

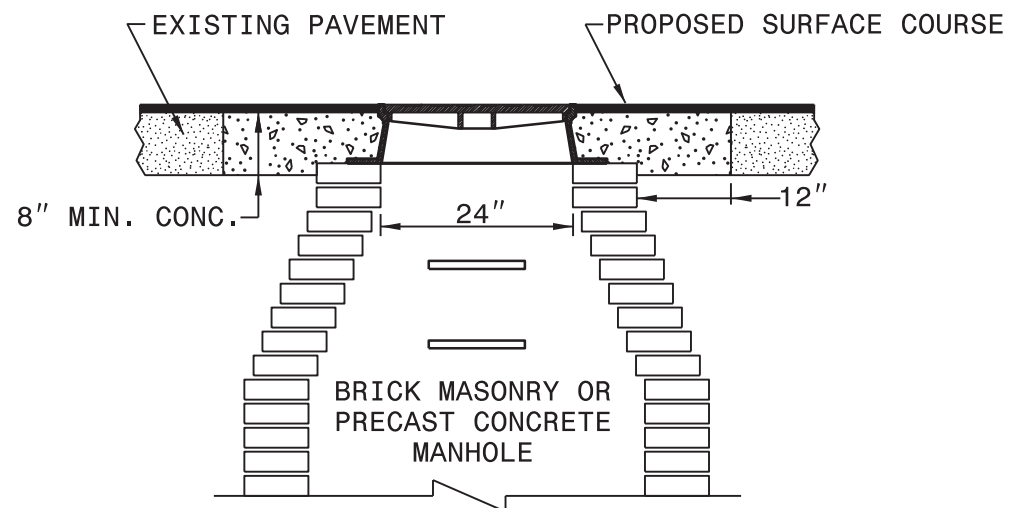
STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

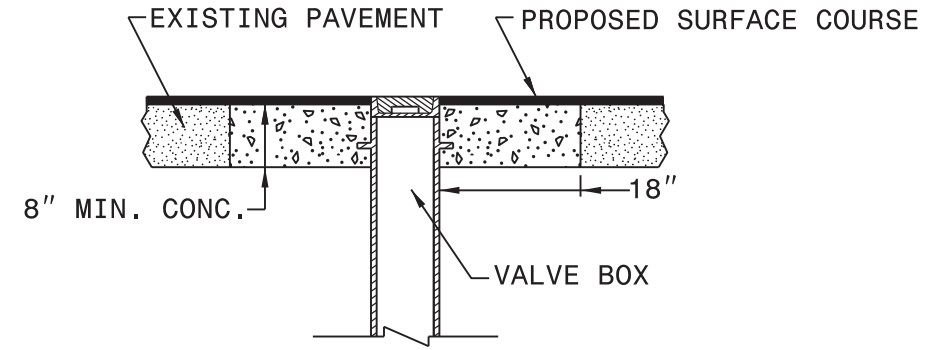
SHEET 1 OF 1
840D55

GENERAL NOTES:

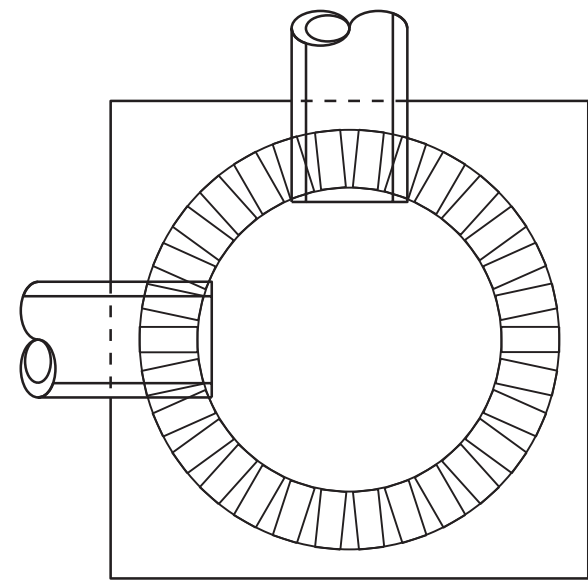
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS $\frac{1}{2}$ " +/- $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2018 STANDARD SPECIFICATIONS.



MANHOLE CONCRETE ENCASEMENT



VALVE BOX CONCRETE ENCASEMENT



ELEVATION VIEW

PLACE BRICK ACCORDING TO ELEVATION VIEW

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

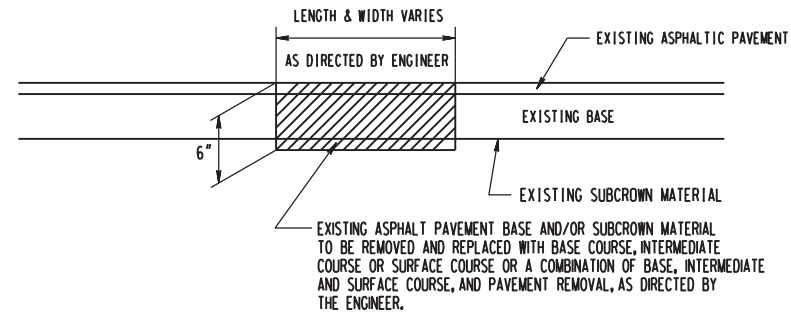
SHEET 1 OF 1
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STATE OF NORTH CAROLINA
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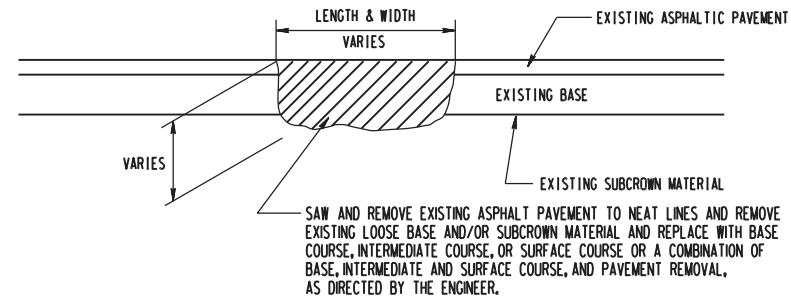
REVISIONS

28-NOV-2017 10:49
S:\Division Resurfacing\z Design Standards & CADD\CADD_Specs.Dwg\Typicals_Curb, Etc.dwg
8/17/99

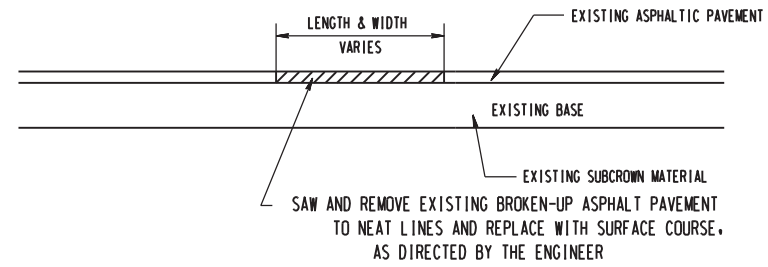
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



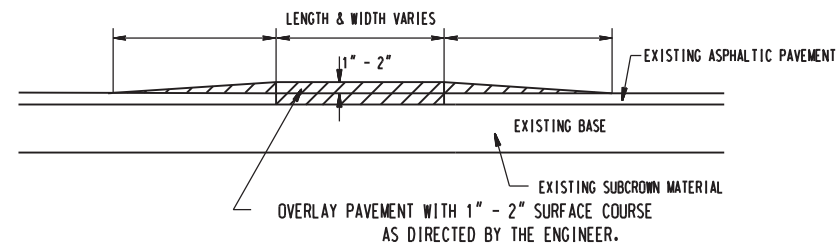
DETAIL NO. 1



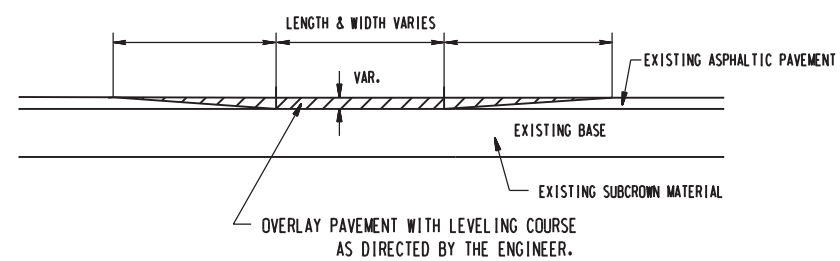
DETAIL NO. 2



DETAIL NO. 3



DETAIL NO. 4

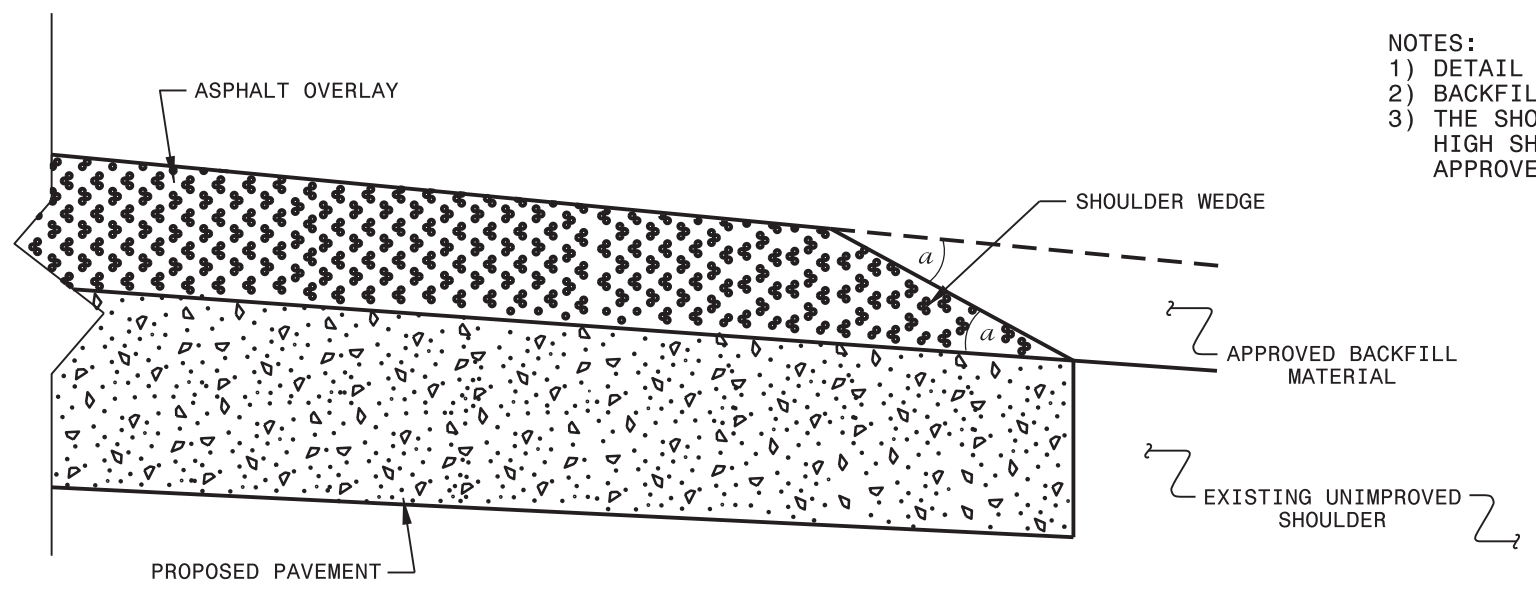


DETAIL NO. 5

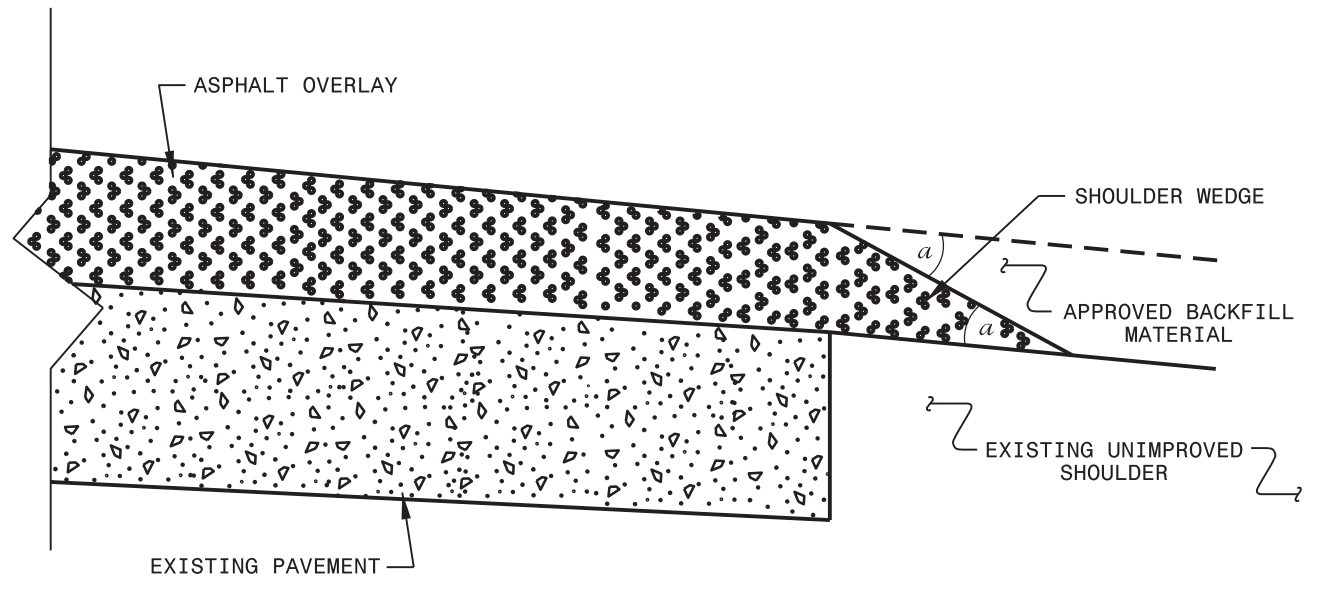
REVISIONS

20-APP-2015 ILEF
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 \$\$\$USERNAME\$\$\$

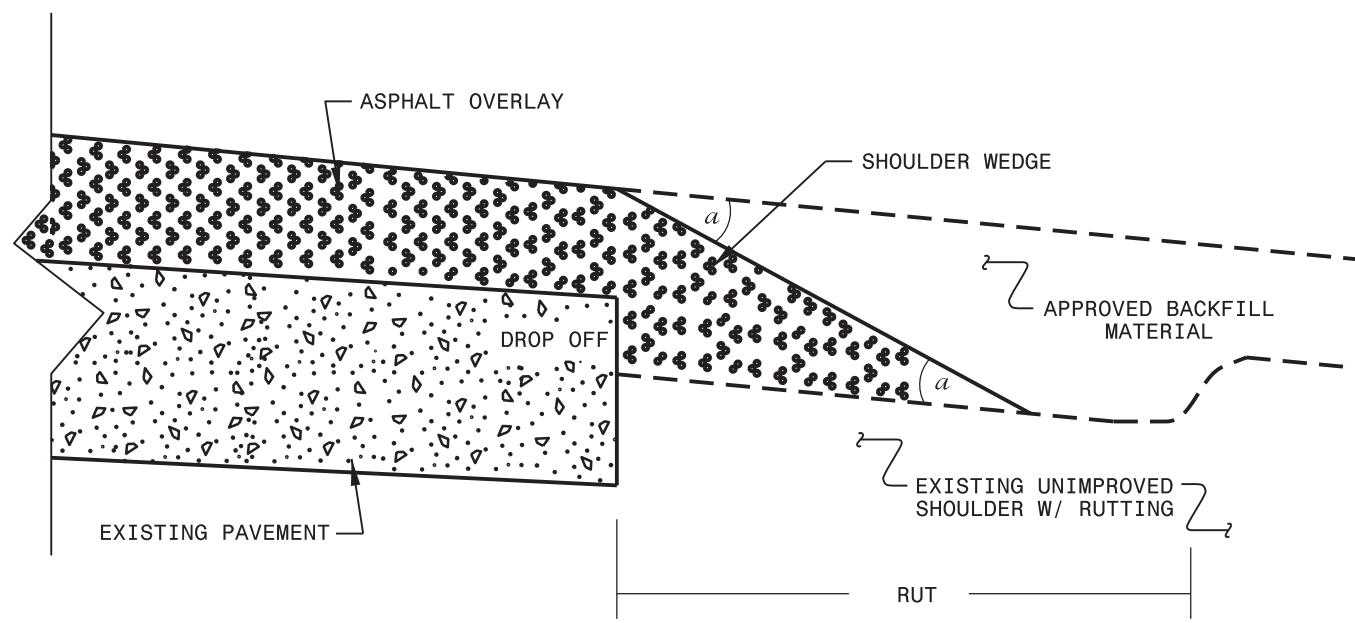
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22
 s:\usr\details\stand\shoulderwedgedetail.dgn
 3\Sampson August 2018 Revised Shoulder Wedge Detail.dgn
 10/25/12

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

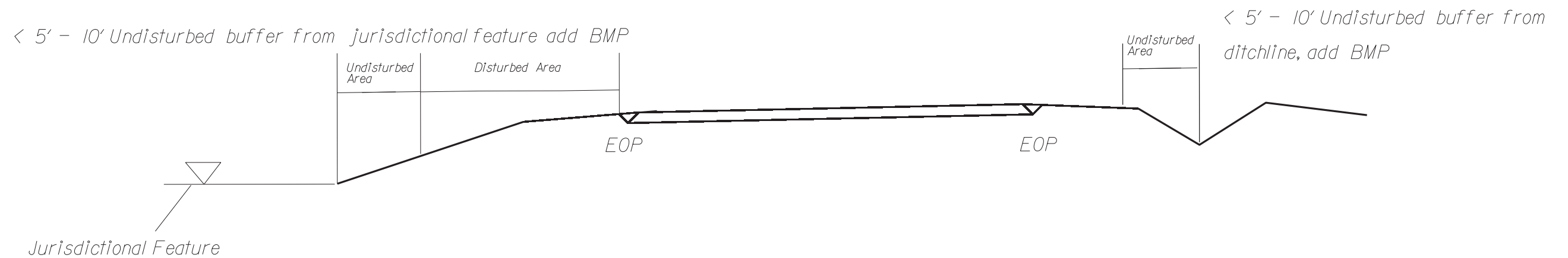
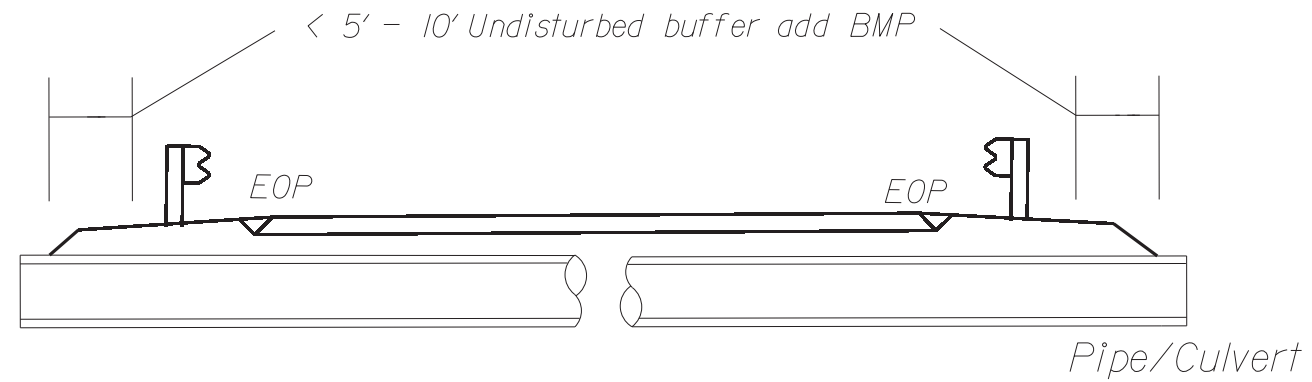
SOIL STABILIZATION TIMEFRAMES

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

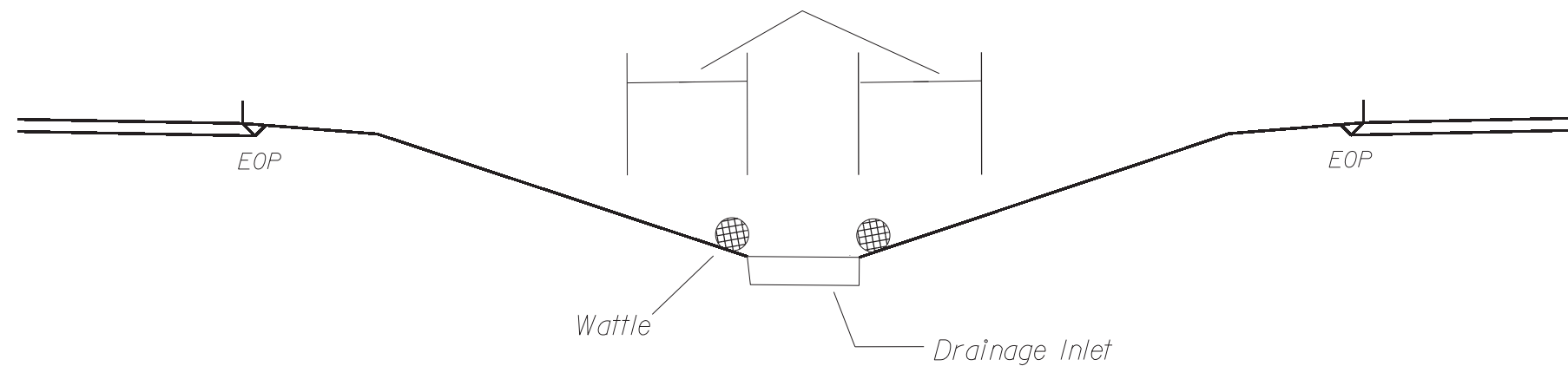
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

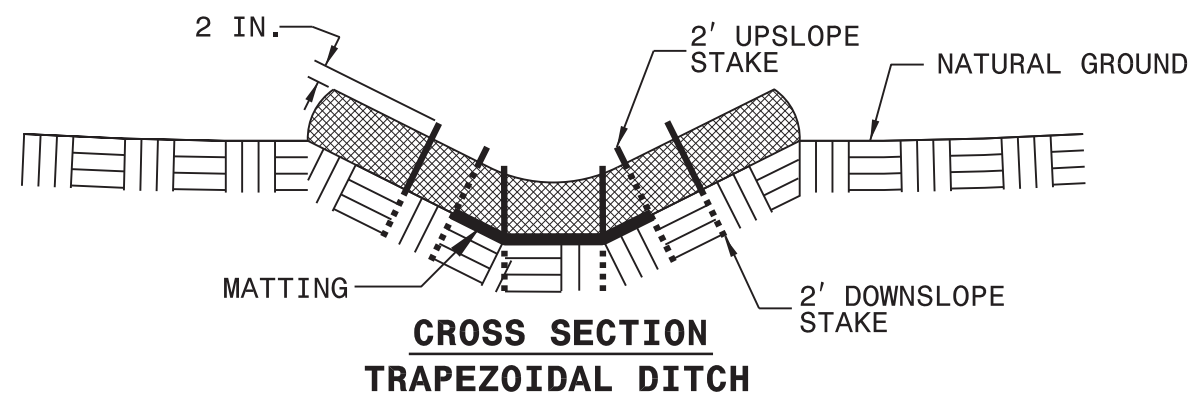
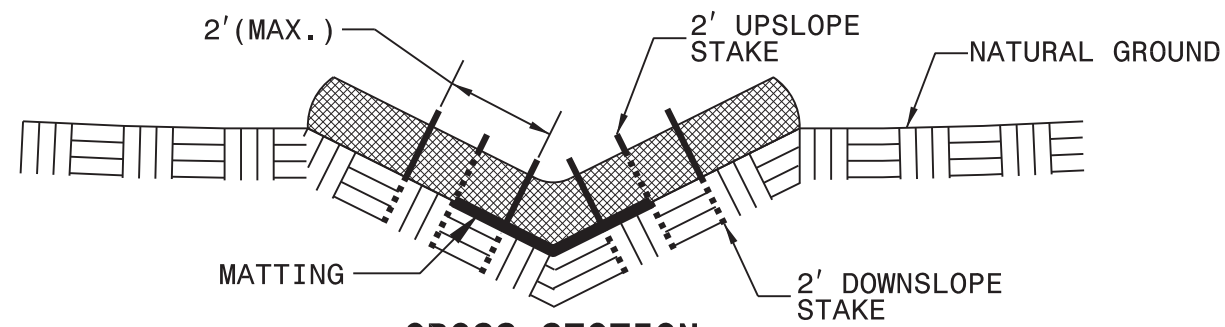
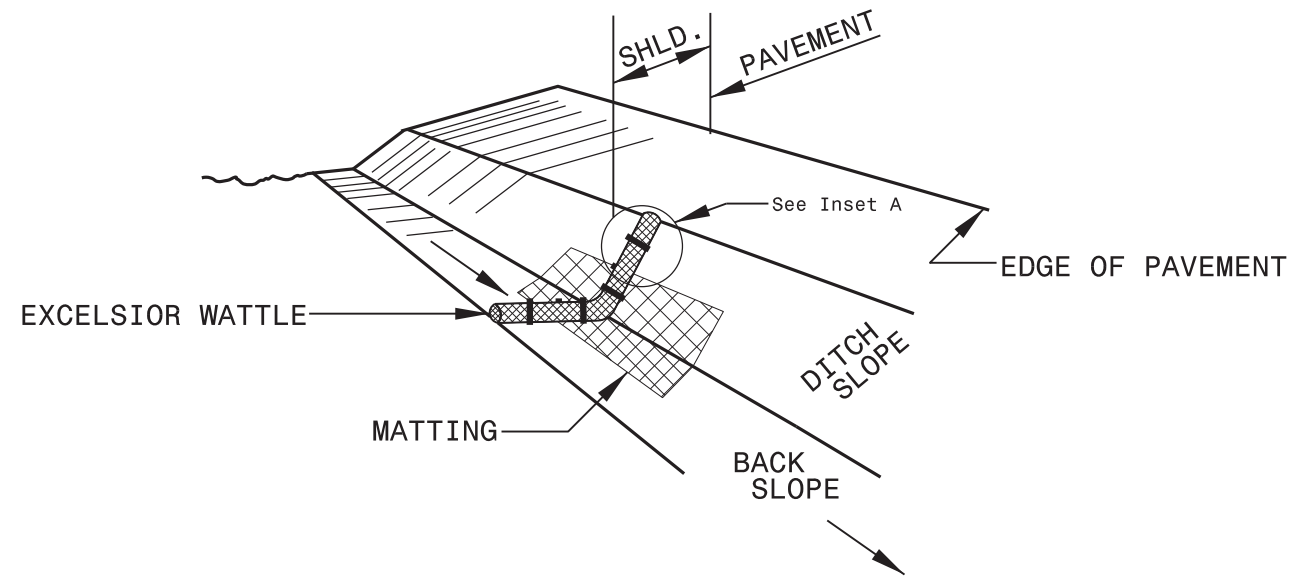


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

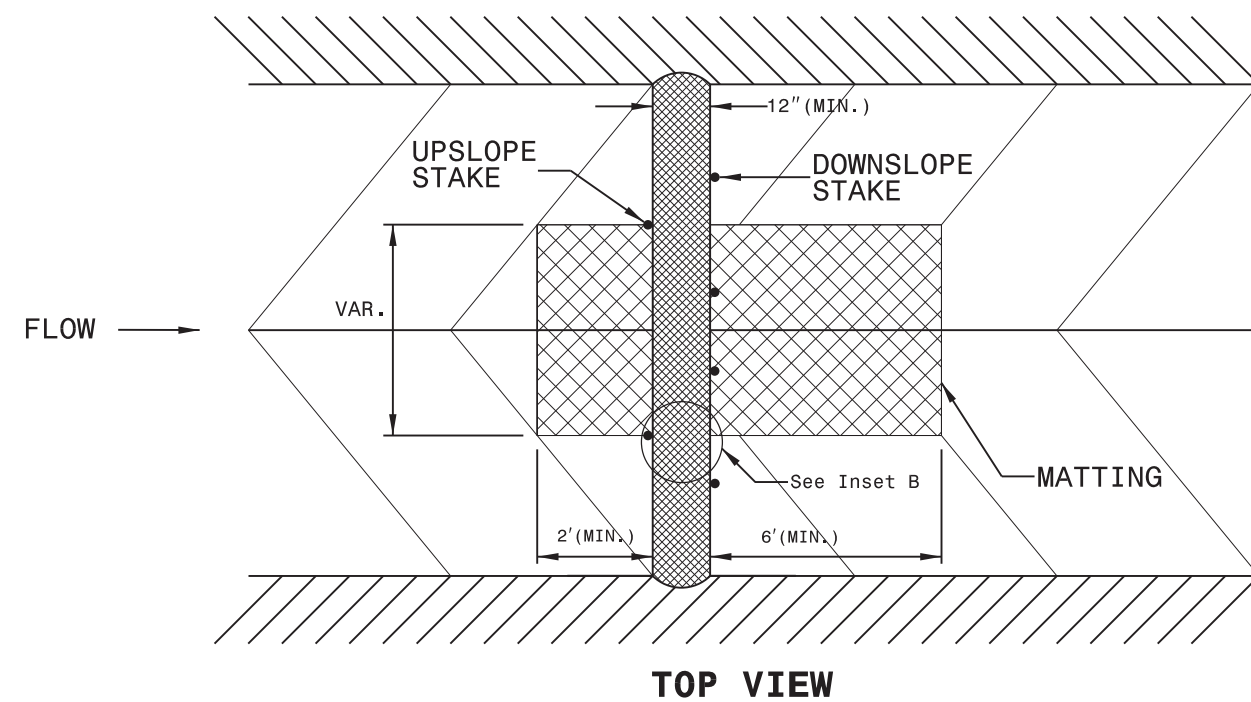
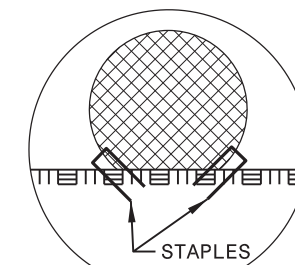
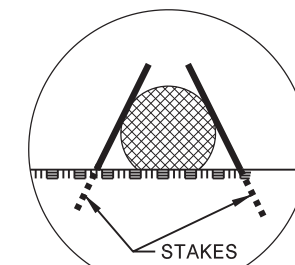
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



PROJECT NO.	SHEET NO.
2023CPT.03.06.10821, Etc.	6

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0264000000-E	1220000000-E	1308000000-E	1330000000-E	1519000000-E		1520000000-E	1575000000-E	1705000000-E	1705500000-E	1775500000-E		
												SHOULDER RECONSTRUCTION (STATE FURNISHED BORROW)	INCIDENTAL STONE BASE	0" - 1.5" MILLING (GENERIC)	INCIDENTAL MILLING	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5B SHORT OVERLAY	LEVELING COURSE, S9.5B	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT (FULL DEPTH)	PATCHING EXISTING PAVEMENT (MILL)	ASPHALT SURFACE TREATMENT, MATCOAT, #67 GRANITE		
											MI	FT	SMI	TON	SY	SY	TON	TON	TON	TON	TON	TON	SY	
2023CPT.03.06.10821	Sampson	1	NC 24 (MARTIN LUTHER KING JR. BLVD/SOUTHWEST BLVD.)	US 421 (SOUTHWEST BLVD.) TO US 701 BUS.(SOUTHEAST BLVD.) [MP 20.58 - MP 21.69]	1	2	2WU	NO	NO	1.11	38	2.22	20			2,320	2,479			168		40		
TOTAL FOR MAP NO. 1											1.11		2.22	20			2,320	2,479			168		40	
2023CPT.03.06.10821	Sampson	2	NC 24 (SOUTHWEST BLVD.) EXIT RAMP	FROM .13 MI. S OF SR 1227 (TRAM RD./BUTLER AVE.) TO US 421 (SOUTHWEST BLVD.) [MP 10.35 - MP 10.60]	1	2	2WU	NO	NO	0.25	18	0.50				138	230			16		10		
TOTAL FOR MAP NO. 2											0.25		0.50				138	230			16		10	
TOTAL FOR PROJ NO. 2023CPT.03.06.10821											1.36		2.72	20			2,458	2,709			184		50	
																	2,709							
2023CPT.03.06.20821	Sampson	3	SR 1740 (WEST DARDEN ROAD)	FROM SR 1742 (N. MCCULLEN RD.) TO SR 1722 (SUTTONTOWN RD.) [MP 8.18 - MP 12.53]	2	2	2WU	NO	NO	4.35	21	8.70	130			357	4,448	180	50	311	40	53,592		
TOTAL FOR MAP NO. 3											4.35		8.70	130			357	4,448	180	50	311	40	53,592	
2023CPT.03.06.20821	Sampson	4	SR 1262 (AIRPORT ROAD)	FROM NC 24 (SUNSET AVE.) TO SR 1228 (W. MAIN ST.) [MP 0.04 - MP 1.32]	1	2	2WU	NO	NO	1.28	21	2.56	16	330	141	1,309				100		248		
TOTAL FOR MAP NO. 4											1.28		2.56	16	330	141	1,309				100		248	
2023CPT.03.06.20821	Sampson	5	SR 1735 (BURCH ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1725 (GIDDENSVILLE RD.) [MP 0.02 - MP 0.94]	3	2	2WU	NO	NO	0.92	21	1.84	86			117	1,564			107	50			
TOTAL FOR MAP NO. 5											0.92		1.84	86			117	1,564			107	50		
2023CPT.03.06.20821	Sampson	6	SR 1901 (CARROLL A. BRITT ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1900 (BREWER RD.) [MP 0.01 - MP 0.73]	2	2	2WU	NO	NO	0.72	20	1.44	20			117	701	250	68	80		8,448		
TOTAL FOR MAP NO. 6											0.72		1.44	20			117	701	250	68	80		8,448	
2023CPT.03.06.20821	Sampson	7	SR 1734 (SPENCER ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1725 (GIDDENSVILLE RD.) [MP 0.02 - MP 1.40]	2	2	2WU	NO	NO	1.38	21	2.76	30			117	1,411	20	97		50	17,002		
TOTAL FOR MAP NO. 7											1.38		2.76	30			117	1,411	20	97		50	17,002	
2023CPT.03.06.20821	Sampson	8	SR 1947 (BOBBY HOPE ROAD)	FROM NC 903 TO DUPLIN CO. LINE [MP 0.02 - MP 1.77]	2	2	2WU	NO	NO	1.75	20	3.50	34			117	1,134	61	82	38		20,533		
TOTAL FOR MAP NO. 8											1.75		3.50	34			117	1,134	61	82	38		20,533	
TOTAL FOR PROJ NO. 2023CPT.03.06.20821											10.4		20.80	316	330		966	10,567	241	320	765	208	298	99,575
																	10,808							
GRAND TOTAL											11.76		23.52	336	330		3,424	13,276	241	320	949	208	348	99,575
																	13,517							

PROJECT NO.	SHEET NO.
2023CPT.03.06.10821, Etc.	8

SUMMARY OF QUANTITIES

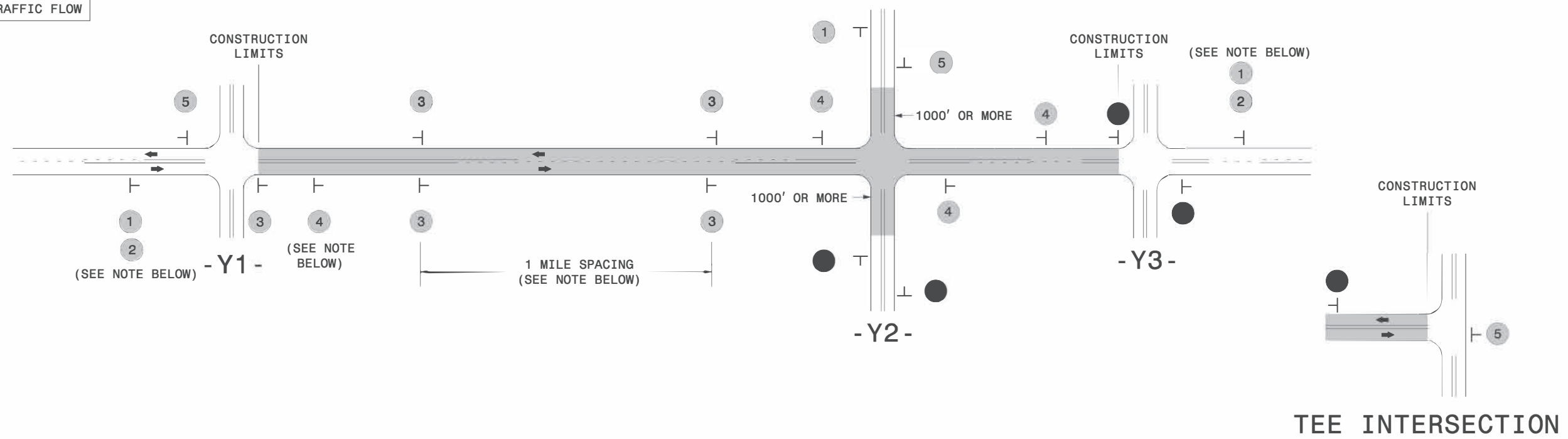
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	6021000000-E	6036000000-E	6042000000-E	6071010000-E	6084000000-E	6090000000-E	6093000000-E	6117000000-N	7444000000-E	
												FERTILIZER FOR TEMPORARY SEEDING	MATTING FOR EROSION CONTROL	1/4" HARDWARE CLOTH	WATTLE	SEED & MULCHING	SEED FOR REPAIR SEEDING	FERTILIZER FOR REPAIR SEEDING	RESPONSE FOR EROSION CONTROL	INDUCTIVE LOOP SAWCUT	
											MI	FT	TON	SY	LF	LF	AC	LB	TON	EA	LF
2023CPT.03.06.10821	Sampson	1	NC 24 (MARTIN LUTHER KING JR. BLVD/SOUTHWEST BLVD.)	US 421 (SOUTHWEST BLVD.) TO US 701 BUS.(SOUTHEAST BLVD.) [MP 20.58 - MP 21.69]	1	2	2WU	NO	NO	1.11	38	0.28	10	56	20	1.08	56	0.28	4	1,020	
TOTAL FOR MAP NO. 1											1.11		0.28	10	56	20	1.08	56	0.28	4	1,020
2023CPT.03.06.10821	Sampson	2	NC 24 (SOUTHWEST BLVD.) EXIT RAMP	FROM .13 MI. S OF SR 1227 (TRAM RD./BUTLER AVE.) TO US 421 (SOUTHWEST BLVD.) [MP 10.35 - MP 10.60]	1	2	2WU	NO	NO	0.25	18	0.06	20	20	10	0.24	13	0.06	2		
TOTAL FOR MAP NO. 2											0.25		0.06	20	20	10	0.24	13	0.06	2	
TOTAL FOR PROJ NO. 2023CPT.03.06.10821											1.36		0.34	30	76	30	1.32	69	0.34	6	1,020
2023CPT.03.06.20821	Sampson	3	SR 1740 (WEST DARDEN ROAD)	FROM SR 1742 (N. MCCULLEN RD.) TO SR 1722 (SUTTONTOWN RD.) [MP 8.18 - MP 12.53]	2	2	2WU	NO	NO	4.35	21	1.09	50	218	70	4.22	218	1.09	16		
TOTAL FOR MAP NO. 3											4.35		1.09	50	218	70	4.22	218	1.09	16	
2023CPT.03.06.20821	Sampson	4	SR 1262 (AIRPORT ROAD)	FROM NC 24 (SUNSET AVE.) TO SR 1228 (W. MAIN ST.) [MP 0.04 - MP 1.32]	1	2	2WU	NO	NO	1.28	21	0.32	20	70	20	1.24	64	0.32	4		
TOTAL FOR MAP NO. 4											1.28		0.32	20	70	20	1.24	64	0.32	4	
2023CPT.03.06.20821	Sampson	5	SR 1735 (BURCH ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1725 (GIDDENSVILLE RD.) [MP 0.02 - MP 0.94]	3	2	2WU	NO	NO	0.92	21	0.23	10	50	20	0.89	46	0.23	4		
TOTAL FOR MAP NO. 5											0.92		0.23	10	50	20	0.89	46	0.23	4	
2023CPT.03.06.20821	Sampson	6	SR 1901 (CARROLL A. BRITT ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1900 (BREWER RD.) [MP 0.01 - MP 0.73]	2	2	2WU	NO	NO	0.72	20	0.18	10	40	20	0.70	36	0.18			
TOTAL FOR MAP NO. 6											0.72		0.18	10	40	20	0.70	36	0.18		
2023CPT.03.06.20821	Sampson	7	SR 1734 (SPENCER ROAD)	FROM NC 403 (FAISON HWY.) TO SR 1725 (GIDDENSVILLE RD.) [MP 0.02 - MP 1.40]	2	2	2WU	NO	NO	1.38	21	0.35	20	70	30	1.34	70	0.35	4		
TOTAL FOR MAP NO. 7											1.38		0.35	20	70	30	1.34	70	0.35	4	
2023CPT.03.06.20821	Sampson	8	SR 1947 (BOBBY HOPE ROAD)	FROM NC 903 TO DUPLIN CO. LINE [MP 0.02 - MP 1.77]	2	2	2WU	NO	NO	1.75	20	0.44	20	100	30	1.70	88	0.44	4		
TOTAL FOR MAP NO. 8											1.75		0.44	20	100	30	1.70	88	0.44	4	
TOTAL FOR PROJ NO. 2023CPT.03.06.20821											10.4		2.61	130	548	190	10.09	522	2.61	32	
GRAND TOTAL											11.76		2.95	160	624	220	11.41	591	2.95	38	1,020

SIGNING FOR RESURFACING PROJECTS

LEGEND

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	● 2	 W20-1 48" X 48" W7-3aP 24" X 18"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. #2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS: 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK. W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER.
	● 3	 SP 13107 48" X 48"	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	● 4	 SP 13106 48" X 48"	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	● 5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	
	THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.			

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



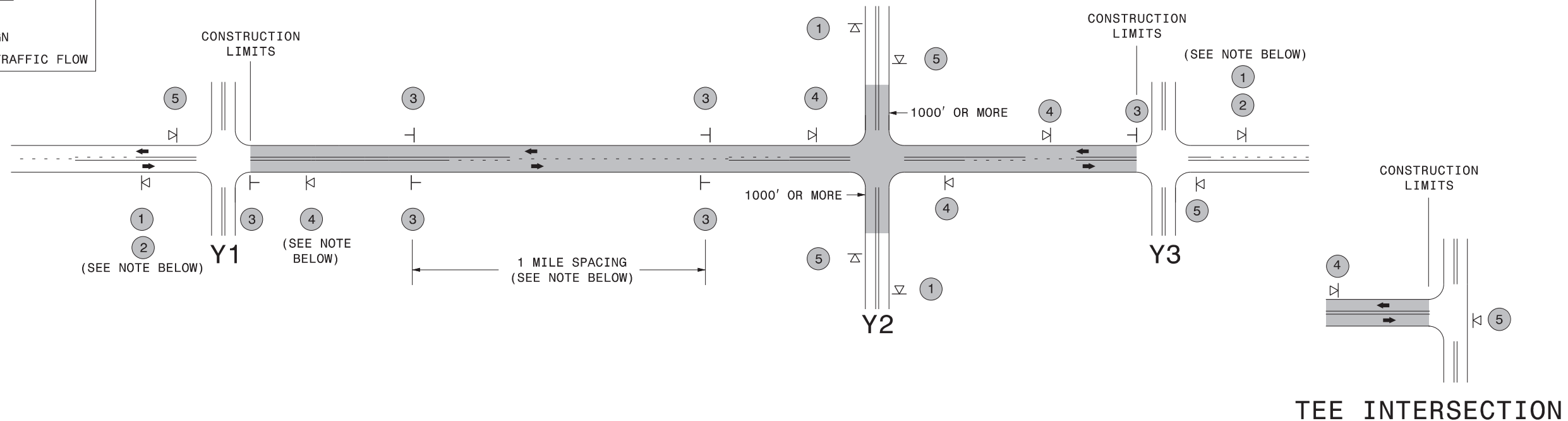
ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

5/15/2017 S:\TLM\WZTC\Resurfacing\2L2W & AST Resurfacing Details\Resurfacing_AdvWarn_2L.n.dgn User:kedals

SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND

- ◻ PORTABLE SIGN
- └ STATIONARY SIGN
- ← DIRECTION OF TRAFFIC FLOW



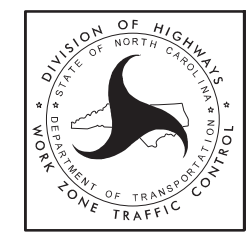
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	 	<ul style="list-style-type: none"> - PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. - SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS. 	<p>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	3	 	<ul style="list-style-type: none"> - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. 	
	4		<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. 	
	5		<ul style="list-style-type: none"> - PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION. 	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>			

MAPS LESS THAN 2 MILES

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.



ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT

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