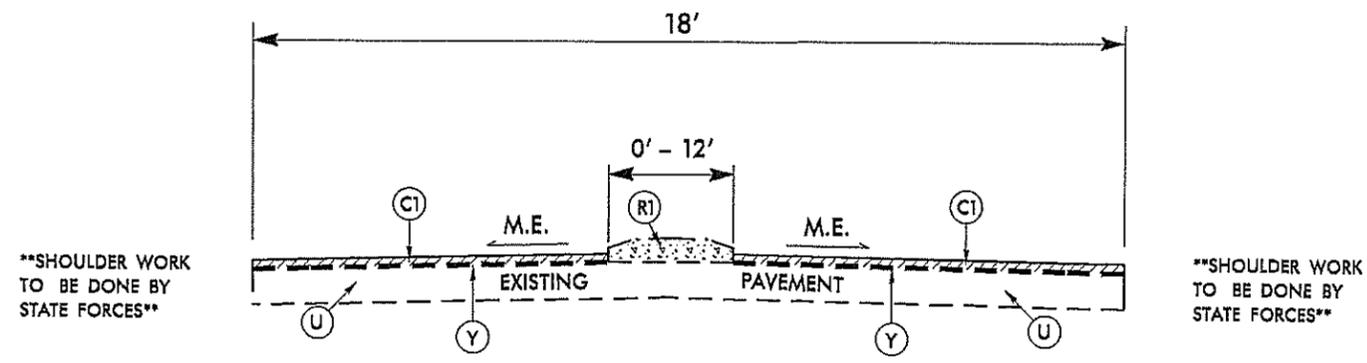


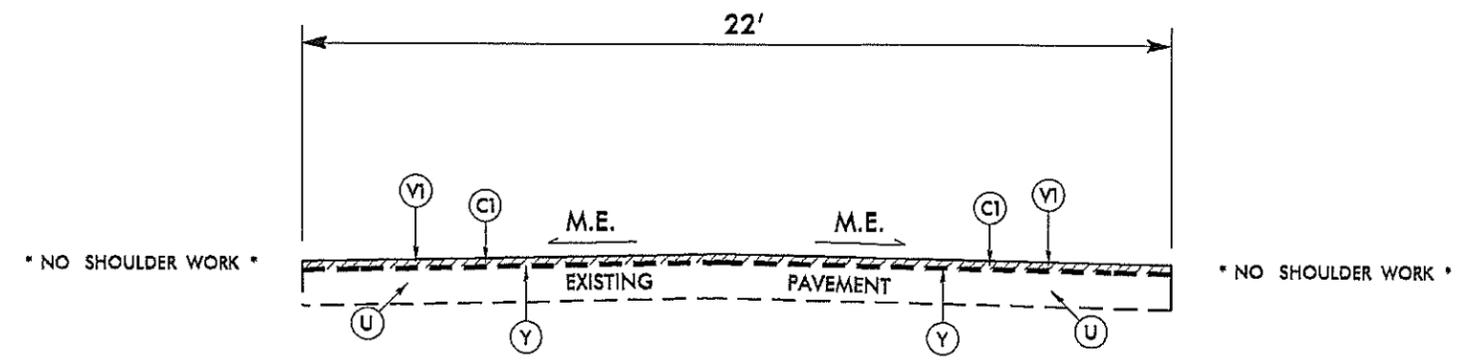
8/17/99

REVISIONS

29-DEC-2014 15:32
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TYPICAL SECTION NO. 1
 MAP NO. 1
 SR 1406 (BELL SWAMP RD.)
 MP 0.00 - MP 3.55



TYPICAL SECTION NO. 2
 MAP NO. 2
 SR 1472 (VILLAGE RD.)
 MP 1.32 - MP 3.77
 (NO WORK BRIDGE #208
 MP 3.17 - MP 3.19)

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1 1/2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.6B, AT AN AVERAGE RATE OF 188 LBS. PER SQ.YD.
R1	EXISTING CONCRETE MONOLITHIC ISLAND
U	EXISTING PAVEMENT
V1	MILLING BITUMINOUS PAVEMENT 1 1/2" DEPTH
Y	PAVEMENT INTERLAYER

NOTE: PLACE PAVEMENT INTERLAYER PRIOR TO RESURFACING. SEE PROJECT SPECIAL PROVISIONS.

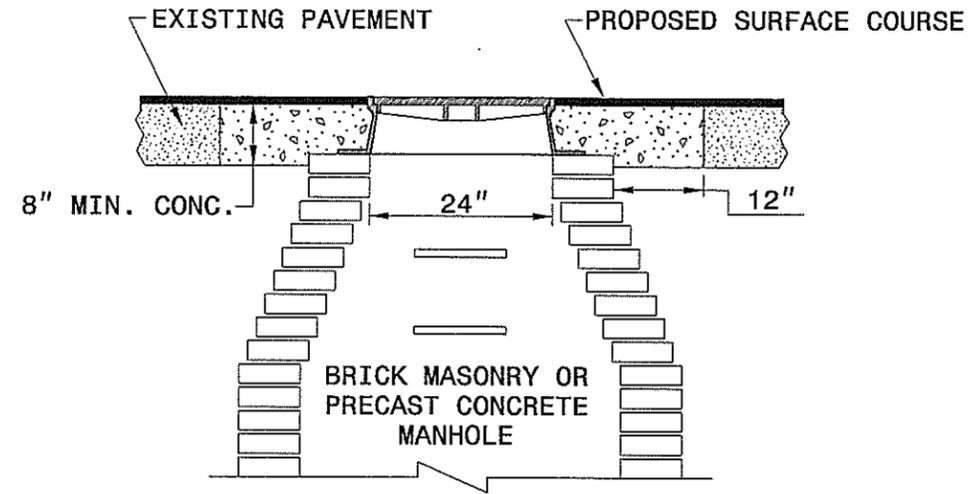
STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

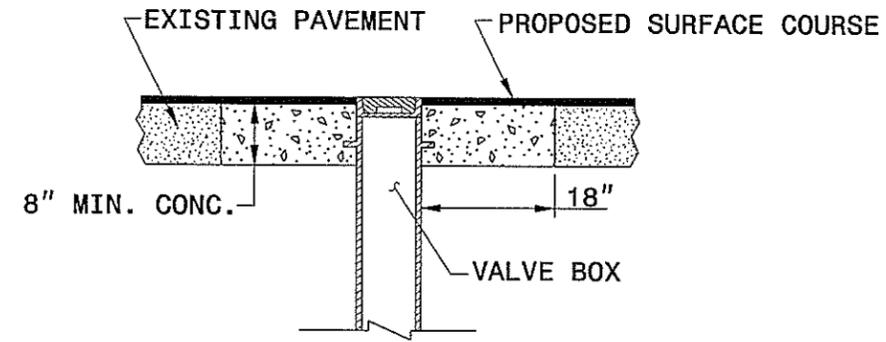
SHEET 1 OF 1
840D55

GENERAL NOTES:

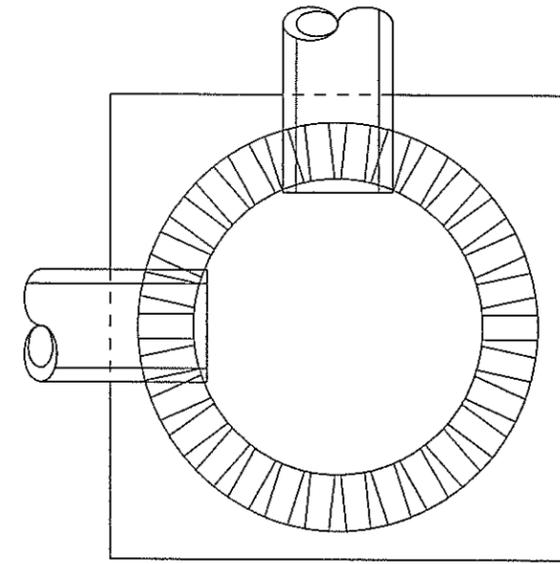
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS 1/2" +/- 1/8"



MANHOLE CONCRETE ENCASEMENT



VALVE BOX CONCRETE ENCASEMENT



ELEVATION VIEW

PLACE BRICK ACCORDING TO ELEVATION VIEW

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

SHEET 1 OF 1
840D55

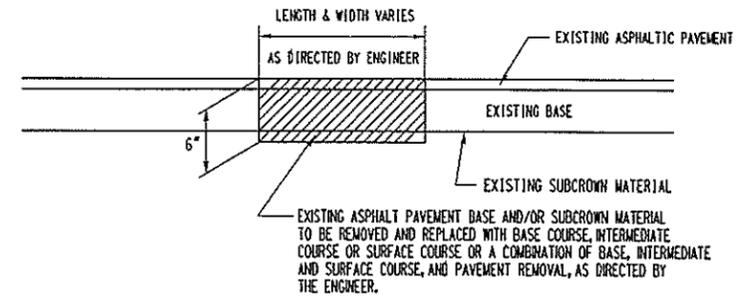
PROJECT SERVICES UNIT
STANDARDS AND SPECIAL DESIGN
Office 919-250-4128 FAX 919-250-4119

SEE PLATE FOR TITLE

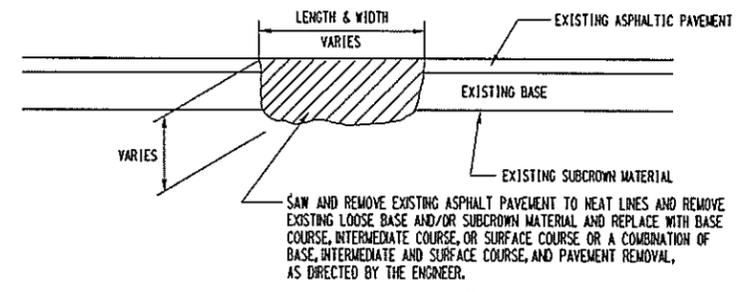
ORIGINAL BY:	DATE:
MODIFIED BY: E.E. WARD	DATE:
CHECKED BY:	DATE:
FILE SPEC.: /usr/details/stand/840d55.dgn	

07-DEC-2005 14:25
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ericward H1 P5222233

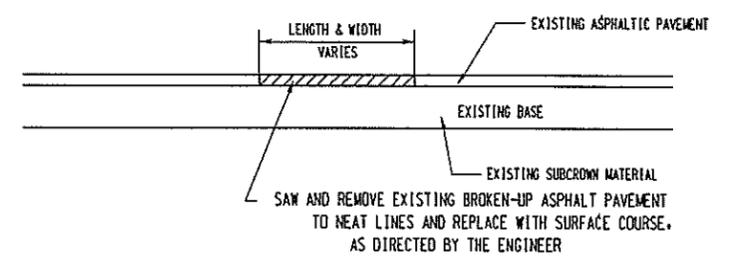
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



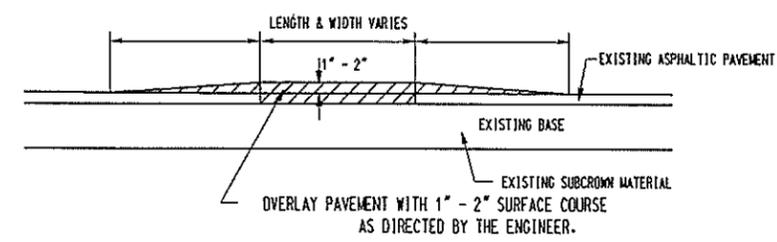
DETAIL NO. 1



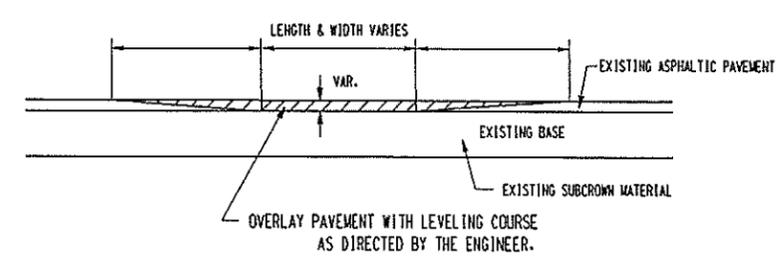
DETAIL NO. 2



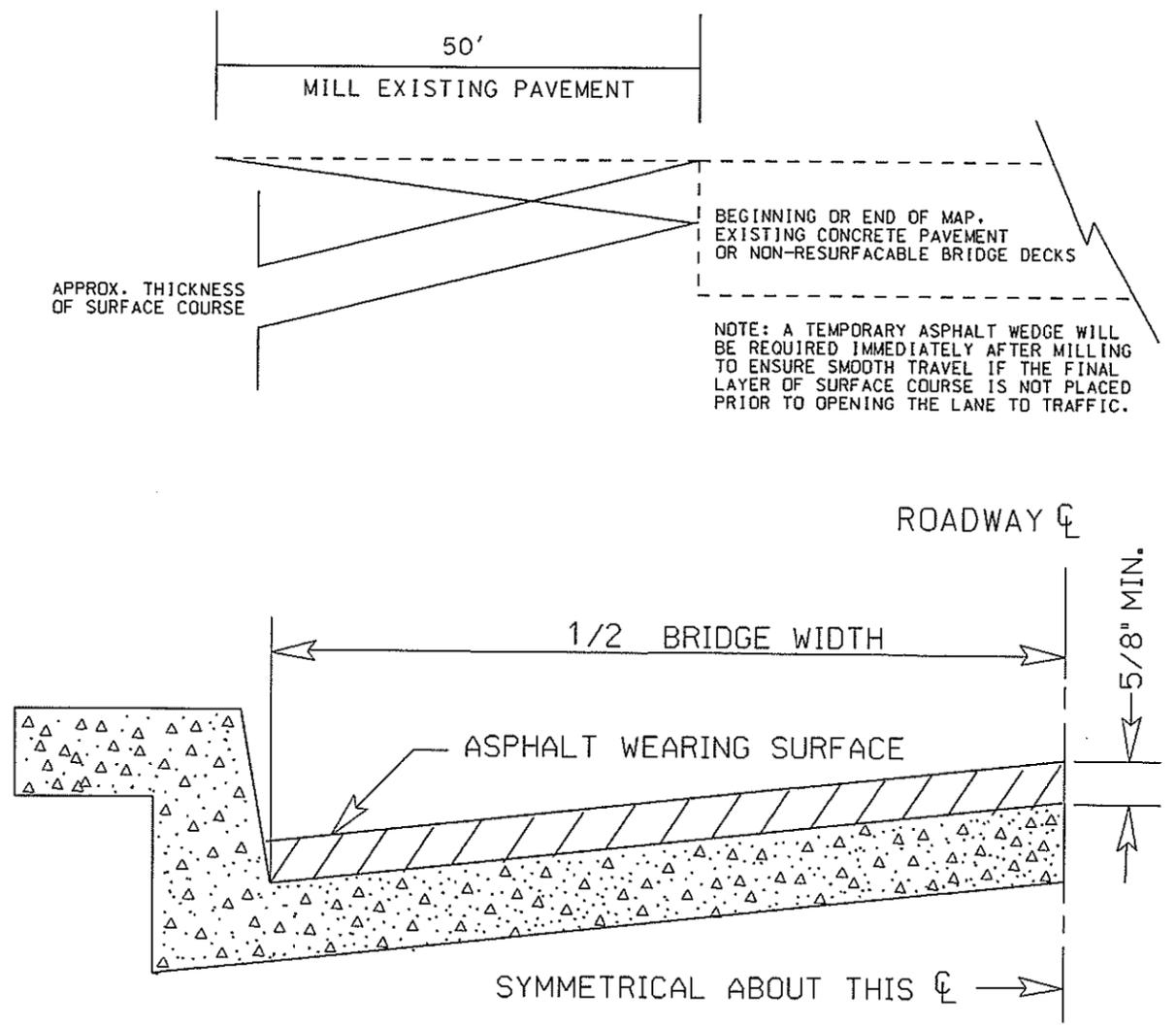
DETAIL NO. 3



DETAIL NO. 4



DETAIL NO. 5



BRIDGE HALF TYPICAL SECTION

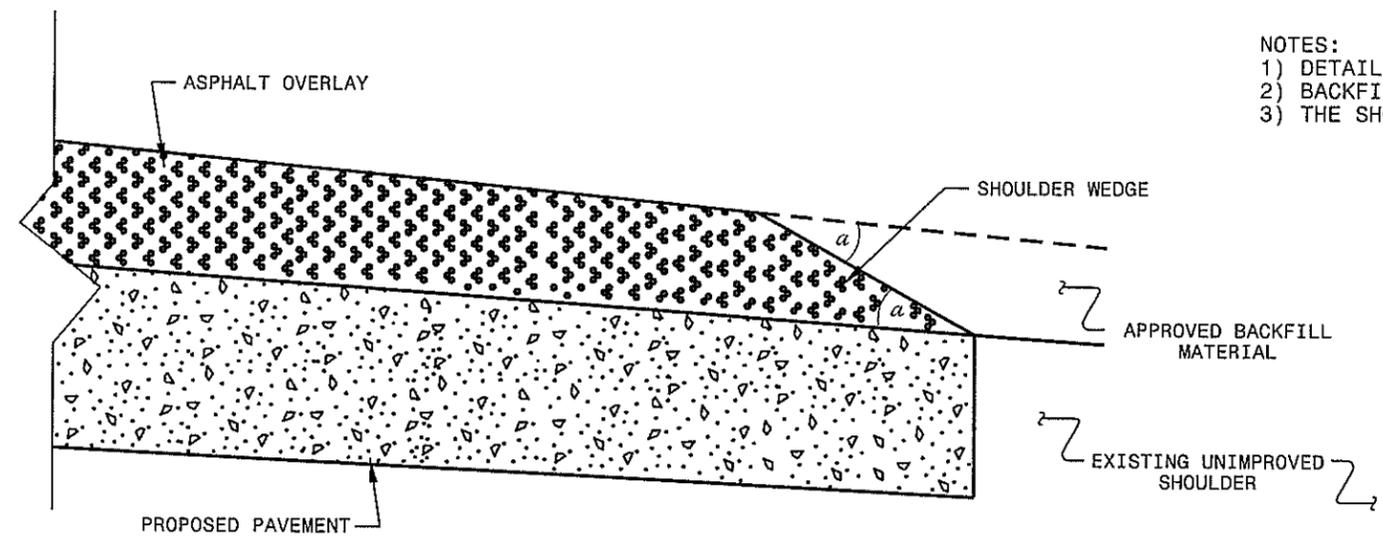
FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 5/8" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1-1/2" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

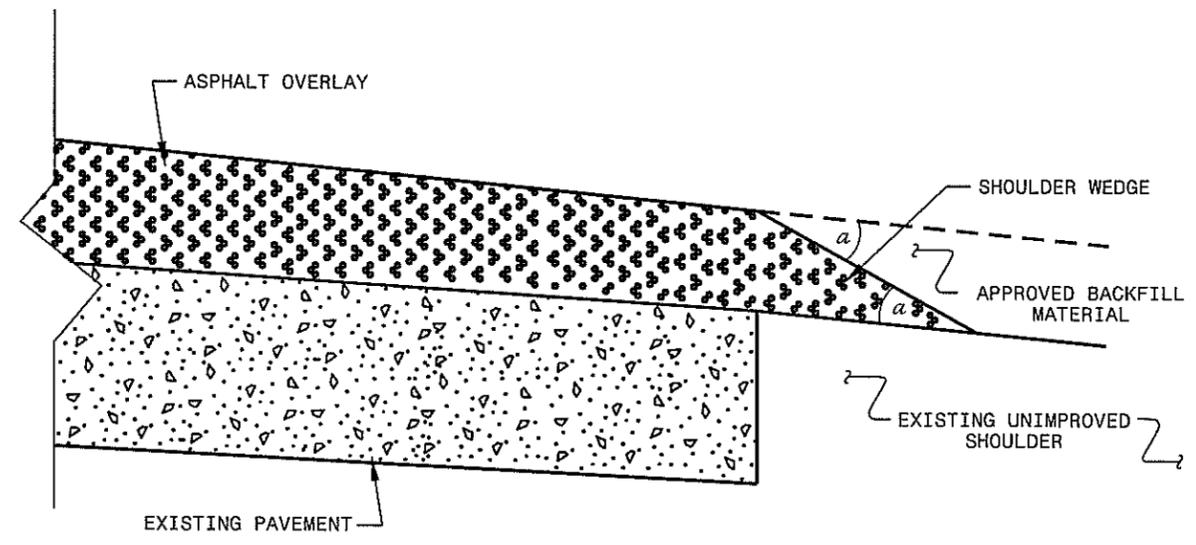
REVISIONS

20-DEC-2014 15:45
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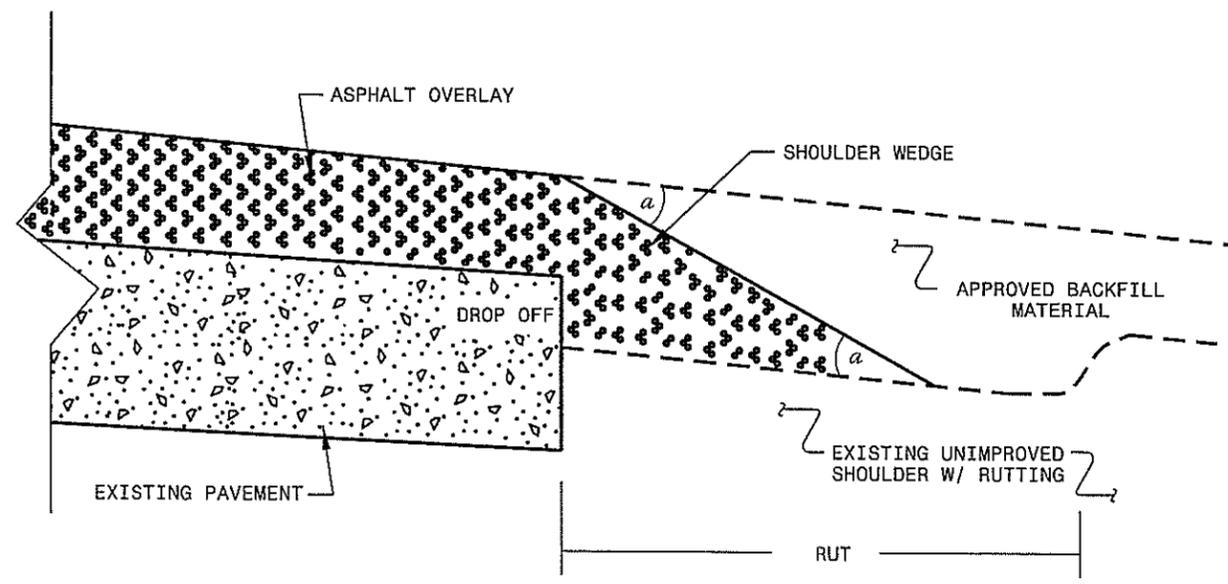
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT Office 919-707-6950 FAX 919-250-4119			
SHOULDER WEDGE DETAILS			
ORIGINAL BY:	T. SPELL	DATE:	7-10-11
MODIFIED BY:		DATE:	10/16/12
CHECKED BY:		DATE:	
FILE SPEC:	s:\usr\details\stand\shoulderwedge\detail.dgn		

 SYSTEMS

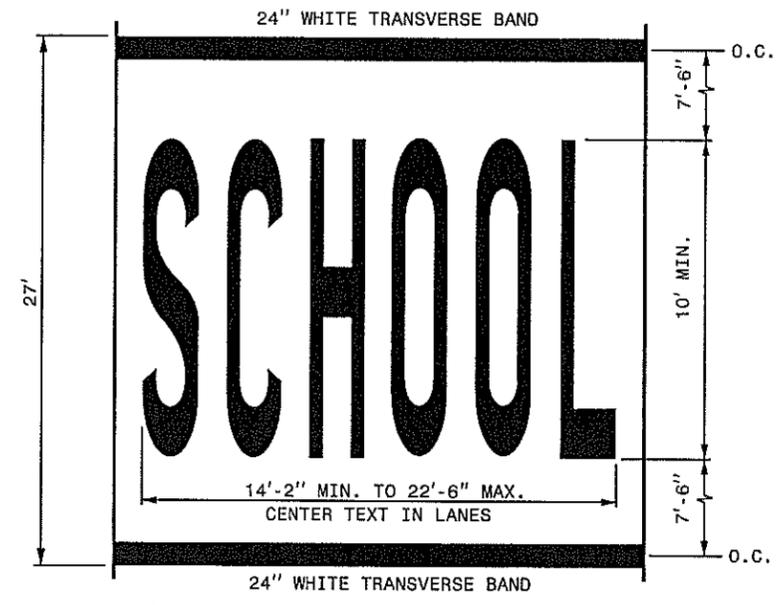
STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

1-12

ENGLISH DETAIL DRAWING FOR
PAVEMENT MARKINGS
SYMBOLS AND WORD MESSAGES

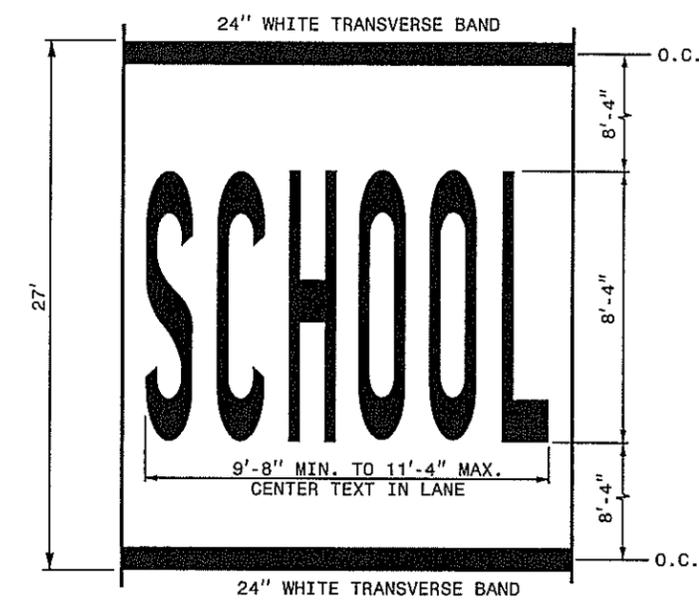
REVISED 9/14/11
SHEET 3 OF 8
1205D08

MULTI-LANE WIDTH "SCHOOL"

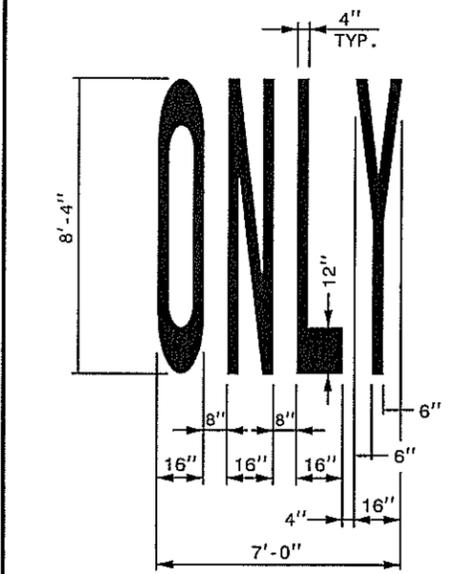


LETTER HEIGHT = 10' MIN.
LETTER WIDTH = 20"
SPACING = 10" MIN./30" MAX. (USE EQUAL SPACING BETWEEN LETTERS)
NOTE: THE TWO-LANE PAVEMENT MARKING DIMENSIONS OF "SCHOOL" SHOWN IN PART 7 OF THE MUTCD MAY ALSO BE USED.

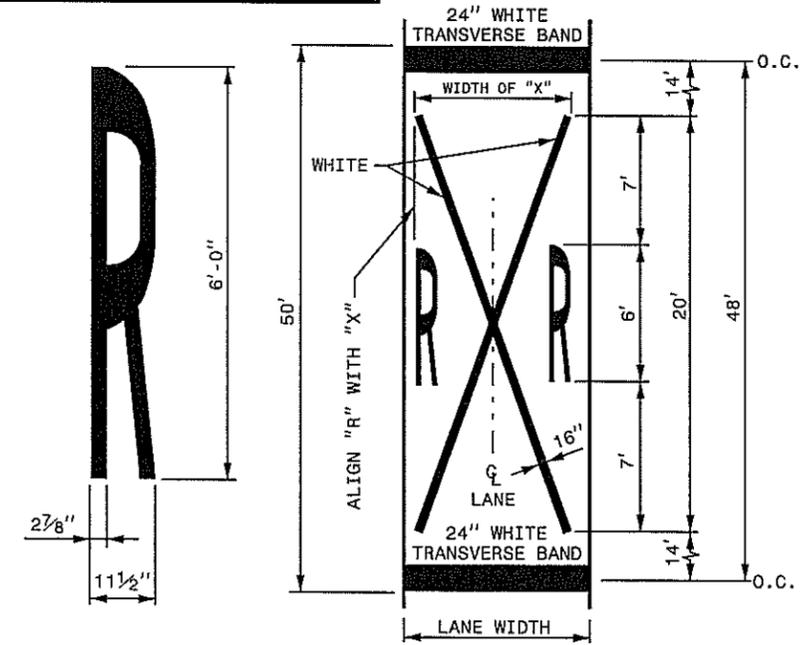
SINGLE LANE WIDTH "SCHOOL"



LETTER HEIGHT = 8'-4"
LETTER WIDTH = 16"
SPACING = 4" MIN./8" MAX. (USE EQUAL SPACING BETWEEN LETTERS)



RAILROAD RXR SYMBOL



LANE WIDTH (FEET)	WIDTH OF "X" (FEET)
8' ≤ W ≤ 9'	7'
9' < W ≤ 12'	8'
W > 12'	10'

GENERAL NOTES:

- 1- THE SCHOOL PAVEMENT MARKING CONSISTS OF SIX (6) CHARACTERS. THE TWO (2) 24" TRANSVERSE BANDS WILL BE PAID FOR UNDER A SEPARATE PAY ITEM. REFER TO ROADWAY STANDARD DRAWING 1205.10 FOR ADDITIONAL PAVEMENT MARKING GUIDANCE.
- 2- PAVEMENT MARKING ADVANCE OF A HIGHWAY-RAIL CROSSING SHALL CONSIST OF TWO (2) CHARACTERS AND TWO (2) 16" LINES (FORMING AN X) WHICH ARE PAID FOR UNDER TWO SEPARATE PAY ITEMS. THE TWO (2) 24" TRANSVERSE BANDS WILL BE PAID FOR UNDER A SEPARATE PAY ITEM. REFER TO ROADWAY STANDARD DRAWING 1205.11 FOR ADDITIONAL PAVEMENT MARKING GUIDANCE.

ENGLISH DETAIL DRAWING FOR
PAVEMENT MARKINGS
SYMBOLS AND WORD MESSAGES

REVISED 9/14/11
SHEET 3 OF 8
1205D08

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

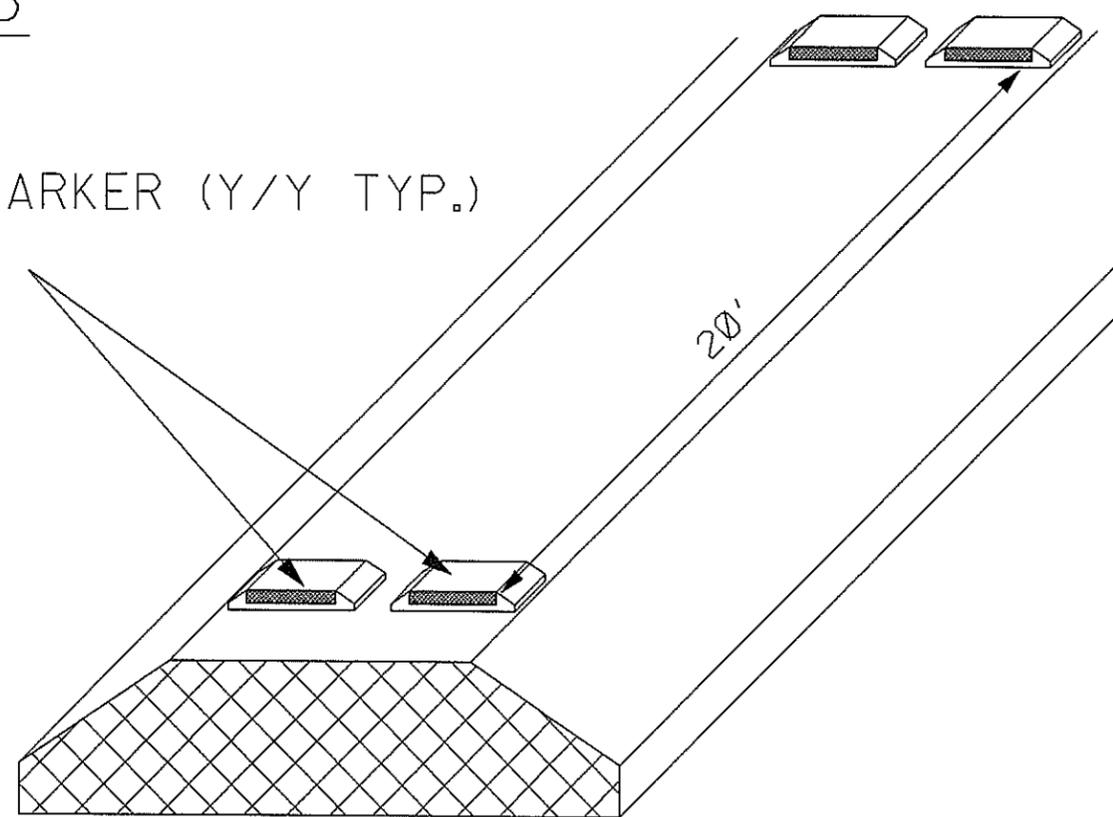
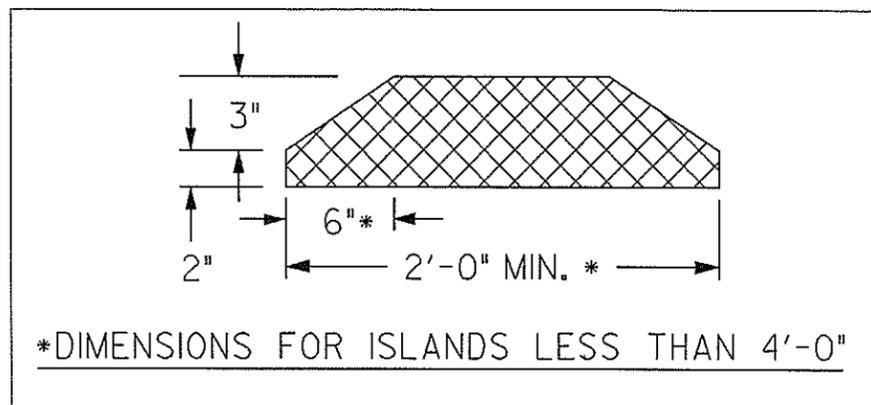
SEE TITLE BLOCK

ORIGINAL BY: J. HOWERTON DATE: 10/5/11
MODIFIED BY: DATE:
CHECKED BY: DATE:
FILE SPEC.: s:\one112 Stds to Special Details\560d01

PAVEMENT MARKING DETAIL

PAVEMENT MARKER DETAIL FOR CONCRETE ISLANDS

RAISED PAVEMENT MARKER (Y/Y TYP.)
(STD. DWG 1251.01)



MONOLITHIC CONCRETE ISLAND

(SEE STANDARD DRAWINGS 852.01, 852.02, & 852.06 FOR DETAILS.)

REVISIONS

8/17/99

30-DEC-2014 15:46
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20-DEC-2014 15:46
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PROJECT NO.	SHEET NO.	TOTAL NO.
3CR.20101.171	8	

SUMMARY OF QUANTITIES

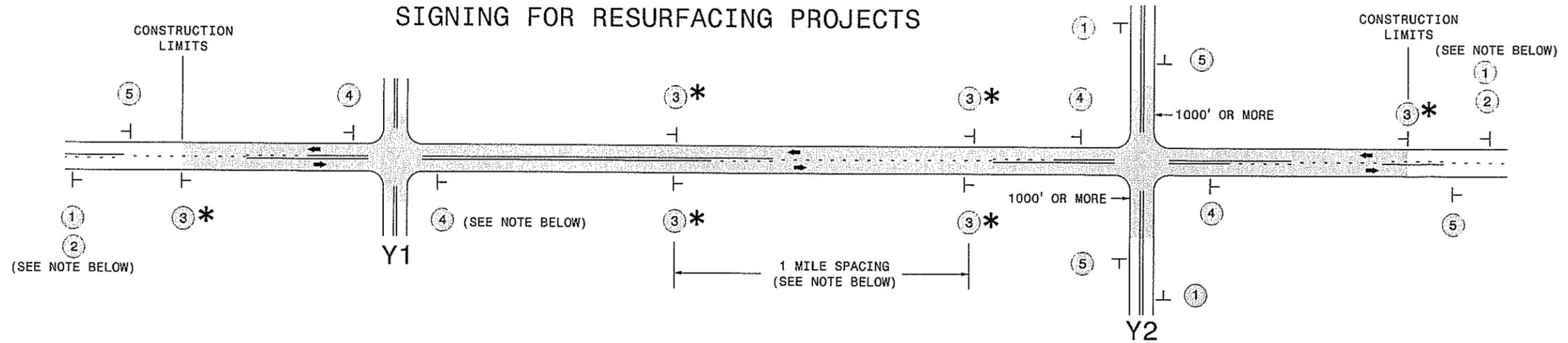
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	INC. STONE BASE TONS	1 1/2" MILLING SY	INC. MILLING SY	SURFACE COURSE, S9.5B TONS	ASPHALT BINDER FOR PLANT MIX TON	PATCHING EXISTING PAVEMENT (FULL DEPTH) TON	PATCHING EXISTING PAVEMENT (MILL) TON	PAVEMENT INTERLAYER SY	ADJ. OF MANHOLES EA
3CR.20101.171	Brunswick	1	SR 1406 (BELL SWAMP RD.)	FROM PVMT CHANGE TO US 17 (MP 0.00 TO MP 3.55)	1	2	2WU	NO	NO	3.55	18	140		1,100	3,329	200	390		37,488.00	
TOTAL FOR MAP NO. 1										3.55		140		1,100	3,329	200	390		37,488.00	
3CR.20101.171	Brunswick	2	SR 1472 (VILLAGE RD.)	FROM 250' WEST OF SR 1486 (POST OFFICE RD. NE) TO PVMT CHANGE (MP 1.32 TO MP 3.77)	2	2	2WU	NO	NO	2.43	22	100	34,400		3,050	183	60	280	34,074.00	2
TOTAL FOR MAP NO. 2										2.43		100	34,400		3,050	183	60	280	34,074.00	2
TOTAL FOR PROJ NO. 3CR.20101.171										5.98		240	34,400	1,100	6,379	383	450	280	71,562.00	2
GRAND TOTAL										5.98		240	34,400	1,100	6,379	383	450	280	71,562.00	2

PROJECT NO.	SHEET NO.	TOTAL NO.
3CR.20101.171	9	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4685000000-E	4686000000-E			4700000000-E	4705000000-E	4710000000-E	4721000000-E	4725000000-E			4850000000-E	4900000000-N				
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 120 M YELLOW THERMO	4" X 120 M WHITE THERMO	12" X 90 M YELLOW THERMO	16" X 120 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO RXR 120 M	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	4" LINE REMOVAL	YELLOW & YELLOW MARKERS	CRYSTAL & RED MARKERS					
										SF	LS	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA			
3CR.20101.171	Brunswick	1	SR 1406 (BELL SWAMP RD.)	FROM PVMT CHANGE TO US 17 (MP 0.00 TO MP 3.55)	1	2	2WU	3.55	18	400	0.60	37,488	37,488											235				
TOTAL FOR MAP NO. 1												3.55												235				
3CR.20101.171	Brunswick	2	SR 1472 (VILLAGE RD.)	FROM 250' WEST OF SR 1486 (POST OFFICE RD. NE) TO PVMT CHANGE (MP 1.32 TO MP 3.77)	2	2	2WU	2.43	22	272	0.40	24,714	25,144	938	177	100	50	4	2	5	424	194	39					
TOTAL FOR MAP NO. 2												2.43													424	194	39	
TOTAL FOR PROJ NO. 3CR.20101.171												5.98			672	1	62,202	62,632	938	177	100	50	4	2	5	424	429	39
GRAND TOTAL												5.98			672	1	62,202	62,632	938	177	100	50	4	2	5	424	429	39
															63,570					7					468			

SIGNING FOR RESURFACING PROJECTS



LEGEND	
	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

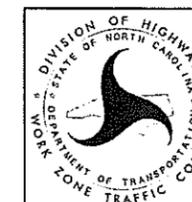
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	①		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p style="text-align: center;">NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p style="text-align: center;">WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p style="text-align: center;">PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	②		#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	③*		PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	④		THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
⑤		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

* SIGNING FOR ASPHALT SURFACE TREATMENTS (ONLY)

SUBSTITUTE LOW/SOFT SHOULDER SIGNS BY ALTERNATING THE FOLLOWING TWO SIGNS:
STARTING WITH "UNMARKED PAVEMENT AHEAD" (SP 06026) FOLLOWED BY "LOOSE GRAVEL" (W8-7).



RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS

C:\Users\jgibson\Documents\Resurfacing\Resurfacing_AdvWarn_2Ln.dgn
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 11/11/2010 10:10:10 AM