

## CATEGORICAL EXCLUSION ACTION CLASSIFICATION FORM

TIP Project No.	<u>BP-5500G</u>
State Project No.	<u>50070</u>
Federal Project No.	<u>BRNHS-000S- (747)</u>

- A. Project Description: (Include project scope and location and refer to the attached project location map.)

**Duplin Bridge 30: Epoxy Overlay on deck, joint replacement, mill and replace asphalt on approaches. Bridge is located on NC 41 0.77 mile west of SR 1101 (Cornwallis Rd) and crosses Rock Fish Creek (C; Sw).**

- B. Purpose and Need:

**Bridge preservation efforts, extend the life of the structures**

- C. Proposed Improvements:

Circle one or more of the following Type II improvements which apply to the project:

1. Modernization of a highway by resurfacing, restoration, rehabilitation, reconstruction, adding shoulders, or adding auxiliary lanes (e.g., parking, weaving, turning, climbing).
  - a. Restoring, Resurfacing, Rehabilitating, and Reconstructing pavement (3R and 4R improvements)
  - b. Widening roadway and shoulders without adding through lanes
  - c. Modernizing gore treatments
  - d. Constructing lane improvements (merge, auxiliary, and turn lanes)
  - e. Adding shoulder drains
  - f. Replacing and rehabilitating culverts, inlets, and drainage pipes, including safety treatments
  - g. Providing driveway pipes
  - h. Performing minor bridge widening (less than one through lane)
2. Highway safety or traffic operations improvement projects including the installation of ramp metering control devices and lighting.
  - a. Installing ramp metering devices
  - b. Installing lights
  - c. Adding or upgrading guardrail
  - d. Installing safety barriers including Jersey type barriers and pier protection
  - e. Installing or replacing impact attenuators
  - f. Upgrading medians including adding or upgrading median barriers
  - g. Improving intersections including relocation and/or realignment
  - h. Making minor roadway realignment
  - i. Channelizing traffic
  - j. Performing clear zone safety improvements including removing hazards and flattening slopes
  - k. Implementing traffic aid systems, signals, and motorist aid

1. Installing bridge safety hardware including bridge rail retrofit
  3. Bridge rehabilitation, reconstruction, or replacement or the construction of grade separation to replace existing at-grade railroad crossings.
    - a. Rehabilitating, reconstructing, or replacing bridge approach slabs
    - b. Rehabilitating or replacing bridge decks
    - c. Rehabilitating bridges including painting (no red lead paint), scour repair, fender systems, and minor structural improvements
    - d. Replacing a bridge (structure and/or fill)
  4. Transportation corridor fringe parking facilities.
  5. Construction of new truck weigh stations or rest areas.
  6. Approvals for disposal of excess right-of-way or for joint or limited use of right-of-way, where the proposed use does not have significant adverse impacts.
  7. Approvals for changes in access control.
  8. Construction of new bus storage and maintenance facilities in areas used predominantly for industrial or transportation purposes where such construction is not inconsistent with existing zoning and located on or near a street with adequate capacity to handle anticipated bus and support vehicle traffic.
  9. Rehabilitation or reconstruction of existing rail and bus buildings and ancillary facilities where only minor amounts of additional land are required and there is not a substantial increase in the number of users.
  10. Construction of bus transfer facilities (an open area consisting of passenger shelters, boarding areas, kiosks and related street improvements) when located in a commercial area or other high activity center in which there is adequate street capacity for projected bus traffic.
  11. Construction of rail storage and maintenance facilities in areas used predominantly for industrial or transportation purposes where such construction is not inconsistent with existing zoning and where there is no significant noise impact on the surrounding community.
  12. Acquisition of land for hardship or protective purposes, advance land acquisition loans under section 3(b) of the UMT Act. Hardship and protective buying will be permitted only for a particular parcel or a limited number of parcels. These types of land acquisition qualify for a CE only where the acquisition will not limit the evaluation of alternatives, including shifts in alignment for planned construction projects, which may be required in the NEPA process. No project development on such land may proceed until the NEPA process has been completed.
- D. Special Project Information: (Include Environmental Commitments and Permits Required.)

**With all work on deck, there will be no 404 or 401 impacts.  
No permits are required.**

E. Threshold Criteria

The following evaluation of threshold criteria must be completed for Type II actions

<u>ECOLOGICAL</u>	<u>YES</u>	<u>NO</u>
(1) Will the project have a substantial impact on any unique or important natural resource?	<input type="checkbox"/>	<u>N</u>
(2) Does the project involve habitat where federally listed endangered or threatened species may occur?	<input type="checkbox"/>	<u>N</u>
(3) Will the project affect anadromous fish?	<input type="checkbox"/>	<u>N</u>
(4) If the project involves wetlands, is the amount of permanent and/or temporary wetland taking less than one-third (1/3) of an acre and have all practicable measures to avoid and minimize wetland takings been evaluated?	<u>N/A</u>	<input type="checkbox"/>
(5) Will the project require the use of U. S. Forest Service lands?	<input type="checkbox"/>	<u>N</u>
(6) Will the quality of adjacent water resources be adversely impacted by proposed construction activities?	<input type="checkbox"/>	<u>N</u>
(7) Does the project involve waters classified as Outstanding Water Resources (OWR) and/or High Quality Waters (HW)?	<input type="checkbox"/>	<u>N</u>
(8) Will the project require fill in waters of the United States in any of the designated mountain trout counties?	<input type="checkbox"/>	<u>N</u>
(9) Does the project involve any known underground storage tanks (UST's) or hazardous materials sites?	<input type="checkbox"/>	<u>N</u>
<u>PERMITS AND COORDINATION</u>	<u>YES</u>	<u>NO</u>
(10) If the project is located within a CAMA county, will the project significantly affect the coastal zone and/or any "Area of Environmental Concern" (AEC)?	<input type="checkbox"/>	<u>N</u>
(11) Does the project involve Coastal Barrier Resources Act resources?	<input type="checkbox"/>	<u>N</u>
(12) Will a U. S. Coast Guard permit be required?	<input type="checkbox"/>	<u>N</u>
(13) Will the project result in the modification of any existing regulatory floodway?	<input type="checkbox"/>	<u>N</u>
(14) Will the project require any stream relocations or channel changes?	<input type="checkbox"/>	<u>N</u>

SOCIAL, ECONOMIC, AND CULTURAL RESOURCES

	<u>YES</u>	<u>NO</u>
(15) Will the project induce substantial impacts to planned growth or land use for the area?	<input type="checkbox"/>	<u>N</u>
(16) Will the project require the relocation of any family or business?	<input type="checkbox"/>	<u>N</u>
(17) Will the project have a disproportionately high and adverse human health and environmental effect on any minority or low-income population?	<input type="checkbox"/>	<u>N</u>
(18) If the project involves the acquisition of right of way, is the amount of right of way acquisition considered minor?	<u>N/A</u>	<input type="checkbox"/>
(19) Will the project involve any changes in access control?	<input type="checkbox"/>	<u>N</u>
(20) Will the project substantially alter the usefulness and/or land use of adjacent property?	<input type="checkbox"/>	<u>N</u>
(21) Will the project have an adverse effect on permanent local traffic patterns or community cohesiveness?	<input type="checkbox"/>	<u>N</u>
(22) Is the project included in an approved thoroughfare plan and/or Transportation Improvement Program (and is, therefore, in conformance with the Clean Air Act of 1990)?	<u>Y</u>	<input type="checkbox"/>
(23) Is the project anticipated to cause an increase in traffic volumes?	<input type="checkbox"/>	<u>N</u>
(24) Will traffic be maintained during construction using existing roads, staged construction, or on-site detours?	<u>Y</u>	<input type="checkbox"/>
(25) If the project is a bridge replacement project, will the bridge be replaced at its existing location (along the existing facility) and will all construction proposed in association with the bridge replacement project be contained on the existing facility?	<u>N/A</u>	<input type="checkbox"/>
(26) Is there substantial controversy on social, economic, or environmental grounds concerning the project?	<input type="checkbox"/>	<u>N</u>
(27) Is the project consistent with all Federal, State, and local laws relating to the environmental aspects of the project?	<u>Y</u>	<input type="checkbox"/>
(28) Will the project have an "effect" on structures/properties eligible for or listed on the National Register of Historic Places?	<input type="checkbox"/>	<u>N</u>
(29) Will the project affect any archaeological remains which are important to history or pre-history?	<input type="checkbox"/>	<u>N</u>

- (30) Will the project require the use of Section 4(f) resources (public parks, recreation lands, wildlife and waterfowl refuges, historic sites, or historic bridges, as defined in Section 4(f) of the U. S. Department of Transportation Act of 1966)? ☐   N
- (31) Will the project result in any conversion of assisted public recreation sites or facilities to non-recreation uses, as defined by Section 6(f) of the Land and Water Conservation Act of 1965, as amended? ☐   N
- (32) Will the project involve construction in, across, or adjacent to a river designated as a component of or proposed for inclusion in the Natural System of Wild and Scenic Rivers? ☐   N

F. Additional Documentation Required for Unfavorable Responses in Part E  
(Discussion regarding all unfavorable responses in Part E should be provided below. Additional supporting documentation may be attached, as necessary.)

**During this project, care should be taken to ensure that no construction debris falls into the water below, which is jurisdictional (Rock Fish Creek) and should not be impacted by any activity. Deck drains will need to be sealed during any work which would allow epoxy or asphalt to enter the water below. Any construction equipment will be operated from the bridge deck and will not impact any part of the creek, creek bank, or wetland areas adjacent to the creek. If it becomes necessary to have equipment under the bridge, or to place any type of fill into the water or wetlands, a permit may be needed and will need to be secured before work can continue.**

G. CE Approval

TIP Project No. BP-5500G  
State Project No. 50070  
Federal-Aid Project No. BRNHS-000S-(747)

Project Description: (Include project scope and location. Attach location map.)


**Duplin Bridge 30: Epoxy Overlay on deck, joint replacement, mill and replace asphalt on approaches. Bridge is located on NC 41 0.77 mile west of SR 1101 (Cornwallis Rd) and crosses Rock Fish Creek (C; Sw).**

Categorical Exclusion Action Classification: (Check one)

<u>X</u>	TYPE II(A)
<u>          </u>	TYPE II(B)

Approved:

8/29/14  
Date

  
Project Engineer

8/29/2014  
Date

*Anneliese Westphal*  
Division Environmental Officer

9/2/14  
Date

L.E. Fosson, Jr  
Division Engineer

For Type II(B) projects only:

Date \_\_\_\_\_ Division Administrator  
Federal Highway Administration



# Duplin County - Bridge #30 - NC 41 Aerial Map





## CATEGORICAL EXCLUSION ACTION CLASSIFICATION FORM

TIP Project No.	<u>BP-5500G</u>
State Project No.	<u>50070</u>
Federal Project No.	<u>BRNHS-000S- (747)</u>

- A. Project Description: (Include project scope and location and refer to the attached project location map.)

**Sampson Bridge 6: Latex Modified Deck Overlay, joint replacement, repair of steel beam ends, cleaning and painting of structural steel, milling and replacement of asphalt on approaches. Paint Removal Containment system is to be installed with air handling machinery stored in uplands area only. Bridge 6 is located on US 701 approximately 1.7 miles southwest of Garland.**

- B. Purpose and Need:

**Bridge preservation efforts, extend the life of the structure.**

- C. Proposed Improvements:

Circle one or more of the following Type II improvements which apply to the project:

1. Modernization of a highway by resurfacing, restoration, rehabilitation, reconstruction, adding shoulders, or adding auxiliary lanes (e.g., parking, weaving, turning, climbing).
  - a. Restoring, Resurfacing, Rehabilitating, and Reconstructing pavement (3R and 4R improvements)
  - b. Widening roadway and shoulders without adding through lanes
  - c. Modernizing gore treatments
  - d. Constructing lane improvements (merge, auxiliary, and turn lanes)
  - e. Adding shoulder drains
  - f. Replacing and rehabilitating culverts, inlets, and drainage pipes, including safety treatments
  - g. Providing driveway pipes
  - h. Performing minor bridge widening (less than one through lane)
2. Highway safety or traffic operations improvement projects including the installation of ramp metering control devices and lighting.
  - a. Installing ramp metering devices
  - b. Installing lights
  - c. Adding or upgrading guardrail
  - d. Installing safety barriers including Jersey type barriers and pier protection
  - e. Installing or replacing impact attenuators
  - f. Upgrading medians including adding or upgrading median barriers
  - g. Improving intersections including relocation and/or realignment
  - h. Making minor roadway realignment
  - i. Channelizing traffic
  - j. Performing clear zone safety improvements including removing hazards and flattening slopes



- k. Implementing traffic aid systems, signals, and motorist aid
  - l. Installing bridge safety hardware including bridge rail retrofit
3. Bridge rehabilitation, reconstruction, or replacement or the construction of grade separation to replace existing at-grade railroad crossings.
- a. Rehabilitating, reconstructing, or replacing bridge approach slabs
  - b. Rehabilitating or replacing bridge decks
  - c. Rehabilitating bridges including painting (no red lead paint), scour repair, fender systems, and minor structural improvements
  - d. Replacing a bridge (structure and/or fill)
4. Transportation corridor fringe parking facilities.
5. Construction of new truck weigh stations or rest areas.
6. Approvals for disposal of excess right-of-way or for joint or limited use of right-of-way, where the proposed use does not have significant adverse impacts.
7. Approvals for changes in access control.
8. Construction of new bus storage and maintenance facilities in areas used predominantly for industrial or transportation purposes where such construction is not inconsistent with existing zoning and located on or near a street with adequate capacity to handle anticipated bus and support vehicle traffic.
9. Rehabilitation or reconstruction of existing rail and bus buildings and ancillary facilities where only minor amounts of additional land are required and there is not a substantial increase in the number of users.
10. Construction of bus transfer facilities (an open area consisting of passenger shelters, boarding areas, kiosks and related street improvements) when located in a commercial area or other high activity center in which there is adequate street capacity for projected bus traffic.
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12. Acquisition of land for hardship or protective purposes, advance land acquisition loans under section 3(b) of the UMT Act. Hardship and protective buying will be permitted only for a particular parcel or a limited number of parcels. These types of land acquisition qualify for a CE only where the acquisition will not limit the evaluation of alternatives, including shifts in alignment for planned construction projects, which may be required in the NEPA process. No project development on such land may proceed until the NEPA process has been completed.
- D. Special Project Information: (Include Environmental Commitments and Permits Required.)

Please see section F.

E. Threshold Criteria

The following evaluation of threshold criteria must be completed for Type II actions

<u>ECOLOGICAL</u>	<u>YES</u>	<u>NO</u>
(1) Will the project have a substantial impact on any unique or important natural resource?	<input type="checkbox"/>	<u>N</u>
(2) Does the project involve habitat where federally listed endangered or threatened species may occur?	<input type="checkbox" value="Y"/>	<u>          </u>
(3) Will the project affect anadromous fish?	<input type="checkbox"/>	<u>N</u>
(4) If the project involves wetlands, is the amount of permanent and/or temporary wetland taking less than one-third (1/3) of an acre and have all practicable measures to avoid and minimize wetland takings been evaluated?	<u>N/A</u>	<input type="checkbox"/>
(5) Will the project require the use of U. S. Forest Service lands?	<input type="checkbox"/>	<u>N</u>
(6) Will the quality of adjacent water resources be adversely impacted by proposed construction activities?	<input type="checkbox"/>	<u>N</u>
(7) Does the project involve waters classified as Outstanding Water Resources (OWR) and/or High Quality Waters (HQW)?	<input type="checkbox" value="Y"/>	<u>          </u>
(8) Will the project require fill in waters of the United States in any of the designated mountain trout counties?	<input type="checkbox"/>	<u>N</u>
(9) Does the project involve any known underground storage tanks (UST's) or hazardous materials sites?	<input type="checkbox"/>	<u>N</u>
<u>PERMITS AND COORDINATION</u>	<u>YES</u>	<u>NO</u>
(10) If the project is located within a CAMA county, will the project significantly affect the coastal zone and/or any "Area of Environmental Concern" (AEC)?	<input type="checkbox"/>	<u>N</u>
(11) Does the project involve Coastal Barrier Resources Act resources?	<input type="checkbox"/>	<u>N</u>
(12) Will a U. S. Coast Guard permit be required?	<input type="checkbox"/>	<u>N</u>
(13) Will the project result in the modification of any existing regulatory floodway?	<input type="checkbox"/>	<u>N</u>
(14) Will the project require any stream relocations or channel changes?	<input type="checkbox"/>	<u>N</u>

SOCIAL, ECONOMIC, AND CULTURAL RESOURCES

	<u>YES</u>	<u>NO</u>
(15) Will the project induce substantial impacts to planned growth or land use for the area?	<input type="checkbox"/>	<u>N</u>
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(21) Will the project have an adverse effect on permanent local traffic patterns or community cohesiveness?	<input type="checkbox"/>	<u>N</u>
(22) Is the project included in an approved thoroughfare plan and/or Transportation Improvement Program (and is, therefore, in conformance with the Clean Air Act of 1990)?	<u>Y</u>	<input type="checkbox"/>
(23) Is the project anticipated to cause an increase in traffic volumes?	<input type="checkbox"/>	<u>N</u>
(24) Will traffic be maintained during construction using existing roads, staged construction, or on-site detours?	<u>Y</u>	<input type="checkbox"/>
(25) If the project is a bridge replacement project, will the bridge be replaced at its existing location (along the existing facility) and will all construction proposed in association with the bridge replacement project be contained on the existing facility?	<u>N/A</u>	<input type="checkbox"/>
(26) Is there substantial controversy on social, economic, or environmental grounds concerning the project?	<input type="checkbox"/>	<u>N</u>
(27) Is the project consistent with all Federal, State, and local laws relating to the environmental aspects of the project?	<u>Y</u>	<input type="checkbox"/>
(28) Will the project have an "effect" on structures/properties eligible for or listed on the National Register of Historic Places?	<input type="checkbox"/>	<u>N</u>
(29) Will the project affect any archaeological remains which are important to history or pre-history?	<input type="checkbox"/>	<u>N</u>

- (30) Will the project require the use of Section 4(f) resources (public parks, recreation lands, wildlife and waterfowl refuges, historic sites, or historic bridges, as defined in Section 4(f) of the U. S. Department of Transportation Act of 1966)? ☐   N
- (31) Will the project result in any conversion of assisted public recreation sites or facilities to non-recreation uses, as defined by Section 6(f) of the Land and Water Conservation Act of 1965, as amended? ☐   N
- (32) Will the project involve construction in, across, or adjacent to a river designated as a component of or proposed for inclusion in the Natural System of Wild and Scenic Rivers? ☐   N

F. Additional Documentation Required for Unfavorable Responses in Part E  
(Discussion regarding all unfavorable responses in Part E should be provided below. Additional supporting documentation may be attached, as necessary.)

**This bridge is the overflow bridge for the South River which is designated as C Sw ORW+. Since this project occurs in an area which drains to ORW classified waters, this project is considered a Type II(B) project.**

**No permits were required for this project as none of the proposed work will have an impact to waters or wetlands. Temporary blocks or pads will be allowed under the bridge to support scaffolding, but must be temporary and not include any fill, rock, etc. Complete containment is required during all aspects of construction so that no construction debris will enter waters or wetlands under the bridge.**

**This project was evaluated by NCDOT's cultural resources specialists in accordance with NCDOT's programmatic agreement (see attached forms).**

**Since there is potential habitat for the federally endangered wood stork (*Mycteria americana*) within the R/W in this area, a Section 7 consultation was required (see attached letter).**

**If the project scope changes to include any work outside of the R/W (including staging of equipment) or removal of trees, this project must be reviewed again by the division environmental office. The land outside of the R/W on both sides of the bridge is in a conservation easement (NC Coastal Land Trust).**

G. CE Approval

TIP Project No. BP-5500G  
State Project No. 50070  
Federal-Aid Project No. BRNHS-000S-(747)

Project Description: (Include project scope and location. Attach location map.)

**Sampson Bridge 6: Latex Modified Deck Overlay, joint replacement, repair of steel beam ends, cleaning and painting of structural steel, milling and replacement of asphalt on approaches. Paint Removal Containment system is to be installed with air handling machinery stored in the highlands area only.**

Categorical Exclusion Action Classification: (Check one)

       TYPE II(A)  
  X   TYPE II(B)

Approved:

8/29/2014 Treron Carroll  
Date Project Engineer

8/29/2014 Anneliese Westphal  
Date Division Environmental Officer

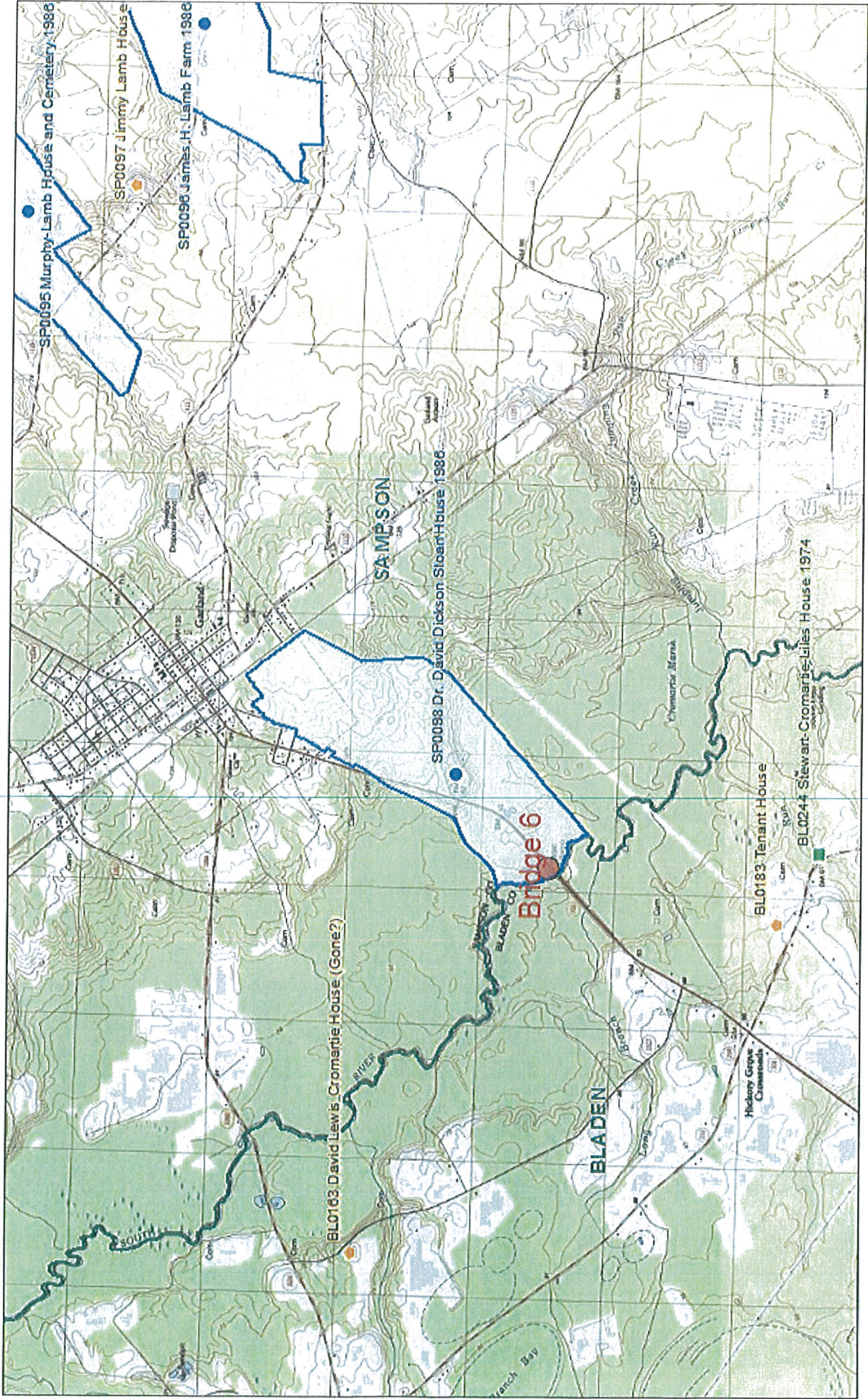
9/2/14 R. E. Fussen, Jr.  
Date Division Engineer

For Type II(B) projects only:

10-16-14 [Signature]  
Date Division Administrator  
Federal Highway Administration

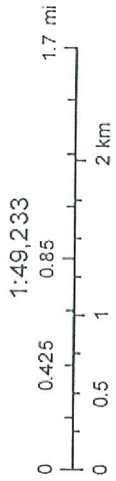


# Sampson County Bridge 6 Topo Map



June 4, 2014

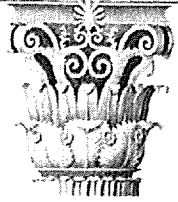
- NR Individual Listing ★ NRHD Center Point
- NR Listing, Gone



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14-06-0031



## HISTORIC ARCHITECTURE AND LANDSCAPES NO HISTORIC PROPERTIES PRESENT OR AFFECTED FORM

This form only pertains to Historic Architecture and Landscapes for this project. It is not valid for Archaeological Resources. You must consult separately with the Archaeology Group.

### PROJECT INFORMATION

<b>Project No:</b>	BP-5500G	<b>County:</b>	Sampson
<b>WBS No.:</b>	50070.1.1	<b>Document Type:</b>	CE
<b>Fed. Aid No:</b>	BRNHS-000S-(747)	<b>Funding:</b>	<input type="checkbox"/> State <input checked="" type="checkbox"/> Federal
<b>Federal Permit(s):</b>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<b>Permit Type(s):</b>	
<p><b><u>Project Description:</u></b></p> <p>Project to repair Sampson County bridge No. 6 on US 701 near the Bladen County line, just southwest of Garland, NC. Project to include latex modified deck overlay, joint replacement, repair of steel beam ends, cleaning and painting of structural steel, milling and replacement of asphalt on approaches. Paint removal containment system will be used. Most of work to be completed from the deck of the bridge. Items under the bridge may include air handling equipment and wooden pads placed on the ground to support scaffolding. Sampson No. 6 is not eligible for the National Register of Historic Places (NRHP) according to the NCDOT Historic Bridge Inventory. Sampson Bridge No. 6 is the "overflow" bridge which mainly spans wetlands and is just to the northeast of the main bridge (Bladen County Bridge No. 69) spanning the South River. No work is planned for Bladen Bridge No. 69. No work or staging will occur outside of existing right-of-way. Project length is approximately 300 feet. Right-of-way will remain the same. An off-site detour is possible but not yet certain that it will be necessary. Proposed route runs from the southwest toward Garland from US 701 to NC 41 to NC 411 back to US 701.</p>			

### SUMMARY OF HISTORIC ARCHITECTURE AND LANDSCAPES REVIEW

- ☐ There are no National Register-listed or Study Listed properties within the project's area of potential effects.
- ☒ There are no properties less than fifty years old which are considered to meet Criteria Consideration G within the project's area of potential effects.
- ☐ There are no properties within the project's area of potential effects.
- ☐ There are properties over fifty years old within the area of potential effects, but they do not meet the criteria for listing on the National Register.
- ☒ There are no historic properties present or affected by this project. (Attach any notes or documents as needed.)

Date of field visit:

**Description of review activities, results, and conclusions:**

Review of HPO quad maps, HPOweb GIS mapping, historic designations roster, and indexes was conducted on 6/24/14. Based on this review, there is one NR (National Register) property, the Dr. David Dickson Sloan House (SP0098) within the Area of Potential Effects (APE). There are no SL, DE, SS or LL properties within the APE. Sampson County property and tax records were reviewed which revealed no other properties within the APE. Sampson County Bridge No. 6 sits just within (approximately 325 feet) the historic boundary for the Sloan House property. The architectural historian corresponded with the project engineer and bridge maintenance engineer to confirm specifics about the project and the following details: because of the project APE's far proximity from any of the contributing resources associated with the historic property, construction activities will not have an adverse effect on the Sloan House the property. Furthermore, there will be no tree removal and no staging of equipment outside of the right-of-way.


Thus, considering all these factors, NCDOT Historic Architecture makes a finding of "No Historic Properties Present/Affected" for this project.

**SUPPORT DOCUMENTATION**

☒ Map(s)    ☐ Previous Survey Info.    ☒ Photos    ☐ Correspondence    ☐ Design Plans

**FINDING BY NCDOT ARCHITECTURAL HISTORIAN**

Historic Architecture and Landscapes – NO HISTORIC PROPERTIES PRESENT OR AFFECTED

  
\_\_\_\_\_  
NCDOT Architectural Historian

7/14/14  
\_\_\_\_\_  
Date



## NO ARCHAEOLOGICAL SURVEY REQUIRED FORM

This form only pertains to ARCHAEOLOGICAL RESOURCES for this project. It is not valid for Historic Architecture and Landscapes. You must consult separately with the Historic Architecture and Landscapes Group.



### PROJECT INFORMATION

Project No: \_\_\_\_\_ County: **Sampson**  
 WBS No: **50070.1.1** Document: **CATEGORICAL EXCLUSION**  
 F.A. No: **BRNHS-000S-(747)** Funding: ☐ State ☒ Federal

Federal Permit Required? ☐ Yes ☐ No Permit Type: \_\_\_\_\_

**Project Description:** Repair Bridge 6 on US 701 over the South River overflow. Area of Potential Effects (A.P.E.) consists of the bridge and the ground surface immediately adjacent to it. The project consists of repairs to the bridge structure. There will be no excavation. No design plans provided. Federally-funded; may require Federal permits; no easements required.

### SUMMARY OF CULTURAL RESOURCES REVIEW

***Brief description of review activities, results of review, and conclusions:***

The review included an examination of topographic map, an aerial photograph, and listings of previously recorded sites, previous archaeological surveys, and previous environmental reviews at the Office of State Archaeology (O.S.A.).

The topographic map (Garland, N.C.) shows the landforms near the bridge have a low to moderate potential for archaeological sites. The bridge is located in a wide river valley, and the nearby landforms appear to be poorly-drained floodplain/swamp. There are no level, well-drained areas near the bridge. The nearest high ground is located approximately 800 meters (2,625 ft.) northeast.

The aerial photograph shows the bridge is located in a heavily wooded area.

A review of information at the O.S.A. shows no previously recorded archaeological sites within or adjacent to the A.P.E. The A.P.E. has not been previously surveyed for archaeological sites, and it is not located in any areas that have been previously reviewed by the State Historic Preservation Office (HPO).

***Brief Explanation of why the available information provides a reliable basis for reasonably predicting that there are no unidentified historic properties in the APE:***

The bridge improvement project consists of improvements to the bridge structure. None of the surrounding land will be impacted. The bridge is located in a wide river valley, and the landforms around the bridge appear to be poorly drained floodplain or swamp. The A.P.E. has a low to moderate potential for archaeological sites. There are no previously recorded sites located within or adjacent to the A.P.E.

### SUPPORT DOCUMENTATION

See attached: ☒ Map(s) ☐ Previous Survey Info ☐ Photos ☐ Correspondence  
☐ Photocopy of County Survey Notes Other: **aerial photograph**

### FINDING BY NCDOT ARCHAEOLOGIST

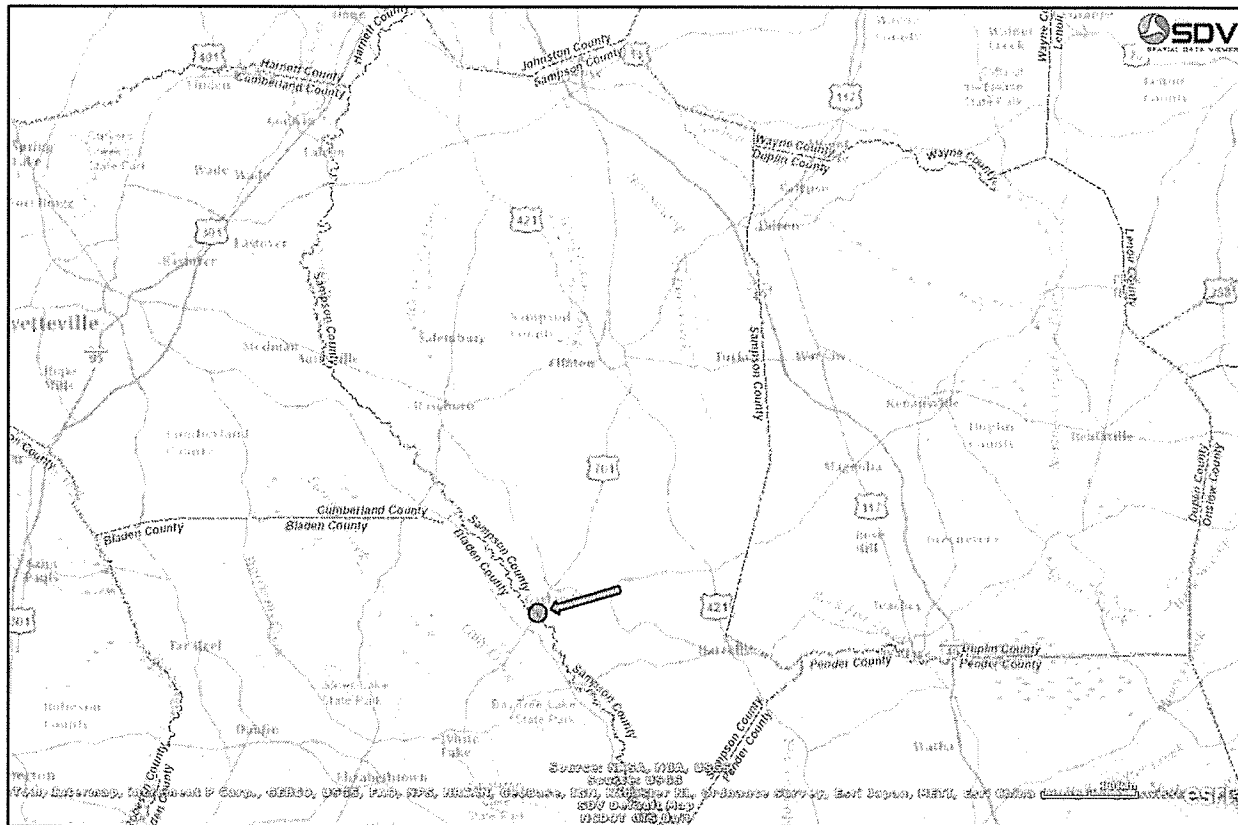
**NO ARCHAEOLOGY SURVEY REQUIRED**

**Caleb Smith**

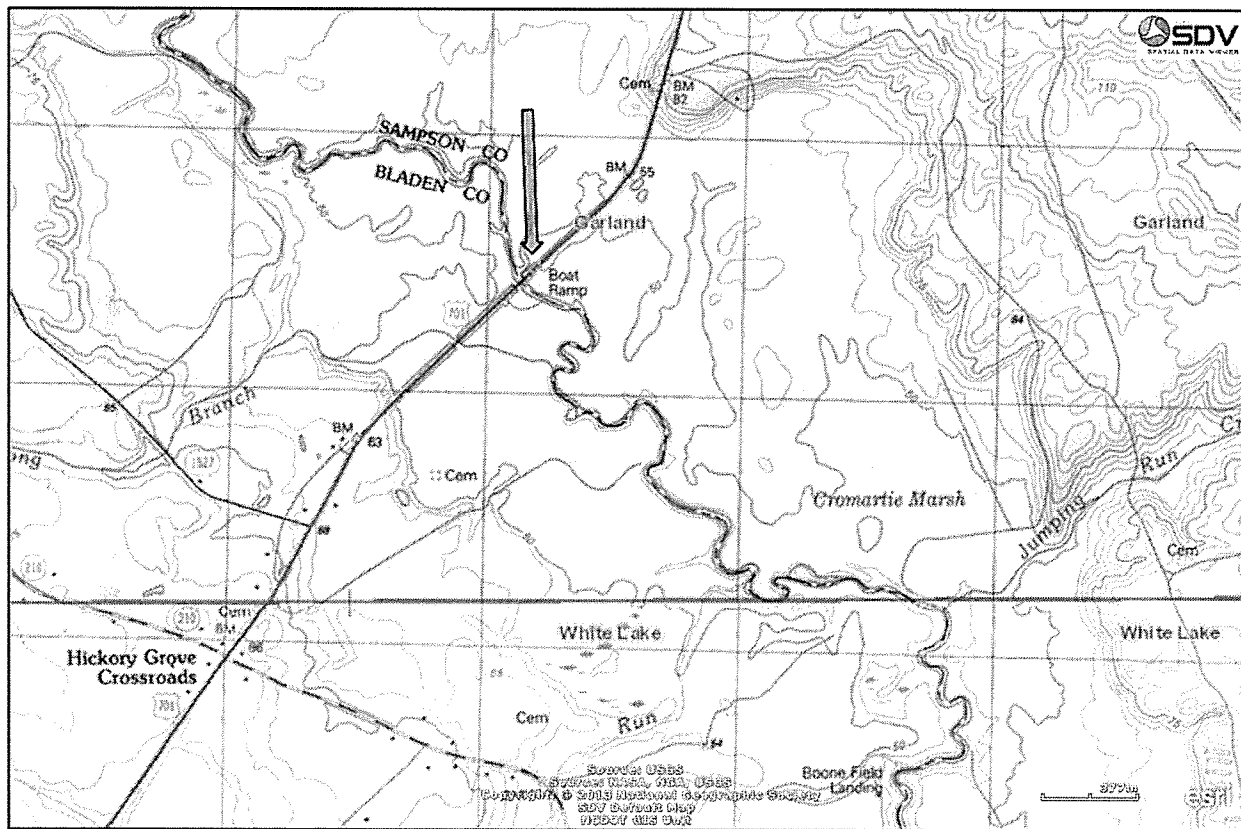
**7/23/2014**

NCDOT ARCHAEOLOGIST II

Date











## United States Department of the Interior

### FISH AND WILDLIFE SERVICE

Raleigh Field Office  
Post Office Box 33726  
Raleigh, North Carolina 27636-3726

August 26, 2014

Anneliese Westphal  
North Carolina Department of Transportation  
5501 Barbados Blvd.  
Castle Hayne, North Carolina 28429

Dear Ms. Westphal:

This letter is in response to your letter of August 20, 2014 which provided the U.S. Fish and Wildlife Service (Service) with the biological conclusion of the North Carolina Department of Transportation that the preservation project for Bridge No. 6 on US 701 over South River overflow in Sampson County (Project BP-5500G) may affect, but is not likely to adversely affect the federally endangered wood stork (*Mycteria americana*). These comments are provided in accordance with Section 7 of the Endangered Species Act (ESA) of 1973, as amended (16 U.S.C. 1531-1543).

According to your submitted information, the site was surveyed for wood storks on August 19, 2014. No wood storks or wood stork nests were observed, although potential foraging habitat was present. Based on available information and on the limited scope of work, the Service concurs with your biological conclusion that the project may affect, but is not likely to adversely affect the wood stork. We believe that the requirements of Section 7(a)(2) of the ESA have been satisfied for this species. We remind you that obligations under Section 7 consultation must be reconsidered if: (1) new information reveals impacts of this identified action that may affect listed species or critical habitat in a manner not previously considered in this review; (2) this action is subsequently modified in a manner that was not considered in this review; or (3) a new species is listed or critical habitat determined that may be affected by this identified action. If you have any questions regarding our response, please contact Mr. Gary Jordan at (919) 856-4520 (Ext. 32).

Sincerely,

for

Pete Benjamin  
Field Supervisor