

PROJECT REFERENCE NO.	SHEET NO.
DB00491	1

# CRAVEN & PITT COUNTY

DB00491

WBS# 2021CPT.02.07.10251

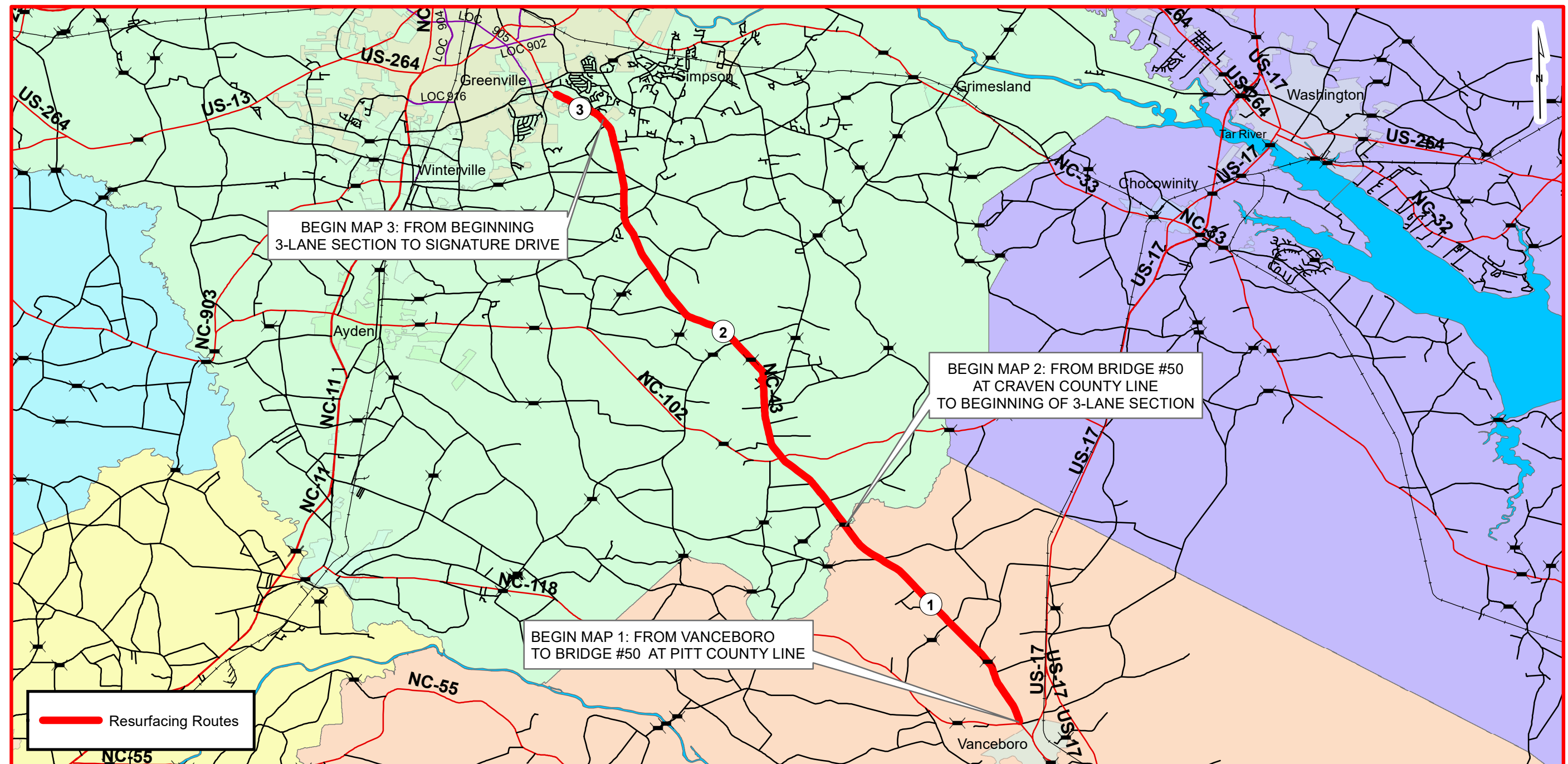
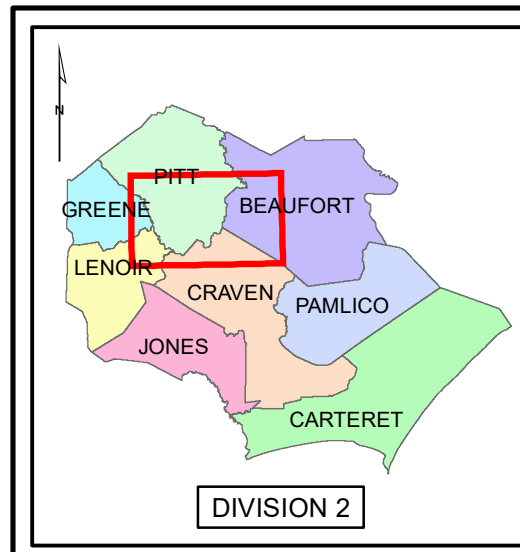
WBS# 2021CPT.02.08.10741

LOCATION: NC-43 FROM VANCEBORO TO SIGNATURE DRIVE

TYPE OF WORK: MILLING, RESURFACING



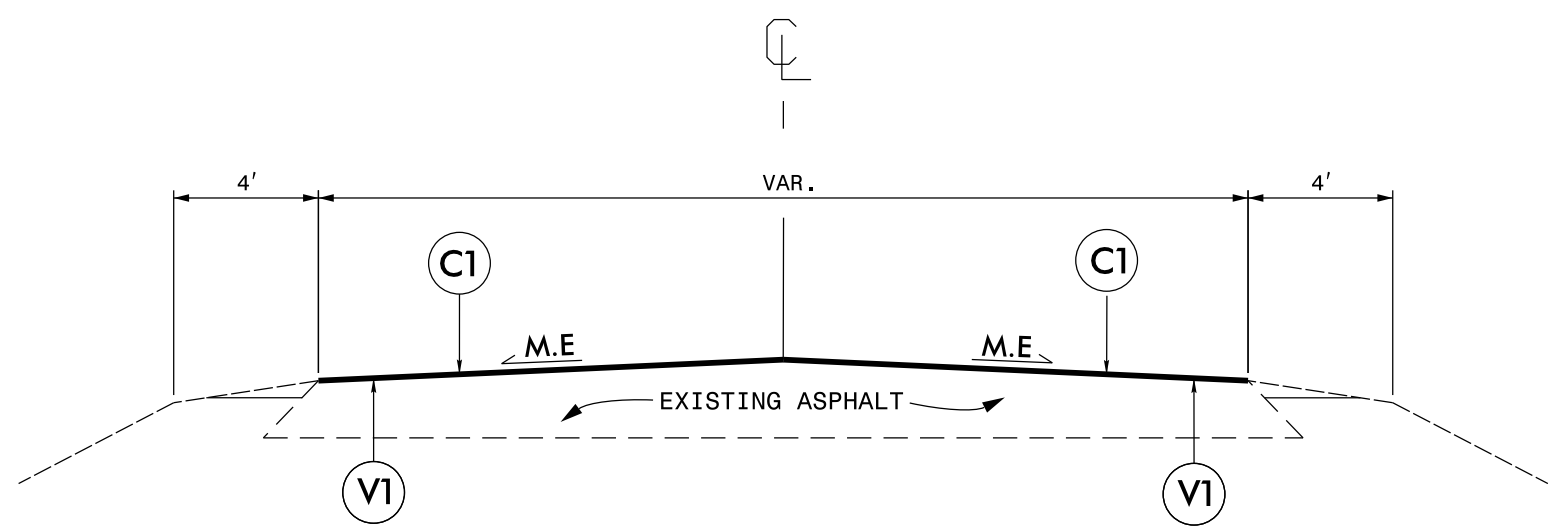
**NCDOT**  
DIVISION 2



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
V1	MILLING DEPTH 1 1/2" FOR THE ENTIRE WIDTH OF ROADWAY.
DRAWINGS NOT TO SCALE	

## TYPICAL SECTION NO. 1

MAP 1, 2, 3



NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

**NOTE:**

1. INCLUDED 1 1/2" MILLING FOR THE ENTIRE WIDTH OF THE ROADWAY, AS DIRECTED BY THE ENGINEER.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.

PROJECT NO.	SHEET NO.	TOTAL NO.
DB00491	3	

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	1297000000-E	1330000000-E	1523000000-E	1575000000-E	1880000000-E	2830000000-N	2845000000-N	
												1½" MILLING	INCIDENTAL MILLING	SURFACE COURSE, \$9.5C	ASPHALT BINDER FOR PLANT MIX	JOINT REPAIR	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	
												MI	FT	SY	SY	TONS	TONS	TONS	EA
2021CPT.02.07.10251	Craven	1	NC-43 (MP 17.80 TO 24.54)	VANCEBORO TO BRIDGE #50 AT THE PITT COUNTY LINE	1	2	2WU	NO	NO	6.74	26	103,000	1,000	8,700	522				
<b>TOTAL FOR MAP NO. 1</b>											<b>6.74</b>		<b>103,000</b>	<b>1,000</b>	<b>8,700</b>	<b>522</b>			
<b>TOTAL FOR PROJ NO. 2021CPT.02.07.10251</b>											<b>6.74</b>		<b>103,000</b>	<b>1,000</b>	<b>8,700</b>	<b>522</b>			
2021CPT.02.08.10741	Pitt	2	NC-43 (MP 0.00 TO 13.00)	BRIDGE #50 AT THE CRAVEN COUNTY LINE TO THE 3-LANE SECTION	1	2	2WU	NO	NO	13.00	26	200,000	1,000	16,800	1,008	425			
<b>TOTAL FOR MAP NO. 2</b>											<b>13.00</b>		<b>200,000</b>	<b>1,000</b>	<b>16,800</b>	<b>1,008</b>	<b>425</b>		
2021CPT.02.08.10741	Pitt	3	NC-43 (MP 13.00 TO 14.20)	BEGIN 3-LANE SECTION TO SIGNATURE DRIVE	1	3	2WU	NO	NO	1.20	44	30,976	1,000	2,805	168	50	1	1	
<b>TOTAL FOR MAP NO. 3</b>											<b>1.20</b>		<b>30,976</b>	<b>1,000</b>	<b>2,805</b>	<b>168</b>	<b>50</b>	<b>1</b>	<b>1</b>
<b>TOTAL FOR PROJ NO. 2021CPT.02.08.10741</b>											<b>14.20</b>		<b>230,976</b>	<b>2,000</b>	<b>19,605</b>	<b>1,176</b>	<b>475</b>	<b>1</b>	<b>1</b>
<b>GRAND TOTAL</b>											<b>20.94</b>		<b>333,976</b>	<b>3,000</b>	<b>28,305</b>	<b>1,698</b>	<b>475</b>	<b>1</b>	<b>1</b>

PROJECT NO.	SHEET NO.	TOTAL NO.
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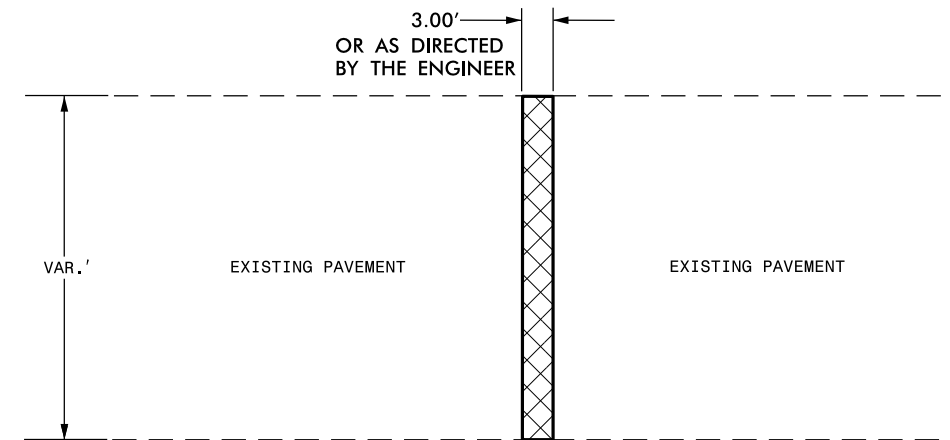
## TRAFFIC CONTROL

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	441300000-E WORK ZONE ADVANCE/GE NERAL WARNING SIGNING	445700000-N TEMPORARY TRAFFIC CONTROL
								MI	FT	SF	LS
								2021CPT.02.07.10251	Craven	1	NC-43 (MP 17.80 TO 24.54)
<b>TOTAL FOR MAP NO. 1</b>								<b>6.74</b>		<b>800</b>	<b>0.32</b>
<b>TOTAL FOR PROJ NO. 2021CPT.02.07.10251</b>								<b>6.74</b>		<b>800</b>	<b>0.32</b>
2021CPT.02.08.10741	Pitt	2	NC-43 (MP 0.00 TO 13.00)	BRIDGE #50 AT THE CRAVEN COUNTY LINE TO THE 3-LANE SECTION	1	2	2WU	13.00	26	1,500	0.63
<b>TOTAL FOR MAP NO. 2</b>								<b>13.00</b>		<b>1,500</b>	<b>0.63</b>
2021CPT.02.08.10741	Pitt	3	NC-43 (MP 13.00 TO 14.20)	BEGIN 3-LANE SECTION TO SIGNATURE DRIVE	1	3	2WU	1.20	44	150	0.05
<b>TOTAL FOR MAP NO. 3</b>								<b>1.20</b>		<b>150</b>	<b>0.05</b>
<b>TOTAL FOR PROJ NO. 2021CPT.02.08.10741</b>								<b>14.20</b>		<b>1,650</b>	<b>1</b>
<b>GRAND TOTAL</b>								<b>20.94</b>		<b>2,450</b>	<b>1</b>

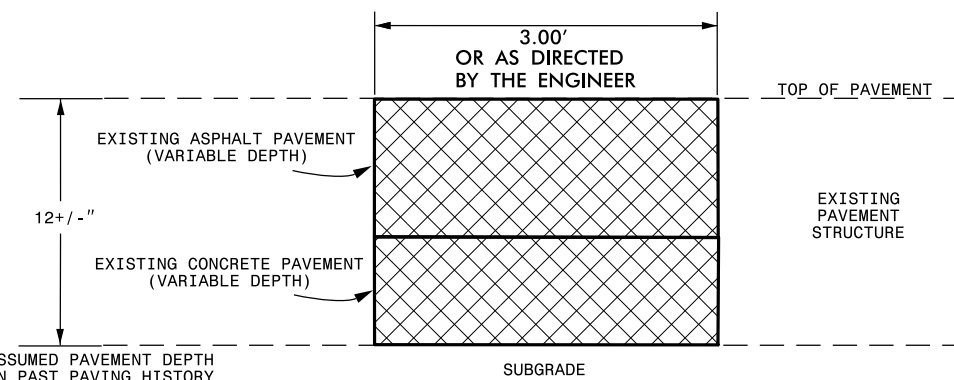
# JOINT REPAIR DETAIL

JOINT SCHEDULE	
MAP	# JOINTS
2, 3	75

CONTRACTOR SHALL COORDINATE WITH RESIDENT ENGINEER'S OFFICE FOR LOCATION OF JOINTS TO BE REPAIRED.



## CROSS-SECTION

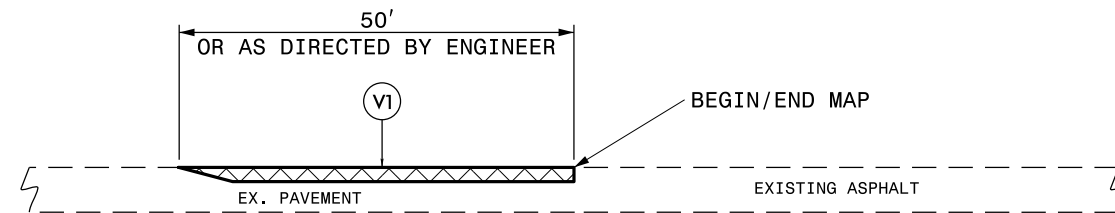


NOTE: ASSUMED PAVEMENT DEPTH BASED ON PAST PAVING HISTORY

**NOTE:**

REMOVE ASPHALT AND CONCRETE AT JOINT LOCATIONS AS DIRECTED BY THE ENGINEER (BY SAWING CLEAN JOINTS). REMOVE A TOTAL WIDTH OF 3' (APPROX. 1.5' EACH SIDE OF JOINT). REMOVE AND REPLACE WITH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C. THERE WILL BE NO DIRECT PAY FOR THIS WORK AS IT WILL BE CONSIDERED INCIDENTAL TO THE LINE ITEM, JOINT REPAIR (TONS)

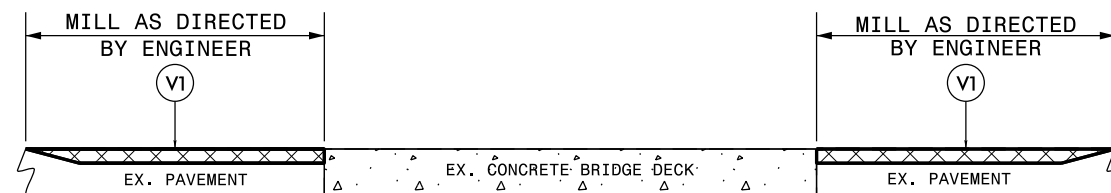
# MILLING TYPICALS



**DETAIL 1**  
BEGIN/END MAP TIE-IN

**NOTE:**

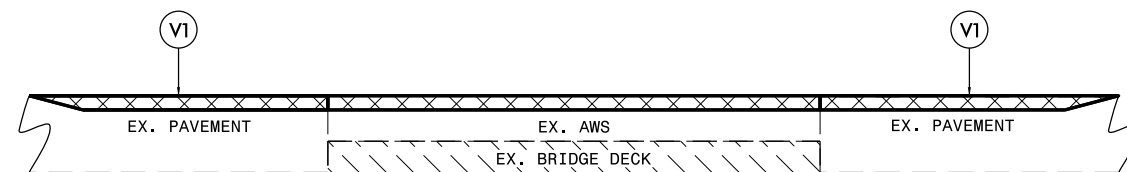
- MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



**DETAIL 2**  
BRIDGE MILLING

**NOTE:**

- MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

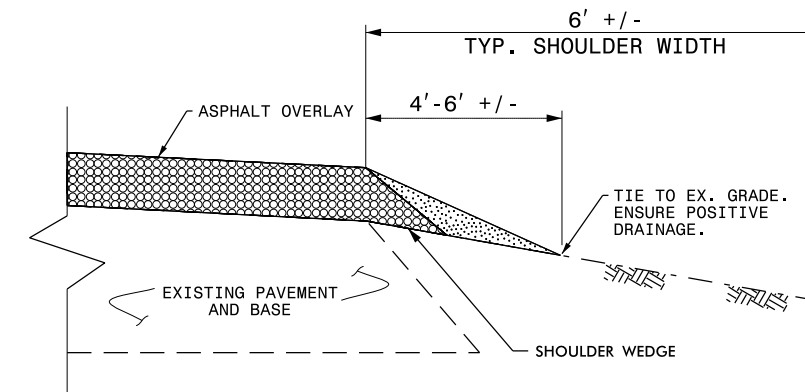


**DETAIL 3**  
BRIDGE MILLING

**NOTE:**

- INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

# SHOULDER RECONSTRUCTION TYPICAL

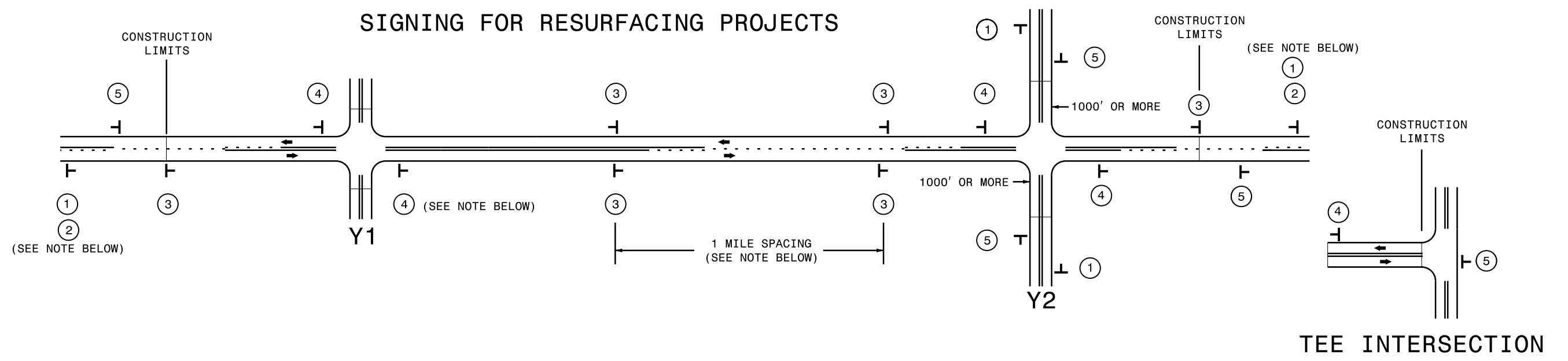


**SHOULDER RECONSTRUCTION DETAIL**

**NOTE:**

- SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

# SIGNING FOR RESURFACING PROJECTS



**LEGEND**  
 STATIONARY SIGN  
 DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	①	 <small>W20-1 48" X 48"</small>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	②	 <small>W7-3aP 24" X 18"</small>	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	③	 <small>SP 13107 48" X 48"</small>	<ul style="list-style-type: none"> <li>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</li> <li>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</li> </ul>	
	④	 <small>SP 13106 48" X 48"</small>	<ul style="list-style-type: none"> <li>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</li> <li>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</li> <li>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</li> <li>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</li> <li>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</li> <li>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</li> </ul>	
	⑤	 <small>G20-2 A 48" X 24"</small>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

**RESURFACING  
ADVANCE WARNING SIGNS  
FOR  
RURAL AND SUBURBAN  
2 LANE ROADWAYS**