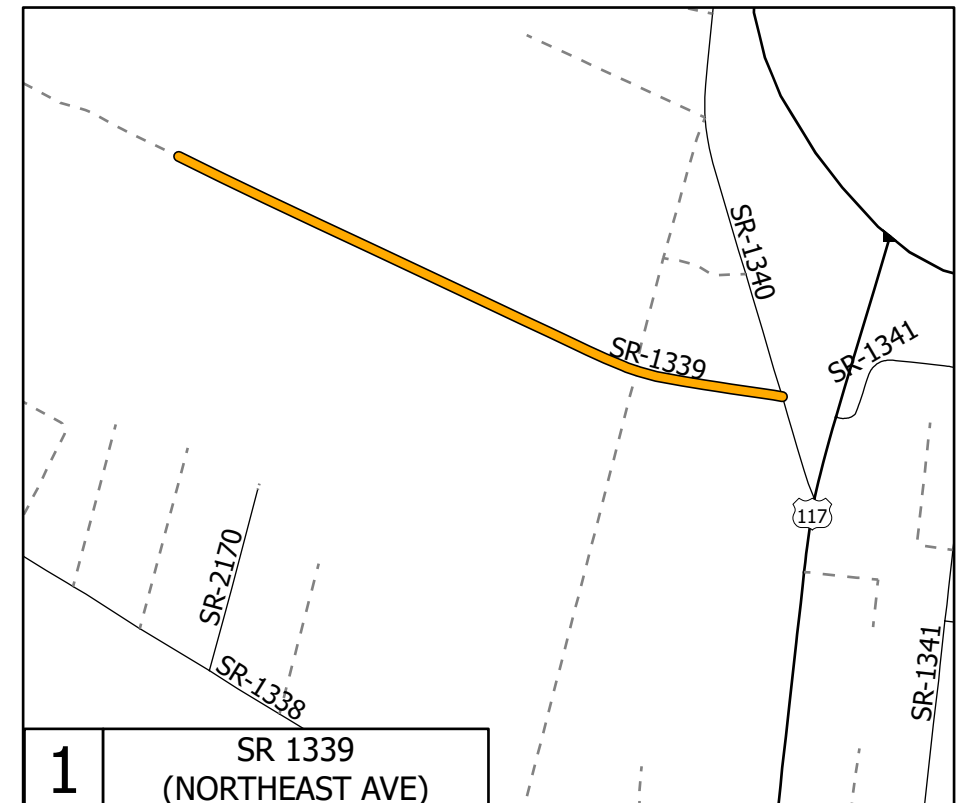
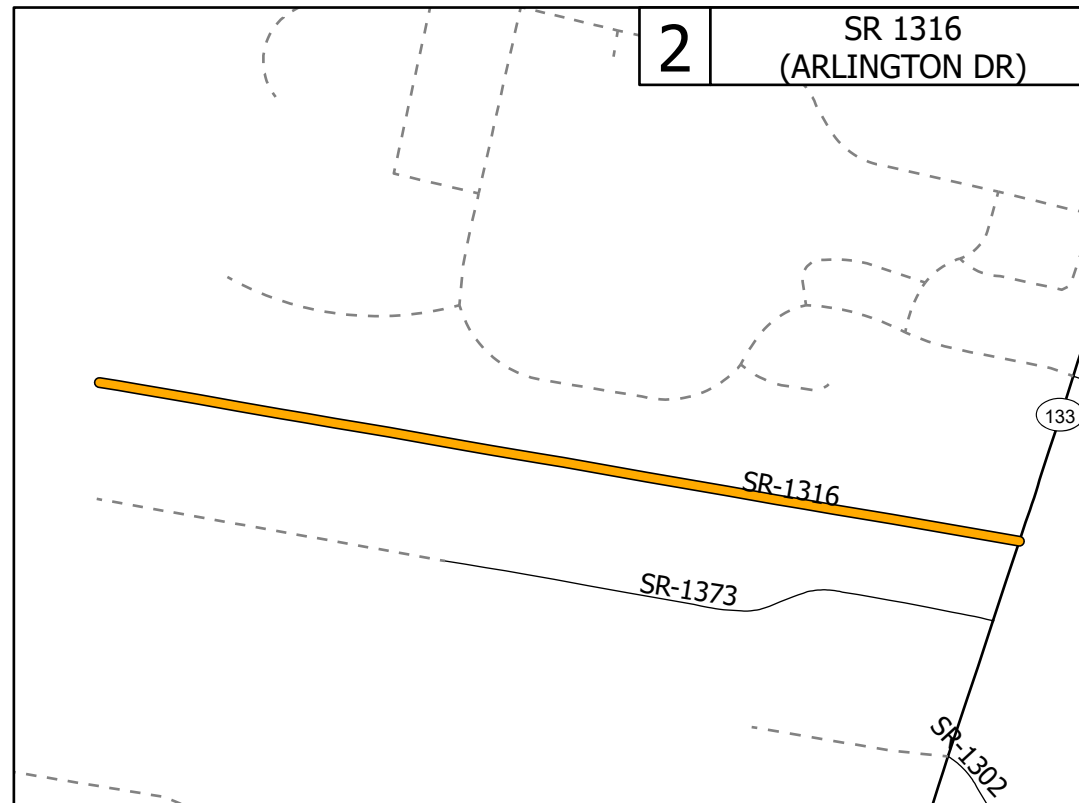
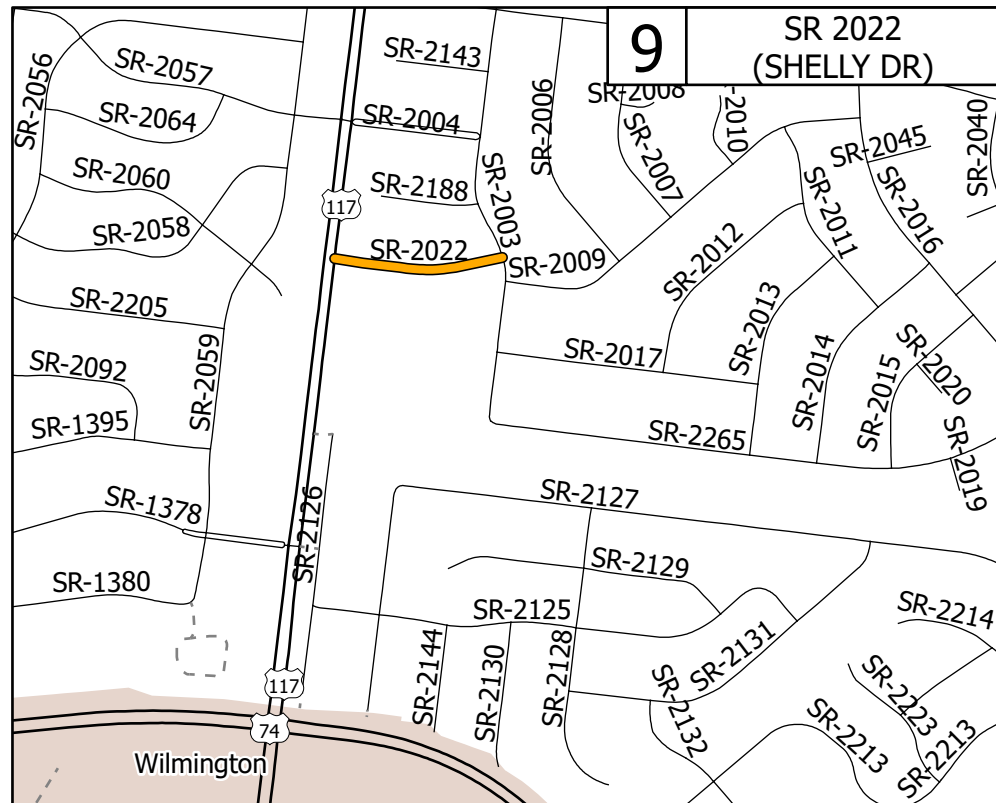
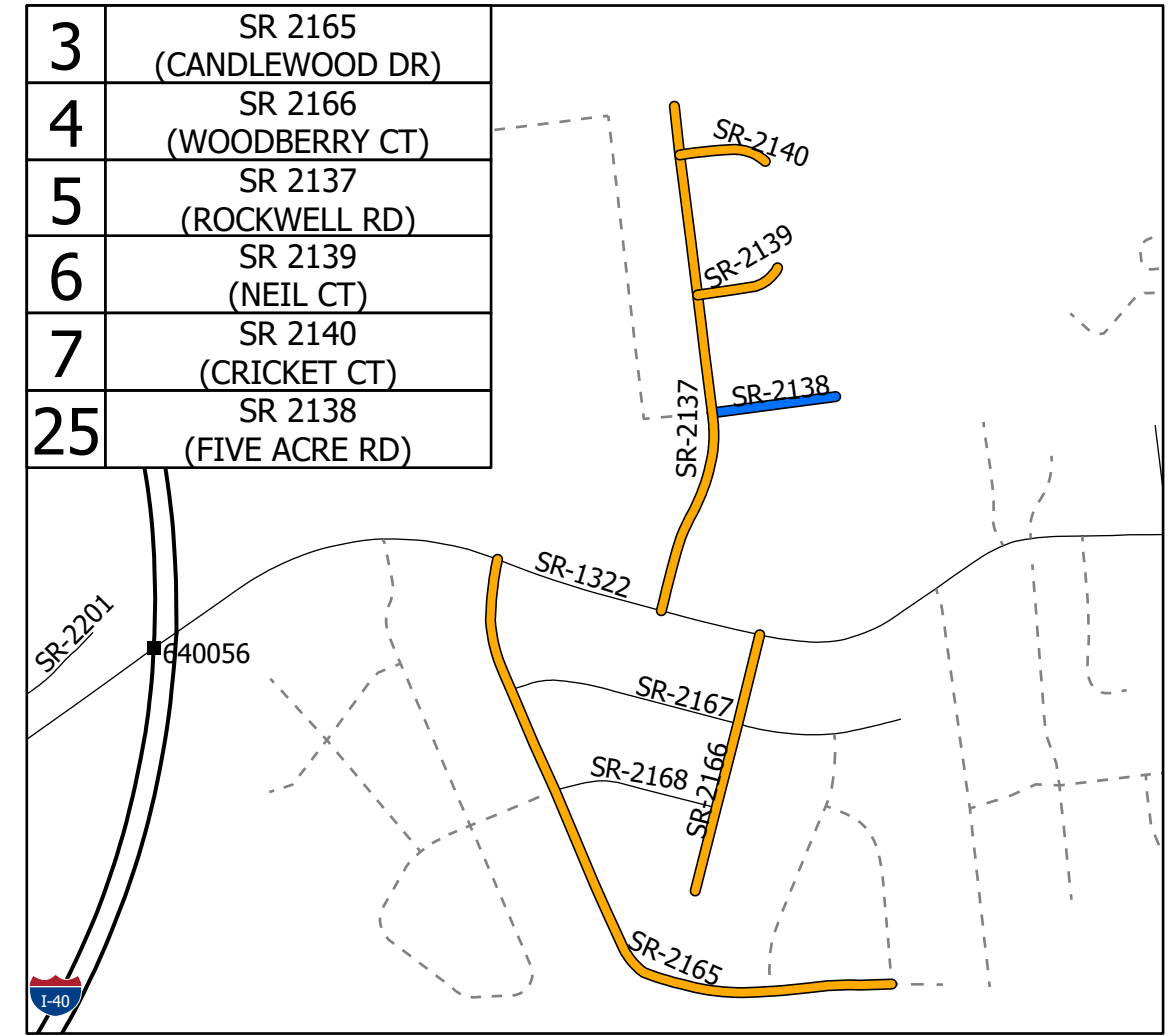
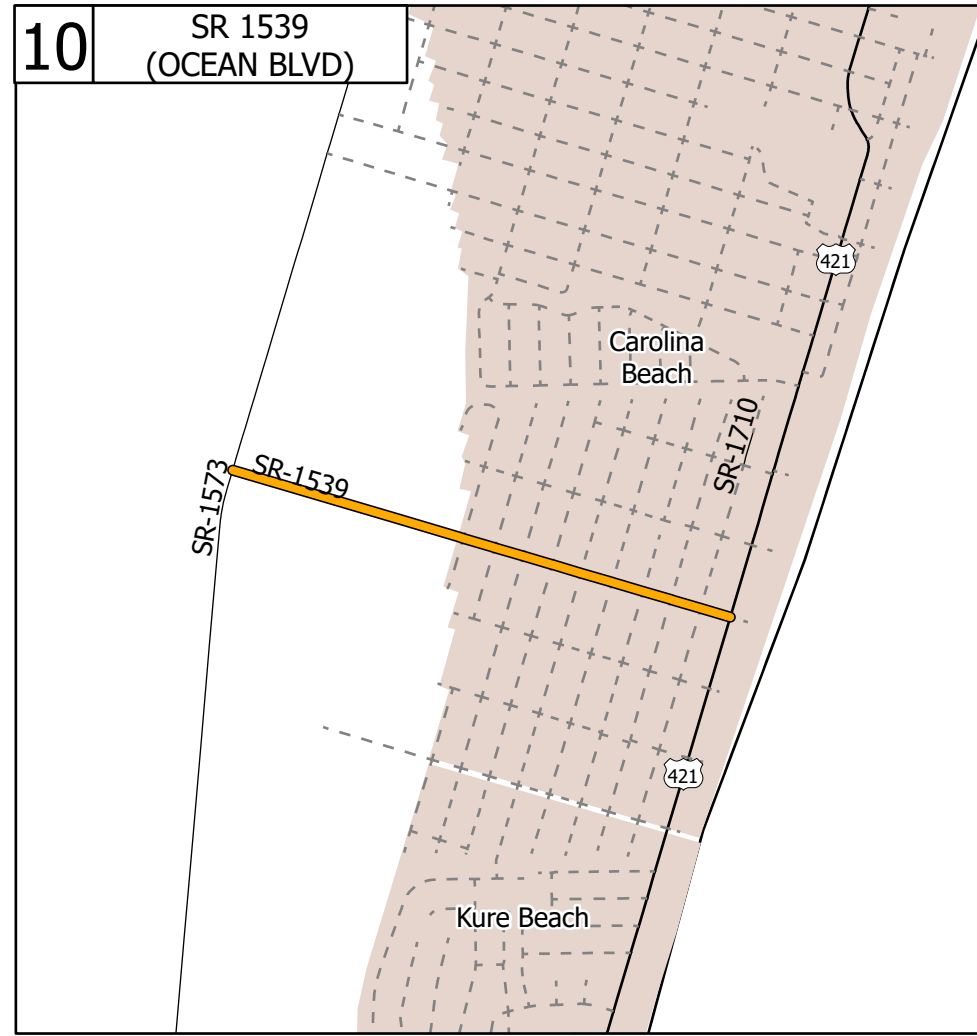
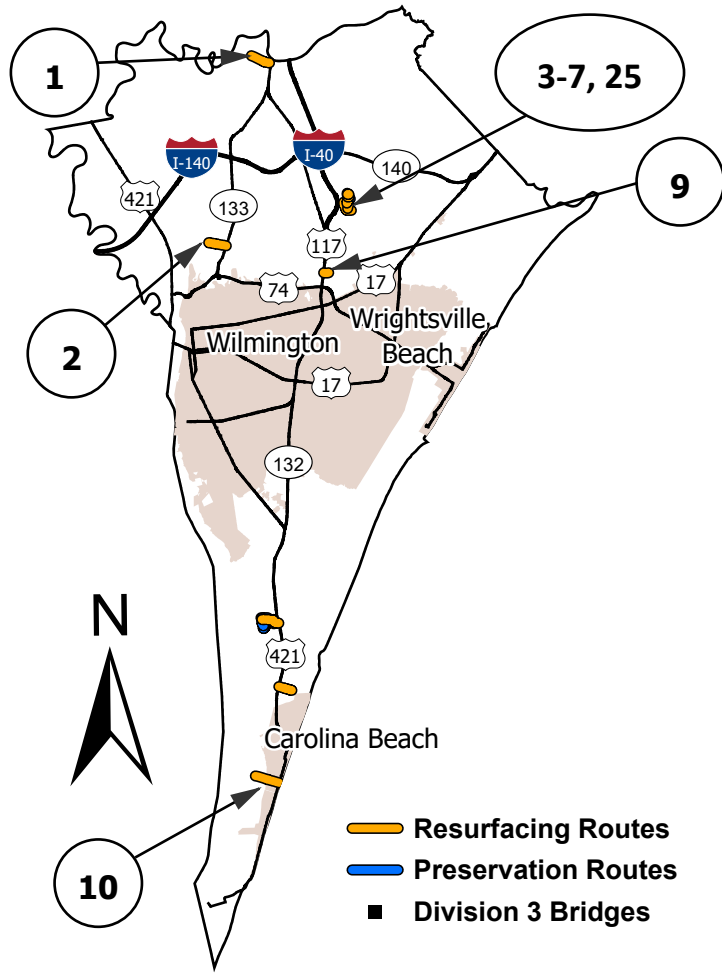


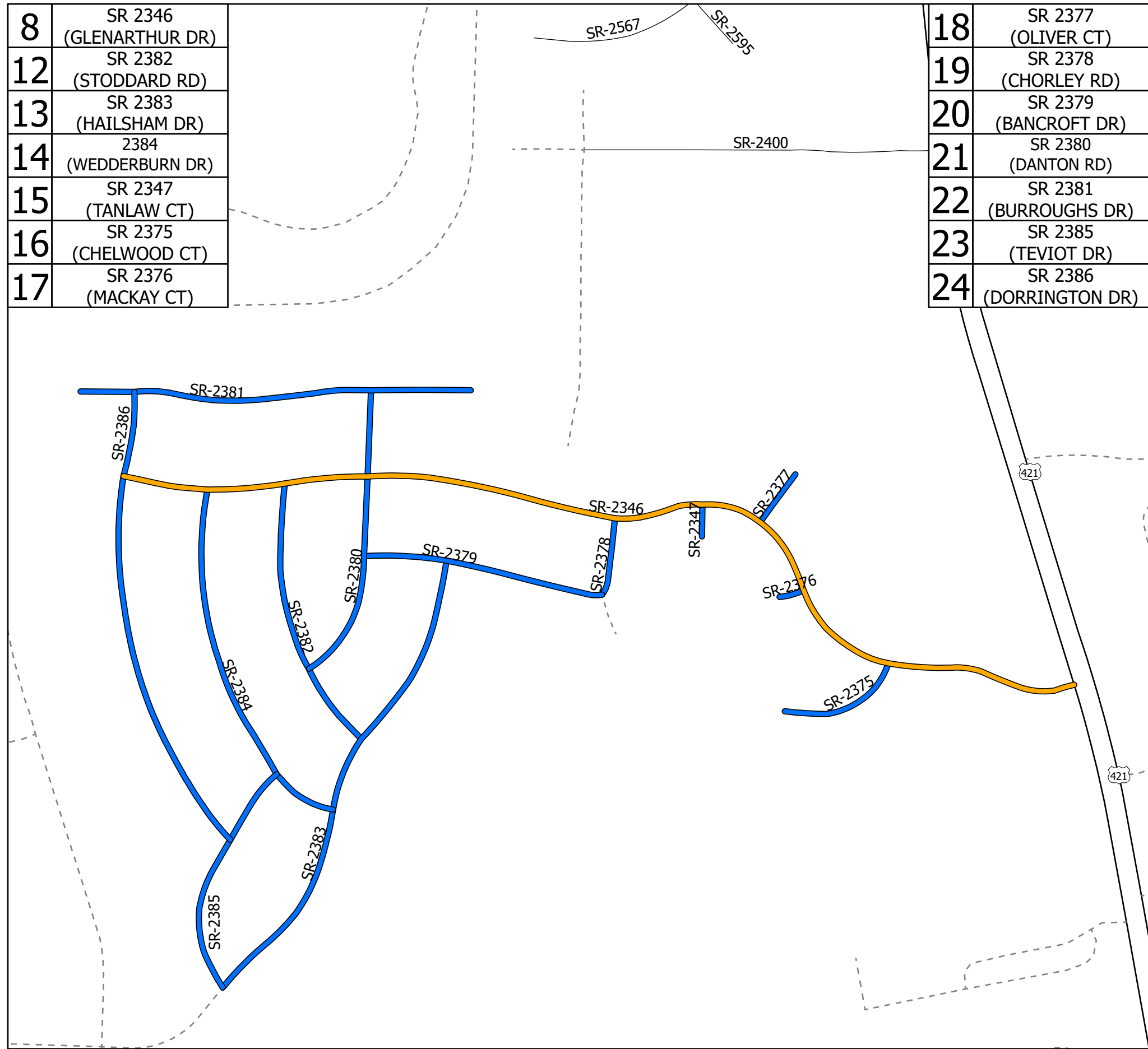
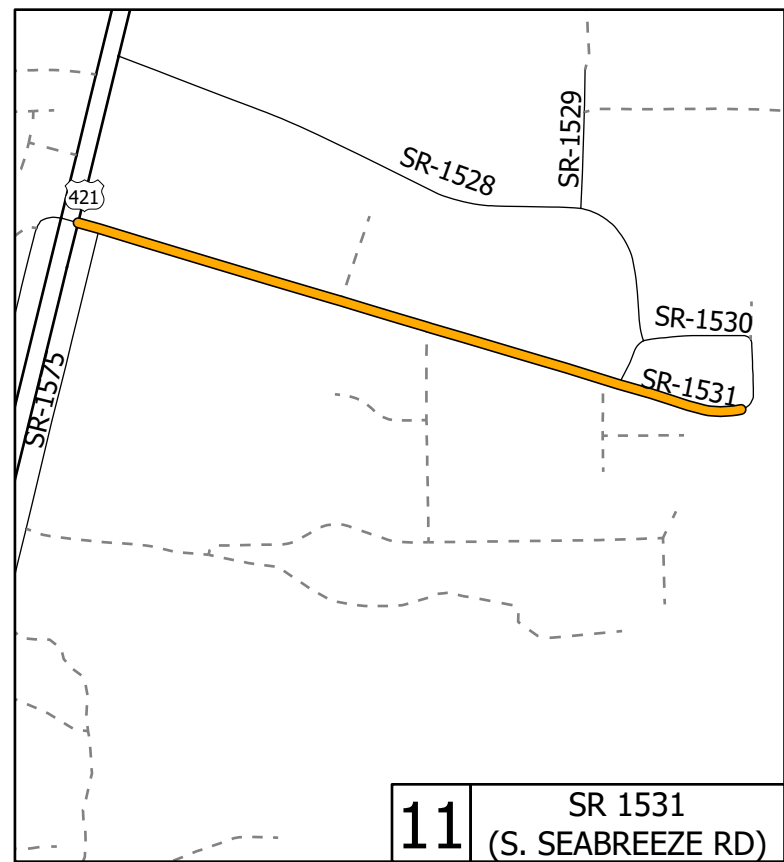
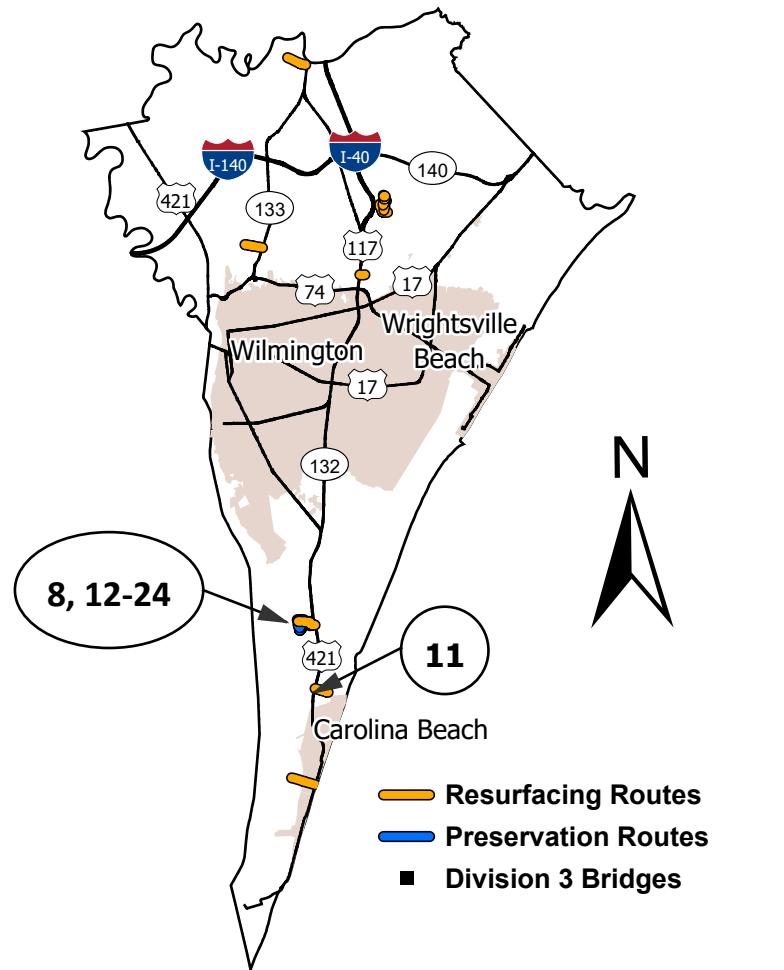
NEW HANOVER 1-7, 9-10, 25

PROJECT REFERENCE NO.	SHEET NO.
2024CPT.03.03.20651, ETC.	1



NEW HANOVER 8, 11-24

PROJECT REFERENCE NO.	SHEET NO.
2024CPT.03.03.20651, ETC.	2



PAVEMENT SCHEDULE

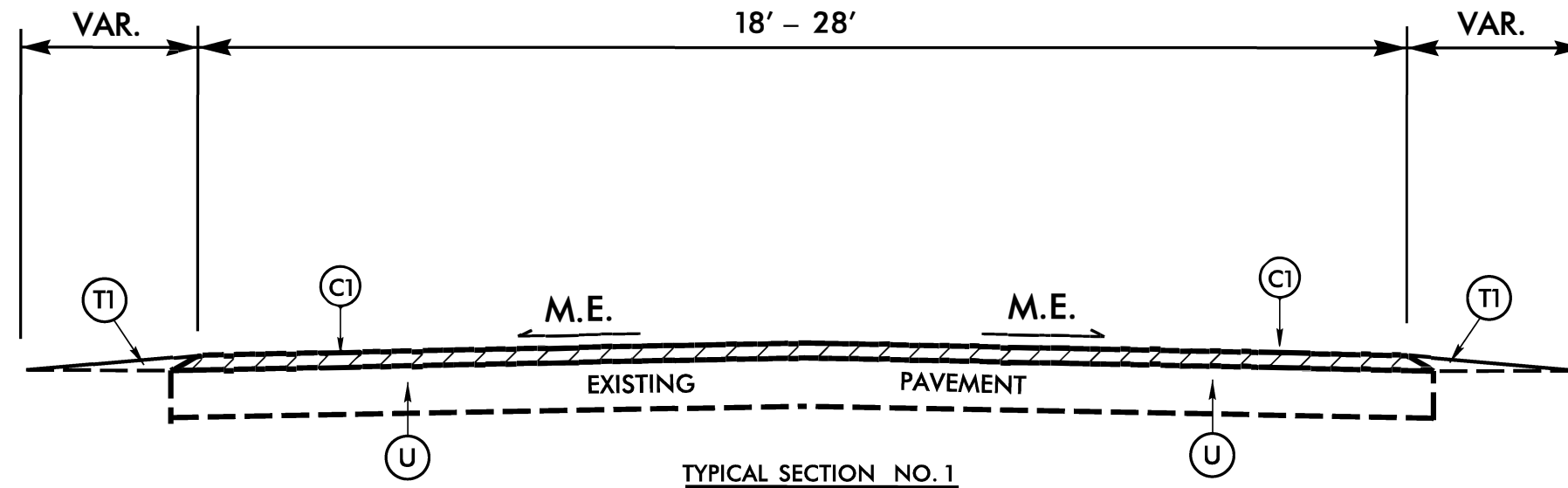
C1	PROP. APPROX. 1 1/4" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ.YD.
C2	PROP. APPROX. 1 1/2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ.YD.
C3	PROP. APPROX. 3/4" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 82.5 LBS. PER SQ.YD.
R1	EXISTING VALLEY CURB
VI	0" TO 1.25" MILLING
T1	EARTH MATERIAL (SHOULDER RECONSTRUCTION)
T2	ASB SHOULDER MATERIAL (SHOULDER RECONSTRUCTION)
U	EXISTING PAVEMENT

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

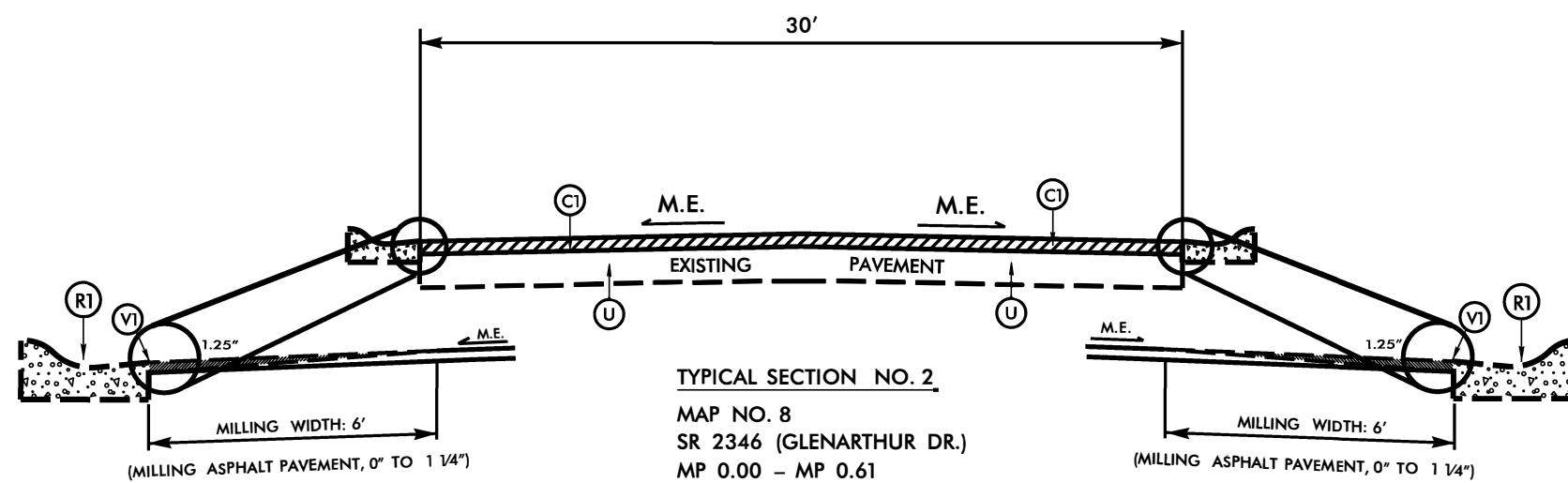
NOTES: SHOULDER WORK ON MAP NO. 1 - 2, 9 - 11 AS NEEDED, TO BE DETERMINED BY ENGINEER.

** MAPS 1 & 11 SHALL HAVE ASB SHOULDER MATERIAL (T2)

NO EROSION CONTROL ITEMS ARE NEEDED FOR MAPS 3 - 8 DUE TO EXISTING ROLLED ASPHALT CURBING.



- ** MAP NO. 1
SR 1339 (NORTHEAST AVE.)
MP 0.00 - MP 0.65
- MAP NO. 2
SR 1316 (ARLINGTON DR.)
MP 0.00 - MP 0.59
- MAP NO. 3
SR 2165 (CANDLEWOOD DR.)
MP 0.00 - MP 0.44
- MAP NO. 4
SR 2166 (WOODBERRY CT.)
MP 0.00 - MP 0.16
- MAP NO. 5
SR 2137 (ROCKWELL RD.)
MP 0.00 - MP 0.33
- MAP NO. 6
SR 2139 (NEIL CT.)
MP 0.00 - MP 0.06
- MAP NO. 7
SR 2140 (CRICKET CT.)
MP 0.00 - MP 0.06
- MAP NO. 9
SR 2022 (SHELLEY DR.)
MP 0.00 - MP 0.17
- ** MAP NO. 11
SR 1531 (S. SEABREEZE RD.)
MP 0.00 - MP 0.45



- MAP NO. 8
SR 2346 (GLENARTHUR DR.)
MP 0.00 - MP 0.61

EFF. 01-16-2018

2018 ROADWAY ENGLISH STANDARD DRAWINGS

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N. C. Department of Transportation - Raleigh, N. C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

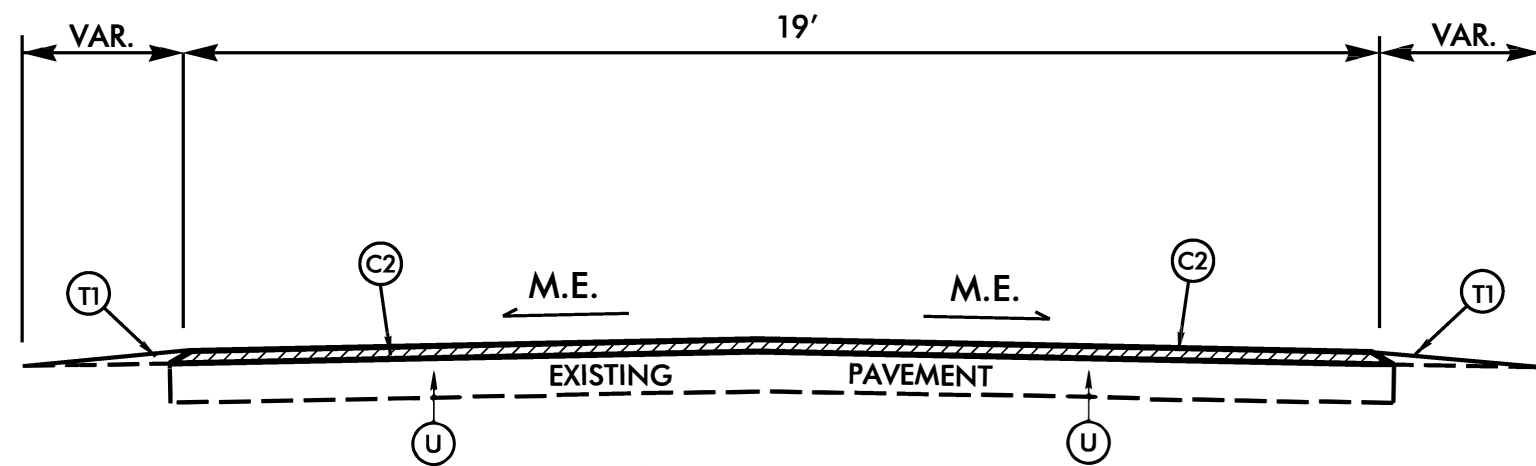
STD.NO. TITLE

DIVISION 12 - PVTM MARKING, MARKERS AND DELINEATION
1205.08 Pavement Markings - Symbols & Word Messages

REVISIONS

SYSTEMS DESIGN

PAVEMENT SCHEDULE	
C2	1 1/2" S9.5B
C3	3/4" S4.75A
T1	EARTH MATERIAL SHOULDER RECONST.
U	EXISTING PAVEMENT

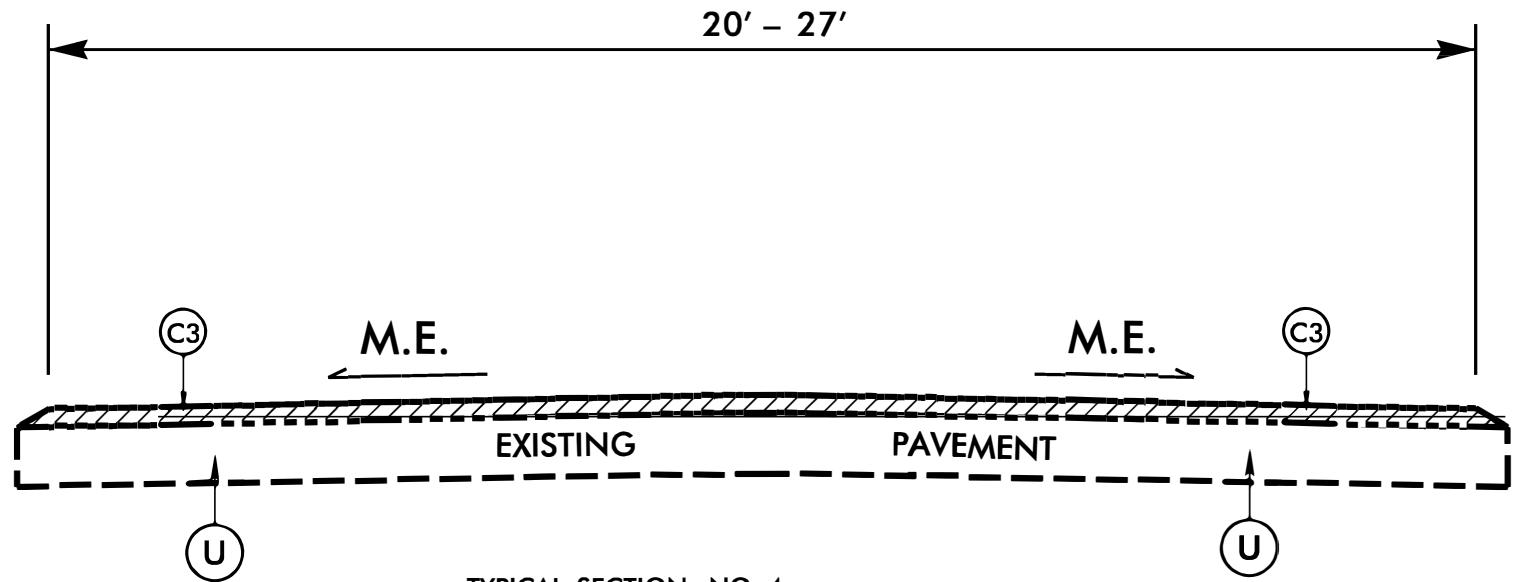


TYPICAL SECTION NO. 3

MAP NO. 10
SR 1539 (OCEAN BLVD.)
MP 0.00 - MP 0.82

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

NOTES: SHOULDER WORK ON MAP NO. 1 - 2, 9 - 11 AS NEEDED, TO BE DETERMINED BY ENGINEER.



TYPICAL SECTION NO. 4

MAP NO. 12
SR 2382 (STODDARD RD.)
MP 0.00 - MP 0.16

MAP NO. 13
SR 2383 (HAILSHAM DR.)
MP 0.00 - MP 0.29

MAP NO. 14
SR 2384 (WEDDERBURN DR.)
MP 0.00 - MP 0.22

MAP NO. 15
SR 2347 (TANLAW CT.)
MP 0.00 - MP 0.02

MAP NO. 16
SR 2375 (CHELWOOD CT.)
MP 0.00 - MP 0.07

MAP NO. 17
SR 2376 (MACKAY CT.)
MP 0.00 - MP 0.02

MAP NO. 18
SR 2377 (OLIVER CT.)
MP 0.00 - MP 0.04

MAP NO. 19
SR 2378 (CHORLEY RD.)
MP 0.00 - MP 0.05

MAP NO. 20
SR 2379 (BANCROFT DR.)
MP 0.00 - MP 0.14

MAP NO. 21
SR 2380 (DANTON ST.)
MP 0.00 - MP 0.18

MAP NO. 22
SR 2381 (BURROUGHS DR.)
MP 0.00 - MP 0.23

MAP NO. 23
SR 2385 (TEVIOT RD.)
MP 0.00 - MP 0.14

MAP NO. 24
SR 2386 (DORRINGTON DR.)
MP 0.00 - MP 0.28

MAP NO. 25
SR 2138 (FIVE ACRE RD.)
MP 0.00 - MP 0.08

REVISIONS

8/17/99

SYSTEMS DESIGN

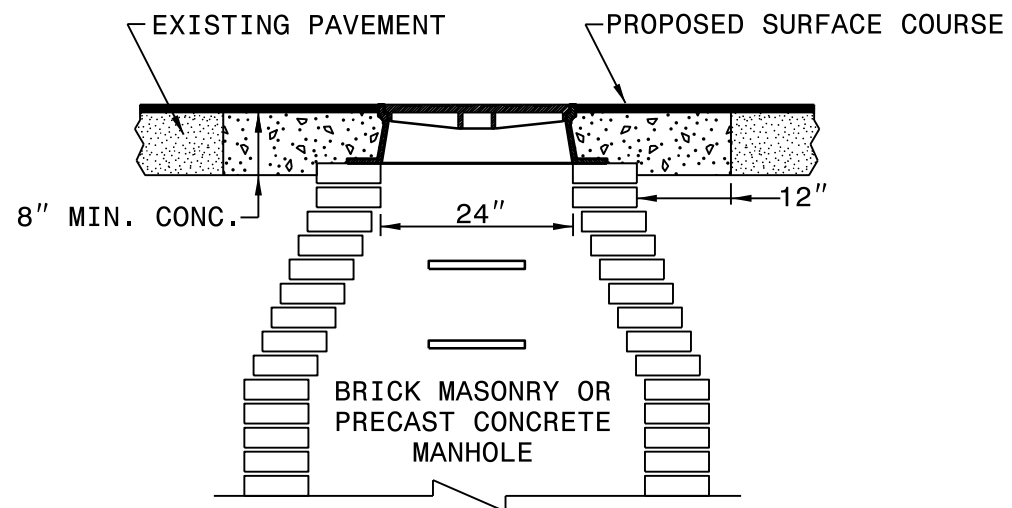
STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

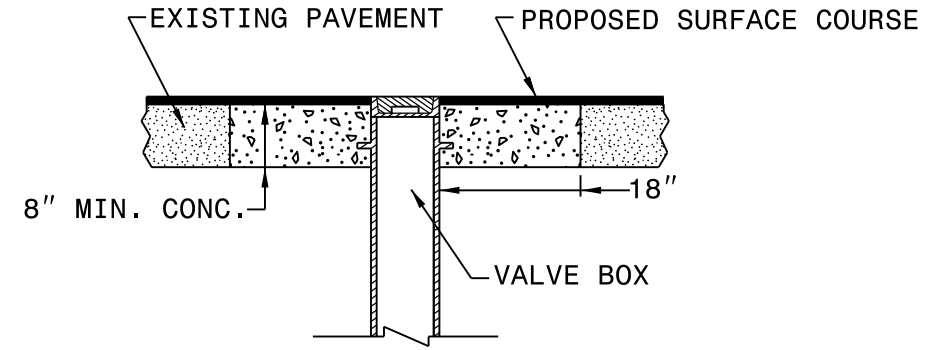
SHEET 1 OF 1
840D55

GENERAL NOTES:

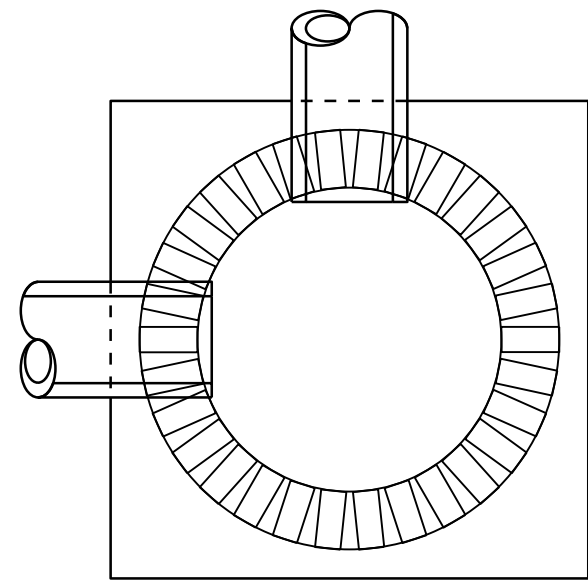
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS $\frac{1}{2}$ " +/- $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2018 STANDARD SPECIFICATIONS.



MANHOLE CONCRETE ENCASEMENT



VALVE BOX CONCRETE ENCASEMENT



ELEVATION VIEW

PLACE BRICK ACCORDING TO ELEVATION VIEW

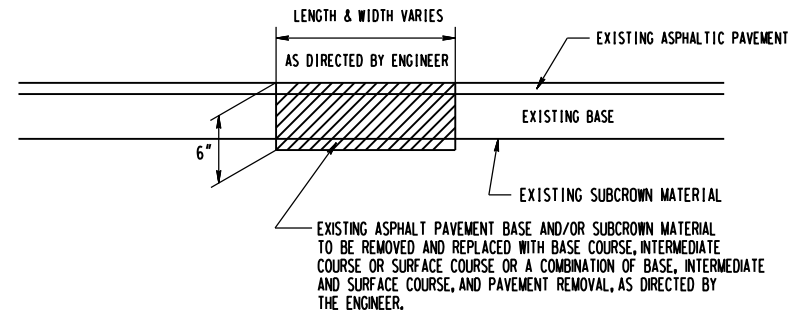
ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

SHEET 1 OF 1
840D55

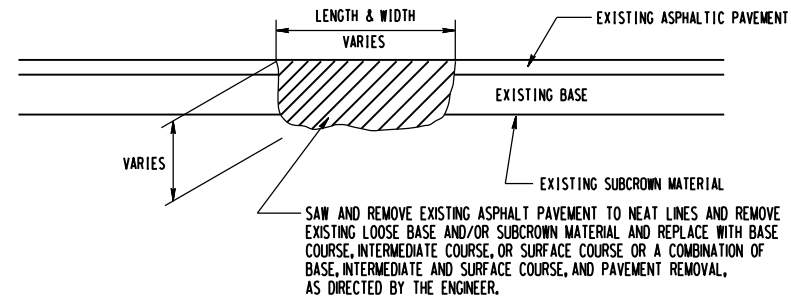
REVISIONS

28-NOV-2017 10:49
S:\Division Resurfacing\z Design Standards & CADD\CADD_Specs.Dwg\Tutorials_Curb, Etc.rxdgn
\$\$\$\$\$USERNAME\$\$\$\$\$

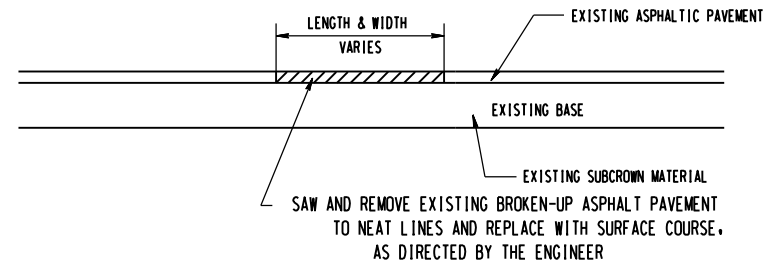
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



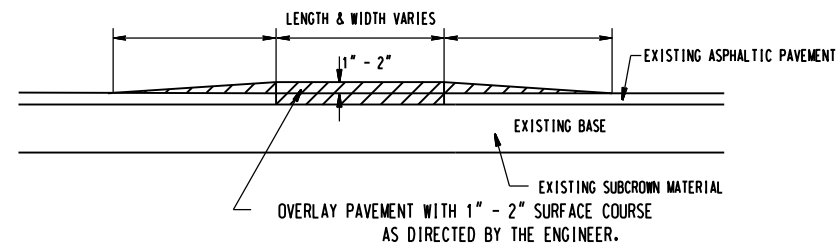
DETAIL NO. 1



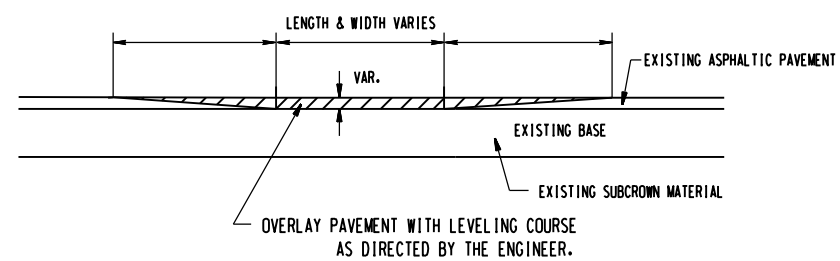
DETAIL NO. 2



DETAIL NO. 3



DETAIL NO. 4

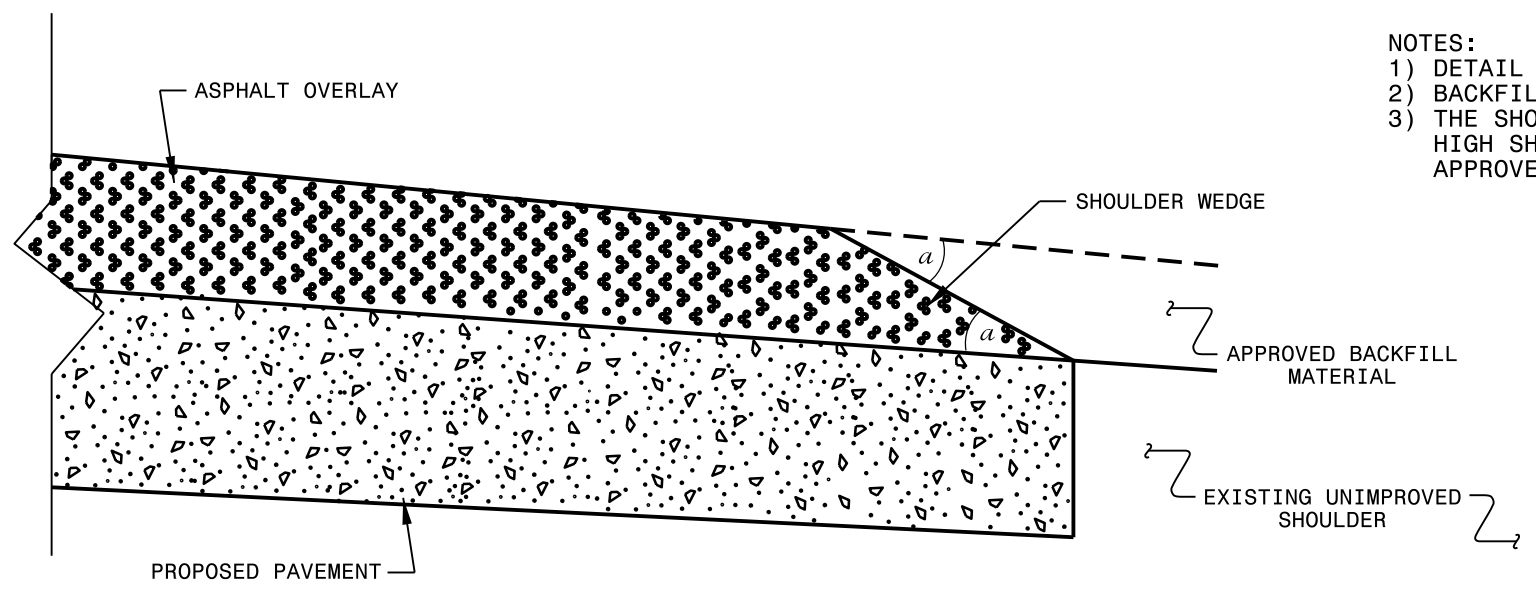


DETAIL NO. 5

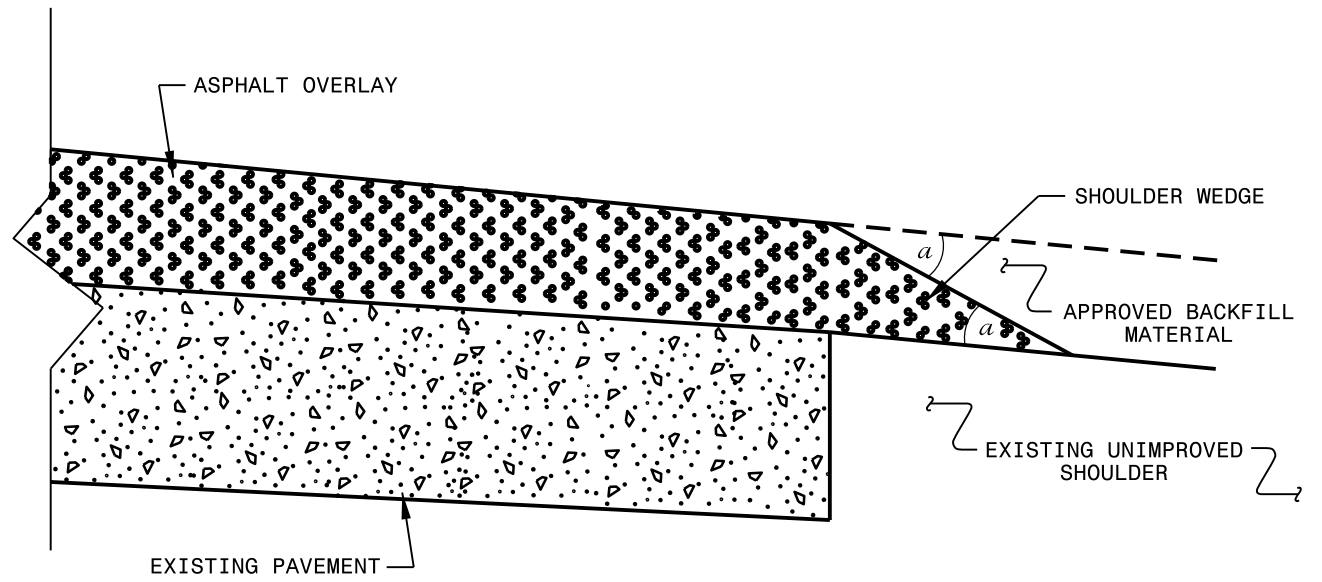
REVISIONS

20-APP-2015-1156
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 \$\$\$SUSFRNME\$\$\$

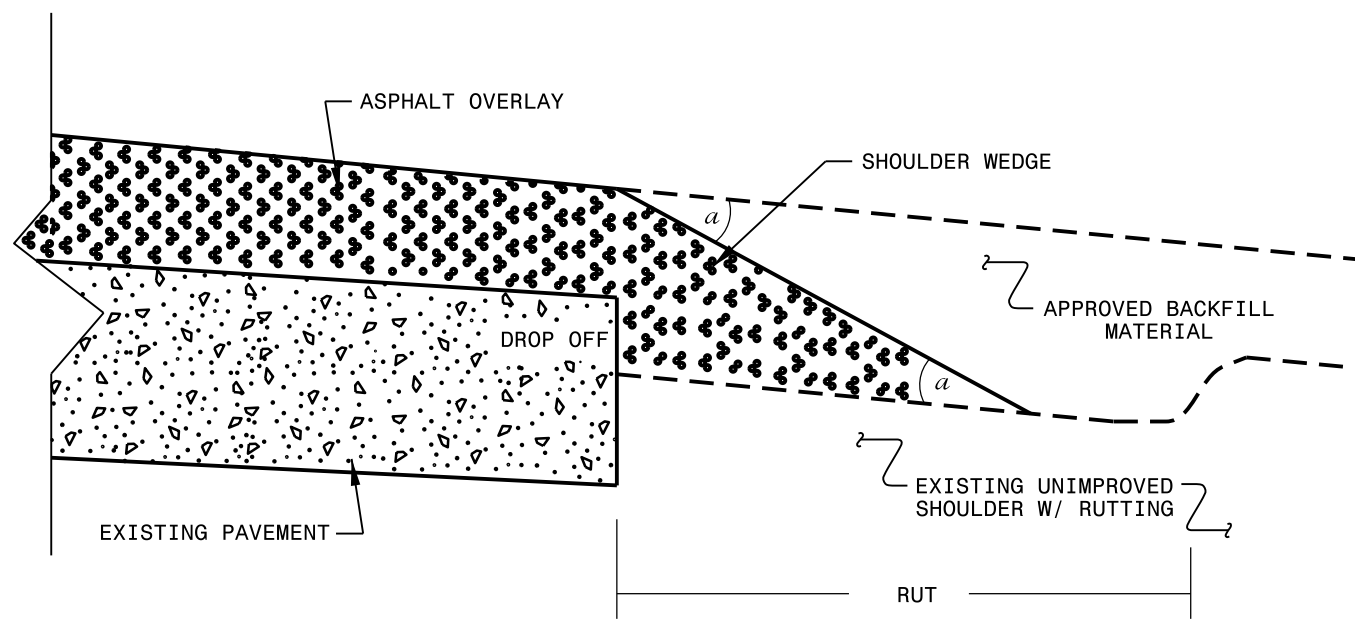
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22
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 3\Sampson August 2018 Revised Shoulder Wedge Detail.dgn
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DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

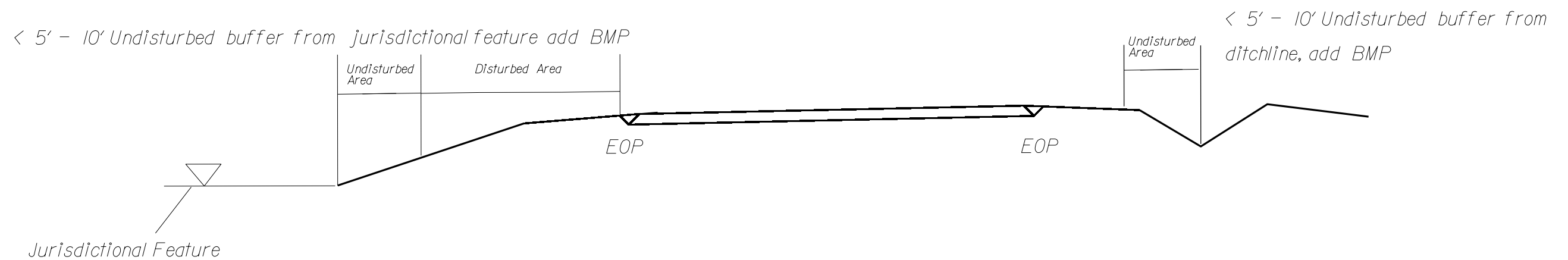
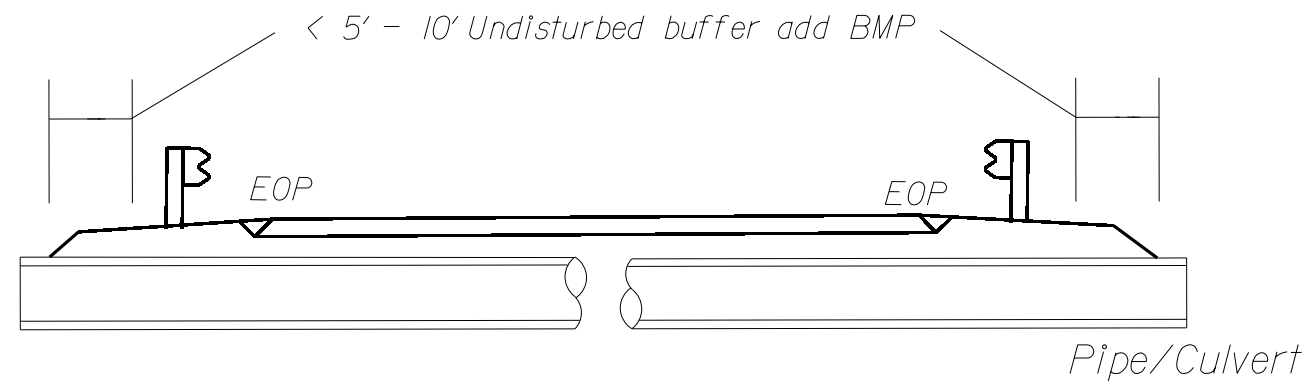
SOIL STABILIZATION TIMEFRAMES

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

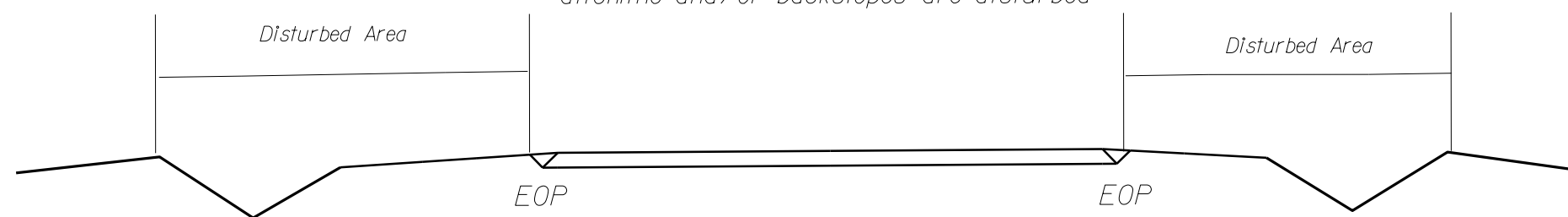
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

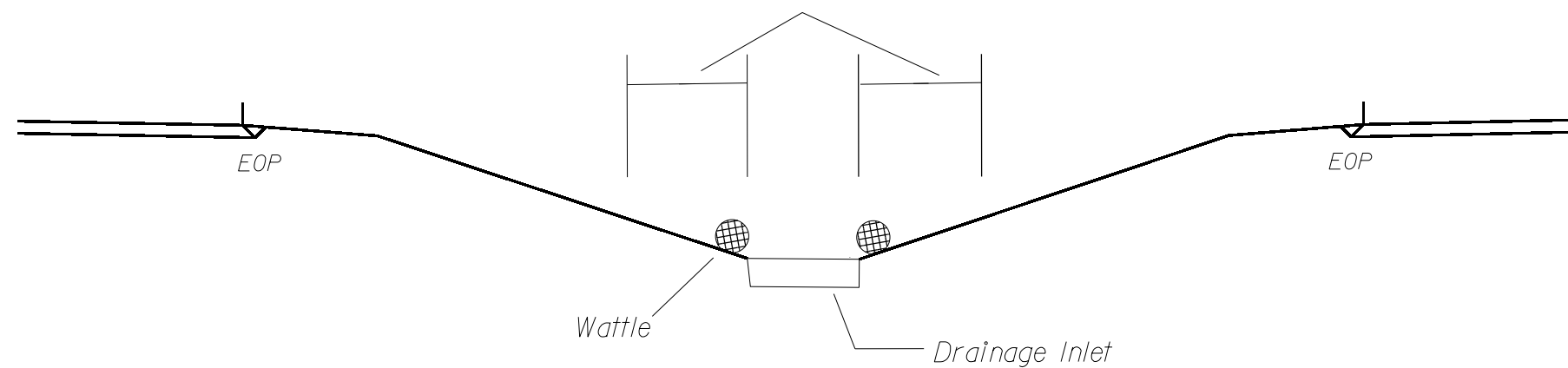
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

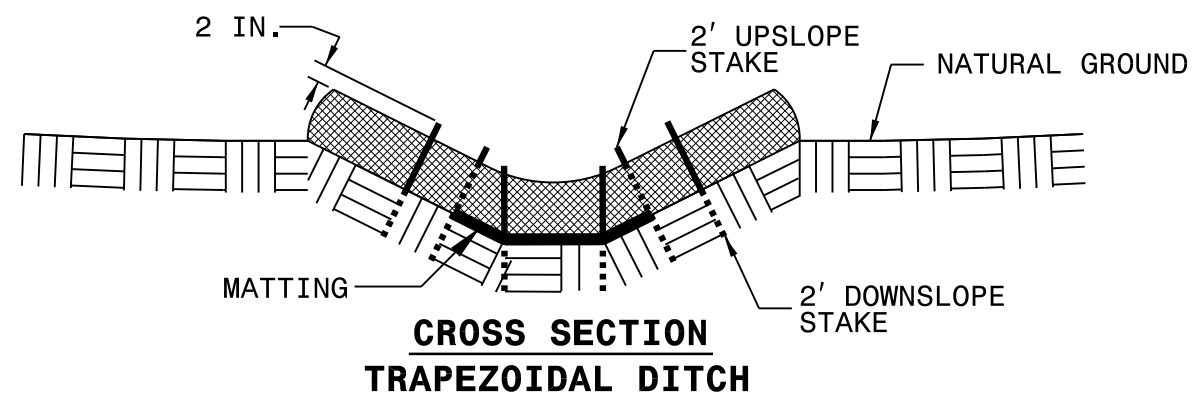
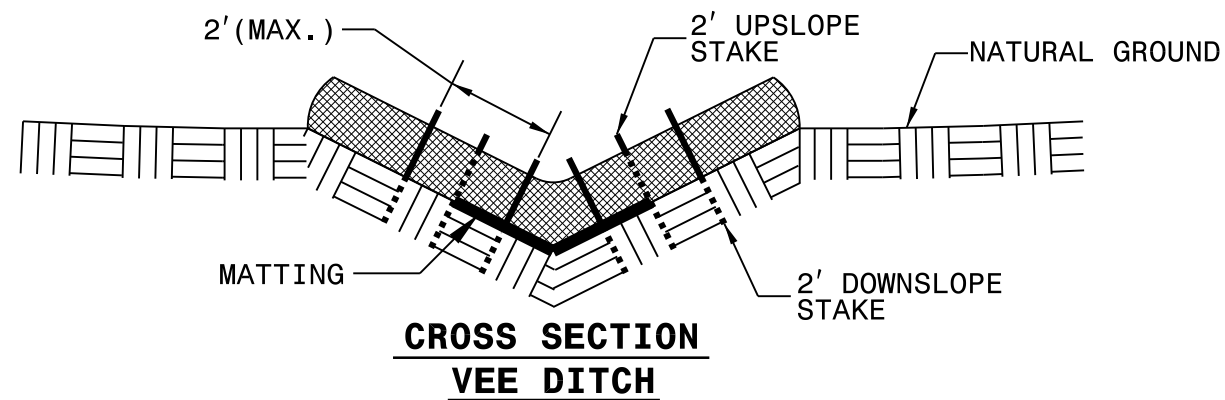
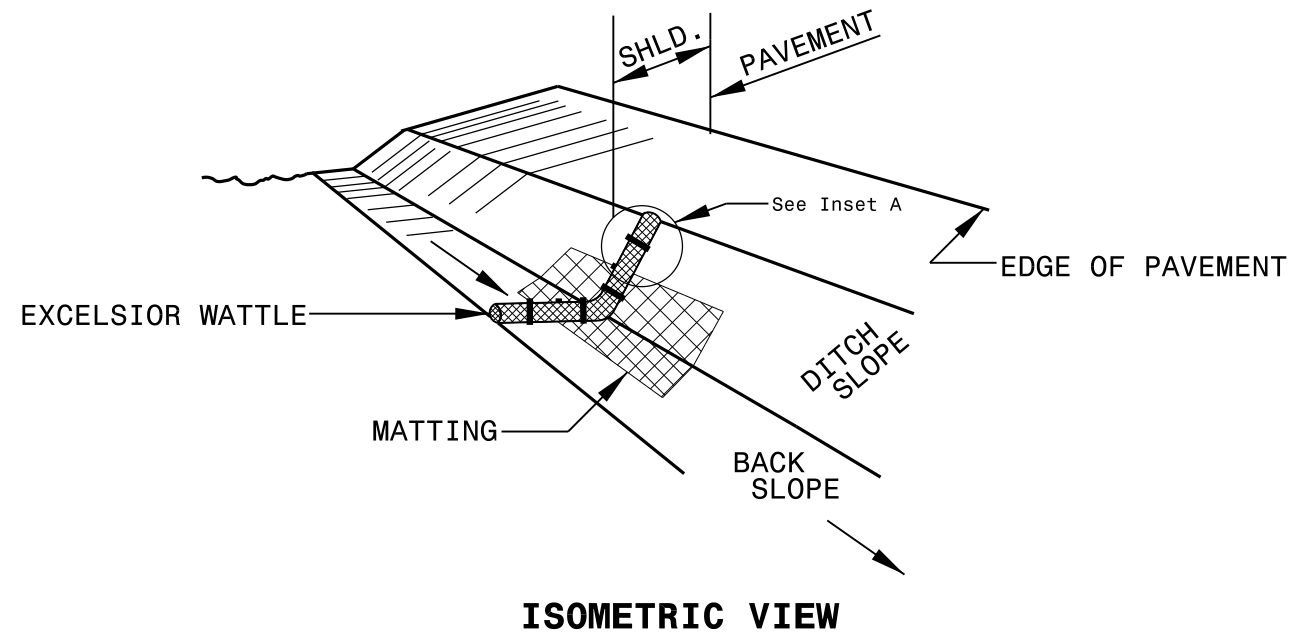


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

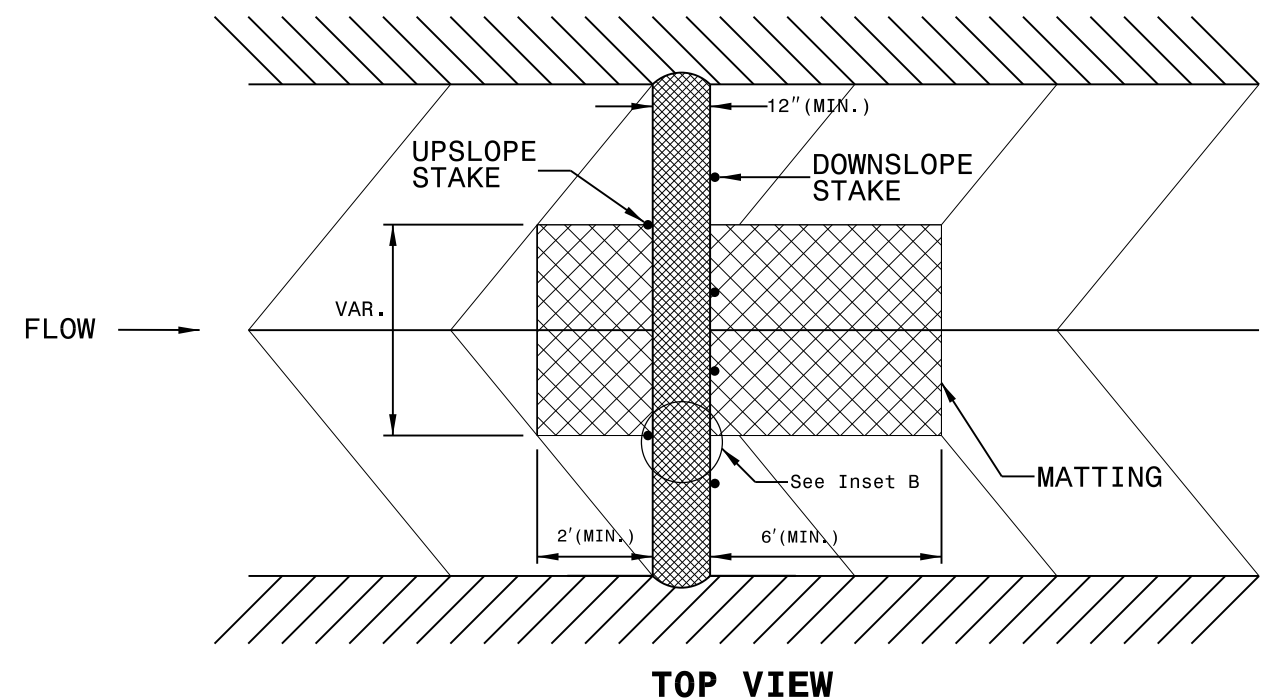
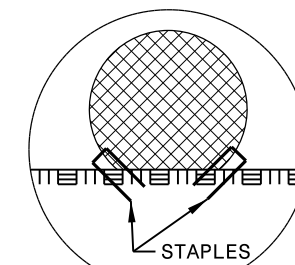
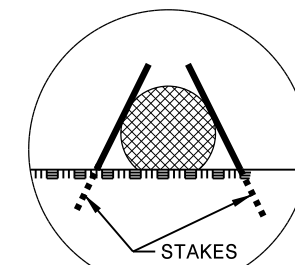
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



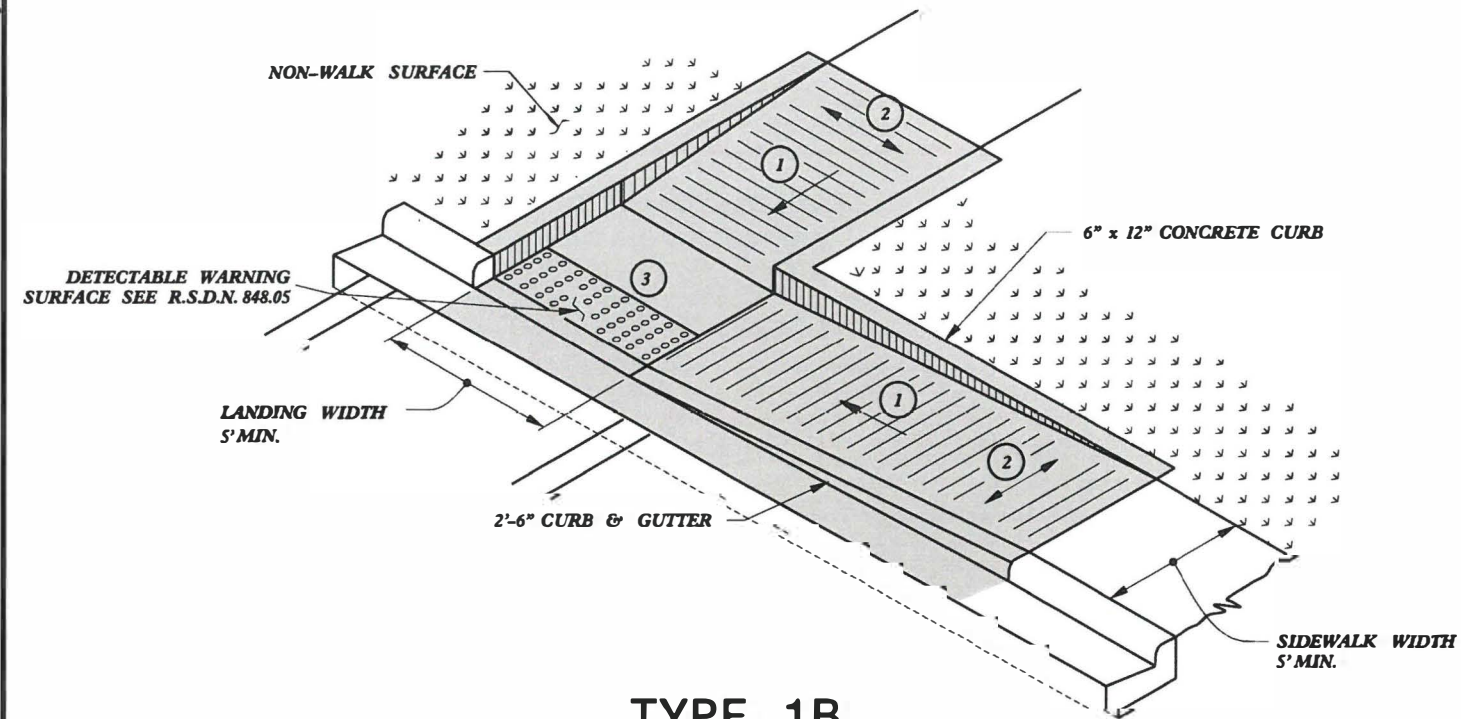
PROJECT NO.	SHEET NO.
2024CPT.03.03.20651, Etc.	9

SUMMARY OF QUANTITIES

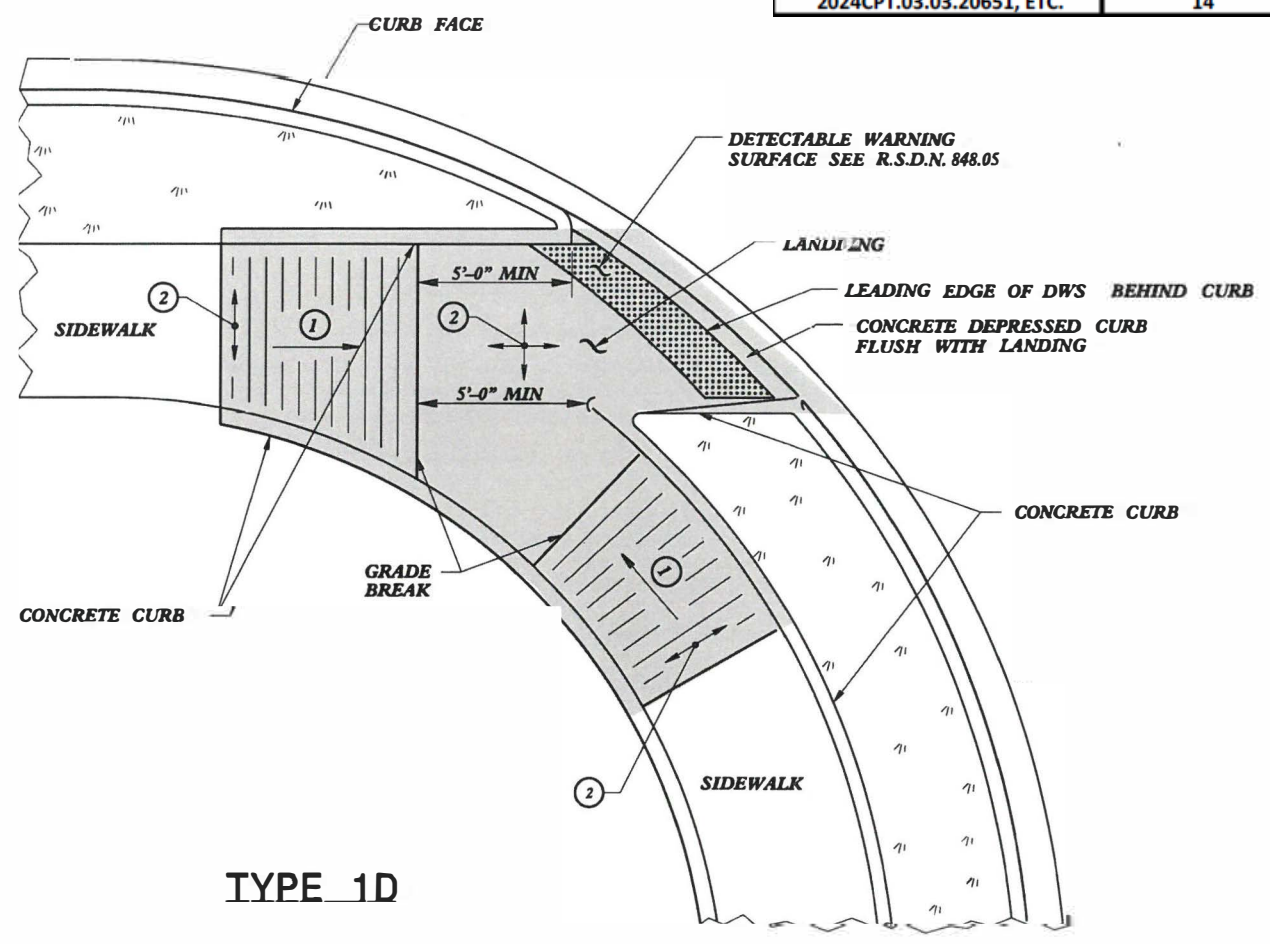
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0106000000-E	1220000000-E	1245000000-E	1260000000-E	1308000000-E	1330000000-E	1519000000-E	1520000000-E	1526000000-E	1575000000-E	1705500000-E	1880000000-E	2591000000-E	2613000000-N	2738000000-E			
												BORROW EXCAVATION	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	AGGREGATE SHOULDER BORROW	0" - 1.25" MILLING (GENERIC)	INCIDENTAL MILLING	SURFACE COURSE, \$9.5B	LEVELING COURSE, \$9.5B	SURFACE COURSE, \$4.75A	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT (MILL)	LEVELING COURSE, \$4.75A GENERIC	4" CONCRETE SIDEWALK	REMOVE & REPLACE CURB RAMPS (GENERIC)	REMOVE CONCRETE SIDEWALK (GENERIC)			
												MI	FT	CY	TON	SMI	TON	SY	SY	TON	TON	TON	TON	TON	SY	EA	SY		
2024CPT.03.03.20652	New Hanover	23	SR-2385 / TEVIOT RD	FROM SR 2384 (WEDDERBURN DR) TO SR 2383 (HAILSHAM DR) [MP 0.00 - MP 0.14]	5	2	2WU	NO	NO	0.14	22									80	6								
TOTAL FOR MAP NO. 23										0.14										80	6								
2024CPT.03.03.20652	New Hanover	24	SR-2386 / DORRINGTON DR	FROM SR 2381 (BURROUGHS DR) TO SR 2385 (TEVIOT RD) [MP 0.00 - MP 0.28]	5	2	2WU	NO	NO	0.28	22									166	12		6						
TOTAL FOR MAP NO. 24										0.28											166	12		6					
2024CPT.03.03.20652	New Hanover	25	SR-2138 / FIVE ACRE RD	FROM SR 2137 (ROCKWELL RD) TO END OF MAINTENANCE [MP 0.00 - 0.08]	5	2	2WU	NO	NO	0.08	27									55	4								
TOTAL FOR MAP NO. 25										0.08											55	4							
TOTAL FOR PROJ NO. 2024CPT.03.03.20652										1.92												1,153	83	20	30				
GRAND TOTAL										6.26												1,153	406	318	313	50	22	50	

THERMOPLASTIC AND PAINT QUANTITIES

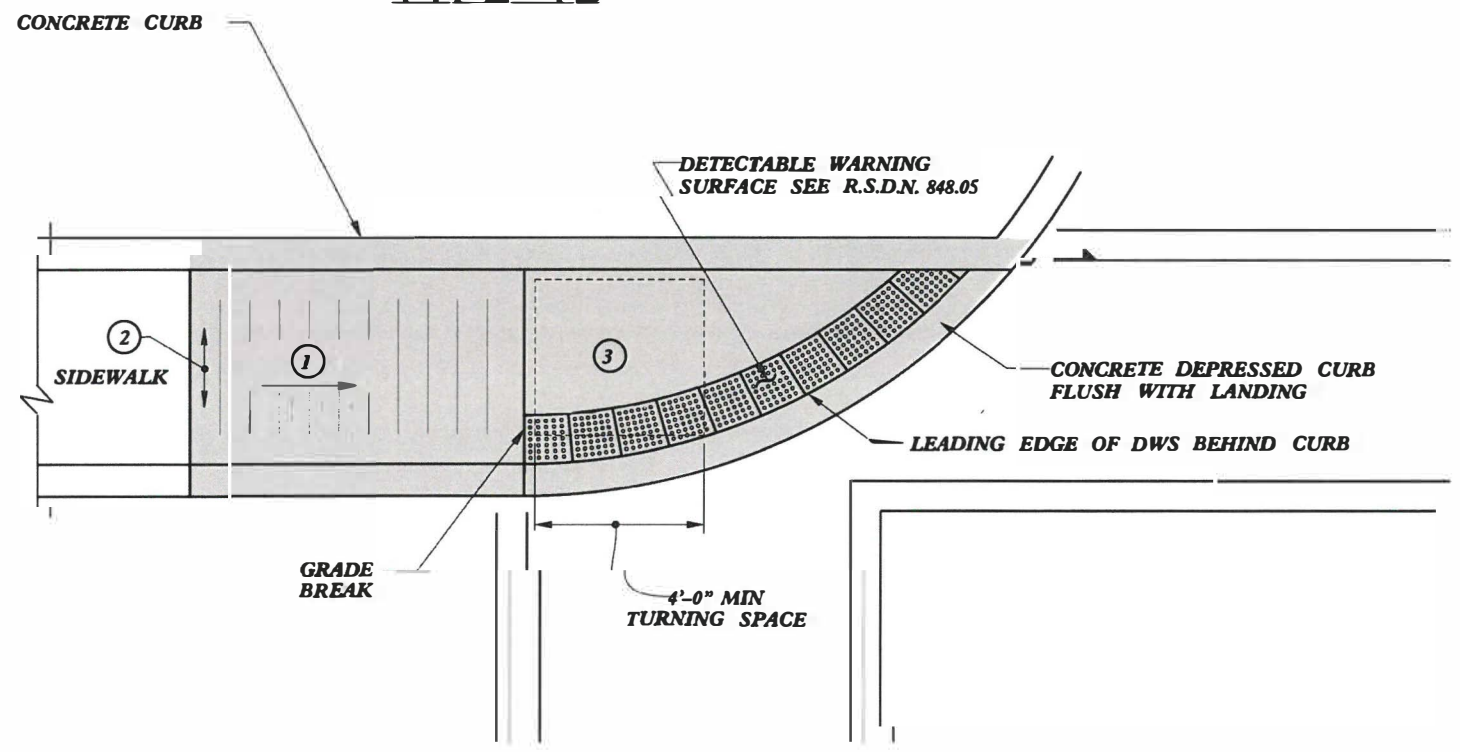
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4457000000-N	4510000000-N	4685000000-E		4695000000-E	4709000000-E	4900000000-N	
										TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	4" X 90 M YELLOW THERMO	4" X 90 M WHITE THERMO	8" X 90 M WHITE THERMO	THERMOPLASTIC PAVEMENT MARKING LINES (24" X 90 MILS)	YELLOW & YELLOW MARKERS	
										LS	HR	LF	LF	LF	LF	EA	
2024CPT.03.03.20651	New Hanover	1	SR-1339 / NORTHEAST AVE	FROM SR 1340 (OLD BRIDGE SITE RD) TO END OF MAINTENANCE [MP 0.00 - MP 0.65]	1	2	2WU	0.65	18	0.08							
TOTAL FOR MAP NO. 1																	
2024CPT.03.03.20651	New Hanover	2	SR-1316 / ARLINGTON DR	FROM NC 133 (CASTLE HAYNE RD) TO END OF MAINTENANCE [MP 0.00 - MP 0.59]	1	2	2WU	0.59	18	0.08							
TOTAL FOR MAP NO. 2																	
2024CPT.03.03.20651	New Hanover	3	SR-2165 / CANDLEWOOD DR	FROM SR 1322 (MURRAYVILLE RD) TO END OF MAINTENANCE [MP 0.00 - MP 0.44]	1	2	2WU	0.44	27	0.06							
TOTAL FOR MAP NO. 3																	
2024CPT.03.03.20651	New Hanover	4	SR-2166 / WOODBERRY CT	FROM SR 1322 (MURRAYVILLE RD) TO CUL-DE-SAC [MP 0.00 - MP 0.16]	1	2	2WU	0.16	26	0.03							
TOTAL FOR MAP NO. 4																	
2024CPT.03.03.20651	New Hanover	5	SR-2137 / ROCKWELL RD	FROM SR 1322 (MURRAYVILLE RD) TO END OF MAINTENANCE [MP 0.00 - MP 0.33]	1	2	2WU	0.33	27	0.05							
TOTAL FOR MAP NO. 5																	
2024CPT.03.03.20651	New Hanover	6	SR-2139 / NEIL CT	FROM SR 2137 (ROCKWELL RD) TO CUL-DE-SAC [MP 0.00 - MP 0.06]	1	2	2WU	0.06	27	0.02							
TOTAL FOR MAP NO. 6																	
2024CPT.03.03.20651	New Hanover	7	SR-2140 / CRICKET CT	FROM SR 2137 (ROCKWELL RD) TO CUL-DE-SAC [MP 0.00 - MP 0.06]	1	2	2WU	0.06	27	0.02							
TOTAL FOR MAP NO. 7																	
2024CPT.03.03.20651	New Hanover	8	SR-2346 / GLENARTHUR DR	FROM US 421 (CAROLINA BEACH RD) TO SR 2386 (DORRINGTON DR) [MP 0.00 - MP 0.61]	2	2	2WU	0.61	30	0.08							
TOTAL FOR MAP NO. 8																	
2024CPT.03.03.20651	New Hanover	9	SR-2022 / SHELLEY DR	FROM US 117 (N COLLEGE RD) TO SR 2003 (KINGS GRANT RD) [MP 0.00 - MP 0.17]	1	2	2WU	0.17	20	0.03							
TOTAL FOR MAP NO. 9																	
2024CPT.03.03.20651	New Hanover	10	SR-1539 / OCEAN BLVD	FROM SR 1573 (DOW RD) TO US 421 (S. LAKE PARK BLVD) [MP 0.00 - MP 0.82]	3	2	2WU	0.82	19	0.09			7,824	8,207		100	
TOTAL FOR MAP NO. 10																	
2024CPT.03.03.20651	New Hanover	11	SR-1531 / S SEABREEZE RD	FROM US 421 (CAROLINA BEACH RD) TO SR 1530 (ELM ST) [MP 0.00 - MP 0.45]	2	2	2WU	0.45	19	0.05	40	3,854	4,752		31	50	
TOTAL FOR MAP NO. 11																	
TOTAL FOR PROJ NO. 2024CPT.03.03.20651																	
																	24,637
2024CPT.03.03.20652	New Hanover	12	SR-2382 / STODDARD DR	FROM SR 2346 (GLENARTHUR DR) TO SR 2383 (HAILSHAM DR) [MP 0.00 - MP 0.16]	5	2	2WU	0.16	22	0.03							
TOTAL FOR MAP NO. 12																	
2024CPT.03.03.20652	New Hanover	13	SR-2383 / HAILSHAM DR	FROM SR 2379 (BANKROFT DR) TO SR 2385 (TEVIOT RD) [MP 0.00 - MP 0.29]	5	2	2WU	0.29	22	0.04				52	22		
TOTAL FOR MAP NO. 13																	
2024CPT.03.03.20652	New Hanover	14	SR-2384 / WEDDERBURN DR	FROM SR 2346 (GLENARTHUR DR) TO SR 2383 (HAILSHAM DR) [MP 0.00 - MP 0.22]	5	2	2WU	0.22	22	0.03							
TOTAL FOR MAP NO. 14																	
2024CPT.03.03.20652	New Hanover	15	SR-2347 / TANLAW CT	FROM SR 2346 (GLENARTHUR DR) TO CUL-DE-SAC [MP 0.00 - 0.02]	5	2	2WU	0.02	18	0.03							
TOTAL FOR MAP NO. 15																	
2024CPT.03.03.20652	New Hanover	16	SR-2375 / CHELWOOD CT	FROM SR 2346 (GLENARTHUR DR) TO CUL-DE-SAC [MP 0.00 - MP - 0.07]	5	2	2WU	0.07	22	0.02							
TOTAL FOR MAP NO. 16																	
2024CPT.03.03.20652	New Hanover	17	SR-2376 / MACKAY CT	FROM SR 2346 (GLENARTHUR DR) TO CUL-DE-SAC [MP 0.00 - MP 0.02]	5	2	2WU	0.02	18	0.02							
TOTAL FOR MAP NO. 17																	
2024CPT.03.03.20652	New Hanover	18	SR-2377 / OLIVER CT	FROM SR 2346 (GLENARTHUR DR) TO CUL-DE-SAC [MP 0.00 - MP 0.04]	5	2	2WU	0.04	22	0.02							
TOTAL FOR MAP NO. 18																	
2024CPT.03.03.20652	New Hanover	19	SR-2378 / CHORLEY RD	FROM SR 2346 (GLENARTHUR DR) TO SR 2379 (BANCROFT DR) [MP 0.00 - MP 0.05]	5	2	2WU	0.05	22	0.03							
TOTAL FOR MAP NO. 19																	



TYPE 1B



TYPE 1D



TYPE 1C

- ① 8.33% (12:1) MAX RAMP SLOPE
- ② CROSS SLOPE: 2.00%
- ③ CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.

PAY LIMITS FOR 1 CURB RAMP

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES



Designed by:
Nicole M. Hecker
588422034164.CS

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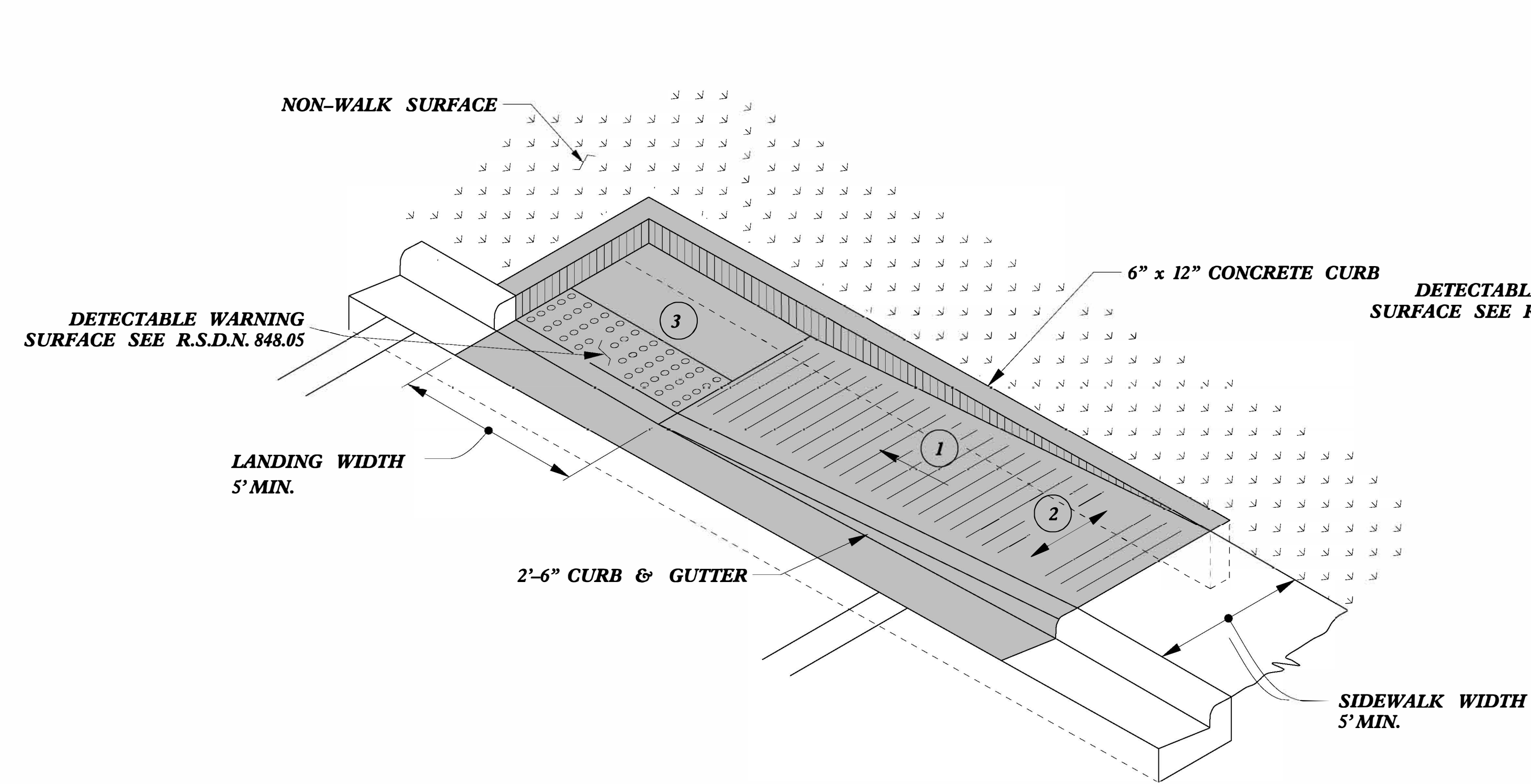
CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 FAX 919-250-4119

CURB RAMPS
Directional Ramps

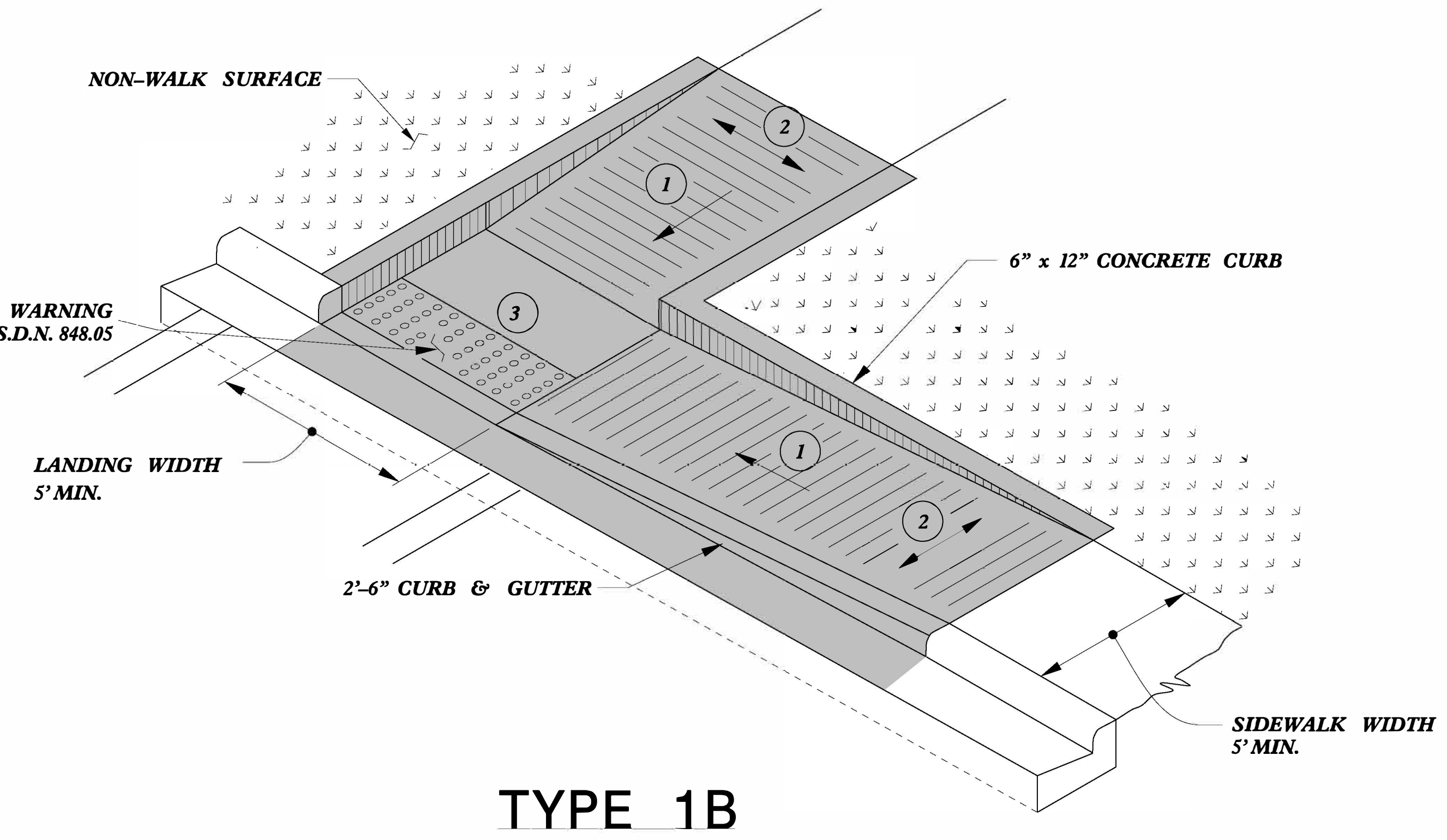
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1/4/99
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5/14/99



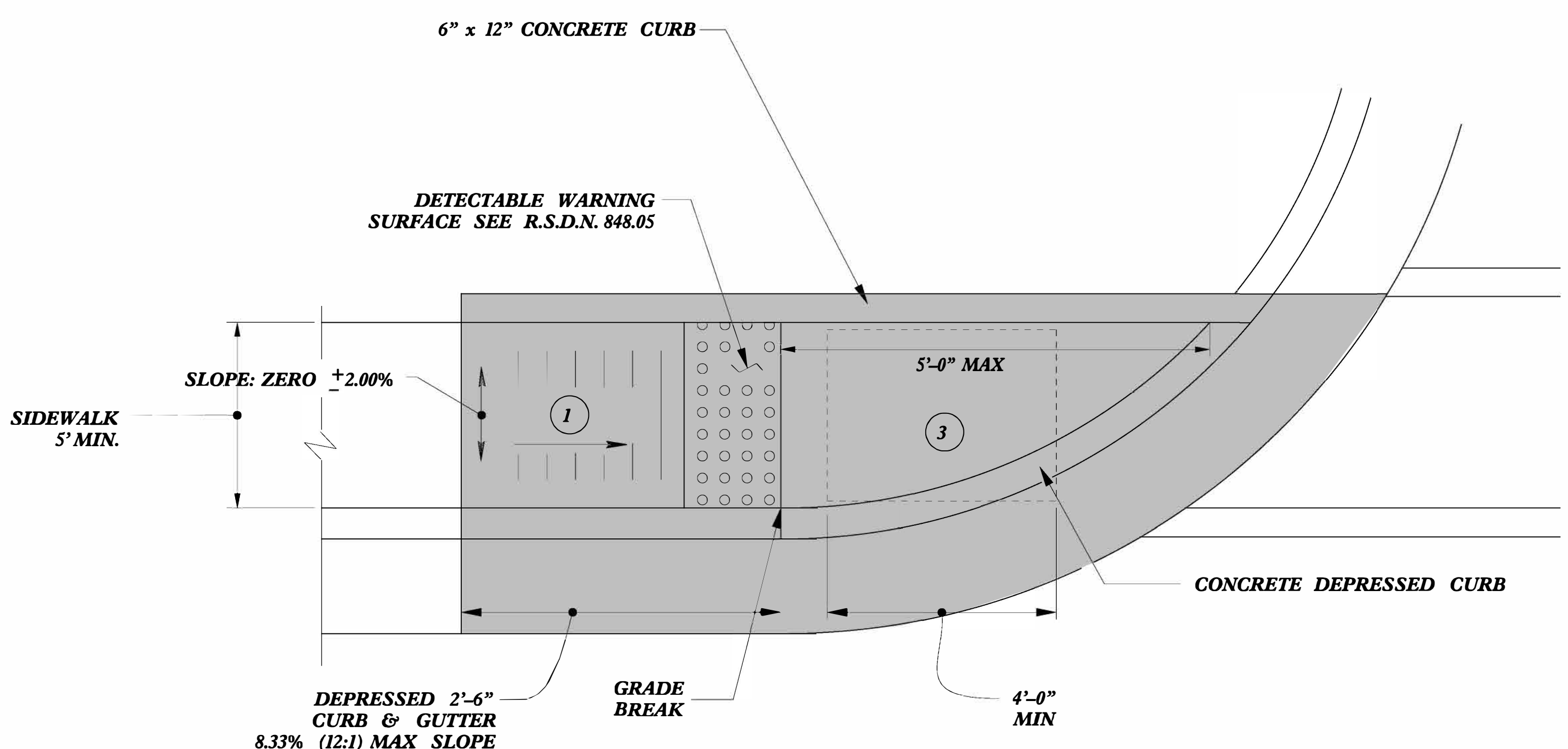
TYPE 1A



TYPE 1B

PAY LIMITS FOR 1 CURB RAMP

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



TYPE 1

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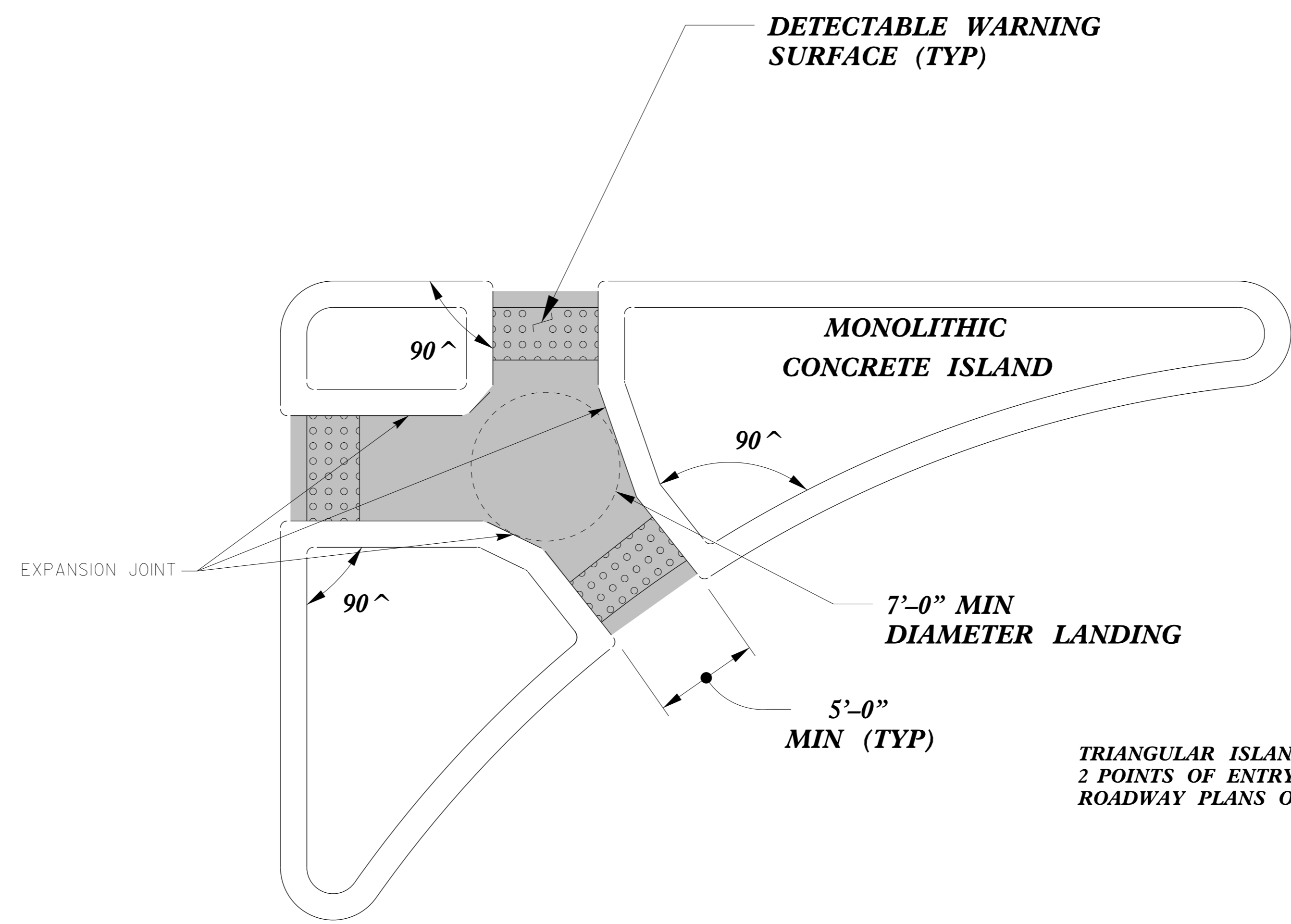
CURB RAMPS
Directional Ramps

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11
MODIFIED BY: _____ DATE: _____
CHECKED BY: _____ DATE: _____
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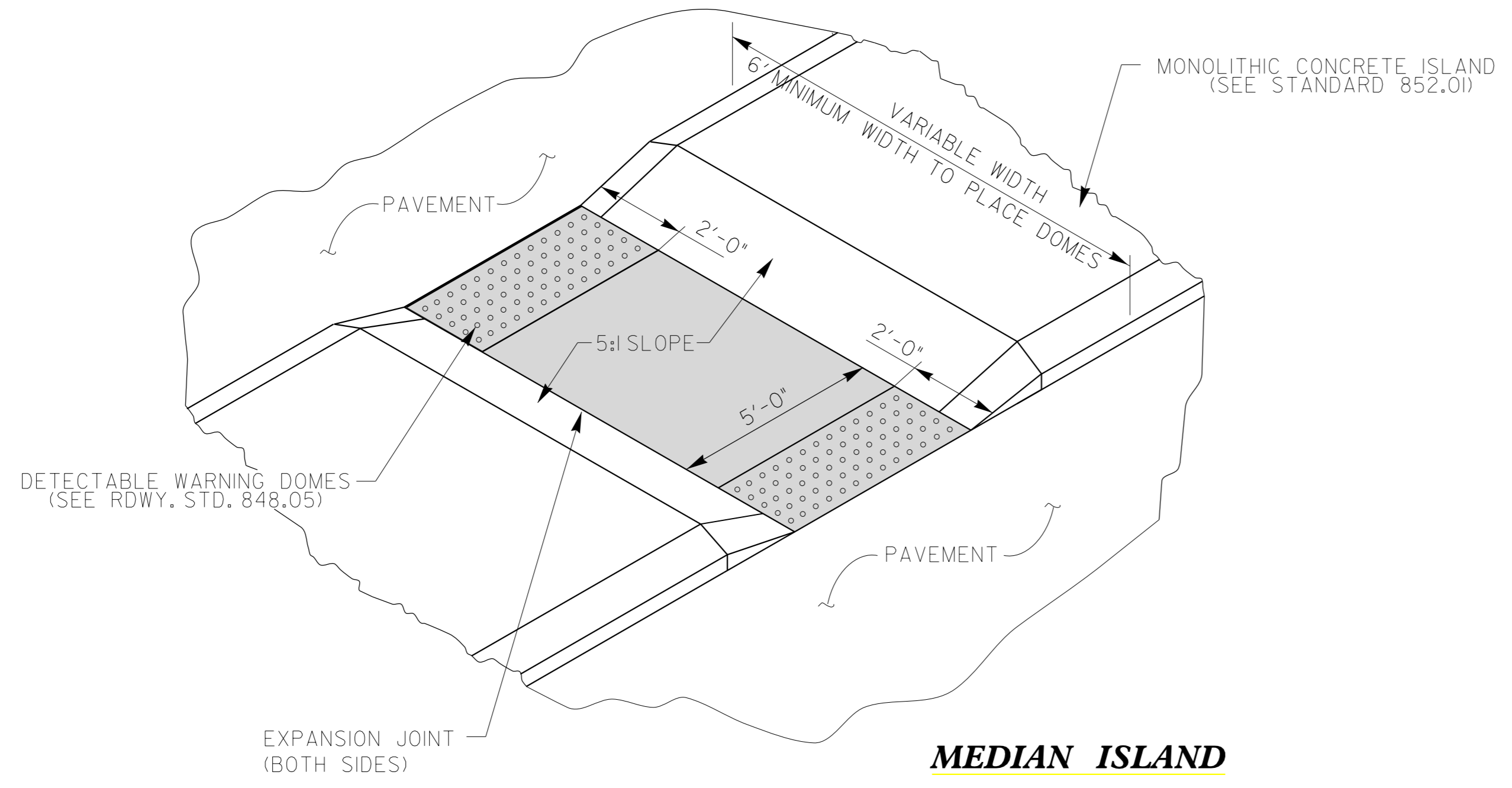
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5/14/99

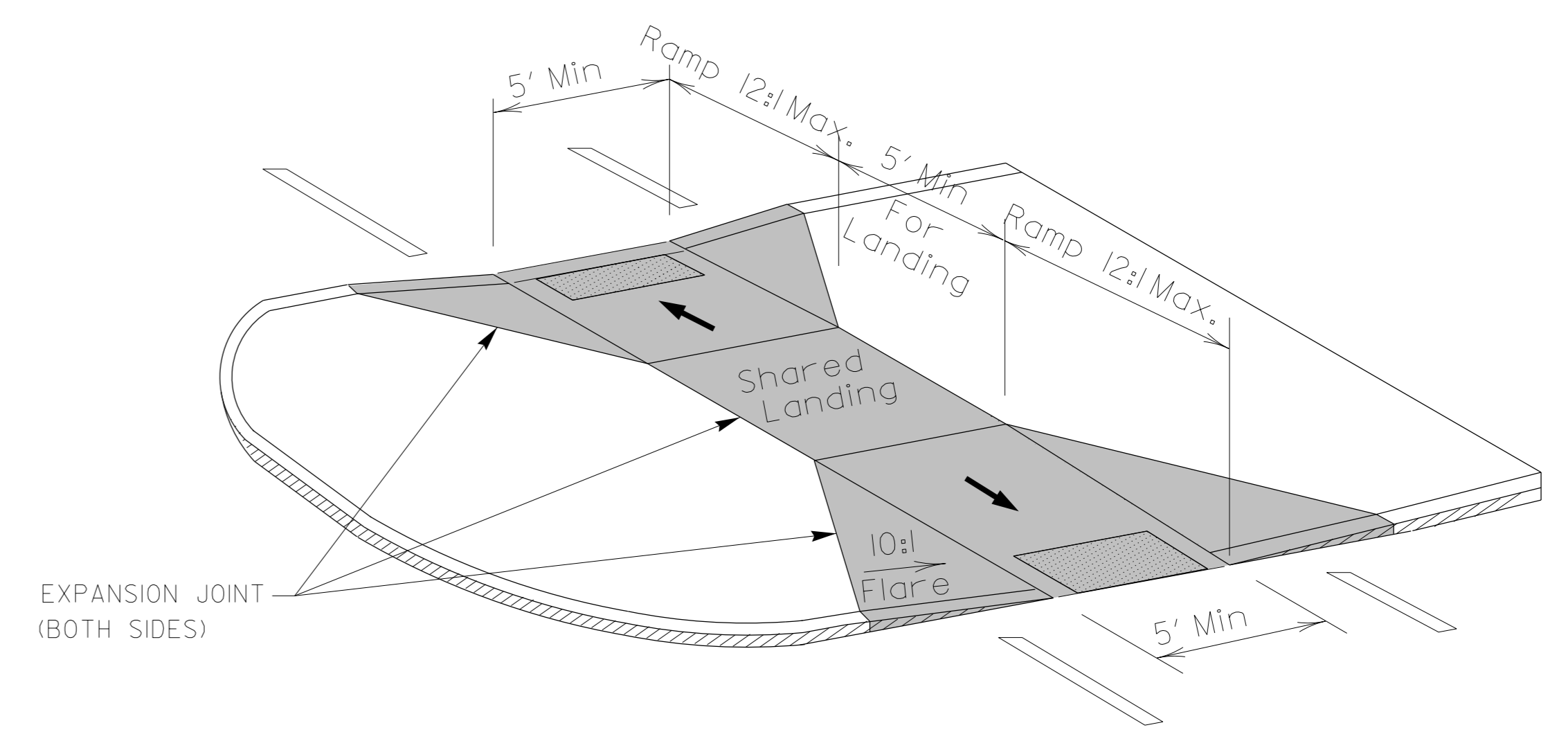
 PAY LIMITS FOR 2 OR 3 CURB RAMPS
(CALCULATE BASED ON NUMBER OF
SETS OF TRUNCATED DOMES)



**TRIANGULAR ISLAND
WITH CUT THROUGH**



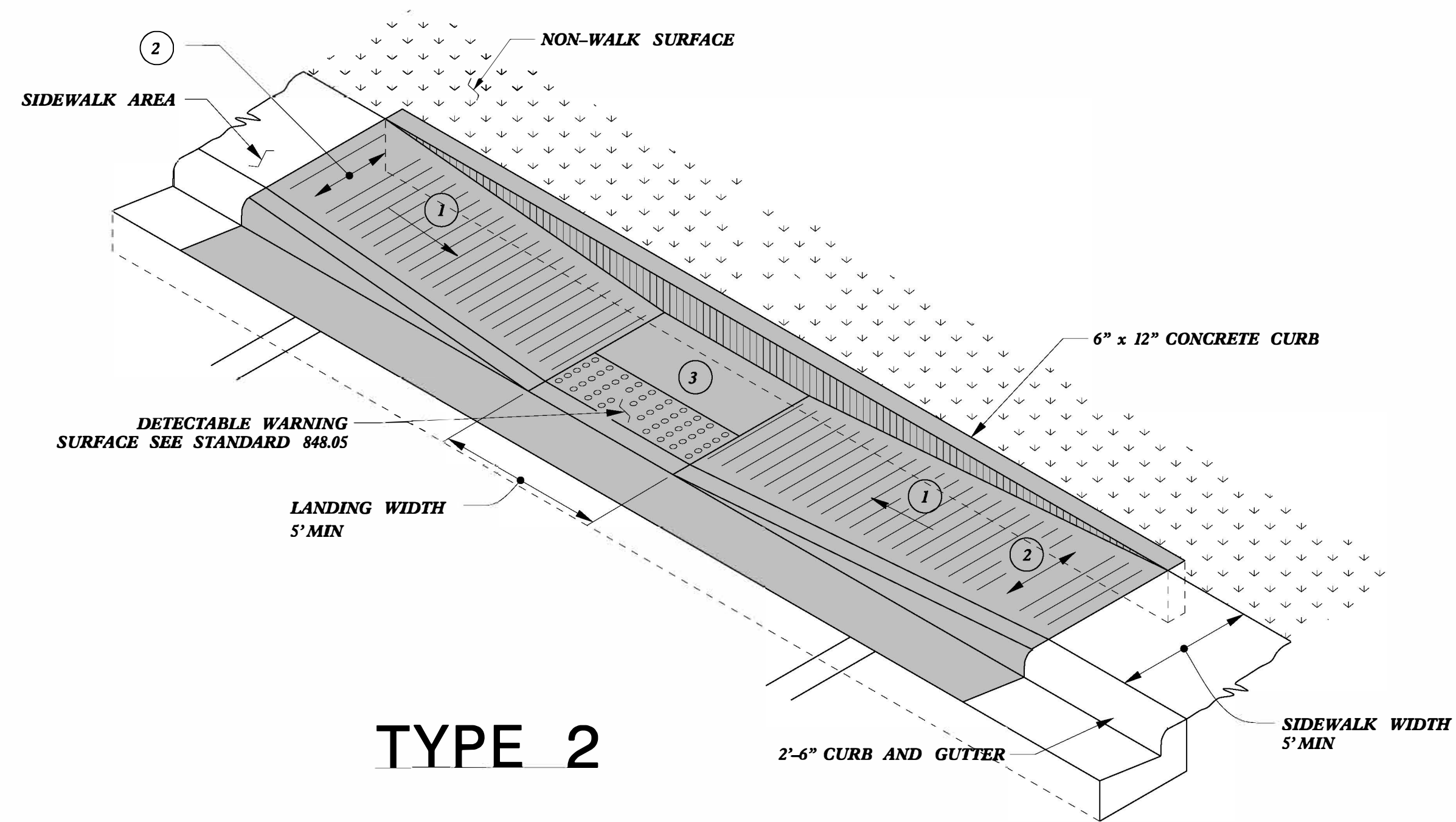
**MEDIAN ISLAND
WITH CUT THROUGH**



**MEDIAN ISLAND
CURB RAMPS**

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
CURB RAMPS	
Median or Turn Lane Islands	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC.: stds/2012CurbRamp/CurbRampDetails.dgn	

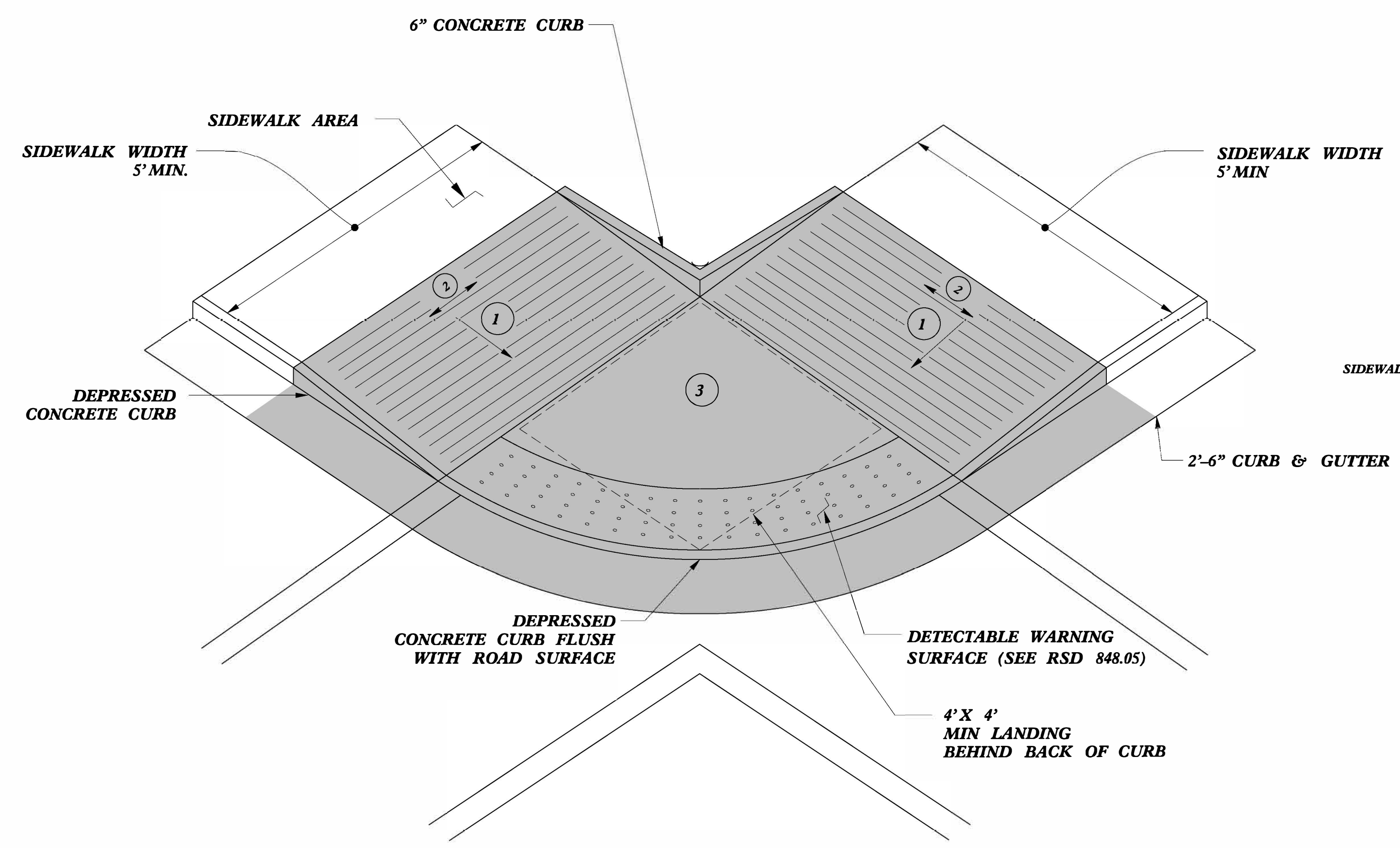
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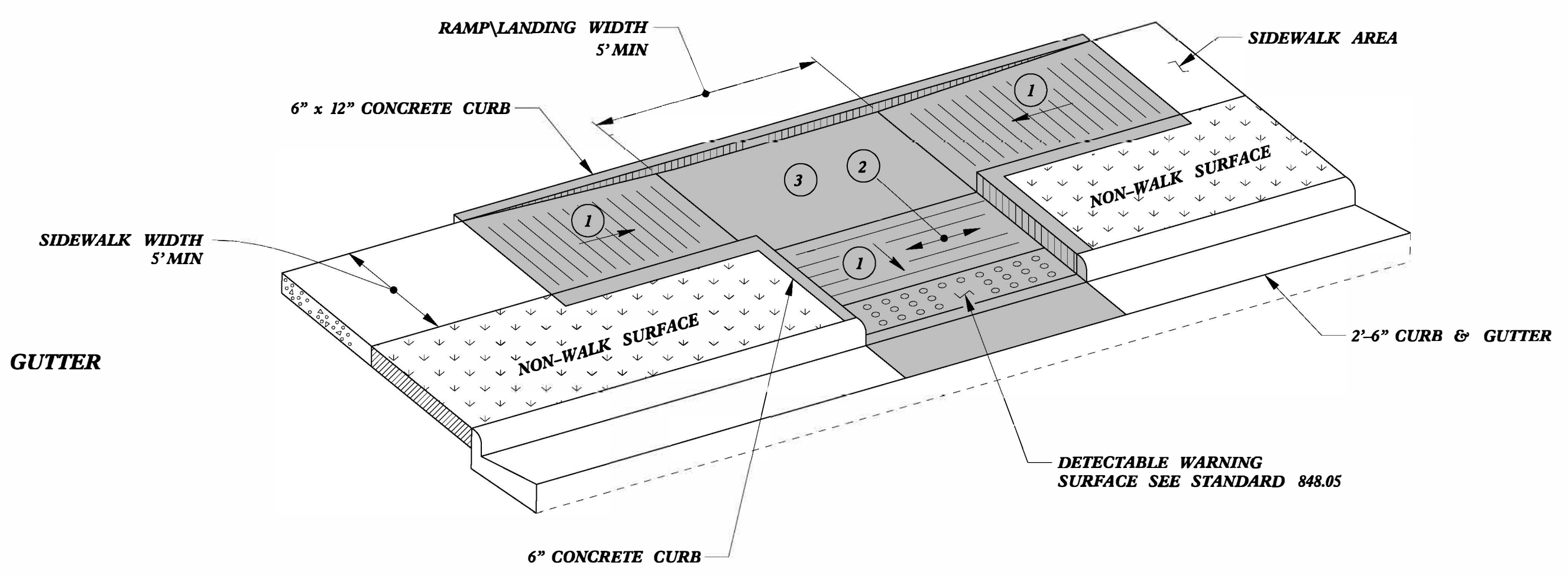
TYPE 2

PAY LIMITS FOR 1 CURB RAMP

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



TYPE 2A



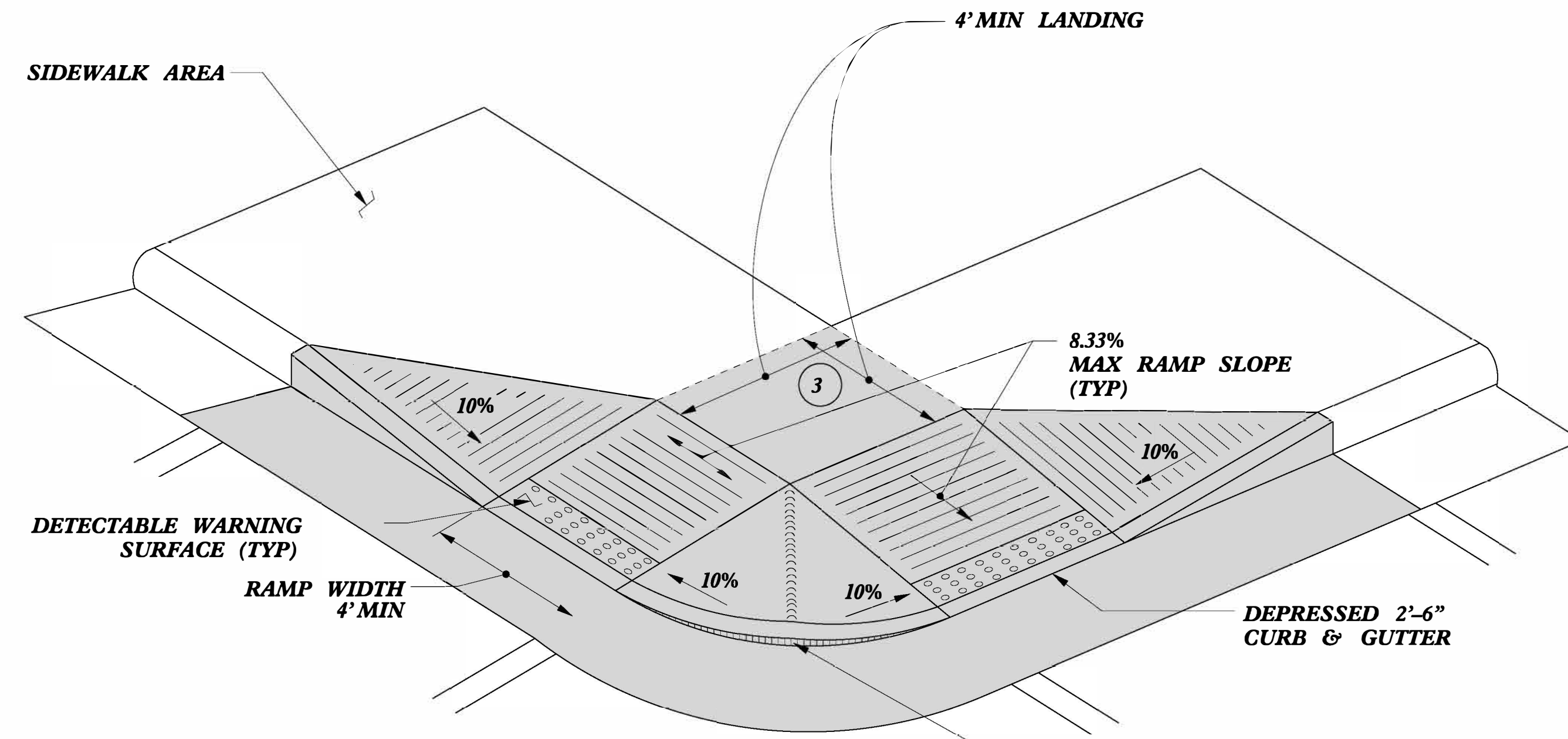
TYPE 3

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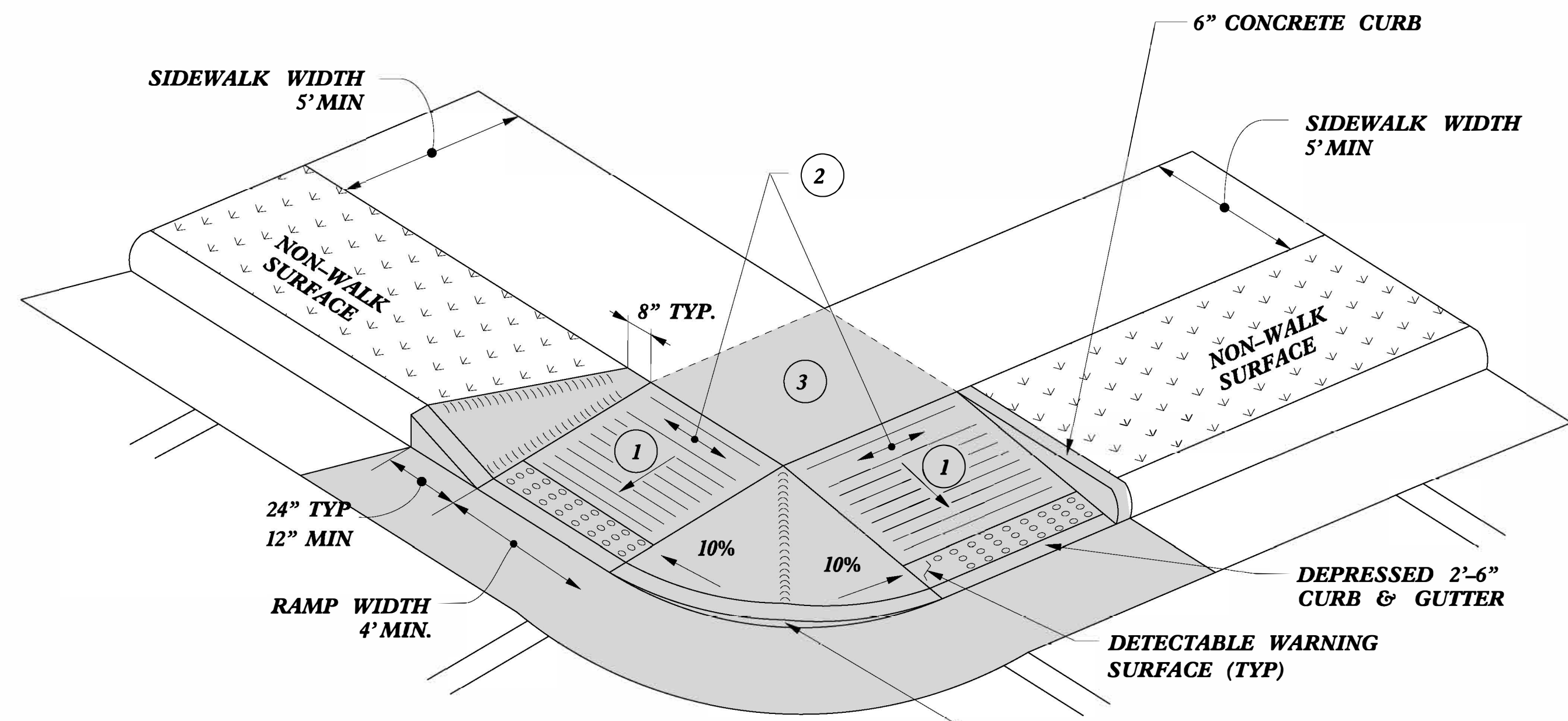
CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950 FAX 919-250-4119	
CURB RAMPS	
Parallel Ramps	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
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REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

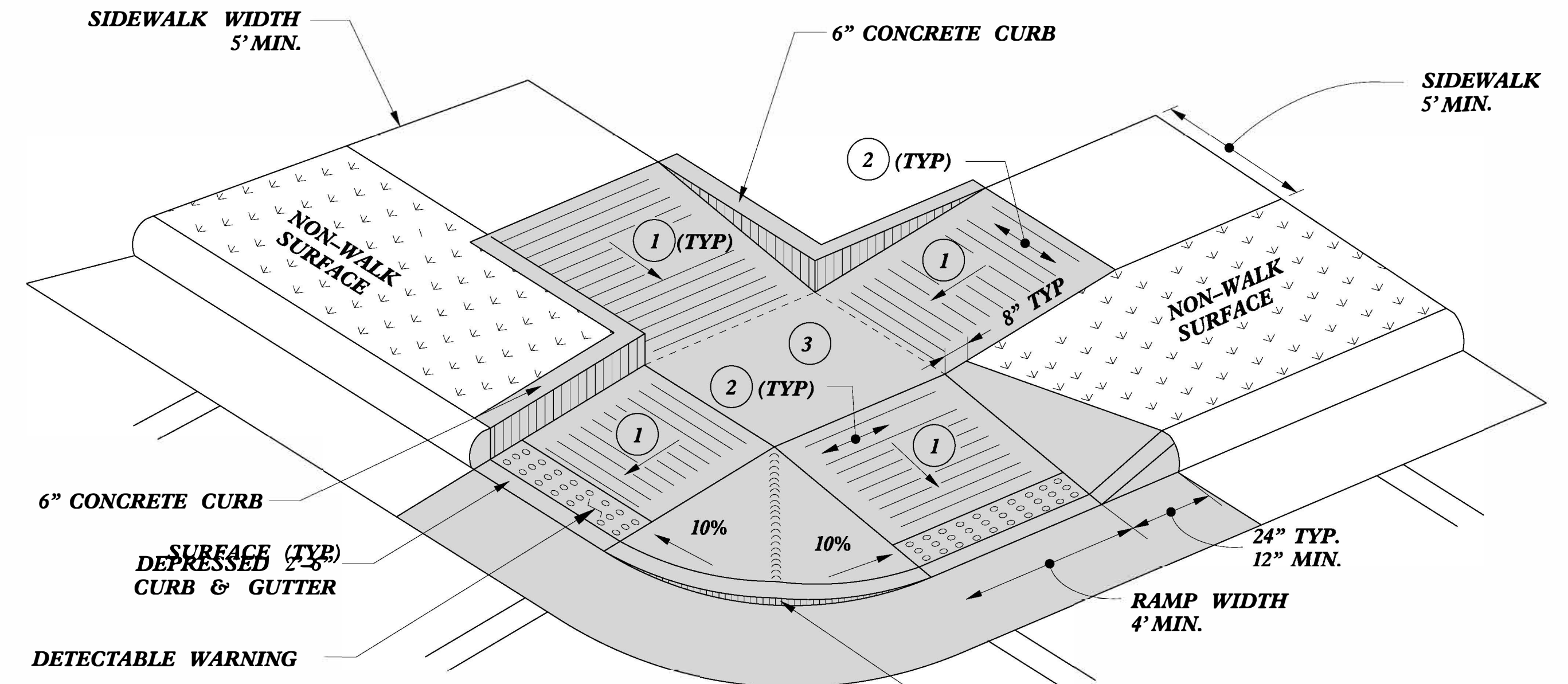
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TYPE 4



TYPE 4A



TYPE 5

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.

PAY LIMITS FOR 2 CURB RAMPS

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AND DEVELOPMENT UNIT**
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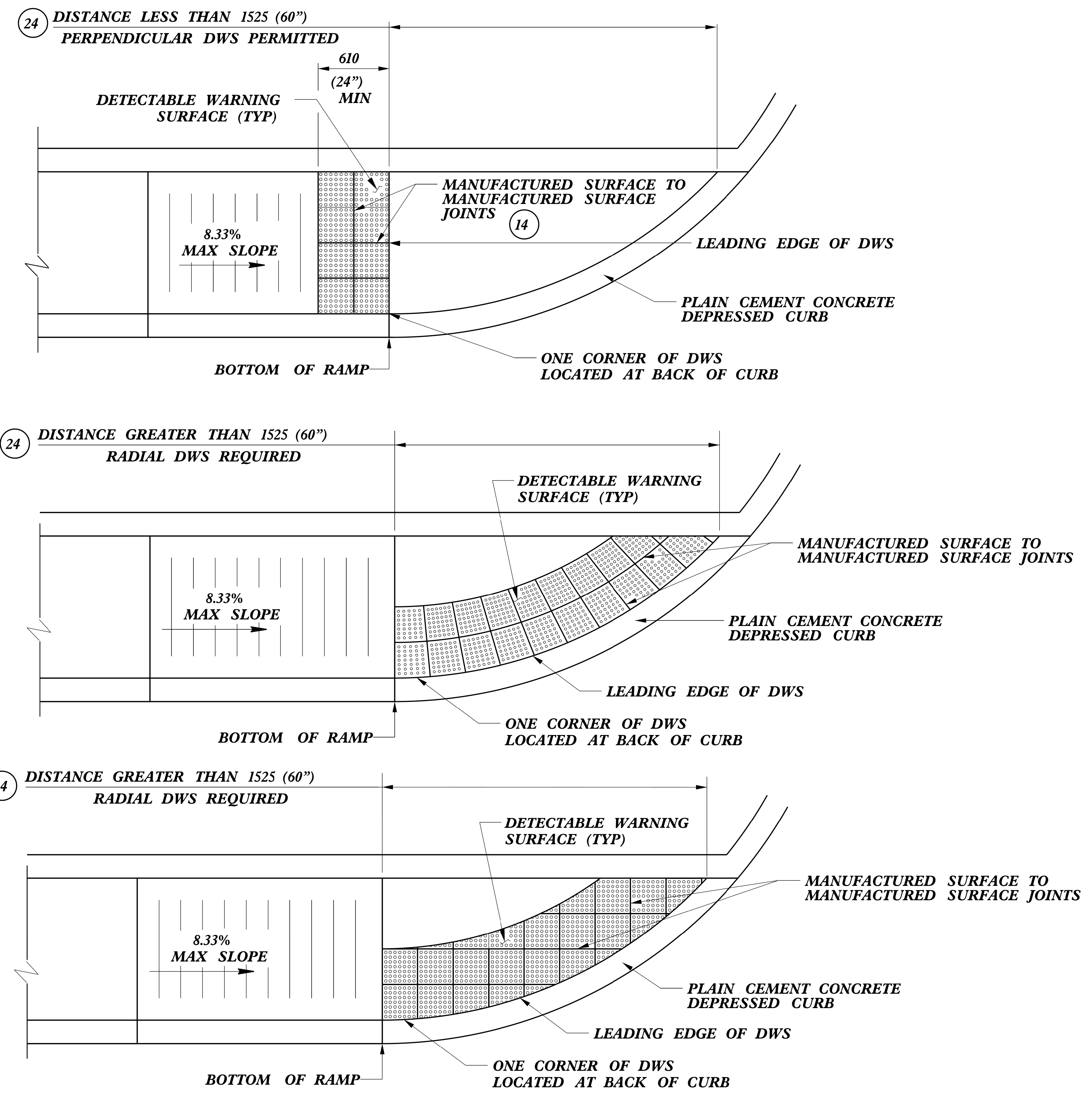
CURB RAMPS
Shared Landing

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11
MODIFIED BY: DATE:
CHECKED BY: DATE:
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dgn

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

5/14/99
\$\$\$\$\$CUT\$\$\$\$\$
\$\$\$\$\$USER\$\$\$\$\$

TRUNCATED DOMES AT RADIUS DETAIL



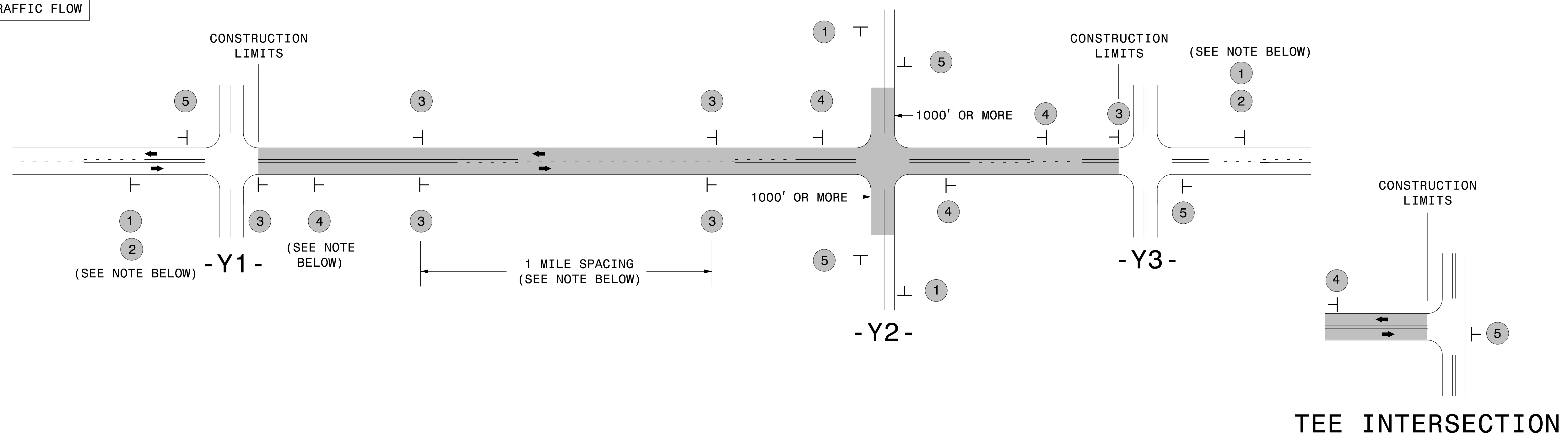
DETECTABLE WARNING SURFACE (DWS)
ON CURVED SURFACES

REVISIONS

14 DEC 2015 08:46:46 USER: J... NDER\SR-5001CB...40924.1.F78.S1.dwg Point Loop MUP\ROADWAY\Proj\Plan_Sheets\3CR.PE_PSH.L2F.dgn
 8/17/2015

SIGNING FOR RESURFACING PROJECTS

LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

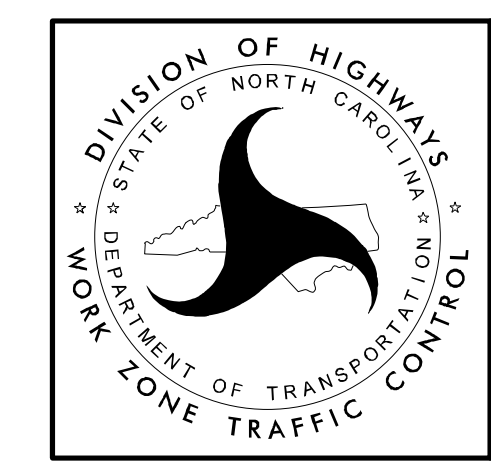
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING