

CRAVEN COUNTY DB00590

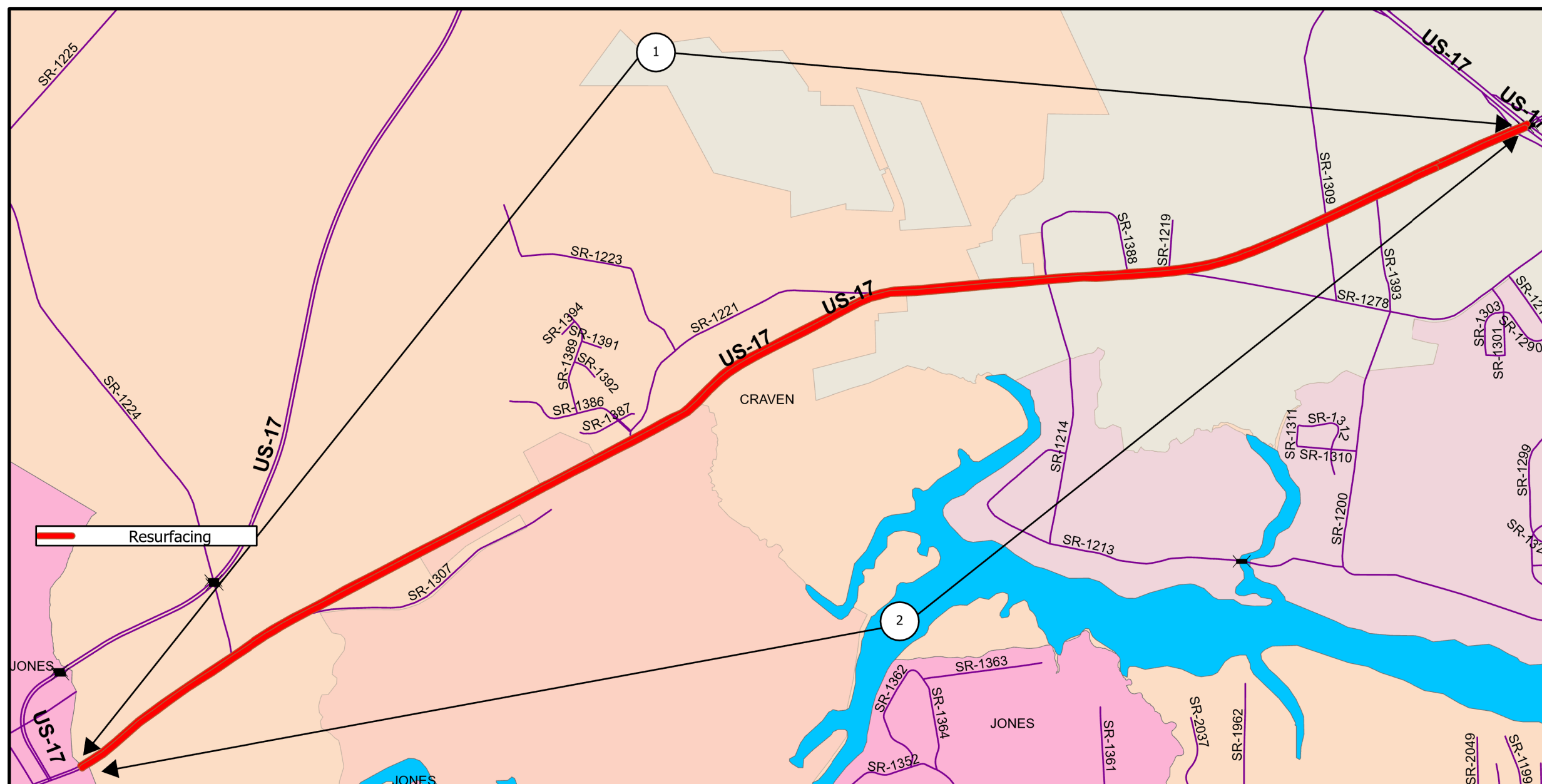
PROJECT REFERENCE NO.	SHEET NO.
DB00590	1

WBS# 2025CPT.02.15.10251



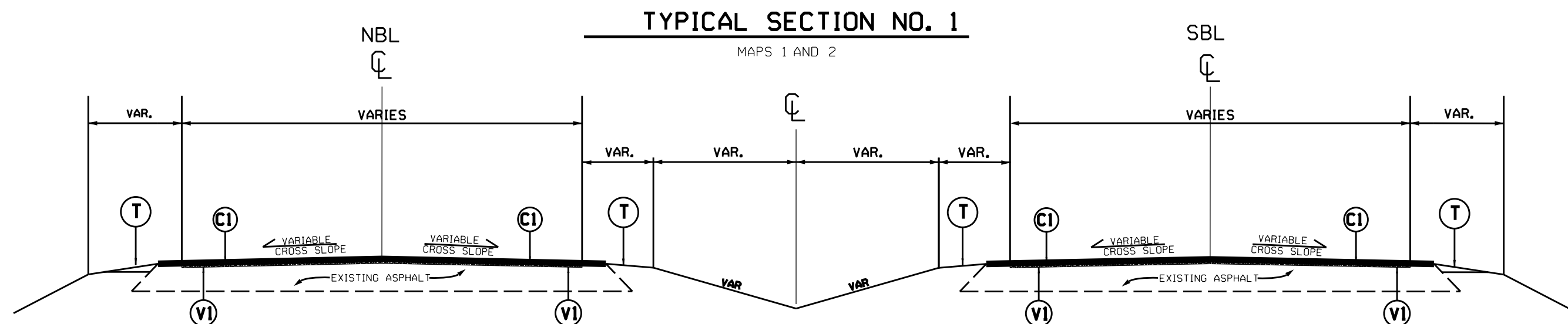
NCDOT
DIVISION 2

TYPE OF WORK : MILLING, RESURFACING, AND SHOULDER RECONSTRUCTION



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	MILLING DEPTH 1.5" FOR ENTIRE WIDTH OF THE ROADWAY.
DRAWINGS NOT TO SCALE	

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.



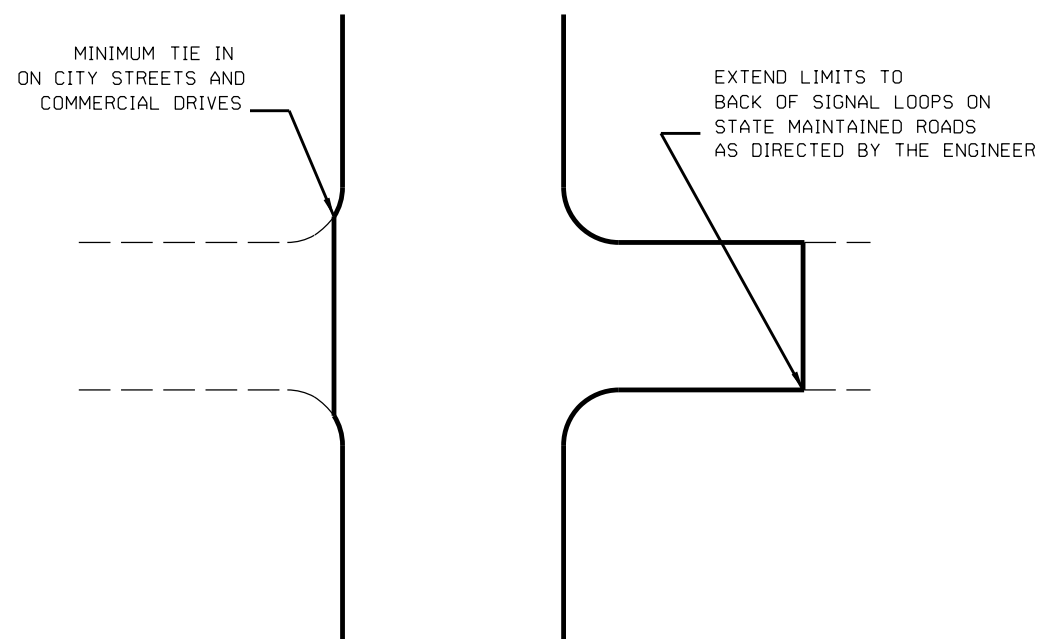
NOTE:

1. MILL FULL WIDTH OF THE ENTIRE ROADWAY TO A DEPTH OF 1.5 INCHES. MILLING TO INCLUDE BOTH NCDOT AND CITY SIDE STREETS TO BACK OF RADIUS.
2. PLACE 1.5 INCHES OF ASPHALT SURFACE COURSE S9.5C AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
4. PERFORM SHOULDER RECONSTRUCTION AFTER PAVING IS COMPLETED.
5. VARIOUS CURB AND GUTTER SECTIONS WILL APPEAR THROUGHOUT BOTH MAPS.
6. VARIOUS MEDIAN ISLANDS WILL APPEAR THROUGHOUT BOTH MAPS.

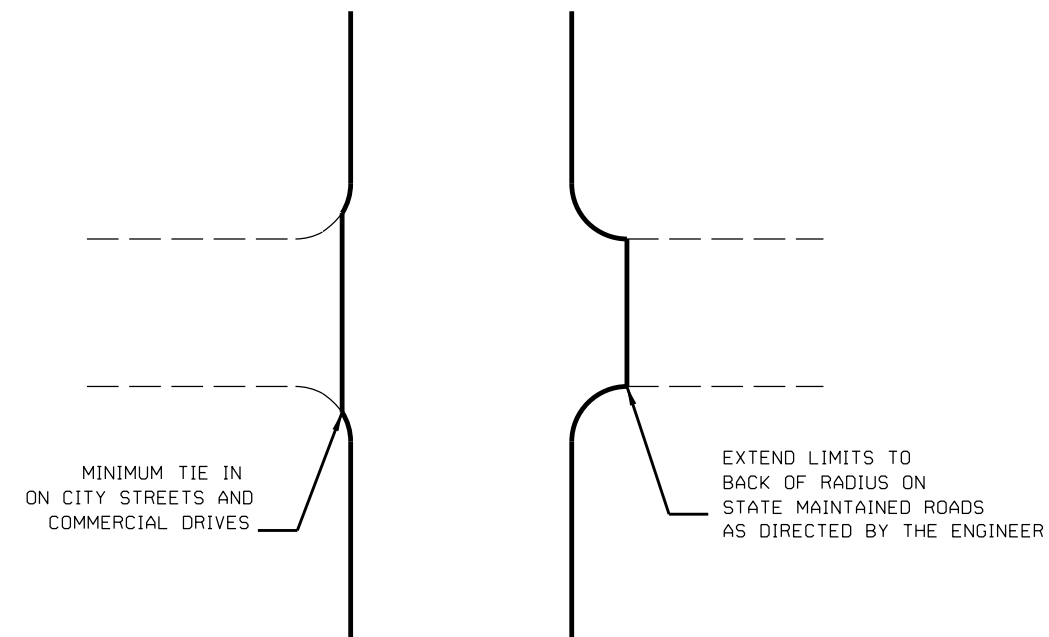
PROJECT NO.	SHEET NO.	TOTAL NO.
DB00590	3	

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	HAULING NCDOT SUPPLIED SHOULDER MATERIAL	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1½" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	TEMPORARY SILT FENCE	WATTLE	SEED & MULCHING	RESPONSE FOR EROSION CONTROL	WORK ZONE ADVANCE/ GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT
								MI	FT	EA	TONS	SMI	SY	SY	TONS	TONS	EA	EA	LF	LF	AC	EA	SF	LS	HR
2025CPT.02.15.10251	Craven	1	US-17 BUS S	FROM 356' SOUTH OF US 70 WEST RAMP TO JONES COUNTY	1	4	MD	6.56	34	232	225	11.62	134,331	16,107	12,637	746	1	1	656	100	5.81	1	525	0.5	180
TOTAL FOR MAP NO. 1								6.56		232	225	11.62	134,331	16,107	12,637	746	1	1	656	100	5.81	1	525	0.5	180
2025CPT.02.15.10251	Craven	2	US-17 BUS N	FROM JONES COUNTY TO 356' SOUTH OF US 70 WEST RAMP	1	4	MD	6.57	34	233	225	11.64	130,126	21,350	12,724	751			657	100	5.82	1	530	0.5	180
TOTAL FOR MAP NO. 2								6.57		233	225	11.64	130,126	21,350	12,724	751			657	100	5.82	1	530	0.5	180
TOTAL FOR PROJ NO. 2025CPT.02.15.10251								13.13		465	450	23.26	264,457	37,457	25,361	1,497	1	1	1,313	200	11.63	2	1,055	1	360
GRAND TOTAL								13.13		465	450	23.26	264,457	37,457	25,361	1,497	1	1	1,313	200	11.63	2	1,055	1	360



TYPICAL DETAIL OF PROJECT LIMITS AT SIGNALIZED Y LINES



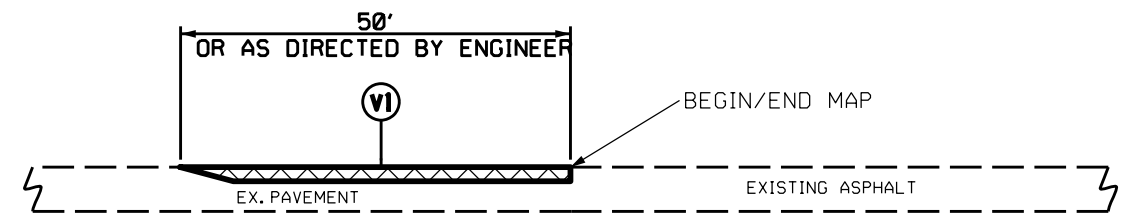
TYPICAL DETAIL OF PROJECT LIMITS AT UNSIGNALIZED Y LINES

ADDITIONAL INTERSECTIONS (NON-TYPICAL)

Extend paving limits to back of radius
or loop on the following intersections:

MAP#	STREET NAME	COMMENTS
1	US 70 EASTBOUND EXIT RAMP	PAVE TO JOINT AT BACK OF RADIUS
1	MALL ENTRANCE NEAR CHICK FIL A	PAVE TO NOSE OF ISLAND
1	MCCARTHY BLVD	PAVE TO BACK OF RADIUS
1	GLENBURNIE RD	PAVE TO JOINT APPROX. 105' FROM -L-
1	JIMMIES CREEK DR	PAVE TO JOINT AT BACK OF ISLAND
1	ACADEMIC DR (NORTH END)	PAVE TO JOINT APPROX. 45' FROM -L-
1	ACADEMIC DR (SOUTH END)	PAVE TO JOINT APPROX. 35' FROM -L-
1	TRENT CREEK RD	PAVE TO JOINT APPROX. 135' FROM -L-
1	EAST CHURCH RD	PAVE TO JOINT APPROX. 90' FROM -L-
2	SHORELINE DR	PAVE TO JOINT APPROX. 100' FROM -L-
2	EFIRD BLVD	PAVE TO JOINT APPROX. 42' FROM -L-
2	N PIRATES RD	PAVE TO JOINT AT BACK OF RADIUS
2	NORBURY DR	PAVE TO JOINT APPROX. 60' FROM -L-
2	TRENT CREEK RD	PAVE TO JOINT APPROX. 75' FROM -L-
2	RIVER RD	PAVE TO NOSE OF ISLAND
2	HARBOR FREIGHT ENTRANCE	PAVE TO JOINT APPROX. 50' FROM -L-

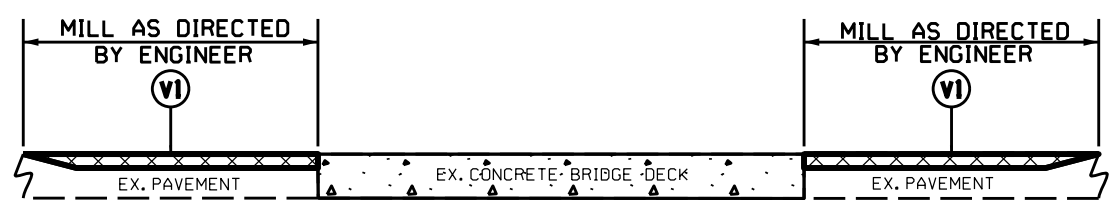
MILLING TYPICALS



DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:

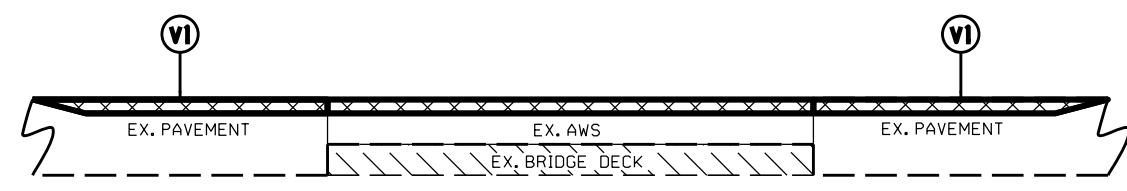
- MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

NOTE:

- MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

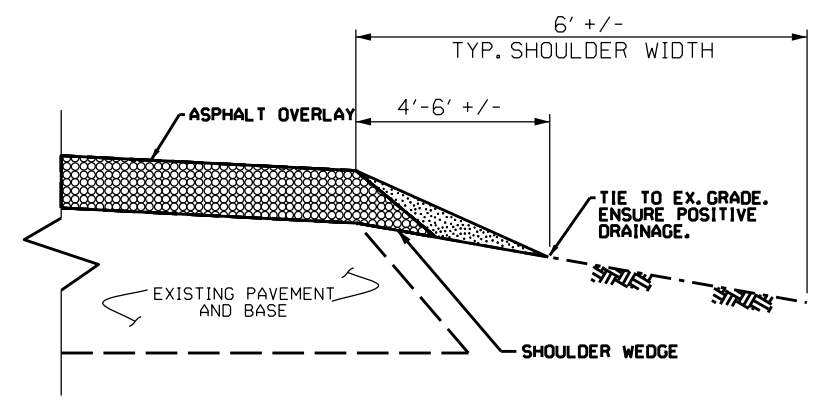


DETAIL 3
BRIDGE MILLING

NOTE:

- INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

SHOULDER RECONSTRUCTION TYPICAL

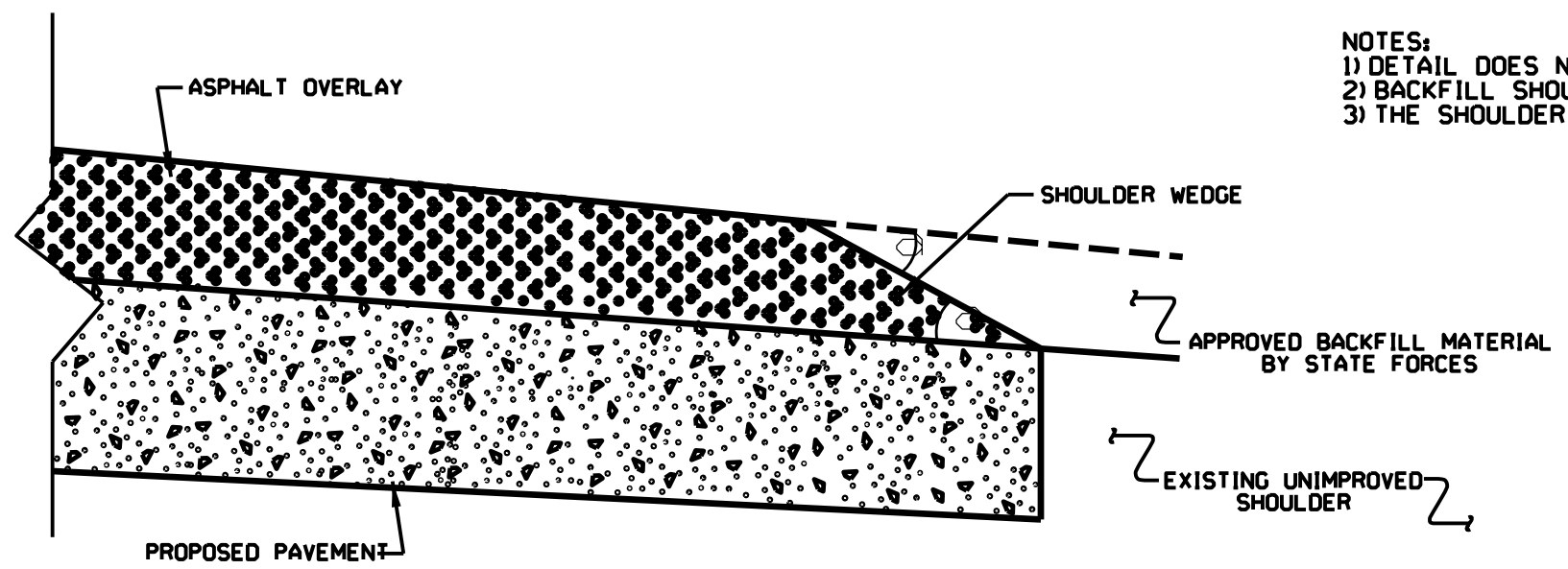


SHOULDER RECONSTRUCTION DETAIL

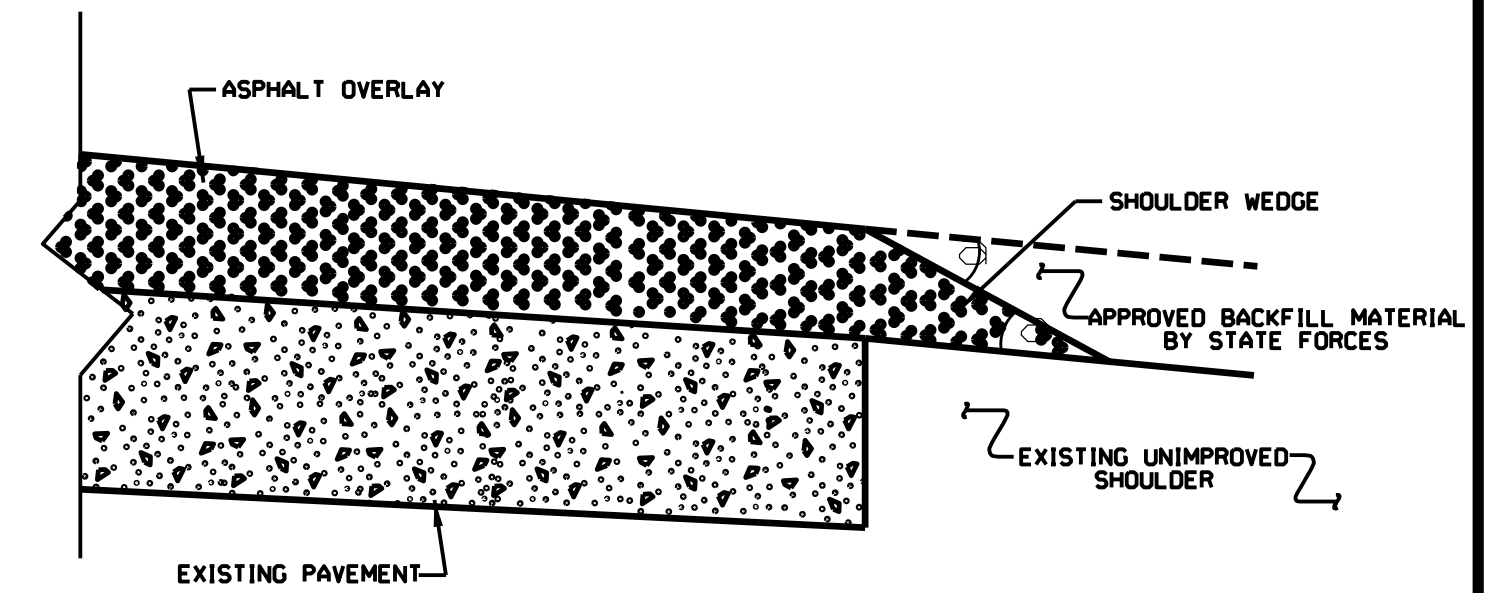
NOTE:

- SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

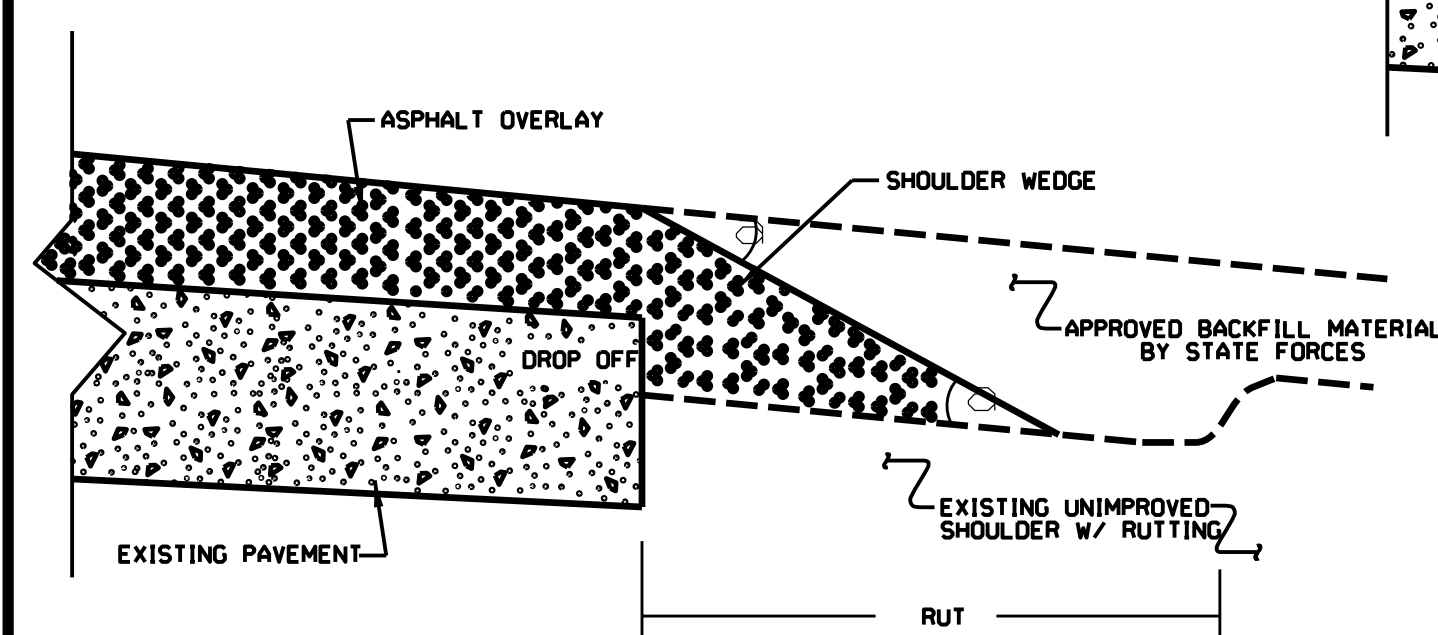
- NOTES:
 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

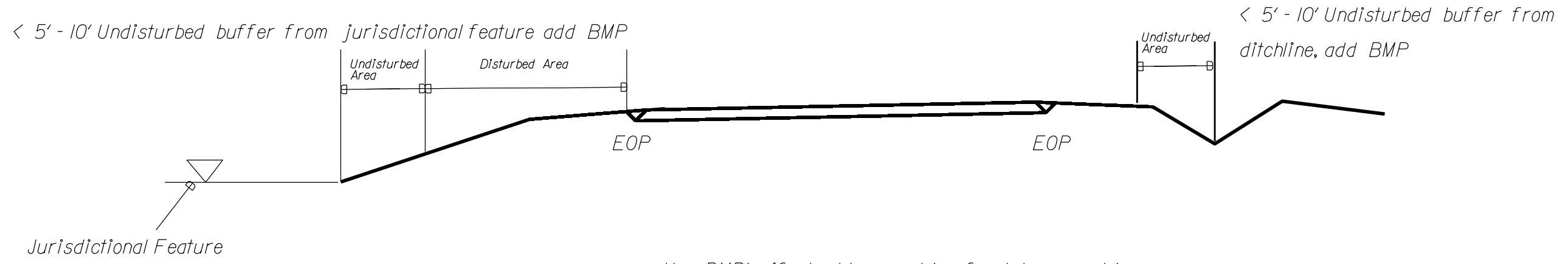
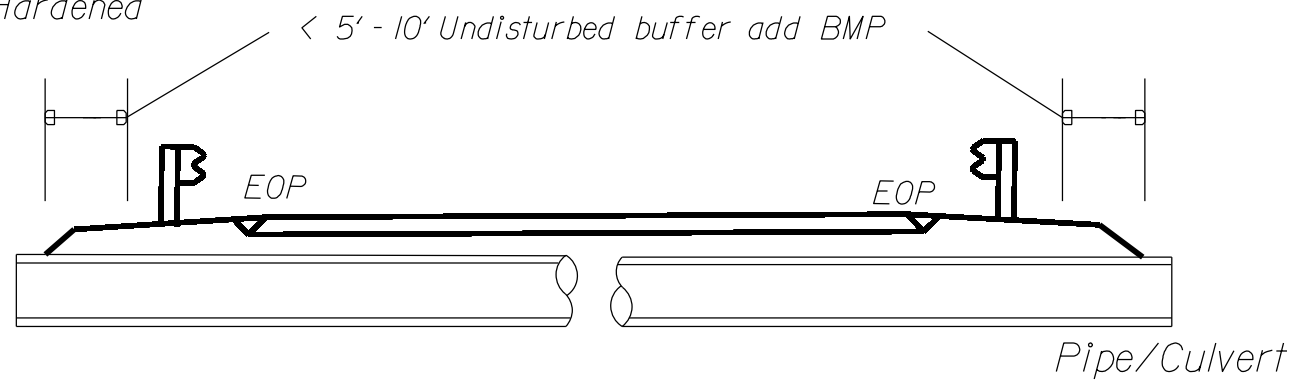
- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-767-6430	Fax 919-230-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: J.SPELL	DATE: 7-13-11
MODIFIED BY:	DATE: 12/16/12
CHECKED BY:	DATE:
FILE SPEC: www.dps.nc.gov/transportation/standards	

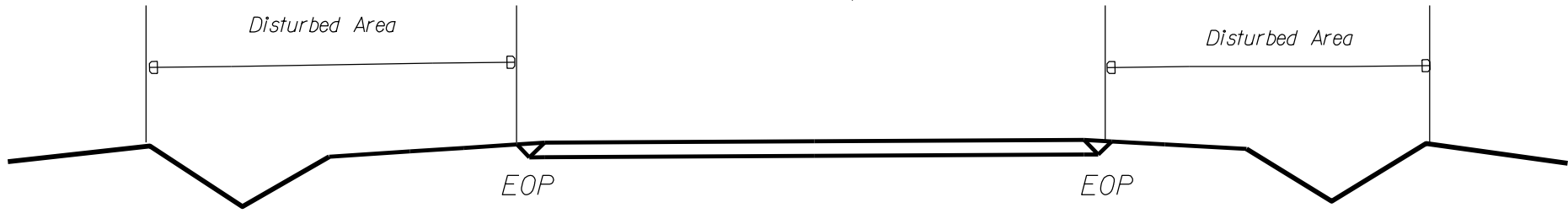
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

EROSION CONTROL DETAIL

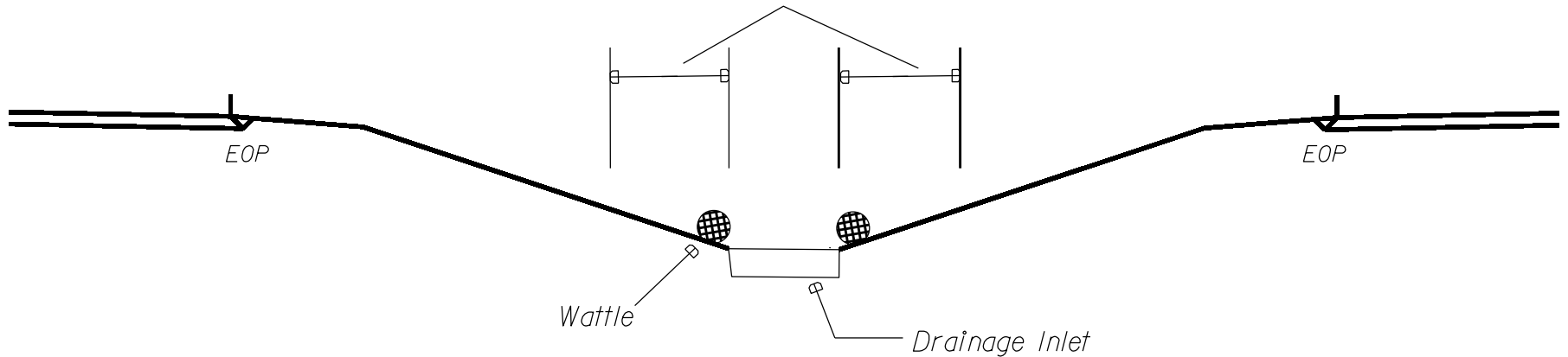
BMP Options: Wattle, Silt Fence or Hardened Aggregate.



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

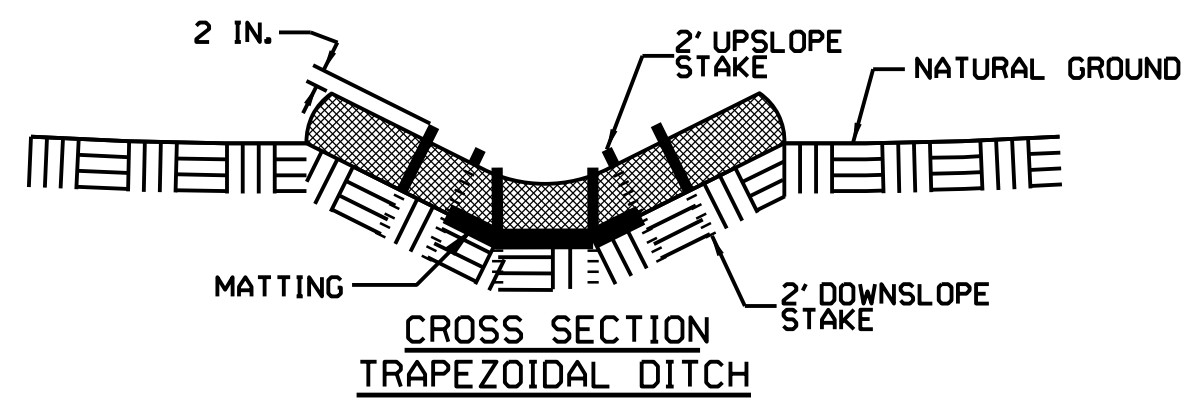
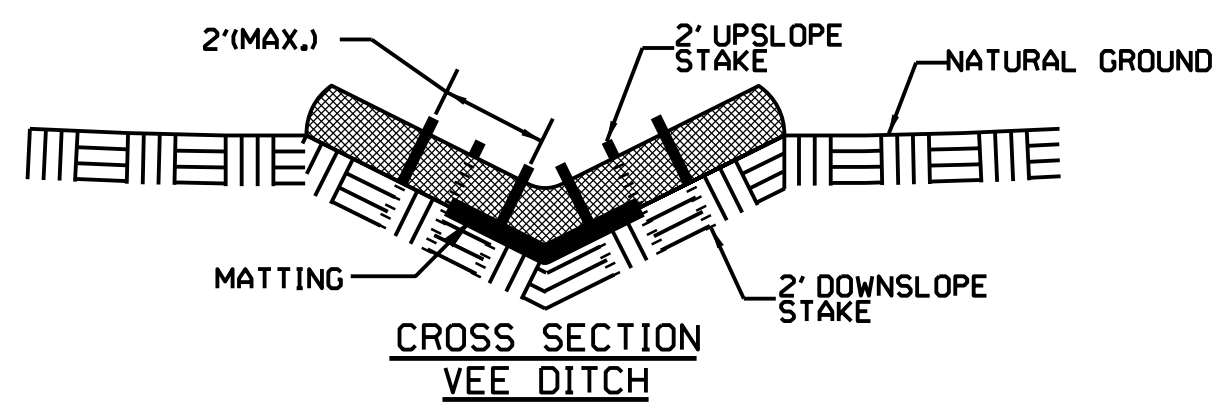
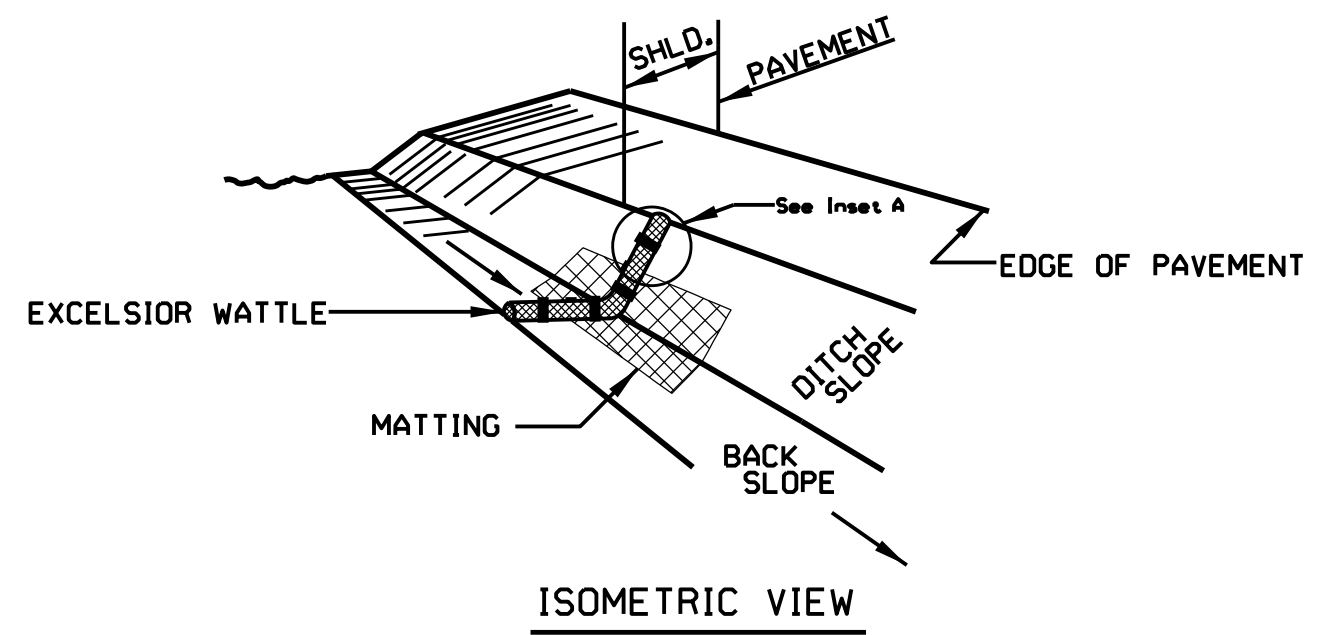


< 5' - 10' Undisturbed buffer from inlet, add wattle

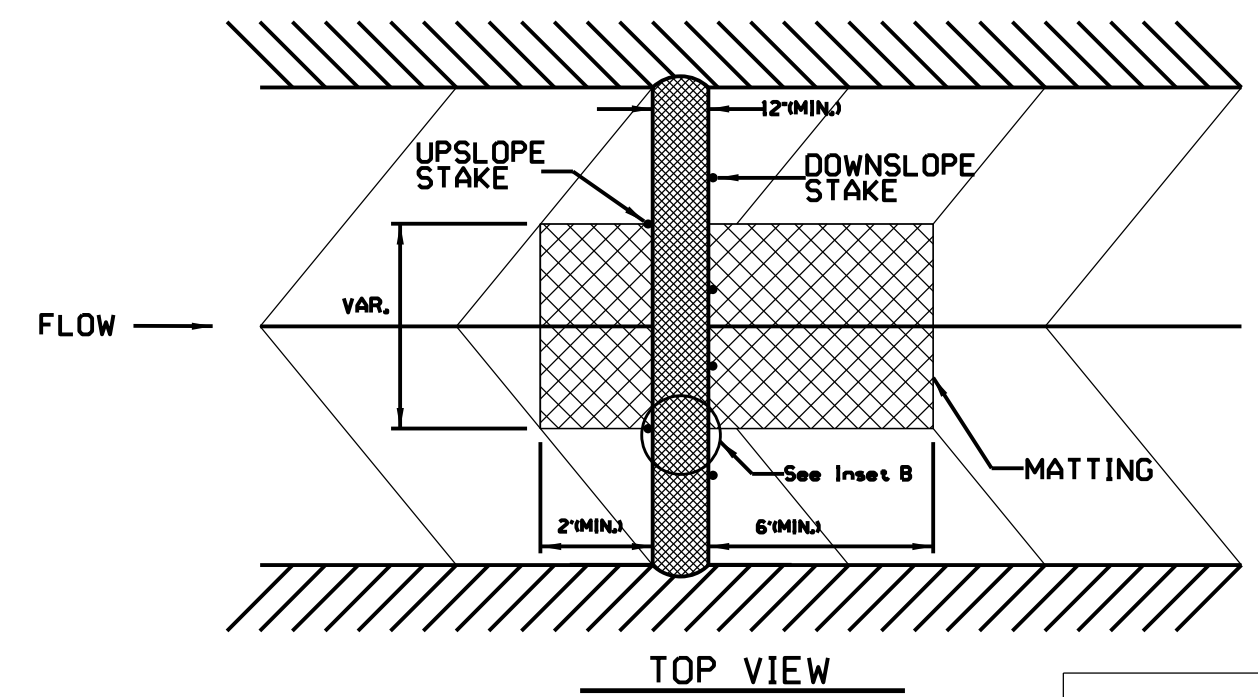
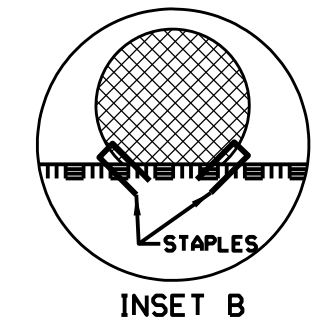
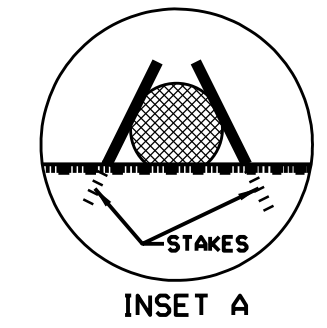


NOT TO SCALE

WATTLE DETAIL

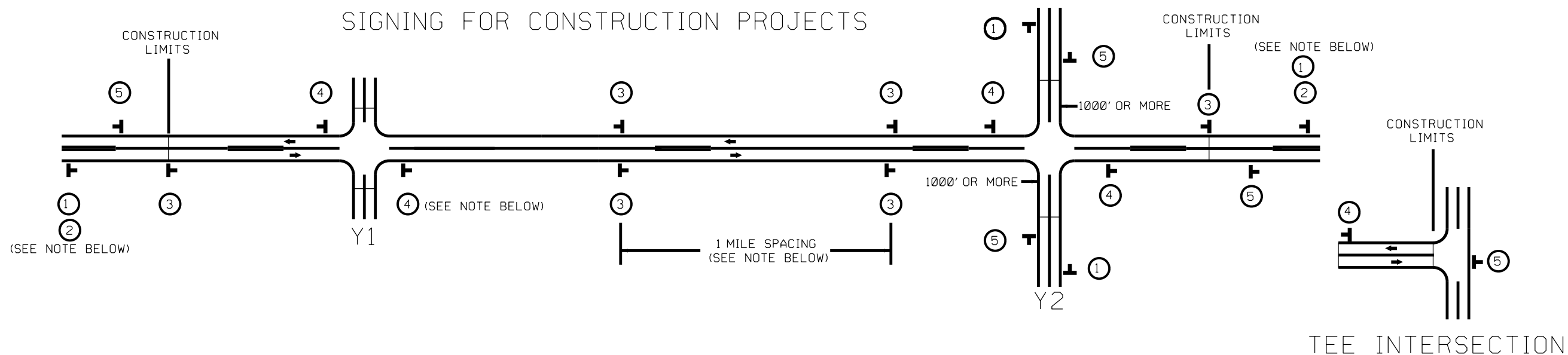


- NOTES:**
- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
 - USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
 - ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
 - INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
 - PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12' IN LENGTH.
 - INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
 - INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE

SIGNING FOR CONSTRUCTION PROJECTS



LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.
	 W20-1 48" X 48" W7-3aP 24" X 18" *2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)	
	 SP 13107 48" X 48" - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	 SP 13106 48" X 48" - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	 G20-2 A 48" X 24" PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.



PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.



CONSTRUCTION PROJECTS
 ADVANCE WARNING SIGNS
 FOR
 RURAL AND SUBURBAN
 2 LANE ROADWAYS