B.1007111
BEAUFORT COUNTY
 BRIDGE NO. : 1

SHEET OF

STATE OF NORTH CAROLINA
 DEPARTMENT OF TRANSPORTATION
 RALEIGH

STANDARD
 16" PRESTRESSED
 CONCRETE PILE

REVISIONS						SHEET NO.
NO.	BY:	DATE:	NO.	BY:	DATE:	3
1			3			TOTAL SHEETS
2			4			4

ASSEMBLED BY : S. T. SANDOR DATE : 07/2012
 CHECKED BY : P. SPROUSE DATE : 07/2012
 DRAWN BY : RH 9/98
 CHECKED BY : LES 10/98

REV. 5/1/06R TLA/GM
 REV. 11/30/10 WMC/GM
 REV. 10/1/11 MAA/GM

STANDARD NOTES

DESIGN DATA:

SPECIFICATIONS	-----	A.A.S.H.T.O. (CURRENT)
LIVE LOAD	-----	SEE PLANS
IMPACT ALLOWANCE	-----	SEE A.A.S.H.T.O.
STRESS IN EXTREME FIBER OF		
STRUCTURAL STEEL - AASHTO M270 GRADE 36	-	20,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50W	-	27,000 LBS. PER SQ. IN.
- AASHTO M270 GRADE 50	-	27,000 LBS. PER SQ. IN.
REINFORCING STEEL IN TENSION		
GRADE 60	--	24,000 LBS. PER SQ. IN.
CONCRETE IN COMPRESSION	-----	1,200 LBS. PER SQ. IN.
CONCRETE IN SHEAR	-----	SEE A.A.S.H.T.O.
STRUCTURAL TIMBER - TREATED OR		
UNTREATED - EXTREME FIBER STRESS	-----	1,800 LBS. PER SQ. IN.
COMPRESSION PERPENDICULAR TO GRAIN OF TIMBER	-----	375 LBS. PER SQ. IN.
EQUIVALENT FLUID PRESSURE OF EARTH	-----	30 LBS. PER CU. FT.
		(MINIMUM)

MATERIAL AND WORKMANSHIP:

EXCEPT AS MAY OTHERWISE BE SPECIFIED ON PLANS OR IN THE SPECIAL PROVISIONS, ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE 2012 "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" OF THE N. C. DEPARTMENT OF TRANSPORTATION.

STEEL SHEET PILING FOR PERMANENT OR TEMPORARY APPLICATIONS SHALL BE HOT ROLLED.

CONCRETE:

UNLESS OTHERWISE REQUIRED ON PLANS, CLASS A CONCRETE SHALL BE USED FOR ALL PORTIONS OF ALL STRUCTURES WITH THE EXCEPTION THAT: CLASS AA CONCRETE SHALL BE USED IN BRIDGE SUPERSTRUCTURES, ABUTMENT BACKWALLS, AND APPROACH SLABS; AND CLASS B CONCRETE SHALL BE USED FOR SLOPE PROTECTION AND RIP RAP.

CONCRETE CHAMFERS:

UNLESS OTHERWISE NOTED ON THE PLANS, ALL EXPOSED CORNERS ON STRUCTURES SHALL BE CHAMFERED 3/4" WITH THE FOLLOWING EXCEPTIONS: TOP CORNERS OF CURBS MAY BE ROUNDED TO 1-1/2" RADIUS WHICH IS BUILT INTO CURB FORMS; CORNERS OF TRANSVERSE FLOOR EXPANSION JOINTS SHALL BE ROUNDED WITH A 1/4" FINISHING TOOL UNLESS OTHERWISE REQUIRED ON PLANS; AND CORNERS OF EXPANSION JOINTS IN THE ROADWAY FACES AND TOPS OF CURBS AND SIDEWALKS SHALL BE ROUNDED TO A 1/4" RADIUS WITH A FINISHING STONE OR TOOL UNLESS OTHERWISE REQUIRED ON PLANS.

DOWELS:

DOWELS WHEN INDICATED ON PLANS AS FOR CULVERT EXTENSIONS, SHALL BE EMBEDDED AT LEAST 12" INTO THE OLD CONCRETE AND GROUTED INTO PLACE WITH 1:2 CEMENT MORTAR.

ALLOWANCE FOR DEAD LOAD DEFLECTION, SETTLEMENT, ETC. IN CASTING SUPERSTRUCTURES:

BRIDGES SHALL BE BUILT ON THE GRADE OR VERTICAL CURVE SHOWN ON PLANS. SLABS, CURBS AND PARAPETS SHALL CONFORM TO THE GRADE OR CURVE.

ALL DIMENSIONS WHICH ARE GIVEN IN SECTION AND ARE AFFECTED BY DEAD LOAD DEFLECTIONS ARE DIMENSIONS AT CENTER LINE OF BEARING UNLESS OTHERWISE NOTED ON PLANS. IN SETTING FORMS FOR STEEL BEAM BRIDGES AND PRESTRESSED CONCRETE GIRDER BRIDGES, ADJUSTMENTS SHALL BE MADE DUE TO THE DEAD LOAD DEFLECTIONS FOR THE ELEVATIONS SHOWN. WHERE BLOCKS ARE SHOWN OVER BEAMS FOR BUILDING UP TO THE SLAB, THE VERTICAL DIMENSIONS OF THE BLOCKS SHALL BE ADJUSTED BETWEEN BEARINGS TO COMPENSATE FOR DEAD LOAD DEFLECTIONS, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER. WHERE BOTTOM OF SLAB IS IN LINE WITH BOTTOM OF TOP FLANGES, DEPTH OF SLAB BETWEEN BEARINGS SHALL BE ADJUSTED TO COMPENSATE FOR DEAD LOAD DEFLECTION, VERTICAL CURVE ORDINATE, AND ACTUAL BEAM CAMBER.

IN SETTING FALSEWORK AND FORMS FOR REINFORCED CONCRETE SPANS, AN ALLOWANCE SHALL BE MADE FOR DEAD LOAD DEFLECTIONS, SETTLEMENT OF FALSEWORK, AND PERMANENT CAMBER WHICH SHALL BE PROVIDED FOR IN ADDITION TO THE ELEVATIONS SHOWN. AFTER REMOVAL OF THE FALSEWORK, THE FINISHED STRUCTURES SHALL CONFORM TO THE PROFILE AND ELEVATIONS SHOWN ON THE PLANS AND CONSTRUCTION ELEVATIONS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR FALSEWORK OR FORMS FOR BRIDGE SUPERSTRUCTURE AND ANY STRUCTURE OR PARTS OF A STRUCTURE AS NOTED ON THE PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE CONSTRUCTION OF THE FALSEWORK OR FORMS IS STARTED.

REINFORCING STEEL:

ALL REINFORCING STEEL SHALL BE DEFORMED. DIMENSIONS RELATIVE TO PLACEMENT OF REINFORCING ARE TO CENTERS OF BARS UNLESS OTHERWISE INDICATED IN THE PLANS. DIMENSIONS ON BAR DETAILS ARE TO CENTERS OF BARS OR ARE OUT TO OUT AS INDICATED ON PLANS.

WIRE BAR SUPPORTS SHALL BE PROVIDED FOR REINFORCING STEEL WHERE INDICATED ON THE PLANS. WHEN BAR SUPPORT PIECES ARE PLACED IN CONTINUOUS LINES, THEY SHALL BE SO PLACED THAT THE ENDS OF THE SUPPORTING WIRES SHALL BE LAPPED TO LOCK LEGS ON ADJOINING PIECES.

STRUCTURAL STEEL:

AT THE CONTRACTOR'S OPTION, HE MAY SUBSTITUTE 7/8" Ø SHEAR STUDS FOR THE 3/4" Ø STUDS SPECIFIED ON THE PLANS. THIS SUBSTITUTION SHALL BE MADE AT THE RATE OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS, AND STUD SPACING CHANGES SHALL BE MADE AS NECESSARY TO PROVIDE THE SAME EQUIVALENT NUMBER OF 7/8" Ø STUDS ALONG THE BEAM AS SHOWN FOR 3/4" Ø STUDS BASED ON THE RATIO OF 3 - 7/8" Ø STUDS FOR 4 - 3/4" Ø STUDS. STUDS OF THE LENGTH SPECIFIED ON THE PLANS MUST BE PROVIDED. THE MAXIMUM SPACING SHALL BE 2'-0".

EXCEPT AT THE INTERIOR SUPPORTS OF CONTINUOUS BEAMS WHERE THE COVER PLATE IS IN CONTACT WITH BEARING PLATE, THE CONTRACTOR MAY, AT HIS OPTION, SUBSTITUTE FOR THE COVER PLATES DESIGNATED ON THE PLANS COVER PLATES OF THE EQUIVALENT AREA PROVIDED THESE PLATES ARE AT LEAST 5/16" IN THICKNESS AND DO NOT EXCEED A WIDTH EQUAL TO THE FLANGE WIDTH LESS 2" OR A THICKNESS EQUAL TO 2 TIMES THE FLANGE THICKNESS. THE SIZE OF FILLET WELDS SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT ANSI/AASHTO/AWS "BRIDGE WELDING CODE". ELECTROSLAG WELDING WILL NOT BE PERMITTED.

WITH THE SOLE EXCEPTION OF EDGES AT SURFACES WHICH BEAR ON OTHER SURFACES, ALL SHARP EDGES AND ENDS OF SHAPES AND PLATES SHALL BE SLIGHTLY ROUNDED BY SUITABLE MEANS TO A RADIUS OF APPROXIMATELY 1/16 INCH OR EQUIVALENT FLAT SURFACE AT A SUITABLE ANGLE PRIOR TO PAINTING, GALVANIZING, OR METALLIZING.

HANDRAILS AND POSTS:

METAL STANDARDS AND FACES OF THE CONCRETE END POSTS FOR THE METAL RAIL SHALL BE SET NORMAL TO THE GRADE OF THE CURB, UNLESS OTHERWISE SHOWN ON PLANS. THE METAL RAIL AND TOPS OF CONCRETE POSTS USED WITH THE ALUMINUM RAIL SHALL BE BUILT PARALLEL TO THE GRADE OF THE CURB.

METAL HANDRAILS SHALL BE IN ACCORDANCE WITH THE PLANS. RAILS SHALL BE AS MANUFACTURED FOR BRIDGE RAILING. CASTINGS SHALL BE OF A UNIFORM APPEARANCE. FINISHES AND OTHER DEFORMATIONS RESULTING FROM CASTING OR OTHERWISE SHALL BE REMOVED IN A MANNER SO THAT A UNIFORM COLORING OF THE COMPLETED CASTING SHALL BE OBTAINED. CASTINGS WITH DISCOLORATIONS OR OF NON-UNIFORM COLORING WILL NOT BE ACCEPTED. CERTIFIED MILL REPORTS ARE REQUIRED FOR METAL RAILS AND POSTS.

SPECIAL NOTES:

GENERALLY, IN CASE OF DISCREPANCY, THIS STANDARD SHEET OF NOTES SHALL GOVERN OVER THE SPECIFICATIONS, BUT THE REMAINDER OF THE PLANS SHALL GOVERN OVER NOTES HEREON, AND SPECIAL PROVISIONS SHALL GOVERN OVER ALL. SEE SPECIFICATIONS ARTICLE 105-4.

ENGLISH

JANUARY, 1990

STD. NO. SN