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Acknowledgements

The participants in the Bryson City Pedestrian Plan wish to thank Town Council and the citizens of Bryson City for making this Plan a reality. We would also like to thank the Partnership for the Future of Bryson City and the North Carolina Department of Transportation Bicycle and Pedestrian Transportation Division.

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Executive Summary

The Bryson City Comprehensive Pedestrian Plan is intended as a guiding document to make Bryson City a more pedestrian-friendly location. Partially funded by a grant from NCDOT and matching funds from the Town of Bryson City and the Partnership for the Future of Bryson City, the Pedestrian Plan serves several purposes, including providing guidance for future pedestrian-related projects and improvements in the City. The benefits of this plan are as varied as the population it serves, including improved air quality, a healthier and more physically active population, reduced traffic congestion, and improved pedestrian safety for children and the elderly. All of these benefits amount to an overall improvement in quality of life which can make a city very attractive to newcomers and visitors, thus boosting the town's economy and vitality.

The Bryson City Pedestrian Plan was begun in October 2006 and completed in Fall 2007. During that time, the Town hired a consultant (the Louis Berger Group) to assist with plan preparation and public involvement. The Town also conducted a public involvement process which included establishing a stakeholder committee and holding two public meetings.

Major focus areas for the Plan are the Downtown Bryson City area, the Super Saver Commercial area, Veteran's Boulevard, Swain County Middle School, the intersection of Hughes Branch Road and US19, and the Deep Creek Recreation Center. Due to safety and accessibility needs, the Town's top priorities should be:

- ◆ Improving pedestrian safety for crossing Veteran's Boulevard,
- ◆ Providing safe pedestrian crossings in the Swain County Middle School area extending to the nearby residential neighborhoods and to the intersection of Veteran's Boulevard and School Drive,
- ◆ Creating pedestrian access and crossings from Downtown to the intersection of US 19 and Hughes Branch Road

The plan provides recommendations for each of these focus areas and organizes them into an implementation plan which includes short-term (1 – 5 years in the future) to long-term (more than 10 years in the future) actions, and cover a range of costs. A summary of the recommendations are provided in the following tables. As part of an implementation program, the Plan also provides general cost estimates for each project, and potential partners that the Town could pursue working with to begin its projects and programs.

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Table ES - 1. Short-Term Actions (0 - 5 years)

Projects			
Reference Number	Description	Primary Responsible Party	Potential Additional Partners
1	Install striping and pedestrian signals for pedestrian crossings at the intersection of Everett and Main Streets	NCDOT/Town Manager and Public Works	Partnership**
2	Install striping for pedestrian crossings at the intersection of Academy and Rector Streets	NCDOT/Town Manager and Public Works	Partnership
7	Construct sidewalk from downtown to Super Saver Shopping Center and commercial area	NCDOT/Town Manager and Public Works	Partnership
8	Provide pedestrian signals at intersection of Veteran's Boulevard and Slope Street	NCDOT/Town Manager and Public Works	Partnership
9	Provide pedestrian signals at intersection of Veteran's Boulevard and Main Street	NCDOT/Town Manager and Public Works	Partnership
16	Sidewalk along US 19 to Hughes Branch Road from Downtown*	NCDOT/Town Manager and Public Works	Partnership
17	Install pedestrian signals at the intersection of Hughes Branch Road and US 19*	NCDOT/Town Manager and Public Works	Partnership
<i>*Note: NCDOT TIP Project No. R-4751 is planned for US 19 from the intersection of US 19 and Hughes Branch Road to US 441 in Cherokee.</i>			
<i>"Partnership": Partnership for the Future of Bryson City and Swain County</i>			

Table ES - 1 continued. *Short Term Actions (0 – 5 years).*

Programs		
Description	Primary Responsible Party	Potential Additional Partners
Walkabout/Historic Walking Tour	Town Manager and Visitor's Bureau/Chamber of Commerce	Partnership
Safe Routes to School Program	Town Manager	SCSS
Spot Improvement and Maintenance Programs	Town Manager/Public Works	NCDOT
Policies		
Description	Primary Responsible Party	Potential Partners
Require new development to construct sidewalk and connect to existing sidewalk.	Town Manager/Planning Board	Developers
Require all new signals to include pedestrian signal heads and crosswalks.	Town Manager/Planning Board/NCDOT/Public Works	Developers
Coordinate with Land Use Plan and future ordinances: As part of the policy recommendation to encourage mixed-use and pedestrian-friendly development, ensure that the recommendations of the Pedestrian Plan are incorporated into the Land Use Plan and other future recommendations.	Town Manager/Planning Board	Land Use committee and Planning Board
Assure safe pedestrian access through parking lots: as part of the policy recommendation to assure safe pedestrian access through parking lots, the Town should begin immediately to review future development plans to evaluate their pedestrian-friendliness and encourage more pedestrian-friendly design.	Town Manager/Planning Board	Developers
<i>"Partnership": Partnership for the Future of Bryson City and Swain County</i> <i>SCSS: Swain County School System</i>		

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Table ES - 2. Mid-Term Actions (6 – 10 years).

Projects			
Reference Number	Description	Primary Responsible Party	Potential Partners
3	Construct pedestrian connection from Island Park to the Train Depot with sidewalk along Ramseur Street and Bryson Street	Town Manager and Public Works	Partnership Visitor's Bureau Chamber SC Rec. Depart.
10	Provide pedestrian warning signage along Veteran's Boulevard	NCDOT /Public Works	Town Manager
13	Provide signed and marked pedestrian crossings from sidewalk on School Drive for access to Swain County Middle School	Town Manger and Public Works	Partnership SCSS SC Rec. Depart.
18	Construct sidewalk connection to Deep Creek Recreation Center to Downtown	NCDOT/Public Works/Town Manager	Partnership NCDOT SC Rec. Depart.
Programs			
Description		Primary Responsible Party	Potential Partners
Crosswalk/Traffic Safety Stings		Police Dept.	Town Manager
Promotional/Educational Materials		Town Manger	Chamber Visitor's Bureau Partnership SC Rec. Depart.
Policies			
Description		Primary Responsible Party	Potential Partners
Consider requiring new development to set aside land for greenways		Planning Board/Town Manager	Developers
<i>"Partnership": Partnership for the Future of Bryson City and Swain County</i> <i>"Chamber": Chamber of Commerce</i> <i>"SC Rec. Depart.": Swain County Recreation Department</i>			

Table ES - 3. Long-term Actions (10 or more years).

Projects			
Reference Number	Description	Primary Responsible Party	Potential Partners
4	Sidewalk along Academy Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
5	Sidewalk along Island Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
6	Sidewalk along Fryemont Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
11	Consider signalized mid-block crossing along Veteran's Boulevard	NCDOT/Public Works	
12	Construct sidewalk on the northeast side of Veteran's Boulevard	NCDOT/Public Works	
14	Widen existing sidewalk along School Drive to Swain County Middle School	Public Works	NCDOT SCSS
15	Construct sidewalk from Downtown Bryson City to Swain County High School along Fontana Drive.	NCDOT/Public Works	SCSS
Programs			
Description		Potential Partners	
None.			
Policies			
Description		Potential Partners	
None.			
"Chamber": Chamber of Commerce "Partnership": Partnership for the future of Bryson City and Swain County "SCSS": Swain County School System			

Bryson City Pedestrian Plan

Executive Summary

Bryson City has some of the elements necessary for a more walkable community. With careful planning, deliberate steps, and persistence, Bryson City can become a more pedestrian-friendly community. The Town's next steps should begin to immediately address the short-term priority program, policy, and project recommendations. At the same time, the Town should also start to lay the groundwork for the longer term recommendations by mentioning them to potential partners and starting already to budget for projects in the future. Most importantly, the Town should continue its efforts to raise awareness about the importance of making a community more walkable in order to continue to cultivate support for more pedestrian improvements and programs. Residents, visitors, and local leaders should be familiar with the economic, health, and environmental benefits of a community in which there is less dependence on autos and more reliance on foot travel as not only a form of recreation, but also as a form of transportation.

The Bryson City Comprehensive Pedestrian Plan is a tool to create a more pedestrian-friendly town through recommended programs, policies, and plans. The benefits of this plan include improved air quality, a healthier and more physically active population, reduced traffic congestion, and improved pedestrian safety for children and the elderly.

Section 1. Introduction, Goals, and Objectives

1.1. Introduction

The Bryson City Comprehensive Pedestrian Plan is intended as a guiding document to make Bryson City a more pedestrian-friendly location. Partially funded by a grant from NCDOT and matching funds from the Town of Bryson City and the Partnership for the Future of Bryson City, the Pedestrian Plan serves several purposes, including providing guidance for future pedestrian-related projects and improvements in the City. But the plan is more than a project list, it is also a tool to create a more pedestrian-friendly atmosphere through recommended programs, policies, and plans. The results of the Plan will be safe, accessible facilities and programs and policies that encourage residents and visitors alike to walk, rather than drive, around town.

This plan is intended to address the needs of Bryson City's varied population, including those of current and future residents, visitors, and tourists. The benefits of this plan are as varied as the population it serves, including improved air quality, a healthier and more physically active population, reduced traffic congestion, and improved pedestrian safety for children and the elderly. All of these benefits amount to an overall improvement in quality of life which can make a city very attractive to newcomers and visitors, thus boosting the town's economy and vitality.

The following chapters of the Plan provide recommendations for projects, programs, and policies that will help to improve the pedestrian conditions in Bryson City and encourage walking. The Plan also provides design guidelines that are tailored to the specific needs of Bryson City. Finally, the Plan presents a list of priorities and a recommended schedule, as well as cost estimates and potential funding sources, to assist with implementation of the Plan's recommendations.

1.2. Plan Process

The Bryson City Pedestrian Plan was begun in October 2006 and completed in Fall 2007. During that time, the Town hired a consultant (the Louis Berger Group) to assist with plan preparation and public involvement. The Town also conducted a public involvement process which included establishing a stakeholder committee and holding two public meetings.

Bryson City Pedestrian Plan

Section 1: Introduction, Goals, and Objectives

The members of the Stakeholder Committee were:

Chester Bartlett	Wally Treadway
Troy Burns	Brad Walker
Larry Callicutt	Scot Warf
Diana B. Clampitt	Kate Welch
Freda Livingston	Loomis White
Pat Montieth	Robert White

There were a total of four stakeholder committee meetings. The Stakeholder Committee served as a standing committee to provide direct input on the plan. Members of the Stakeholder Committee were selected to represent a variety of interests, including Bryson City's schools, the development and hospitality community, and planning board.

In their first meeting, the Stakeholder Committee identified their Goals and Objectives for the Plan. These goals and objectives served as guidance for the Plan and assisted with making decisions about priorities and emphasis in the Plan. **Overall, the main goal for the pedestrian plan is to, in the long term, have sidewalk on all streets in the town.** This goal should be achieved by implementing projects, programs, and policies that construct sidewalk in town starting from work in the downtown and spreading out into the whole of Bryson City. Most of the additional goals identified by the stakeholder committee focused on projects the Town would like to be constructed. The following goals are those identified by the Stakeholder Committee:

- ◆ Make pedestrian improvements along School House Hill (School Drive).
- ◆ Identify methods to make Everett Street more pedestrian-friendly and reduce congestion.
- ◆ Coordinate with future changes to the Train Depot area, including converting Fry Street to a pedestrian mall and providing pedestrian improvements to Bryson Walk which would connect to the proposed railroad turntable.
- ◆ Provide pedestrian access to Ingles area and Hughes Branch Road/HWY 19 intersection.
- ◆ Examine the potential for a riverwalk along the Tuckaseegee River from Island Park to the Administration Building.
- ◆ Address parking/congestion issues in town.
- ◆ Identify potential pedestrian improvements to Main Street including more connected sidewalk, better lighting, and better crossings.

Overall, the main goal for the pedestrian plan is to, in the long term, have sidewalk on all streets in the town. This goal should be achieved by implementing projects, programs, and policies that construct sidewalk in town starting from work in the downtown and spreading out into the whole of Bryson City.

- ◆ Determine improvements which would draw pedestrians to other parts of Town beyond Everett Street and the Train Depot area.
- ◆ Maintain and build upon the improvements and successes that have been accomplished in the Town over the past few years.
- ◆ Establish a walking tour, potentially of historic areas in Bryson City.
- ◆ Recommend sidewalk to the High School from Downtown
- ◆ Recommend improvements to draw more people to Downtown, including constructing a park for children, promoting mixed uses of residential and commercial development, and providing incentives for businesses to remain open after 5 PM to create a more vibrant, 24-hour downtown area.

In general, the committee agreed that safety – especially to schools – and access to destinations are a top priority for the Plan, while aesthetic improvements and other additions, such as better lighting, are also important.



Figure 1-1. Images from Bryson City. Top Left: Bryson City Town Hall. **Top Right:** Everett Street during the Folkmoot USA Festival.

This section of the plan provides:

- ◆ **A Demographic Analysis** of the Town's population
- ◆ A discussion of **community concerns**
- ◆ A summary of the **existing pedestrian facilities** and major pedestrian **destinations** in the town.

Section 2. Evaluating Current Conditions

2.1. Introduction

Located in the Appalachian Mountains near to the North Carolina-Tennessee border, Bryson City is the county seat for Swain County in North Carolina (see Figure 2-4). The Town was originally named Charleston, but in 1889 its name was changed to Bryson City in honor of Colonel Thaddeus Dillard Brysonⁱ. For many years, the Town served as a central shopping and business destination for nearby small communities. Today, the Town remains a local commercial hub for outlying areas, but has also become a major attraction for visitors to the region. The Town is host to the Great Smoky Mountain Railroad, which departs from its downtown station each day for a variety of themed trips to either Dillsboro, NC located approximately 15 miles east, or Nantahala Gorge to the west. Highlights of the train rides include the Horseshoe Curve, Fontana Lake Trestle, Tennessee River, and the Nantahala River Gorgeⁱⁱ. Bryson City has also become a popular destination for visitors looking to capitalize on the many outdoor activities in the area, including hiking and camping in the nearby Great Smoky Mountains National Park or Nantahala National Forest, rafting or kayaking on the Nantahala and Tuckaseegee River, and boating at Fontana Lake. Other nearby attractions include the City of Asheville, a popular destination for its cultural and fine arts attractions; and Western Carolina University in Cullowhee, NC.

In order to provide full and adequate services to all the residents of Bryson City, the Bryson City Pedestrian Plan must address all of the needs of the people it serves, residents and visitors alike. To this end, it is important to first examine and understand the needs of the people in Bryson City and the Town's existing infrastructure. This section of the Plan provides a summary of a demographic analysis of the Town's population, a discussion of community concerns and needs that were identified as part of the public outreach for the Plan, and information on the existing pedestrian facilities and major pedestrian destinations in the town.

Bryson City Pedestrian Plan

Section 2: Evaluating Current Conditions

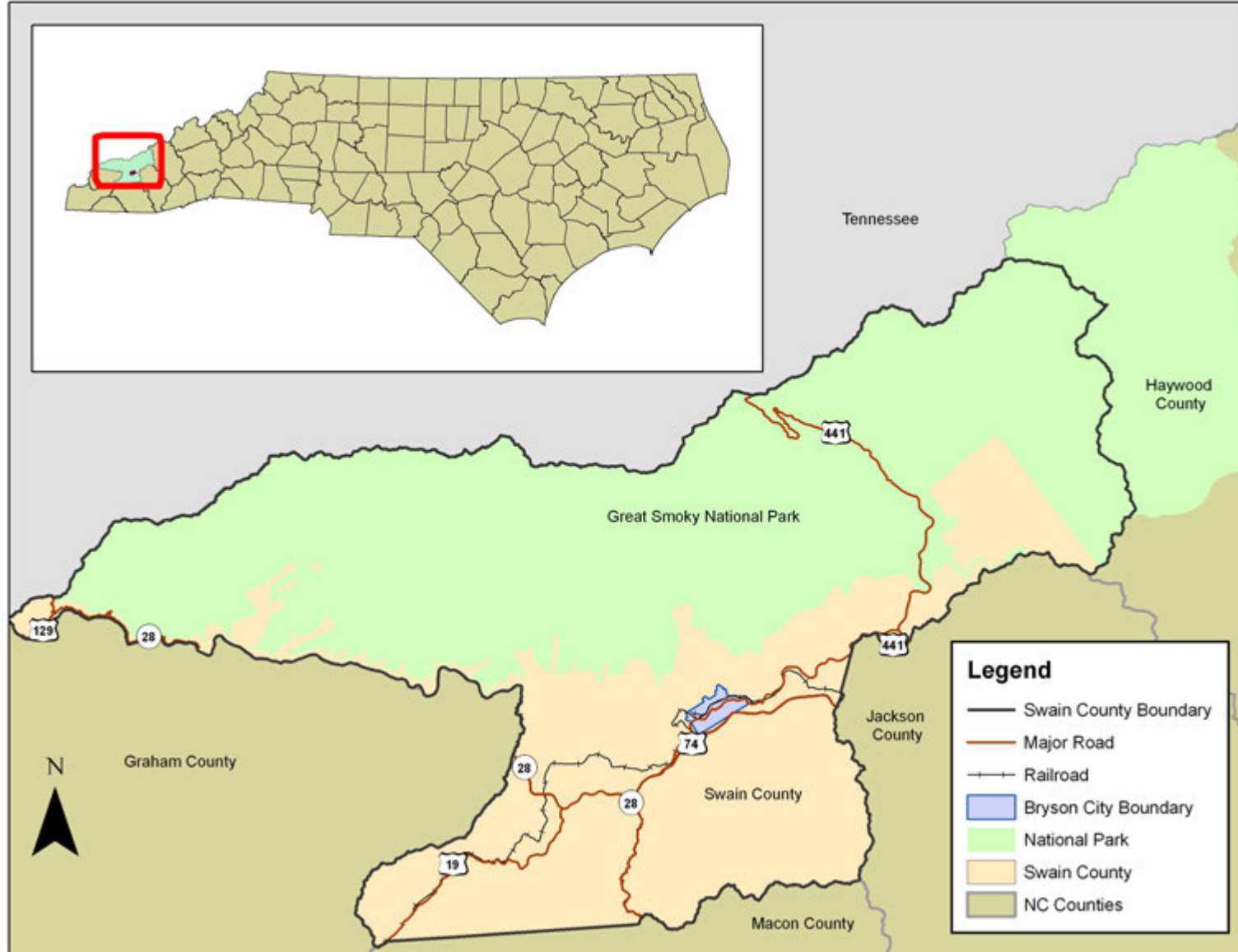


Figure 2-1. Location Map of Bryson City in Western North Carolina

2.2. Demographic Analysis

Appendix 1 contains a complete demographic analysis of the Bryson City population using data from the 2000 US Census. The key points from this analysis highlight the racial, economic, and travel behavior characteristics of Bryson City's population. It is important to identify these characteristics when developing a pedestrian plan in order to understand fully the Town's residents and their potential needs as pedestrians.

The Town of Bryson City has a population of 1,411 as of the 2000 US Census. In general, the Town's population can be described as primarily Caucasian (90 percent) with a small percentage of American Indians (5 percent). This is a different population make-up than Swain County, which is 66 percent Caucasian and 29 percent American Indian. The median household income in Bryson City is \$22,232, nearly half that of the state and nation but consistent with that of the county. In the 2000 US Census, 19 percent of the population in Bryson City lived below the poverty line, while about 18 percent of the population in Swain County lived below the poverty line, and only 12 percent of the state and nation population. Unemployment in Swain County dropped in 2005 to 6.6 percent from 13.0 percent in 2000 and 12.7 percent in 1990 (NC Department of Commerce, <http://data.osbm.state.nc.us>).

The demographic analysis for the Town shows that there are several populations in the Town which have particularly sensitive pedestrian needs: seniors, people with low vehicle availability (often those with low incomes), and a growing population of visitors and tourists. In Bryson City, the percent of the population that are seniors over the age of 60 (30 percent) doubles the percent populations of seniors over the age of 60 in the state and nation. In general, a senior population needs abundant, safe, and well-connected pedestrian facilities to allow them to maintain their mobility even after they have lost their ability to safely operate a car. In the course of the Plan, particular attention will be paid to providing pedestrian access, especially for areas with high populations of seniors in Bryson City, to reach destinations such as grocery stores, churches, and senior centers.

Similar to seniors, lower income populations are particularly sensitive to pedestrian facilities because they are more likely to depend on walking and non-motorized forms of transport for access to work, recreation, and shopping needs. Reflecting the Town's median income and poverty statistics, the Town's 2000 US Census household vehicle availability data shows that over 56 percent of the Town's households had one or no

Bryson City Pedestrian Plan

Section 2: Evaluating Current Conditions

vehicles available, over 10 percentage points of that of the state and nation. In addition, to-work commute characteristics show that over six percent of the working population over the age of 16 walked to work, twice the national average and three times the state average. Because of this, improvements to the pedestrian facilities in the Town should be targeted at work locations and essential shopping destinations, which would greatly benefit those of the lower income population in the Town.

The third population in the Town strongly sensitive to pedestrian improvements are the Town's growing numbers of visitors and tourists. Although it is difficult to estimate the exact number of visitors each year, a good indicator is the ridership for the Great Smoky Mountain Railroad, which was 204,302 people in 2006. Unlike permanent residents, tourists and visitors to a place often have a more relaxed attitude about travel, and therefore are more likely to walk rather than drive to destinations. Any future facilities that the Plan recommends should provide and improve existing pedestrian access to popular tourist/visitor destinations such as the downtown, shopping, the Great Smoky Mountain Railroad train station, hotels and motels, and popular parks or recreational areas.

2.3. Community Concerns and Needs

Through discussions with stakeholders and town staff, several major community concerns and needs were identified that the Pedestrian Plan should address. Several of these concerns mirror the potential needs identified from the demographic analysis.

First, many of Bryson City's seniors live within walking distance of the core downtown area. Comments from the public indicate that many senior citizens would like to be able to walk to a grocery store or to downtown safely. Currently, many seniors live in the area along Bryson Walk and Bryson Avenue. From these locations, the nearest grocery stores are either the Super Saver located at the intersection of NC 19 and Slope Street or the Ingles located at the intersection of NC 19 and Hughes Branch Road. The Plan should consider approaches to improving the pedestrian conditions along the routes to these groceries and to providing access to other potential shopping needs.

Second, although less than 14 percent of Bryson City's population are children under the age of 18, a major concern for the Plan is to provide for safe, adequate pedestrian facilities to schools and youth activity centers. The Swain County Middle School is on Arlington Avenue, which is located on what is popularly known as "School House Hill". There is sidewalk leading up the hill from Veteran's Boulevard, but none beyond the school towards Hughes Branch Road. There is a need to provide safe pedestrian access

for children to the school, especially because parents frequently drop their children off at the bottom of School House Hill in order to avoid the traffic congestion of getting to the school. Any future recommendations should address projects to make walking to school safer for children, and to encourage children and their parents to do so.

Third, as a small mountain town, Bryson City would like to continue to capitalize on its potential as a major attraction for outside visitors and tourists. The Pedestrian Plan should integrate with on-going projects related to boosting the local economy, and also provide some policies for future development which will help to both beautify and create a more pedestrian-friendly town. It is often the case that the more pedestrian-friendly a location, the more attractive it is to visitors.



Figure 2-2. Bryson City is a popular place for visitors, as evidenced by the many places to stay along US 19.

2.4. Inventory of existing facilities

In addition to existing sidewalks and greenways, there are a variety of features that are essential considerations in a pedestrian plan. These features include downtown areas, schools, recreation facilities, employment centers, and community facilities like libraries and health centers.

Sidewalk, Crosswalks, and Pedestrian signals

Bryson City has over 3.76 miles of sidewalk in the town and approximately 22 miles of roadway for vehicles. Most sidewalks are located in the downtown area. Sidewalk is usually between 4 and 6 feet wide, and on some streets it also includes a brick inlay. Bryson City has several signalized intersections with crosswalks and pedestrian signals. Everett Street has multiple mid-block pedestrian crossings and all signalized intersections along it include pedestrian signals. Beyond Everett Street, fewer signalized intersections have pedestrian signals associated with them.

Figure 2-4 shows the location of existing sidewalk, crosswalks, and pedestrian signals in the Town of Bryson City.



Figure 2-3. Pedestrian signals, crosswalks, and sidewalk are shown in this picture, taken at the intersection of Everett and Mitchell Streets.

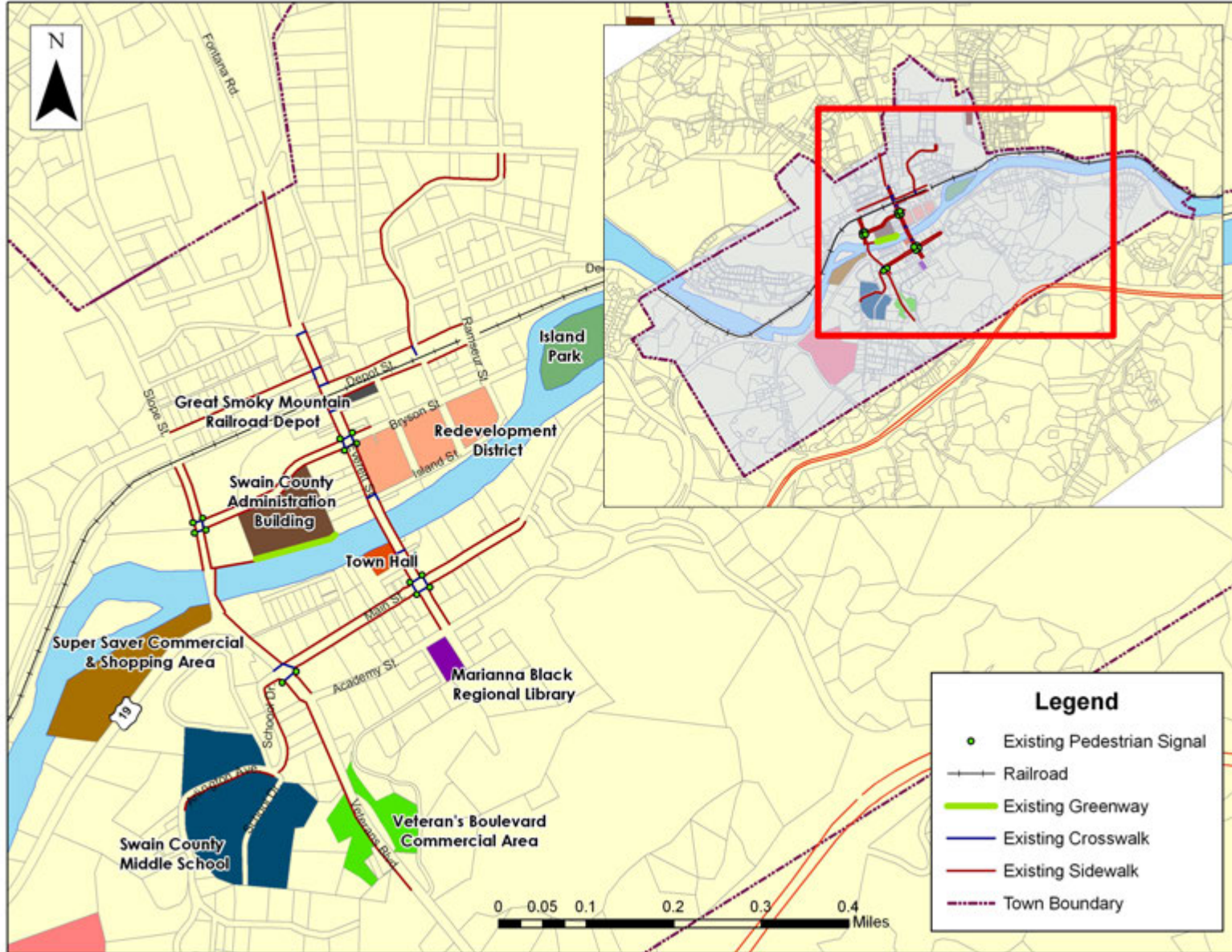


Figure 2-4. A map of Bryson City's existing sidewalk with several major destinations highlighted.

Bryson City Pedestrian Plan

Section 2: Evaluating Current Conditions

Downtown

Bryson City's main pedestrian-oriented area is in its downtown, characterized as the area between Main and Depot Streets and bounded by Ramseur and Slope Street. This area has a variety of restaurants, shops, and attractions for tourists, including the train station for the Great Smoky Mountain Railroad. The Marianna Black Regional Library is located at the corner of Rector Street and Academy Street, within close proximity to several county buildings and the county courthouse. Main Street and Everett Street both serve as the major commercial strips for the area. In general, the downtown has well-developed pedestrian facilities, especially along Everett Street where there are wide sidewalks (between five and ten feet wide) and frequent marked crossings for pedestrians. The Town has recently invested in streetscape renovations, which have been completed on Mitchell and Depot Streets. These renovations include providing "old timey" street lights, decorative pavers in the sidewalks, and crosswalks with brick imprints. When possible, street trees have also been provided. Funding for these streetscape renovations come from NCDOT Enhancement Program Funds. The Town hopes to apply for funding in 2008 for streetscape improvements on Main Street.



Figure 2-5. A view of Downtown Bryson City. Note the crosswalks and wide sidewalks.

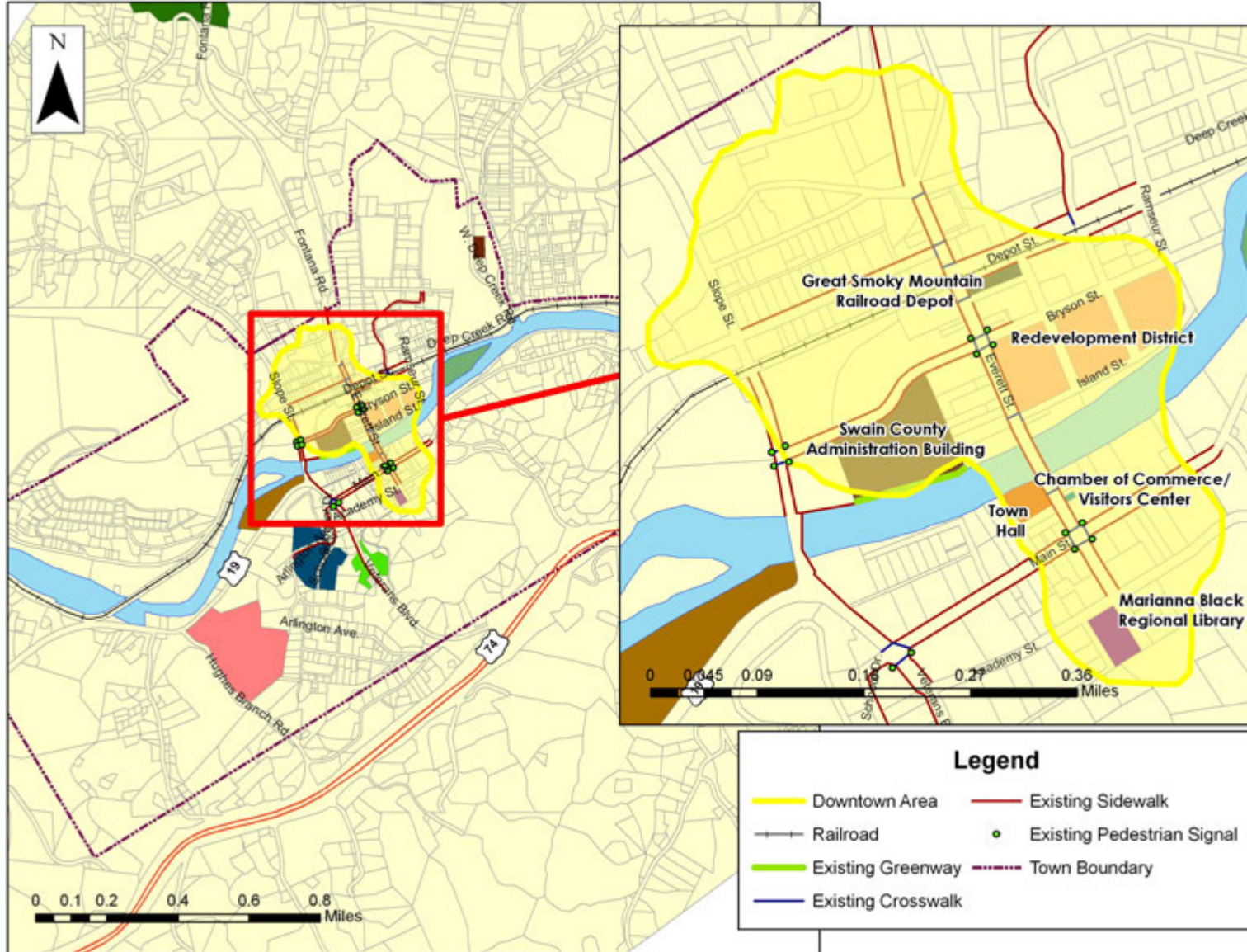


Figure 2-6. Major destinations in Downtown Bryson City.

Bryson City Pedestrian Plan

Section 2: Evaluating Current Conditions

Recreation Facilities

Bryson City has several recreation facilities. It is important to provide pedestrian connections and access to recreation facilities because these locations often attract pedestrians, and also often generate pedestrians. One of Bryson City's main recreational attractions is its Island Park, located on an island in the Tuckasegee River just east of Island Street. This park was accessible at one time via a small footbridge from Island Street onto the island; however, the bridge was destroyed in by flooding due to Hurricane Floyd. In 2007, the Town reconstructed a larger, ADA accessible bridge and plans to redevelop the Island Park as a more permanent and attractive destination for residents and visitors.

At the other end of downtown is a park and recreational area along the edge of the Tuckasegee River adjacent to the Swain County Administration Building. This park features a greenway along the length of the property adjacent to the river, along with a few benches and landscaping. In the future, the Town may consider extending this greenway the length of the river to Island Park to create a "riverwalk". At the time of design for this riverwalk, however, consideration should be made to ensure that the privacy of nearby properties is preserved. The completion of this project could potentially generate local side-businesses, such as a bicycle rental company and snacks/ice cream stores which would cater to people along the boardwalk.

The County also has a recreation facility, the Swain County Recreation Park, within a mile of the downtown area located on Deep Creek Road. The Recreation Park includes a recreational center with swimming pool, playground, tennis courts, baseball and softball fields, a putting green, and batting cages. The playground was developed as the result of a community volunteer effort and built using only donated funds from members of the community.



Figure 2-7. The Swain County Recreation Park along Deep Creek Road.

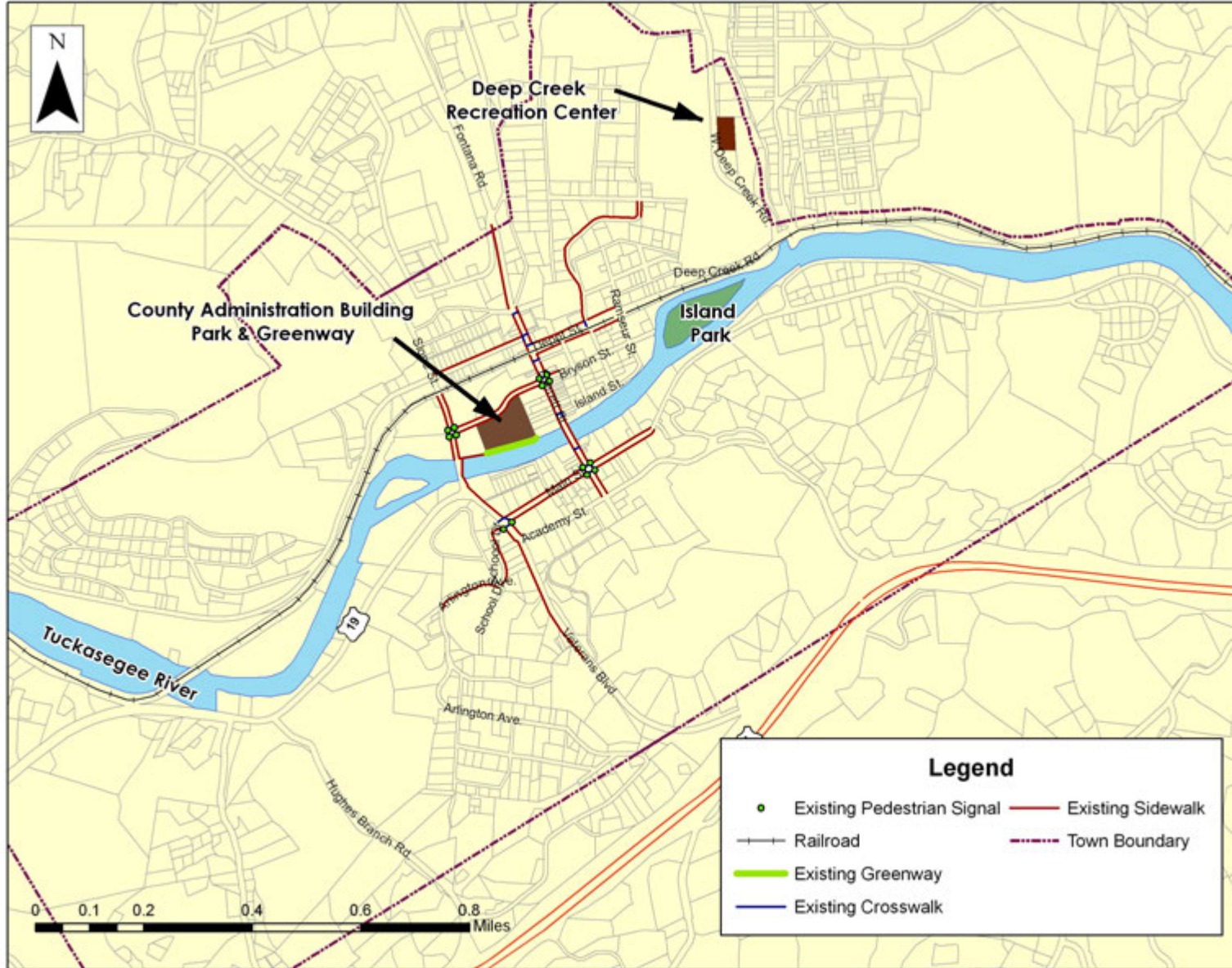


Figure 2-8. Major recreation destinations in Bryson City.

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Section 2: Evaluating Current Conditions

Schools

Bryson City has one school within town limits, the Swain County Middle School, and Swain County High School is just beyond the town limits. The Swain County Middle School hosts children grades 5 through 8, and has an enrollment of approximately 417 students. According to school officials, approximately 75 percent of students ride the bus to school, and the remainder are driven by their parents. As mentioned in the section on community concerns, parents of children at Swain County Middle School often drop-off and pick-up their children at the gas station located at the intersection of School Drive and Main Street in order to avoid drop-off and pick-up traffic. This has some benefits, including reducing traffic and boosting children's physical activity because they must therefore walk up the hill to the school. However, this also creates an issue because children are who dropped off must also sometimes cross Veteran's Boulevard to reach the school or the gas station.

The Swain County High School is located on Fontana Road, about a mile from downtown Bryson City. The school receives children grades 9 through 12, with an average yearly enrollment of approximately 430 students.



Figure 2-9. An early morning view of the entrance to Swain County Middle School.

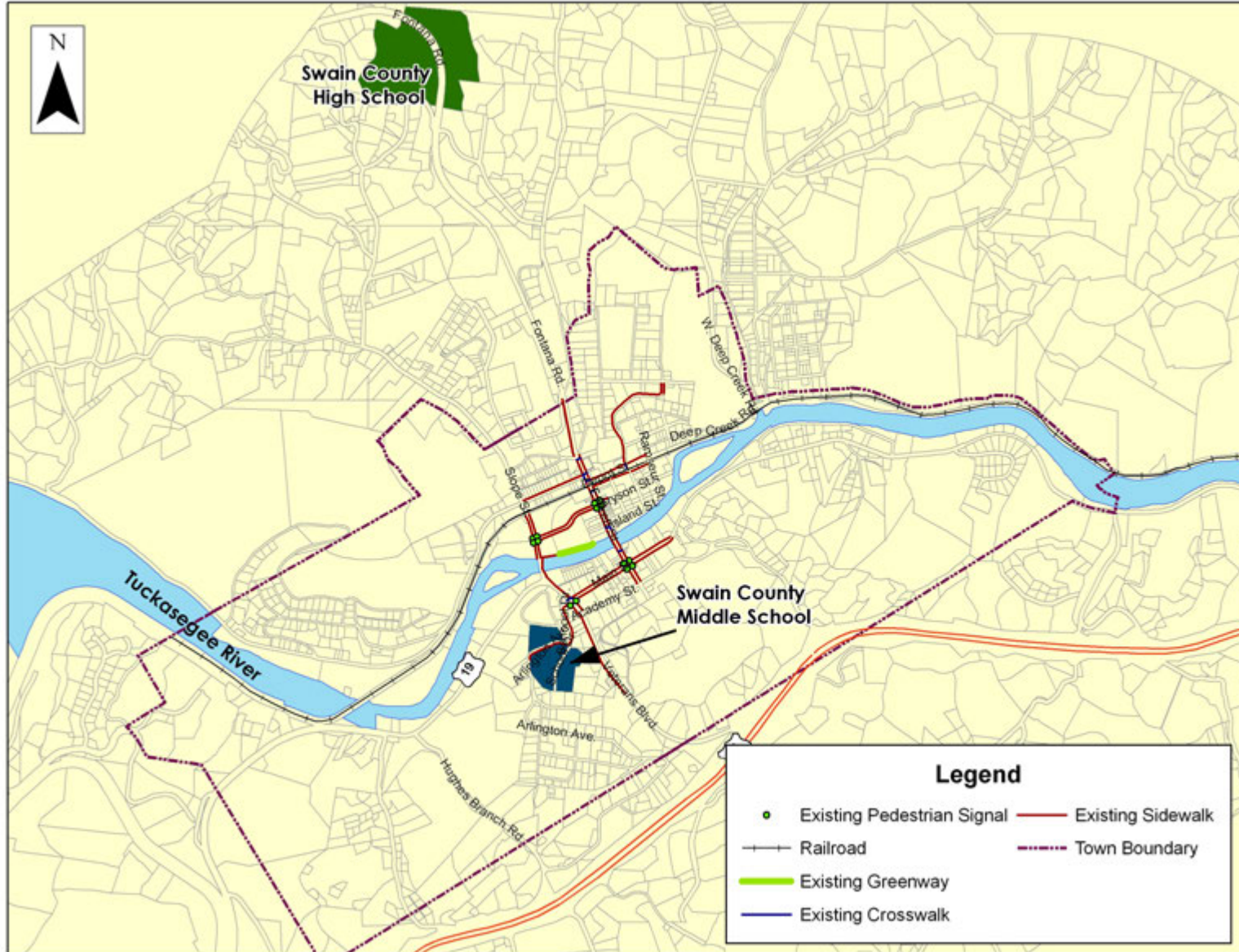


Figure 2-10. Schools in Bryson City.

Bryson City Pedestrian Plan

Section 2: Evaluating Current Conditions

Employment/Commercial Centers

Bryson City has several major commercial/employment centers beyond downtown which are destinations for both employees and also shoppers/patrons. The first commercial/employment center is along Veteran's Boulevard, just as motorists enter the Town from US 74. This area is commonly referred to as "Fast Food Lane" for its many fast food restaurants. The area also has several gas stations and other automobile-related service shops. Another popular commercial location is at the intersection of Slope Street and US 19 where there is the Super Saver shopping center. Even further west of downtown on US 19 is the intersection of US 19 and Hughes Branch Road, where there are several major shopping destinations, including an Ingles (grocery store) and a bank. This destination is less pedestrian accessible due to both its distance from downtown and non-pedestrian-friendly development patterns which are not conducive to walking. Pedestrian improvements should be made around the area to improve pedestrian connectivity within and between each of the shopping destinations.

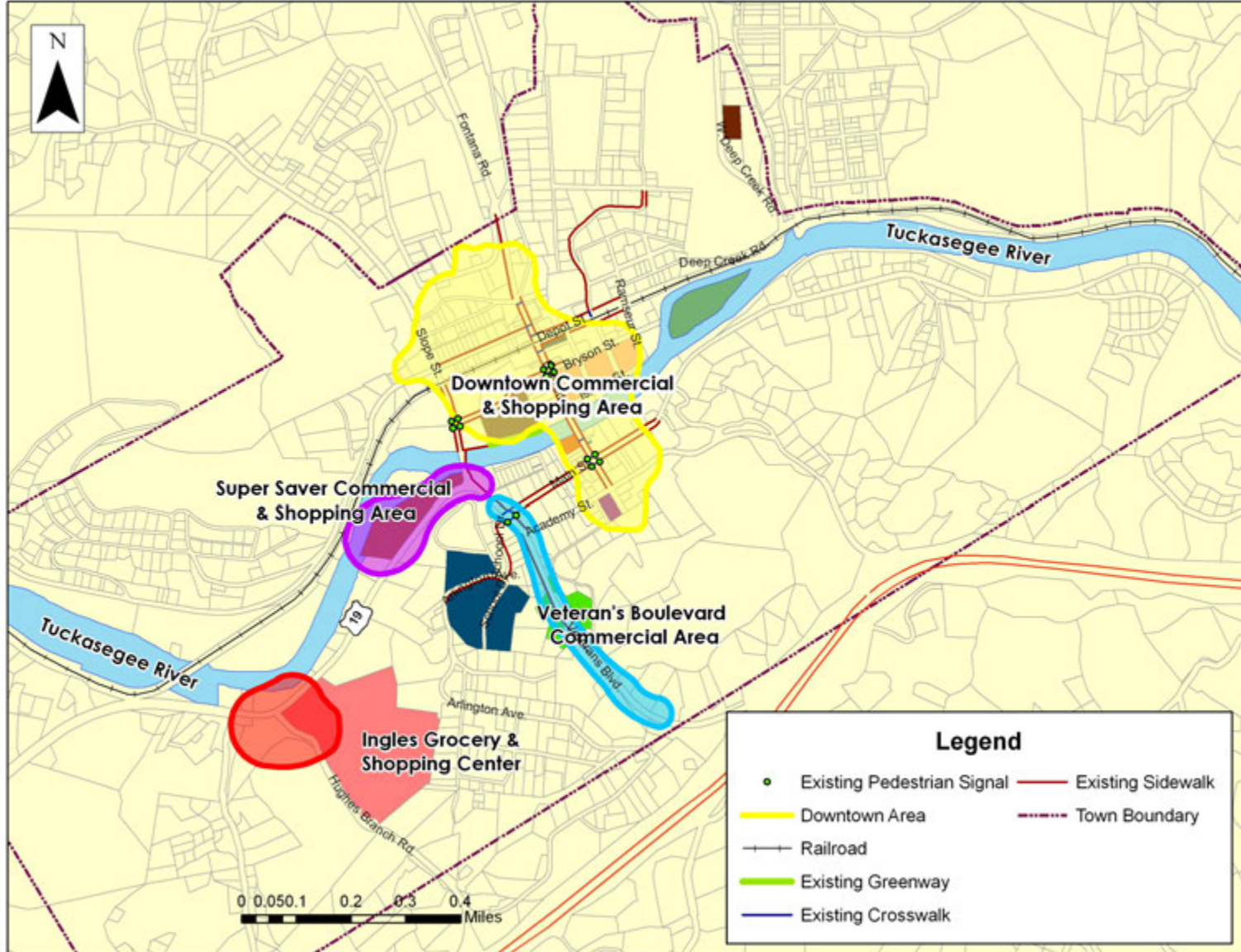


Figure 2-11. Major employment and commercial areas in Bryson City.

2.5. Summary

This section provides a summary of the pedestrian needs in the Town of Bryson City. Based on demographic information, potential uses of the pedestrian system include town residents, especially seniors and low-income populations who have less access to a car, and visitors to the Town. Popular destinations and potential project areas include downtown, near the Swain County Middle School, along Veteran's Boulevard, and along the Tuckaseegee River. Using this information in this section as a basis, the rest of the Pedestrian Plan will provide recommendations for projects, treatments, standards, designs, policies, and programs to help address Bryson City's pedestrian needs.

ⁱ The State Library of North Carolina website. <http://statelibrary.dcr.state.nc.us/>. Accessed 29 November 2006.

ⁱⁱ The Great Smoky Mountain Railroad website. <http://www.gsmr.com/>. Accessed 29 November 2006.

This section discusses existing plan, programs and policies that may effect future pedestrian facilities in Bryson City.



Figure 3-1. NCDOT and the Town have conducted several enhancement projects to install sidewalk in the Downtown area. This picture is of an enhancement project on Mitchell Street.

Section 3. Existing Plans, Programs, and Policies

3.1. Introduction

In addition to the physical infrastructure of Bryson City which was discussed in Section 2, it is also important to understand the existing plans, policies, and programs which may affect pedestrians in the Town. Plans, programs, and policies are just as important as physical infrastructure to creating a pedestrian-friendly town because they create and encourage an atmosphere of acceptance towards acceptance and a perspective of walkability.

3.2. Existing Plans

There are many entities which plan for future improvements in the Town of Bryson City, some of which include: the Town itself, the North Carolina Department of Transportation, the Partnership for the Future of Bryson City/Swain County, and the Economic Development Commission for Swain County. The following paragraphs discuss some of the future plans made by each of these entities' for the Bryson City area.

3.2.1. *The Town of Bryson City and NCDOT*

Much of the transportation-related improvements in the Town of Bryson City are conducted through a joint effort between the Town and the North Carolina Department of Transportation (NCDOT). The Town of Bryson City has recently completed Streetscape Projects funded through NCDOT's Enhancement Funds on both Everett Street and Mitchell Street, and is in the process of conducting a streetscape project on Depot Street. These projects include sidewalk construction, lighting, and pedestrian signal installation at some intersections. The Mitchell Street project is awaiting streetlights.

NCDOT maintains a statewide list of planned projects, the Statewide Transportation Improvements Plan (STIP). In the 2007 STIP, there are two projects which relate to Bryson City and may have an influence on pedestrian facilities in the area:

- ◆ **TIP No. R-4751** – a 9 mile roadway upgrade of US 19 from Hughes Branch Road in Bryson City to US 441 in Cherokee (unfunded).

Bryson City Pedestrian Plan

Section 3: Existing Plans, Policies, and Programs

- ◆ **TIP No. SI-4816 (Hazard Elimination Project)** – installation of a traffic signal on Scope Street and Mitchell Street (construction in progress). This signal includes pedestrian signals and crosswalks. (To be completed by end of 2007)

As will be discussed more thoroughly later in the Plan, the intersection of Hughes Branch Road and US 19 is an important pedestrian crossing area. As such, the design of the roadway at this intersection should give significant consideration for pedestrian needs.

The Town maintains a Capital Improvements Plan, which is mostly focused on water and sewer facility improvements. The Town's most recent Transportation Plan is dated 1993.

Parks and recreation facilities within town limits are under the jurisdiction of the Swain County Parks and Recreation Department; the Town does not have its own Parks and Recreation Department and therefore does not have a Parks and Recreation Plan or Greenways Plan.

In 2007, the Department of Community Assistance began work on preparing the Town's first Land Use Plan, which is expected to be completed in 2008.

3.2.2. *The Partnership for the Future of Bryson City and Swain County*

The Partnership for the Future of Bryson City and Swain County is an umbrella non-profit organization for community projects within Bryson City. The Partnership provides planning, administration and non-profit fiscal services to a variety of public and private community organizations and maintains committees which address issues such as education, health, recreation, business, government and family support. The Partnership supports a variety of projects for Bryson City, however as of August 2007, many of them are in flux due to internal changes to the Partnership. Once this has been settled, the Partnership should resume its role as a major administer of programs in the Town.

3.2.3. *The Economic Development Commission for Swain County*

The Economic Development Commission for Swain County is a county-wide organization which plans for and administers projects and programs to assist with strengthening the economy and businesses in Swain County. The Economic Development Commission maintains a county-wide Economic Development Plan, which lists the Commission's future projects. This Plan was last updated in 2004, and should undergo another update beginning in 2007. The Plan does not have any specific projects for Bryson City, but it does identify two issues of importance in the Town, which are:

1. The need to support the Great Smokey Mountains Railroad Company as they progress with their expansion and,
2. Implementing a greenway along the Tuckaseegee River.

The Economic Development Commission also administers a Redevelopment District in Bryson City located within the boundaries of Island Street, Bryson Street, Everett Street, and Island Park. After the floods caused by Hurricane Floyd in 2004 damaged many of the properties in the area, the Redevelopment District was established through a grant from the North Carolina Rural Center. The Economic Development Commission plans to focus efforts in the Redevelopment District on improving the building stock and marketing the area to future businesses.

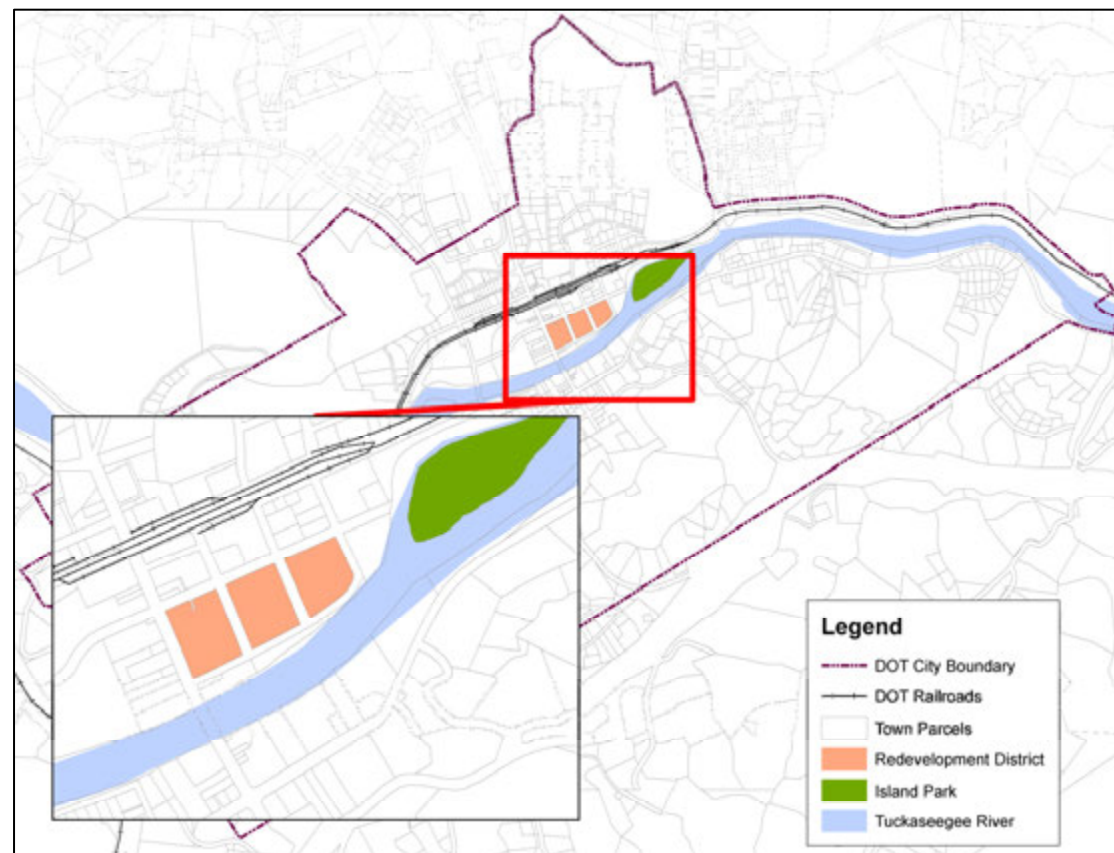


Figure 3-2. Map of Bryson City showing the redevelopment district.

Bryson City Pedestrian Plan

Section 3: Existing Plans, Policies, and Programs

3.2.4. Swain County Chamber of Commerce

The Town of Bryson City is also home to the Swain County Chamber of Commerce, located on Everett Street. The Swain County Chamber of Commerce consists of over 350 members representing retail, wholesale distribution, attractions, services, manufacturing, dining and lodging operations. Located on the town square adjacent to the Old Courthouse, the chamber is a center for business and visitor information. The chamber sponsors special events for the community and visitors, seminars and networking opportunities.

3.2.5. Great Smoky Mountain Railroad Company

Another major entity in the Town is the Great Smoky Mountain Railroad Company (GSMR), which owns several properties in the heart of Downtown Bryson City. The Great Smoky Mountain Railroad Company runs and maintains the Great Smoky Mountain Railroad, which operates passenger train rides between Dillsboro, Bryson City, and Andrews. The GSMR also operates special train events, including the annual Polar Express, the Little Engine That Could Train, Thomas the Tank Train in Dillsboro, and RailFest. The GSMR had a ridership of over 200,000 in 2006, with 46,000 riders for the Polar Express between the beginning of November to the end of December. Another peak period is the month of October, during which time the train had over 30,000 riders in 2006. The GSMR has plans to expand in the future, which includes in the first phase of construction moving the existing shop for trains to their property along Bryson Walk. This shop will include a viewing area for visitors to watch work on the steam engines take place, and a museum for trains. In the second phase of the expansion, the GSMR hopes to construct a hotel and shopping area on the GSMR property.

3.3. Existing Policies

The Town of Bryson City currently has no standard sidewalk requirements, greenway requirements, or crosswalk requirements. In the Downtown Bryson City area, the Town has an informal policy of maintaining a sidewalk width of a minimum of 48 inches with brick edging to create an average sidewalk width of 60 inches (see Figure 3-3). The section on standard designs and guidelines will provide more detailed information about engineering specifics for greenway, sidewalk, and crosswalk design in Bryson City.

The Department of Community Assistance is in the process of preparing the Town's first Land Use Plan, which should be completed by January 2008. In addition, the Bryson City

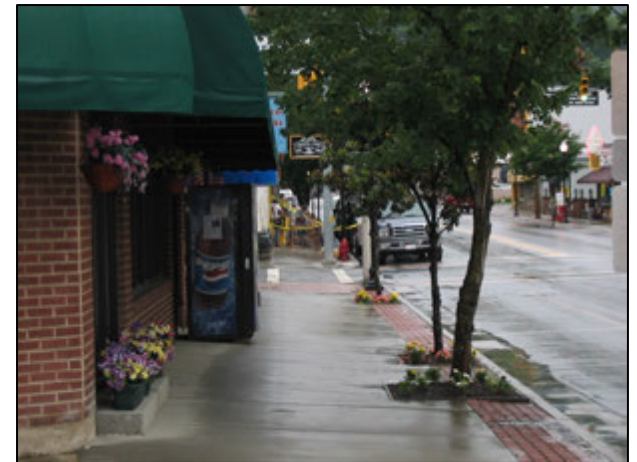


Figure 3-3. Sidewalk in Downtown Bryson City. Note the brick edging that contributes to the sidewalk width.



Figure 3-4. The Great Smoky Mountain Railroad Train is a major attraction for visitors to Bryson City.

Planning Board is currently researching the possibility of implementing a zoning ordinance similar to that applied in Waynesville.

3.4. Existing Programs and Initiatives

Currently the Town has no established programs specifically designed to encourage walking and a more pedestrian-friendly town environment. However, the school system conducts related programs to promote healthy and active living through after-school programs, healthy snack policies, and physical education requirements. It is important to develop programs to encourage walking in conjunction with constructing better walking facilities. Programs can boost the numbers of pedestrians using these facilities while also educating about the benefits of walking for health, the environment, and reducing traffic congestion. Some programs that may be used in Bryson City to encourage walking include creating a Walk-to-Work Day or Week, developing and distributing pamphlets that educate about the benefits of walking, and establishing a Safe Routes to School Program. These recommendations are discussed more thoroughly in Section 6 of this Plan.

3.5. Summary

Bryson City has limited existing policies and programs to guide the construction of pedestrian facilities and to encourage a more pedestrian-friendly community. Rather than hindering the Town's efforts to become more pedestrian-friendly, this provides an opportunity for the Town to establish new policies and programs with the completion of this Plan that will support the community's efforts to encourage pedestrians and walking in the Town. The following sections provide recommendations for those policies, programs, and initiatives that should be implemented to assist the Town in improving its pedestrian environment.

This section provides a brief description of the various areas in town which were discussed as potential project areas through the Stakeholder Committee and public involvement efforts. The section also provides recommendations for improvements in each of these areas.

Section 4. System Plan

4.1. Introduction

This section outlines the focus areas where pedestrian needs have been identified by Town staff, stakeholders, and the public. For each focus area, recommendations are made to address the pedestrian needs. In Section 7, these recommendations are then summarized, prioritized, and cost estimates are provided.

In the discussion of Bryson City's population's demographics contained in Section 2, a variety of populations with special pedestrian needs are addressed. While the focus areas discussed in this Section do not specifically detail recommendations targeted at improving pedestrian access for these populations, any improvement in pedestrian access should assist these populations in their ability to travel from one destination to another.

4.2. Focus areas

Throughout the Plan preparation process, in meetings and through public input, several key locations in Bryson City were highlighted as important pedestrian areas. These focus areas were:

- ◆ Downtown Bryson City Area
- ◆ Super Saver Commercial Area
- ◆ Veteran's Boulevard
- ◆ Swain County Middle School Area
- ◆ Hughes Branch Road/HWY 19 Intersection Area
- ◆ Deep Creek Recreation Center Area

The following paragraphs describe each of these "focus areas" and the recommendations made to address their pedestrian needs. Figure 4-2 shows the focus areas in Bryson City which are discussed in this section and outlines their potential attractions for pedestrians.



Figure 4-1. Train Station for the Great Smoky Mountain Railroad, located on Everett Street at the intersection with Depot Street.

Bryson City Pedestrian Plan
Section 4: System Plan

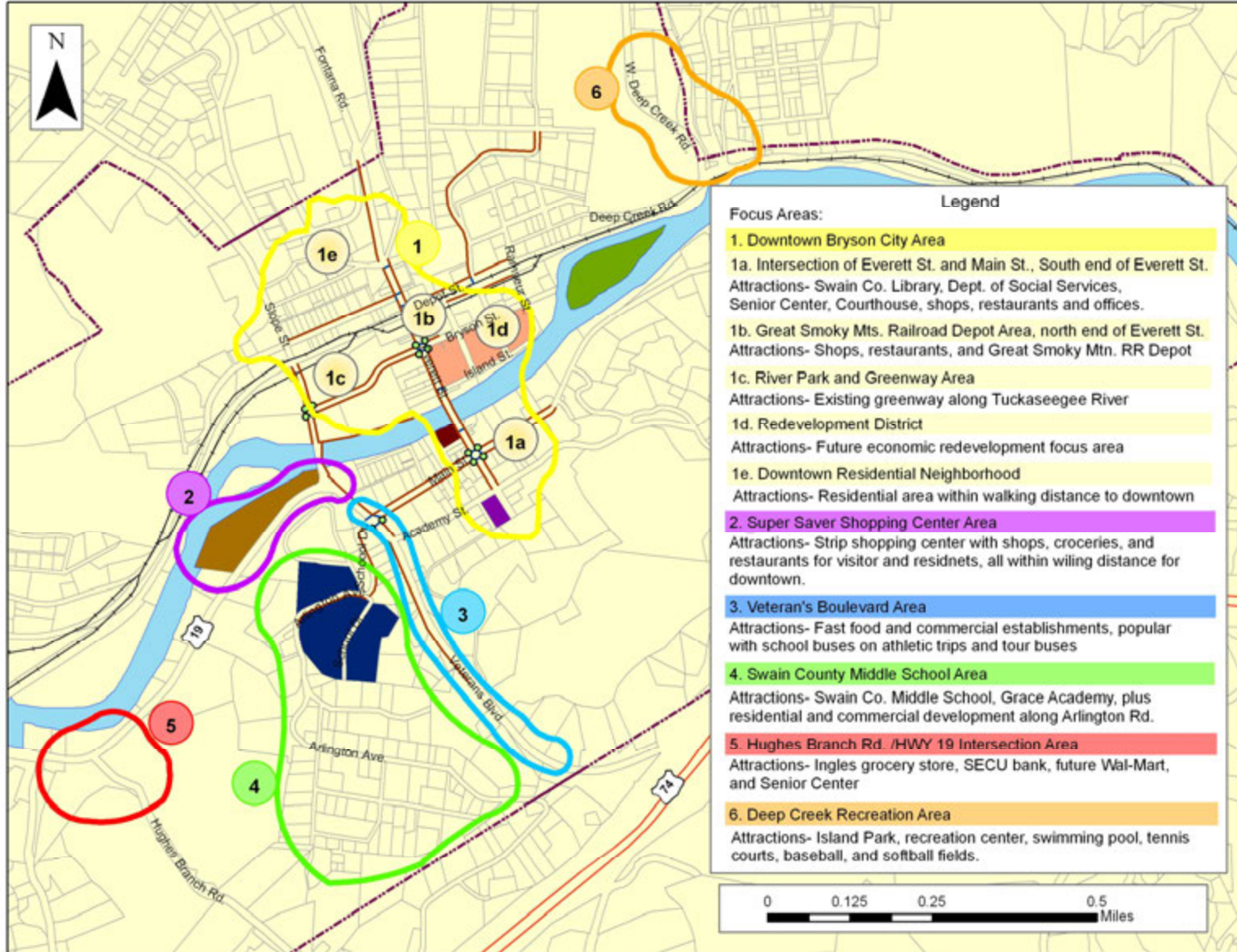


Figure 4-2. Map of Bryson City area showing the general Focus Areas and their potential pedestrian attractions.



Figure 4-3. An image of the existing Island Street streetscape, facing Everett Street.

Downtown Bryson City Area

Downtown Bryson City is the most pedestrian-friendly area in the town, and the destination for most visitors. The Downtown is primarily within the limits of Bryson Walk/Depot Street and Academy Street to the north and south and Slope Street and Ramseur Street to the east and west, with Everett Street serving as the main commercial destination. The Downtown includes shops, restaurants, and services centered around the Great Smoky Mountain Railroad Station. Downtown is also home to many of the county's administration buildings, including the County Courthouse, Department of Social Services, and Administration Building. Nearby residential development along Bryson Walk dates back to prior to 1950 and a time when sidewalks were built with each new house - thus the business area is fairly well-connected to the residential area.

Leaders in Bryson City have several visions for the future of the Downtown area. In private development, the Great Smoky Mountain Railroad company owners intend to develop land around the railroad for new attractions, commercial use, and potentially a hotel. In the property along Bryson Walk between Everett Street and Slope Street, there are plans to construct a functioning railroad turn-table/shop and railroad museum, which will serve as an added attraction to the Great Smoky Mountain Railroad. A hotel is planned in the second phase of expansion, although an exact location has not yet been selected.

In public development, the Town of Bryson City has the on-going streetscape projects discussed in Section 3, and the recently completed Island Park Bridge project. In addition, the Town plans to capitalize on the Redevelopment District designation of the area between Everett Street, Island Street, and Bryson Street.

Everett Street becomes Rector Street after the intersection with Main Street/US 19. There are several major pedestrian attractors and generators along Rector Street and Academy Street, including the Swain County Library, Department of Social Services Building, and County Courthouse Building. Although there is a pedestrian crosswalk in front of the County Courthouse on Main Street, there are no sidewalks or pedestrian signs along Academy Street, Rector Street, and Fryemont Street. On a field visit, consultants and staff witnessed at least five people walking along Academy Street returning to work after lunch. This is an indication that this area has the potential to draw many pedestrians out of the office buildings into downtown. The small park/garden located at the intersection of Main Street and Everett Street could also draw visitors to travel beyond the immediate train station area down to Academy Street, where they may also be

Bryson City Pedestrian Plan

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attracted to the library and potential commercial development along Academy Street and Fryemont Street.

Future improvements will need to address the Train Station Area, Island Park and Economic Redevelopment District, and Department of Social Services/Library/Courthouse Area. In the Train Station area, improvements will need to be made for people crossing the street, perhaps mid-block improvements, and future sidewalk to connect to other attractions. Special sidewalk treatments may also apply to provide a more attractive appearance. At Island Park and along the Economic Redevelopment District, considerations will need to be made for construction of a new greenway, construction of additional sidewalk, and the institution of pedestrian-related requirements (such as sidewalk and crossing treatments) when future residential and commercial development and re-development occurs. In the area around the Department of Social Services/Library/Courthouse, projects will need to provide additional sidewalk, intersection improvements, and better signage.

The following are recommendations for the Downtown area:

- ◆ Improve the intersections of Everett and Main Street, and Academy Street and Rector Streets for better pedestrian convenience and safety.
- ◆ Provide sidewalk along Fryemont Street.
- ◆ Provide pedestrian connection from Island Park to the Train Depot, and consider a pedestrian connection from Island Park along the river to the Administration Building and River Park greenway.

Super Saver Commercial Area

Just west of downtown is the popular Super Saver commercial area, a strip mall which begins near to the intersection of Slope Street/Veteran's Boulevard and HWY 19. Although this is slightly west of downtown, it is still within walking distance for many people. Improved pedestrian access should be provided to this location from downtown to allow employees from the residential neighborhoods near downtown to walk to their jobs, and to encourage visitors and local shoppers to walk, rather than drive, to the destination. This will both reduce traffic congestion in the area and also make the area even safer for pedestrians while promoting a physically active lifestyle.

Recommendations for this area are:

- ◆ Provide a direct pedestrian connection from downtown to Super Saver Shopping Center; and,



Figure 4-4. The new pedestrian bridge to connect downtown to Island Park. Above: The bridge in construction. **Below:** The bridge completed.

- ◆ Improve pedestrian safety and accessibility at the intersection of Slope Street/Veteran's Boulevard and HWY 19.

Veterans Boulevard Area

Light-heartedly referred to as “Fast Food Lane”, this focus area is located along Veteran's Boulevard beginning at the exit from US 74 all the way to Main Street to enter downtown. As discussed in Section 2, along the length of this corridor are many fast food and commercial establishments which are popular locations for school buses on athletic trips and tour buses to drop-off riders to pick up something to eat. Often, a bus will stop at one location and its riders will disperse to various locations along the length of Veterans Boulevard. There is sidewalk along Veteran's Boulevard from Slope Street to US 74, a pedestrian signal at the signalized intersection of Slope Street and Veteran's Boulevard, and another at the intersection of Main Street and Veteran's Boulevard. In spite of this, crossing the street can be hazardous especially since the corridor has high traffic volumes, is heavily traveled by trucks and similar large vehicles, and has a speed limit which varies from 45 mph near US 74 to 25 mph at the intersection with Main Street. In addition, there are no mid-block crossings along the length of Veteran's Boulevard and although there is a sidewalk, it is narrow, and not well-defined through parking lots and near to other shopping areas. The cross-section for Veteran's Boulevard is shown in Figure 4-5.



Figure 4-5. Cross-section of Veteran's Boulevard.

Since many of the bus riders are either children or senior citizens, it is critical that improvements are made to provide additional pedestrian facilities to accommodate and make safer this area for pedestrians. At the same time, it is important to balance these improvements with the vehicle demands for the corridor due to its function as the primary entrance into town. Future improvements should be considered to warn drivers of pedestrians in the area, and to provide safer facilities, such as more frequent mid-block crossings, wider sidewalks, and reduced speeds.

Recommendations for this area are:

- ◆ Provide pedestrian improvements at the following locations:
 - Veteran's Boulevard and Main Street;
 - Veteran's Boulevard and Slope Street;
- ◆ Improve pedestrian safety along Veteran's Boulevard and for pedestrian crossing the Boulevard

Schools Areas: Swain County Middle School and Swain County High School

The Swain County Middle School area is located on School Drive and Arlington Road at the top of what is popularly known as "School House Hill". Although most students are driven or bused to school, at the start and finish of each school day there can be between 20 and 30 students who walk to and from school. As discussed in Section 2, many parents prefer to drop their children off at the gas station located at the intersection of School Drive and Veteran's Boulevard at the base of the hill due to the school's location on such a steep hill and the narrowness of the drive leading to the school. Children who are dropped off at the gas station can walk to school up the hill on the sidewalk along School Drive, which ends at the Middle School. Improvements should be made for crossings and signage to warn drivers of the potential for students to be walking to school. In addition to the Middle School, the nearby Grace Academy also generates many pedestrians, especially from the residential development along Arlington Road and from the Middle School.

Future projects along this area should provide safe pedestrian facilities such as sidewalks and crossings for children walking to the two schools from nearby residential development and the gas station at School Drive and Arlington Road, as well as signage to warn drivers of the potential for pedestrians. In addition, an educational program should be developed to teach children and parents proper pedestrian behavior for walking to school. The Town could also consider implementing a Safe Routes to School program (discussed more in Section 6).

In addition to the Swain County Middle School, access is needed from downtown Bryson City to Swain County High School, which is located on Fontana Road outside of town limits. Sidewalk exists from downtown along Everett Street to Fontana Road to the town limits, however, further improvements will need to be coordinated with the County and NCDOT.

Recommendations for schools are:

- ◆ Provide safer pedestrian access to Swain County Middle School and Grace Academy from nearby residential areas and commercial area along Arlington Ave;
- ◆ Improve pedestrian access and safety along School Drive.
- ◆ Provide complete access along Fontana Road to Swain County High School.

Hughes Branch Road/HWY 19 Intersection Area

As discussed in Section 2, an emerging commercial center in Bryson City is developing at the intersection of Hughes Branch Road and HWY 19. As of August 2007, an Ingles and a bank are at this location, and future development plans call for a Wal-Mart and Senior Center. In addition, an NCDOT TIP Project (TIP No. R-4751) is planned which will include upgrades to US 19 beginning at Hughes Branch Road to US 441 in Cherokee. This TIP project may include an improvement to the intersection of Hughes Branch Road and US 19. Although this location is beyond the traditional limits of downtown, pedestrian-friendly design and facilities should still be provided here to ensure improved safety, increase accessibility, and reduce traffic congestion. In particular, the future Senior Center should have well-connected pedestrian facilities to the nearby shopping to make the area safer and more accessible for those seniors who can no longer drive and the intersection of Hughes Branch Road and US 19 should have pedestrian-activated signals and crosswalks.

Recommendations for this location are as follows:

- ◆ Provide safe pedestrian connection and crossings from Downtown to the intersection of Hughes Branch Road and US 19 and the Ingles/commercial area nearby

Deep Creek Recreation Center Area

The Deep Creek Recreation Center is located about a ½ mile to the east of downtown along Deep Creek Road. As discussed in Section 2, the Recreation Center includes a swimming pool, playground, tennis courts, baseball and softball fields, a putting green, and batting cages. Pedestrian access, including sidewalks, and pedestrian signage should be provided from downtown to this location to allow visitors and residents to walk to this destination. The Town should also consider extending the proposed greenway from Island Park along the river front to the facility.

Recommendations for this area are as follows:

- ◆ Provide safe pedestrian connection and crossings from Downtown and Island Park to the Deep Creek Recreation Center
- ◆ Extend proposed greenway from Island Park to Deep Creek Recreation Center

4.3. Summary

This section provides general recommendations for various focus areas around Bryson City which require pedestrian improvements. The following sections will provide

Bryson City Pedestrian Plan

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additional design, program, policy recommendations to improve the pedestrian-friendliness of Bryson City. In addition, the project recommendations made in this section will be prioritized and cost estimates will be made to help with implementation.

This section provides a set of design guidelines for best practices to make Bryson City more pedestrian-friendly.

Section 5. Design Standards and Guidelines

5.1. Introduction

This section provides guidance for the Town of Bryson City as they construct new pedestrian facilities and reconstruct existing pedestrian facilities to meet better standards. This section is divided into the following topics:

- ◆ legal rights of pedestrians
- ◆ pedestrian facilities and their design
 - sidewalks
 - crossings: signalized or unsignalized
 - greenways
- ◆ ADA requirements
- ◆ downtown area standards
- ◆ school standards
- ◆ sidewalk construction policy and maintenance
- ◆ parking lots

Currently, the Town does not have any standards for pedestrian facilities – sidewalks, crosswalks, and other pedestrian-related amenities are constructed on an ad-hoc, as needed basis. This section of the Plan is important because it provides a consistent set of guidelines within the Town to help create a uniform appearance to Bryson City's sidewalks and a more connected system.

5.2. Legal Rights of Pedestrians.

It is important to understand the legal rights of pedestrians because these guide and define how pedestrian facilities are constructed and provided. Some of the legal rights of pedestrians are defined in Sections 20-172 through 20-175.2 of the North Carolina General Statutes.

More information can also be found in the NC Bike/Pedestrian Laws Guidebook, available at the NCDOT's Division of Bicycle and Pedestrian Transportation webpage: <http://www.ncdot.org/transit/bicycle/laws/resources/BikePedLawsGuidebook-Part-1.pdf>.

Some of the items which should be considered are the following:

- ◆ Drivers must yield to pedestrians (or cyclists) crossing a driveway, alley exit, or parking garage exit on a sidewalk. (§20-173)

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Section 5: Design Standards and Guidelines

- ◆ Pedestrians crossing any roadway other than at a marked crosswalk must yield to vehicles.
- ◆ Pedestrians should cross at street intersections or in marked crosswalks.
- ◆ If there are sidewalks, pedestrians are not to walk in the roadway. Where sidewalks are not provided, any pedestrian walking along the roadway will walk to the extreme left, facing in the direction of approaching traffic.
- ◆ Every driver must consider pedestrians at all times, especially exercising care in the presence of children or incapacitated persons on the roadway. (§20-174)
- ◆ Special emphasis on leaving adequate crossing room at intersections is noted for visually handicapped persons. (§20-175.2)

In addition, pedestrian access is also governed by the requirements of the American Disabilities Act of 1990, a civil rights law which prohibits discrimination against people with disabilities in all aspects of life. As done throughout the US, the Town of Bryson City must provide transportation facilities, including sidewalks and other pedestrian facilities, which comply with the guidelines set forth in the ADA Accessibility Guidelines (ADAAG) in order to meet the standards of the American Disabilities Act. Some of the major items related to pedestrian facilities that are addressed by ADAAG include curb ramps and cross-slopes. The following bullets describe ADAAG-compliant design for these items:

- ◆ **Curb ramps: design and placement.**

- *DESIGN:* Curb ramps are a significant and required feature of accessible pedestrian transportation systems, and must be designed carefully to fulfill their function and the requirements of the Americans with Disabilities Act. Curb ramps should not have a slope greater than 1:12, meaning that for every foot of travel, the slope should not rise more than one inch. To provide a tactile warning to the visually impaired, raised truncated domes with a color contrast to the background material (typically concrete) should be used, with measurements shown in Figure 5-1.i The ADA Accessibility Guidelines for Buildings and Facilities (<http://www.access-board.gov/adaag/html/adaag.htm#A4.29.2>) has an easy-to-use format for locating specific design criteria related to curb ramps, rise/run restrictions on ramps, and figures illustrating basic concepts.ii
- *PLACEMENT:* Curb ramps should be placed entirely within the area of a marked crosswalk, so that a pedestrian can enter the ramp space at an angle perpendicular to the direction of travel. Generally, the standard is to have separate curb ramps on each corner; if a shared (sometimes called corner or diagonal) curb ramp is constructed, then the width and radius should accommodate the user so that entry onto the ramp is parallel to the direction of travel. Figure 5-3 provides examples of the

acceptable relationship between crosswalk and curb ramp location/widths.

- ♦ **Cross-Slopes.** Cross-slopes, or a slope along the travelway surface which is perpendicular to the direction of travel, can often make it very difficult for wheelchair travel. In addition, it can also make for treacherous walking conditions for individuals with problems with their balance and coordination. Cross-sloping most frequently occurs in conditions in which a driveway meets a sidewalk, but can also occur in other situations. In order to minimize the risk of a dangerous and difficult travel condition for some, cross-sloping is regulated by ADAAG such that cross-slopes should not exceed 2 percent, and preferably not exceed 1.5 percent where possible. Figure 5-2 indicates the preferred (top), conditionally acceptable (middle), and unacceptable (bottom) design solutions for new driveways as they interface with sidewalks.

For a complete guide to ADA requirements, please see the National Access Board's website: www.access-board.gov.

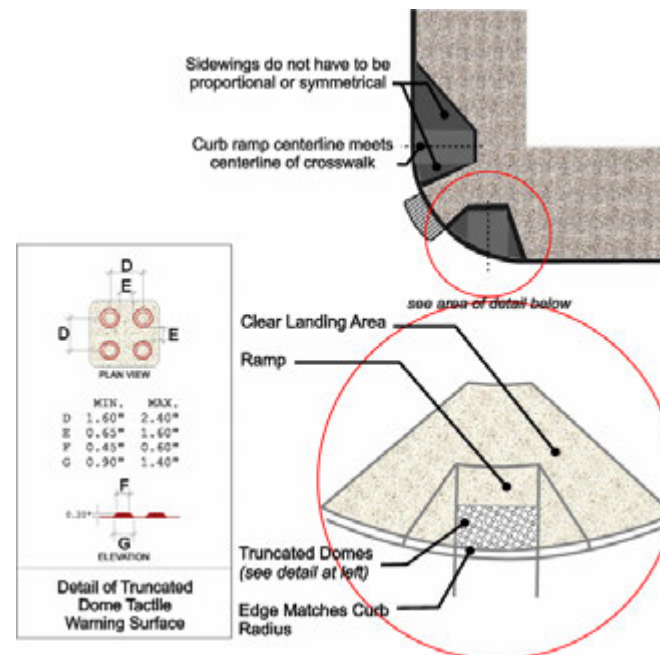


Figure 5-1. Detail of an ADA-complaint curb ramp design with truncated dome measurements.

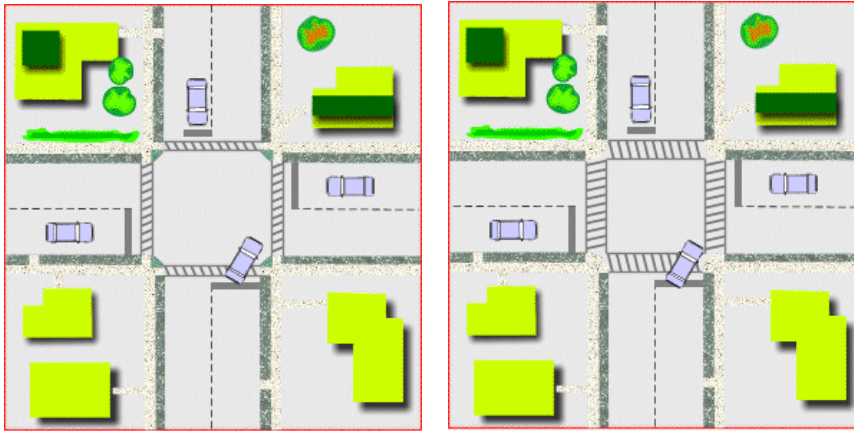


Figure 5-3. Drawing of appropriate curb ramp placements. In each image, the curb ramps are located completely within the crosswalk limits.

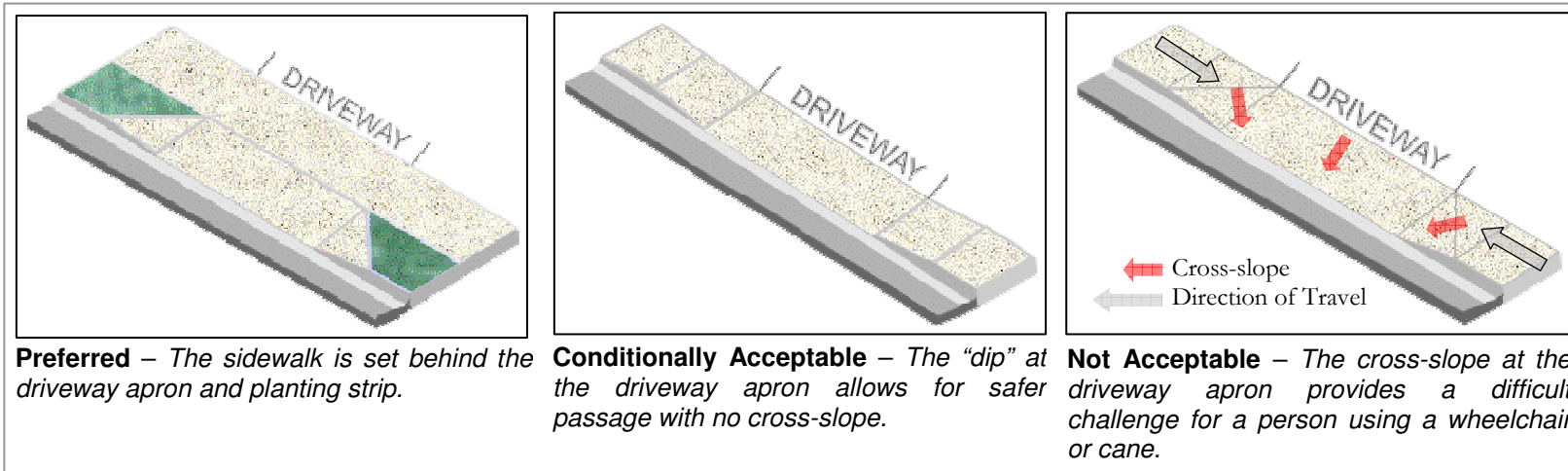


Figure 5-2. Examples of acceptable and unacceptable design solutions for minimizing cross-sloping at a driveway and sidewalk interface.

5.3. Pedestrian Facilities and their Design

There are a variety of sources for design guidance for pedestrian facilities, including the following:

- ◆ NCDOT (Draft, 1997)
- ◆ The American Association of State Highway and Transportation Officials' *Guide for the Planning, Design, and Operation of Pedestrian Facilities* (AASHTO, 2004)
- ◆ Manual on Uniform Traffic Control Devices (MUTCD), frequently updated
- ◆ Federal Highway Administration (FHWA)

The North Carolina Department of Transportation adheres to the design guidelines provided in the AASHTO and MUTCD guidebooks. In general, pedestrian facilities can be described in the following categories:

- ◆ sidewalks
- ◆ crossings
- ◆ greenways

The Town currently does not have its own standards for pedestrian facilities. The following paragraphs provide national standards and best practices for pedestrian facilities by category.

5.3.1. Sidewalks

Standard sidewalk is usually is five feet minimum in width, concrete, and placed along roadways with curb and gutter. In general, the width of sidewalks should accommodate two persons walking past one another, a width generally perceived to be five feet, at a minimum. Other circumstances that may require additional sidewalk width are: (1) to accommodate the overhang of parked vehicles from off-street or angled on-street parking areas; and (2) additional buffer from traffic when a planting strip cannot be installed.

Additional design considerations for on-street sidewalk facilities include the following:

- ◆ Eliminating both high and low contact points with tree branches, mast-arm signs, overhanging edges of amenities or furniture, and
- ◆ Providing clear space between walls on one side of the walkway and amenities, parking overhang, or plantings on the curb side of the walkway (see Figure 5-4 which diagrams the relationships between pedestrian features, building facades, and roadway).

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In general, standard sidewalks should be concrete, which is more durable than asphalt. A more flexible material, such as rubberized paving, can be considered in situations in which there is the potential for tree roots to crack and lift the concrete. Using these types of materials can reduce the risk of a tripping hazard, and also lower maintenance costs. More permeable materials, such as porous pavers, can also be considered for all pedestrian-ways, and in particular for greenways near streams, in order to reduce run-off from storm events.

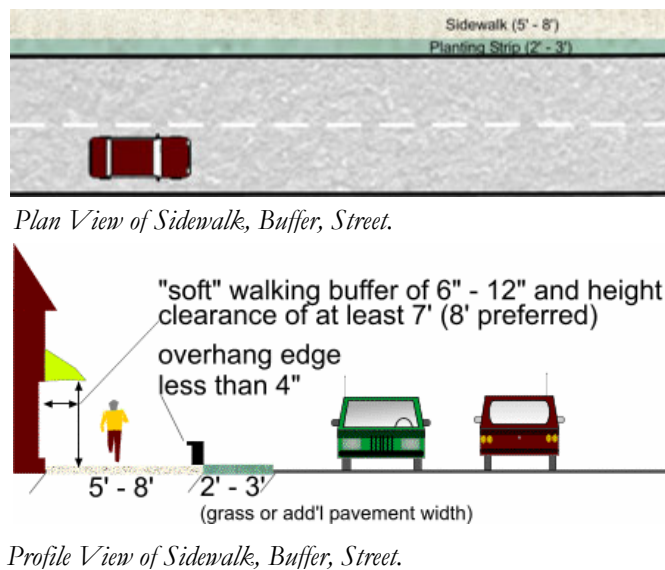


Figure 5-4. A diagram of the relationship between pedestrian features, building facades, and the roadway.

5.3.2. Crossings

Crossings are a critical feature in a well-connected pedestrian system because they provide the linkages between one segment of sidewalk to another as a pedestrian may cross a street, connect to another existing piece of sidewalk, or pass to a new development. A well-placed crossing can dramatically reduce pedestrian travel time and improve pedestrian safety – greatly increasing the convenience of walking as a mode of travel. Crossings can be both signalized and unsignalized, as well as located at intersections or at mid-block locations. The Town of Bryson City has several signalized and unsignalized crossings at various intersections throughout town, and on Everett Street through Downtown Bryson City there are many mid-block crossings to alert motorists to crossing pedestrians.

There are a variety of designs for unsignalized crossings, including striped crosswalks, zebra crosswalks, and raised platform crosswalks. There are also a variety of designs for signalized crossings, including:

- ◆ Pedestrian Signals and crosswalks
- ◆ Pedestrian Signals, crosswalks, and audible signals
- ◆ Pedestrian Signals, crosswalks, and countdown signals
- ◆ High Intensity Activated Crosswalk (HAWK) Signal

It is recommended that for all signalized intersections in the Town, the Town should have signals for pedestrians. In the Downtown area, pedestrian signals should include audible and visible cues and perhaps even countdown displays.



Figure 5-5. Examples of pedestrian-activated signalized mid-block crossings.
Top image: An example of a pedestrian-activated signalized mid-block crossing.
Bottom image: An at-site guide for pedestrians to assist them in understanding the meaning of the push-button signals.

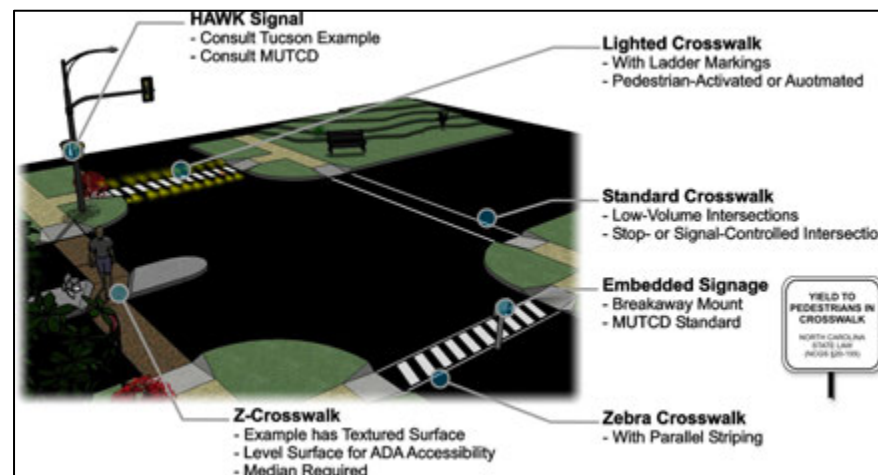


Figure 5-6. A diagram of various crossing treatments Bryson City can consider to improve pedestrian accessibility and safety crossing the street.

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For mid-block crossings, there is still no national consensus for when a crossing should be created mid-block, and when the mid-block crossing should be signalized. The City of Charlotte Department of Transportation has created a set of guidelines for assessing mid-block crossings, based in part on the work of FHWA and Charles Zegeer of the Pedestrian and Bicycle Information Center. In addition to numbers of pedestrians, vehicle speed, and vehicle volume on the roadway, there are a variety of other considerations which must be accounted for when determining whether to construct a mid-block crossing. These considerations include: lighting conditions, sight distance, numbers of lanes, and roadway width. Figure 5-7 shows the “solution space” identified by the City of Charlotte for considering a mid-block crossing. Table 1 shows the decision matrix created by the City of Charlotte for determining when to construct a mid-block crossing and identifying appropriate treatments.

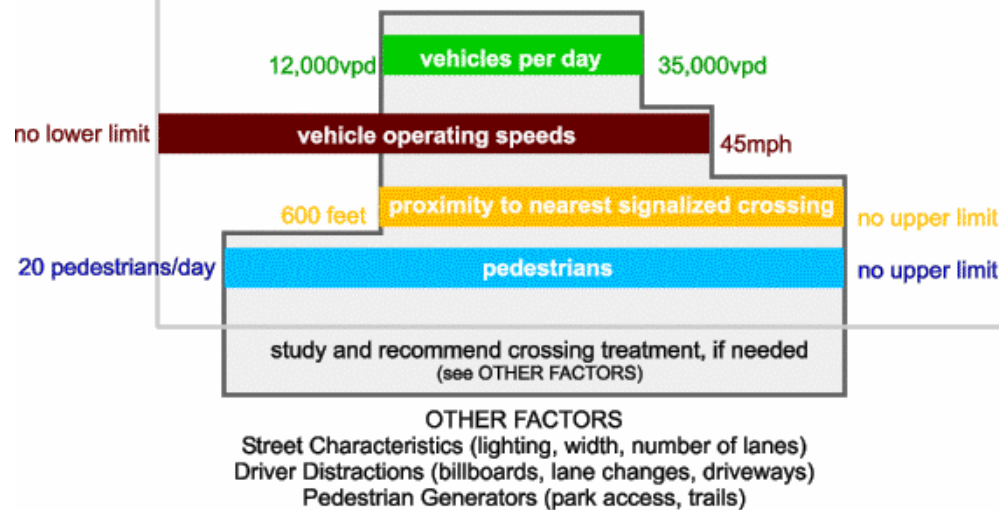


Figure 5-7. The City of Charlotte's solution space for considering when to apply signalized mid-block pedestrian crossings.

Table 5-1. Mid-Block Crossing Treatment Design Criteria (Charlotte DOT, 2005).

**Note: MUTCD recommends pedestrian volumes of at least 400 for a four-hour period. **A HAWK (High-Intensity Activated Crosswalk) signal is a pedestrian-activated system used for high-volume crossings found to be useful in increasing the rate of driver responses to pedestrian crossings, especially in Tucson, AZ where they have been utilized extensively.ⁱⁱⁱ*

Pedestrian Mid-block Crossing Treatment	AADT	Operating Speed	Approx. Cost
Signs	5,000 – 35,000	Less than 45 mph	\$250 - 350
High-Visibility Markings	5,000 – 12,000	Less than 35 mph	\$500 – 1,500
Colored and Textured Markings	5,000 – 12,000	Less than 35 mph	\$5,000+
Curb Extensions	5,000 – 12,000	Less than 35 mph	\$5,000 – 25,000
Raised Crosswalks	5,000 – 15,000	Less than 30 mph	\$2,000 – 15,000
Refuge Island	12,000 – 30,000	Less than 40 mph	\$10,000 – 40,000
Median	15,000 – 35,000	35 - 45 mph	Varies greatly
In-Pavement Illumination	5,000 – 15,000	Less than 35 mph	\$40,000
Pedestrian-Only Signal*	15,000 – 35,000	35 – 45 mph	\$40,000 – 75,000
HAWK Signal**	15,000 – 35,000	35 – 45 mph	\$35,000 – 60,000

Given the sensitive nature of mid-block crossings, every new mid-block crossing treatment will require a specific investigation by the Town prior to initiating design and construction. In spite of this, mid-block treatments can be useful in improving safety in areas with fairly high pedestrian crossings and low numbers of vehicles and vehicle speeds, if located and designed properly.

5.3.3. Signage

In addition to sidewalks and crossings, pedestrian facilities also include signage along major pedestrian routes. Signs serve primarily to notify motorists and others of the presence of pedestrians. The intended effect is to cause motorists drive more cautiously and reduce their speeds, thereby improving the safety for pedestrians in the given area.

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Signs can be used in a variety of places, including at crosswalks, at intersections, in-street, and near schools. National standards for sign placement and use can be found in the Manual for Uniform Traffic Control Devices (MUTCD). The MUTCD provides guidance for warning signs which can be used at both crosswalks, or along the roadway:

“Nonvehicular signs may be used to alert road users in advance of locations where unexpected entries into the roadway or shared use of the roadway by **pedestrians**, animals, and other crossing activities might occur.” (Page 2C – 21, 2003 Edition)

The following are some recommended signs which Bryson City should consider installing. For more signs and more detailed guidelines for sign installation and use, Bryson City should consult the MUTCD.

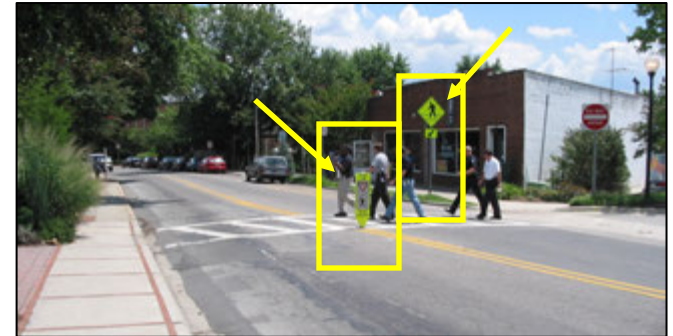


Figure 5-8. An example of two types of signs used to notify motorists of a pedestrian crossing.

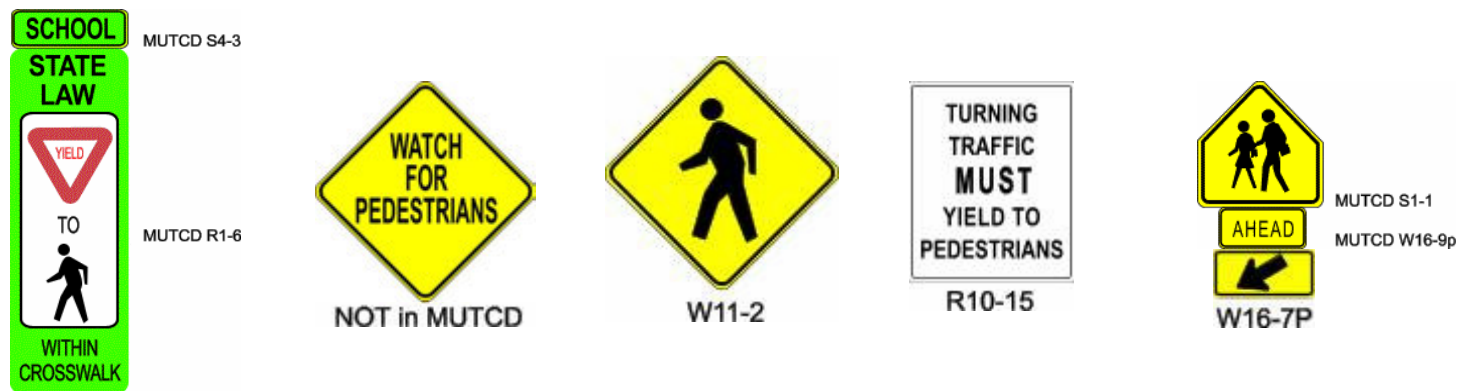


Figure 5-9. Example standard pedestrian warning signs. The first sign is usually installed within the street to warn motorists to yield to pedestrians in a crosswalk - it does not have to be near a school. The second and third signs are common general pedestrian warning signs, while the fourth and fifth signs notify motorists of specific instances to watch for pedestrians. The fourth sign, “Turning Traffic”, is usually placed at intersections to warn motorists that are turning right or left to yield to pedestrians in crosswalks. For the fifth sign, the top sign can either be combined with the smaller “ahead” sign or the arrow symbol to indicate the presence of a crosswalk to motorists in a school zone.

5.3.4. Greenways

Greenways, sometimes also considered multi-use trails, are one of the most popular pedestrian facilities, especially for recreation. Greenways can be both paved or unpaved paths, often unassociated with a roadway. They can be used by pedestrians, cyclists, and other non-motorized users. Greenways are typically no less than 10 feet wide and asphalt if paved. Additional guidance on greenway design and standards can be found at:

http://www.ncdot.org/transit/bicycle/projects/project_types/Multi_Use_Pathways2.pdf

An example greenway cross-section is provided in Figure 5-10.

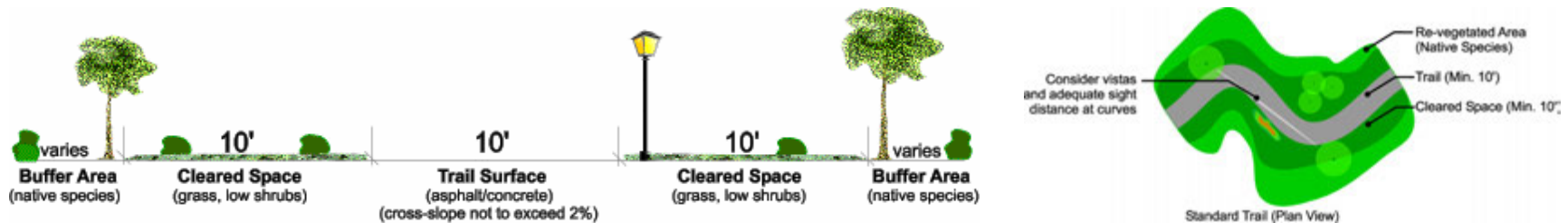


Figure 5-10. Plan and cross-sections views of an example greenway. Left Image: Example cross-section for a typical greenway. **Right Image:** A plan view of a typical greenway.

5.4. Downtown Area Standards

Downtown Bryson City is already one of the most pedestrian-friendly areas in the Town. In order to maintain its pedestrian-oriented nature, and also to enhance the area's attractiveness and visual appeal, the Downtown area should have certain standards which may or may not be required beyond the downtown area. Some of these recommendations are as follows:

- ◆ **Maintain and require brick and concrete sidewalk theme.** Already, the Downtown Area has a consistent brick and concrete sidewalk theme – this theme should be maintained throughout downtown and required whenever new sidewalk is constructed. Figure 5-11 shows an example of this brick and concrete theme.

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- ◆ **Provide wide sidewalk.** Currently, the sidewalk in the downtown area is approximately 8 to 10 feet wide, with a 5-foot wide travelway and additional width for a brick borders. New sidewalk, or reconstructed sidewalk, should be kept at a minimum of 10 feet, if not wider, in the downtown area. This width is necessary to accommodate the numbers of pedestrians expected in a Town such as Bryson City which has many visitors. Pedestrians need space to window shop, stroll, walk side-by-side with their families, and even stop for a rest in the sidewalk space. The Town should also consider accommodating restaurants or cafes interested in creating outdoor, on-street seating, which is often a major booster to making a street look more popular and pedestrian-friendly. It also attracts even more visitors and potential shoppers and diners.
- ◆ **Provide many pedestrian amenities.** In addition to sidewalk width, the Town should also provide pedestrian amenities such as benches, trash cans, and water fountains to make walking in downtown more comfortable for the numerous visitors that come to Bryson City each year. The Town should even consider adding street trees or other means for shade. The more pedestrian amenities available in a particular area, the more inviting the area for pedestrians and visitors.
- ◆ **Provide frequent pedestrian crossings.** The Downtown area also already has many crosswalks and pedestrian crossings. In order to maintain the accessibility of the downtown area, crosswalks should be required at various intervals along major streets that are uninterrupted by intersections.
- ◆ **Require pedestrian signals with audible and visible signals as well as countdown displays at all intersections.** Pedestrian signals should be required with audible and visible signals at all intersections, especially within the downtown area. The Town may also wish to consider pedestrian signals with countdowns in order to assist with safer crossings.

5.5. Schools

In addition to Downtown, another area in Bryson City that merits special treatment is the area around schools. Schools require special treatment because of the presence of both children and very high levels of traffic during drop-off and pick-up. Especially during drop-off and pick-up, traffic near schools can be incredibly varied - consisting of small and large personal vehicles, school and other activity buses, pedestrians, and cyclists. Specific design features should be required around schools to improve safety for all in the area. Some of these design features include:

- ◆ Requiring sidewalks on both sides of the street;



Figure 5-11. Sidewalk on Everett Street in downtown Bryson City. The yellow arrow indicates the brick and concrete treatment.

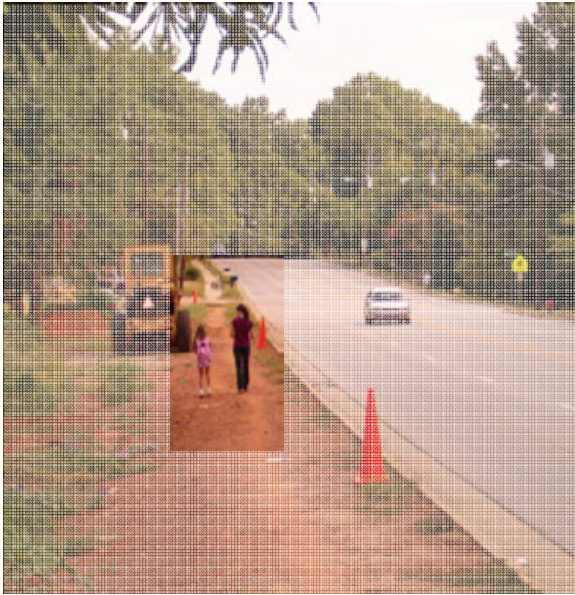


Figure 5-12. Poor pedestrian access at a construction site in Cary, NC.

- ◆ Placing crosswalks and pedestrian signals at all intersections near the school;
- ◆ Reducing speed limits along adjacent streets; and,
- ◆ Providing signage to warn drivers of the school's presence and the potential for children in the street.

5.6. Construction Zones

It is important that during construction of any kind, convenient and safe pedestrian access to destinations remain open and accessible. During the construction or expansion of private development, roadways, utilities, the entity responsible for the construction is also responsible for providing adequate pedestrian access through or around the site as well as signage that provides advance warning to pedestrians and motorists of the closure. Both the MUTCD (Manual on Uniform Traffic Control Devices)^{iv}, NCDOT's Planning and Designing Local Pedestrian Facilities^v, and the ADA (Americans with Disabilities Act)^{vi} stipulate that safe passage should be maintained throughout a temporary closure unless it occurs during an extreme situation such as a natural or man-made emergency. During private construction within Town limits, it is the responsibility of the Town of Bryson City to ensure compliance with these rules by regular monitoring.

The following should be considered whenever a sidewalk or trail will be closed temporarily:

- ◆ *Accessibility for Mobility Impaired Citizens.* At least one accessible route should be provided to transportation or transit facilities; accessible parking areas/spaces; public streets/sidewalks; and public parking areas to an accessible entrance of the building. This route(s) will comply with all other accessibility provisions contained in the ADA regardless of whether they are temporary or permanent. A barrier shall be placed across the full width of the sidewalk or trail to be detectable by a visually impaired person using a cane. An audible information device may be needed in cases where there are especially high traffic volumes challenging a visually impaired person making a street crossing.
- ◆ *Temporary Obstructions.* Parked construction equipment, erosion control fencing, storage of materials/construction debris, and other potential obstructions should be kept away from roadside pedestrian access and pedestrian or multi-use trails so as to keep a permanent passageway open for pedestrians crossing the site. Signs and other devices should not protrude more than 4" into the pedestrian passageway and 7' or less above a sidewalk (8' min. preferred).

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- ◆ *Advance Warning and Signage.* Advance warning may consist of a single sign to a flashing strobe, depending on the nature of the construction or context (such as vehicular volumes) of the work area. Advance signage should be placed so that pedestrians have an opportunity to read the sign and make a safe crossing at a street intersection to the opposite side of the roadway. Smaller, mid-block closures will require fewer treatments, but will still retain the "Sidewalk Closed Ahead Cross Street" advance warning at an appropriate and safe crossing point in advance of the closure, at a minimum.
- ◆ *Route Design.* Temporary traffic barriers like jersey barriers (although not intermittent short sections of jersey barriers) and breakaway bollards should be considered as tools to help delineate a buffer from moving vehicles in areas with high pedestrian traffic volumes and/or to help ensure worker safety.

5.7. Parking Lot Design

Everyone becomes a pedestrian once they park their car, but there are many examples of poor parking lot design. Poor parking lot design at the least will deter customers that may be walking or riding transit to a store, and at the most can create a dangerous safety hazard by increasing pedestrian-vehicle interaction. The most common design issue is that the primary carriageway for vehicles in the parking lot happens to coincide with where the greatest number of pedestrians cross: directly in front of the main entrance. Other issues include poor sight lines to spot pedestrians; bad transition areas from the public domain (e.g., streets) to the private parking area; and inconvenient pedestrian access between parking areas, shops, and adjacent communities. Figure 5-13 indicates a preferred set of suggestions to overcome these common problems. The larger the parking lot, the more vehicles and pedestrians, and therefore the more important it is to carefully design treatments to minimize vehicle-pedestrian interaction. Some suggested treatments:

1. **Parking in the rear.** One way to attract pedestrians to a store and to reduce pedestrian-vehicle interaction is to minimize the amount of parking lot that a pedestrian must walk through to get to the store entrance. This can be done by placing parking in the rear which will reduce travel time for pedestrians approaching the store from the street-front and sidewalk. It will also minimize pedestrian-vehicle interaction by keeping pedestrian customers separate from vehicles by allowing the pedestrian customers to access the store directly from the sidewalk rather than through a parking lot. Parking lots in the rear also create a more attractive streetscape – something that also encourages pedestrian use.

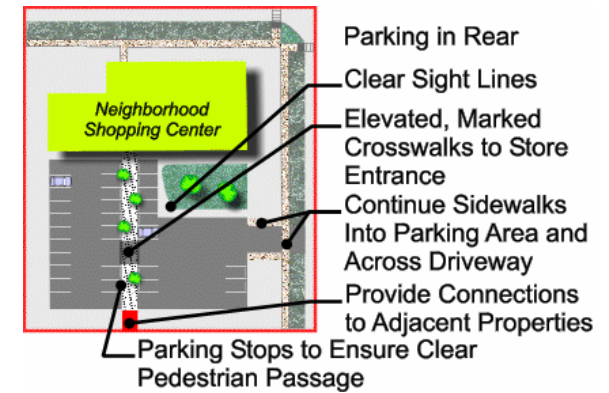


Figure 5-13. An example of pedestrian-friendly parking lot design.

2. **Create safe “landing areas”.** Provide continuous transitions from the street into a safe “landing” area in the parking lot; don’t just “dump” pedestrians into the throat of a driveway.
3. **Maintain good sight lines** at major turning points inside the parking area.
4. **Provide well-marked pedestrian access perpendicular to store fronts.** Whenever possible, provide perpendicular pedestrian access into the front of a high volume land use such as major retail uses. The final crossing to the store entrance(s) should be well-marked, preferably with a raised crosswalk and/or colored demarcations to provide good visual cues to the driver. Moving the main parking aisle away from the principal entrance is another option.
5. **Supply adequate, pedestrian-scale lighting.** Adequate lighting is often perceived as a personal security issue in many large parking areas, and should be provided while avoiding disabling glare (looking into a direct light source and being partially blinded) or causing light pollution to adjoining properties. In order to make customers and pedestrians feel more comfortable, lighting should also be provided at a pedestrian scale. This means lowering the height of some light poles and providing lighting at key locations, such as the entrances and exits to stores, and not just in the parking lots.
6. **Provide awnings.** Especially for some “big box” stores, it is important that the transition for customers from inside the store to the outside be gradual and protected as much as possible from conflicts with vehicles. By providing awnings, a store protects its customers from the rain while allowing for a more comfortable pedestrian environment for customers to window shop and wait for rides or a bus to arrive. This can make a store seem much more comfortable while encouraging customers to remain within the protected awning area and out of conflict with vehicles in the travelway.

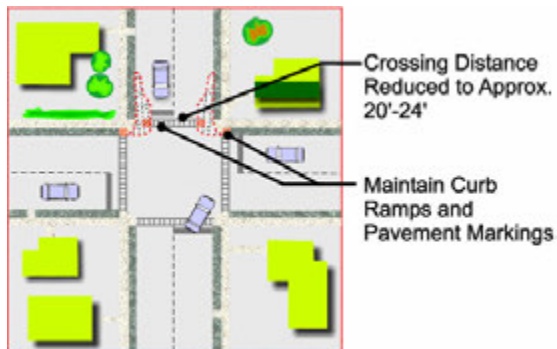


Figure 5-14. An example of bulb-out placement to reduce curb radii and shorten travel distance for pedestrians crossing at an intersection.

Bryson City has several shopping centers and areas with large parking lots, and others may be on the way. It is important that the Town keep the pedestrian's access and safety in mind when reviewing development proposals. Through better design and better design review, the Town will be able to create parking lots that are both convenient for a car and comfortable for a pedestrian.

5.8. Traffic Calming Considerations

Traffic calming is the term used to describe a toolbox of improvements that can be used to “calm”, or slow, traffic along a street, usually in a neighborhood or similar area with low traffic speeds and relatively lower traffic volumes. Although not directly pedestrian-

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related, traffic calming efforts can help to create a safer, more comfortable pedestrian environment by reducing vehicle speeding. Traffic calming comes in a variety of forms. Some of the most common techniques are described in the paragraphs below.

5.8.1. Curb Extensions (Bulb-Outs) and Curb Radii

The primary purpose of bulb-outs is to shorten the distance that pedestrians must travel to cross a street. In addition, they may encourage motorists to drive slower by narrowing the travel lane and reducing vehicular speeds during turning movements at intersections. Motorists will travel more slowly around corners with smaller curb radii even without the use of curb extensions. Landscaping and other aesthetic treatments such as special paving textures should be carefully designed to avoid hazards to drivers and visually-impaired citizens while minimizing maintenance costs. Figure 5-14 shows an example image bulb-out placement to reduce curb radii and make an intersection more pedestrian-friendly.

5.8.2. Medians and Refuge Islands

Figure 5-15 indicates the design and markings associated with refuge islands. Note that pavement markings delineate the approach to the islands; that the islands are “split” to allow for a level platform for wheelchair use; and that in cases where there are wide roads and high traffic volumes, a push-button pedestrian signal may be mounted in the refuge area to allow a pedestrian to split their trip into two halves as they cross the street. Note that the crosswalk on the right side of the diagram is configured at a skewed angle as it crosses the median. This allows pedestrians to have a better angle of sight as they approach and cross each side of the street. In all cases, a minimum 10-foot travel lane is maintained. Sensitivity to large vehicles (buses, trucks and fire equipment) dictates some elements of the median design, curb style, and placement. Median-controlled roadways reduce the number of turning conflicts and are generally preferred for both pedestrians and cyclists over a two-way, left-turn lane (TWLTL) roadway.

5.9. Summary

Pedestrian facility use is a function of a variety of factors, including the connectivity of the facilities, their safety, their convenience, and their comfort. For this reason, pedestrian facility design should be thoughtful and sensitive to the needs of its users. By following the guidelines provided in this section for sidewalk, crossing, and trail design, as well as other items associated with pedestrian facilities, Bryson City should be able to create a built environment that will promote walking and increase the number of pedestrians in the Town.

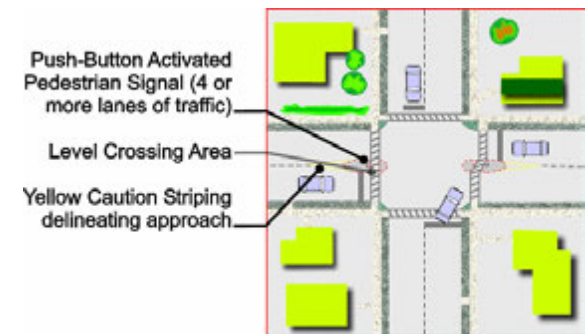


Figure 5-15. An example of well-designed median and refuge islands.

ⁱ Vanguard Company, accessed November, 2005
(<http://www.vanguardonline.com/downloads.asp>)

ⁱⁱ United States Access Board, ADA Accessibility Guidelines Homepage, accessed November, 2005.
(<http://www.access-board.gov/adaag/html/adaag.htm#A4.29.2>)

ⁱⁱⁱ James W. Glock, Letter Correspondence to Regina McElroy, Director, FHWA Office of Transportation Operations, January 11, 2006.

^{iv} *Manual on Uniform Traffic Control Devices for Streets and Highways*, 2003 Edition. Federal Highway Administration, 2003. Especially Sections 6B-1, 6D, 7, and Figures 6H-28, 6H-29, 7A-1, and 7B-4.

^v *Planning and Designing Local Pedestrian Facilities*, North Carolina Department of Transportation Office of Bicycle and Pedestrian Transportation. February, 1997, Chapter 10.

^{vi} Americans with Disabilities Act, US Code 28 CFR Part 36: ADA Standards for Accessible Design. Page 496
(www.usdoj.gov/crt/ada/adastd94.pdf).

This section provides program and policy recommendations to assist in making Bryson City a more pedestrian-friendly community. Recommendations cover the three E's of pedestrian planning: Encouragement, Education, and Enforcement.

Section 6. Program and Policy Recommendations

6.1. Introduction

In addition to physical facilities, it is also important to have strong programs and policies which encourage making the Town more pedestrian-friendly. Programs often fall into one of the four E's of pedestrian planning: Encouragement, Enforcement, Education, and Engineering, and can encourage pedestrian-friendly behavior and more walking in the Town. Similarly, policies can encourage pedestrian-friendly development and design. The following section provides recommendations for some of the programs and policies which the Town can consider implementing to become more pedestrian-friendly.

6.2. Program Recommendations

Pedestrian facilities alone do not make a town pedestrian-friendly; a variety of programs can also be implemented to create and support a pedestrian-friendly culture. A pedestrian-friendly culture has several different characteristics, including the behavior of the people in the town, the attitude of motorists in the town towards pedestrians, and the role of police and other law officials to enforce pedestrian safety. For this reason, programs are often created to fit within the three E's of pedestrian planning: education, encouragement, and enforcement. Education programs teach others about safe pedestrian behaviors, the benefits of walking, and are necessary to assist people in feeling more comfortable with their "new" mode of travel. Education programs can also be used to teach motorists how to interact safely with pedestrians. Encouragement programs, like education programs, can also teach about the benefits of walking and serve to promote walking and pedestrian-friendly behavior through activities and incentives. Finally, enforcement programs provide the "teeth" of creating a safe and legal pedestrian environment. When law enforcement officers and other officials protect pedestrians and encourage walking, this sends a clear message that the presence of pedestrians is a legitimate and permanent condition in the town's transportation network.

The following items are some programs which the Town should consider implementing to create a more pedestrian-friendly environment.

♦ Walkabout/Historic Walking Tour *(Encouragement and Education)*

Walkabouts and Historic Walking Tours are very useful tools for encouraging walking in a Town, and also for improving the marketability and attraction of the Town to visitors. A

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potential walking tour in Bryson City could include stops such as Everett Street, Island Park, the greenway near the Administration Building, and the Bryson City Cemetery. Sometimes historic walking tours are marked by icons painted on sidewalks and streets, or sometimes they are signed. The Town may consider working with the Chamber of Commerce and other visitor-related entities to develop a tour which would encompass a wide variety of attractions. Maps of the tour could be distributed at the Visitor's Center/Chamber of Commerce, Town Hall, Great Smoky Mountain Railroad Depot, and other popular destinations for visitors.

◆ **Safe Routes to School Program** (*Encouragement*)

According to the Federal Highway Administration's website for Safe Routes to School, in 1969, about half of all students walked or bicycled to school. Today, however, over half of all children arrive at school in private automobiles and only 15 percent of all school trips are made by walking or bicycling¹. As a response to these dramatic statistics, the Safe Routes to School Program was designed to create and promote safe walking and cycling to school. Its goals are to improve safety near schools, promote active lifestyles, and reduce pollution and congestion caused by school traffic.

Did you know? In 1969, about half of all students walked or bicycled to school. Today, however, only 15 percent of all school trips are made by walking or bicycling¹.

In North Carolina, the North Carolina Department of Transportation (NCDOT), in conjunction with the Federal Highway Administration, has established the North Carolina Safe Routes to School program, which is a state-wide program to promote safe walking and bicycling to elementary and middle schools in North Carolina. The North Carolina Safe Routes to School program provides opportunities for schools to apply for funding for both programs and capital improvement projects to encourage walking and cycling to school. The program also conducts Safe Routes to School training sessions to be at interested schools to assist with planning and preparation of an extended Safe Routes to School effort at the school. This free training session brings together school administrators, faculty, staff, and representatives from related agencies such as health departments, law enforcement, engineering, and town planning, to educate them about Safe Routes to School, its purpose, and techniques that can be used to create a successful program. Once the training session has been conducted, the school will be prepared to start an effective Safe Routes to School program, and is eligible to apply for grants for both programs and capital improvements.

There are five "E's", or elements, to a Safe Routes to School Program: Engineering, Education, Encouragement, Enforcement, and Evaluation:

¹ FHWA Safety: Safe Routes to School Program. <http://safety.fhwa.dot.gov/saferoutes/>

- ◆ **Engineering** refers to physical improvements such as crosswalks, sidewalks, and signals.
- ◆ **Enforcement** incorporates programs and law enforcement efforts to ensure drivers and pedestrians obey traffic laws and practice safe behavior.
- ◆ **Encouragement** describes programs and other efforts, such as Walk to School Day or a Walking School Bus to entice children, parents, and others to walk or bicycle to school.
- ◆ **Education** identifies methods to teach children, parents, and others about the benefits of walking or bicycling to school and safe behaviors for doing so.
- ◆ **Evaluation** uses measurements or indicators such as the number of children walking or bicycling to school, or the volume of vehicles on nearby school road to determine the success of the SRTS program.

Some of the activities which are often conducted as part of a Safe Routes to School Program include a Walk to School Day and a Walking School Bus. The Walk to School Day program involves designating a single day for students, parents, and teachers to walk or bicycle to school. Students who walk to school are given stickers or a similar award to show that they walked to school, and usually an assembly or some sort of other celebratory activity is planned to highlight the day. A Walking School Bus is similar to a regular school bus in that children travel to school together; however, in the walking school bus, children walk instead of ride the bus to school. The children are escorted by volunteers – usually parents – who collect the children at designated “stops”. The advantage of a Walking School Bus is that it encourages walking to school while also ensuring that children do so safely. It is especially useful for younger children who need to be escorted by an adult. Bryson City should consider implementing these programs to encourage more walking to school.

It is recommended that the Town of Bryson City join with Swain County Public Schools to start a Safe Routes to School program at Swain County Middle School, located within town limits. This program could help to target some of the school's issues with student drop-off at the nearby gas station, and congestion at pick-up. It would also encourage healthier and more active lifestyles in students and their parents.

◆ **Crosswalk/Traffic Safety Stings (Enforcement)**

One of the major issues which was identified during the Plan's preparation process was that of speeding and unsafe pedestrian conditions along Everett Street and Main Street, especially during the summer. One approach to improving safety is for the Bryson City Police Department to conduct pedestrian “stings”. In a sting, a police officer attempts to

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cross the street at a crosswalk. Another police officer is stationed in a vehicle, prepared to stop any motorists who fail to yield or exhibit dangerous behavior. In some Towns, officers have been instructed to provide educational material about pedestrian safety and a motorist's responsibilities to yield at a crosswalk instead of tickets to first-time offenders. Conversely, pedestrian stings could also involve heightened enforcement of jaywalking restrictions.

◆ Promotional/educational materials (*Education*)

A simple way to improve pedestrian safety and to encourage more walking is through the distribution of promotional and educational materials. The purpose of these materials would be to educate Bryson City's citizens about safe walking behaviors, safe driving behaviors around pedestrians, the proper use of pedestrian facilities like pedestrian signals, and the benefits of walking on health and the environment. Materials can also be distributed to outdoor groups and outdoor supply vendors, as well as distributed at town events, kiosks, or major visitor destinations. In addition, materials could be created for distribution to developers which would educate them about pedestrian-friendly design and construction.

A subset of these materials could be designed to particularly target school children and their parents. As discussed in Section 4, children and parents need to be educated about proper safe pedestrian behavior while walking to school, as well as the benefits of a more active lifestyle. The materials which target school children and their parents can be distributed at the start of the school year, through children's handouts and packets, and at any school events (such as Safe Routes to School Day) or at schools' front desks. General education materials can be distributed at various venues throughout Town, and to Town staff, major employers, and future residents.

A second subset of promotional/educational materials could be those targeted to law enforcement officials. These materials would educate law enforcement about the legal rights and responsibilities of pedestrians. For more information about the laws and rights of pedestrians, please see:

- Laws:
http://www.ncdot.org/transit/bicycle/laws/laws_pedlaws.html
- Guide:
<http://www.ncdot.org/transit/bicycle/laws/resources/BikePedLawsGuidebook-Full.pdf>

♦ **Spot Improvement and Maintenance Programs (Engineering)**

Many towns and cities have developed spot improvement and maintenance programs to address small, low-cost needs on a regular basis. Spot improvement programs identify locations of short distances of missing sidewalk and then “fill in the gap” by constructing sidewalk in those places. Similarly, maintenance programs also identify locations of small improvements, typically places where the sidewalk is cracked or crumbled such that it is a hazard, and repair those locations. Although Bryson City has limited funding for this activity, the Town should consider coordinating with its NCDOT Division to develop these programs to address specific small improvements. Some of the top locations that the Town should consider include places where parking lot fronts the roadway and no sidewalk exists, as well as potential connections between and adjacent to parking lots where pedestrian access may be limited. As part of the maintenance program, the town should also consider widening the existing sidewalk in places of high pedestrian traffic – such as in the downtown or along streets leading to the downtown (Main Street, US 19). These improvements would address the needs for pedestrian access in parking lots and wider sidewalk which are discussed in Section 4.

6.3. Policy recommendations

As discussed in Section 1, the Town's main goal is to have sidewalk on all roads in Bryson City, starting with those in the downtown and working out into the whole of Bryson City. To that end, the following policies are designed to help Bryson City guide development and construction of new public facilities (such as new roads or widened roads) to have sidewalk and meet pedestrian requirements.

- ♦ **All roads in Bryson City shall have sidewalk.** In the future all roads in Bryson City will have sidewalk and handicap accessible curb ramps. All signalized intersections will have pedestrian signals and marked crosswalks. As the Town works towards this goal, it should start first with improvements in within the downtown, central Bryson City area and then work outwards towards the greater Bryson City area. At the same time, should new private development or public facility projects be constructed anywhere in the Town, the Town should be sure to require that sidewalk and pedestrian-friendly intersections are designed in conjunction with the new projects.
- ♦ **Require new development to construct sidewalk and connect to existing sidewalk.** While Bryson City has had limited new development in the past, there is

Bryson City Pedestrian Plan

Section 6: Program and Policy Recommendations

still the potential for future development given the town's growth as a major visitor attraction in the Smoky Mountains. When development does occur, the town should have ordinances in place which will require the development to have pedestrian amenities including sidewalks, crosswalks, and connections to existing sidewalk, as well as adjacent developments. This will encourage a more walkable form of development which will both promote a healthier, more active lifestyle among residents and visitors, but also reduce traffic congestion and air pollution.

- ◆ **Consider requiring new development to set aside land for greenways.** Many municipalities require developers to set aside land for greenways as part of their development impact fee for parks and recreation facilities. After the Town adopts a formal pedestrian and greenway plan, it should consider doing the same. In addition, the town should require developers to provide connections to existing greenways as they are constructed to allow for better connectivity in the town's pedestrian system.
- ◆ **Require all new signals to include pedestrian signal heads and crosswalks.** One of the major barriers to pedestrian connectivity in any town is the ability to cross the street. The Town should require all new signals to have pedestrian signal heads and crosswalks to accommodate pedestrians and plan for future conditions in which pedestrians may be walking through the intersection. IN addition, the Town should have a retrofit program to provide pedestrian signal heads for signalized intersections that already exist in town.
- ◆ **Assure Safe Pedestrian Access through Parking Lots.** Parking lots can be some of the most unfriendly locations for pedestrians due to their very nature of being a place where both vehicles and pedestrians mix. When new development is constructed, or existing development is renovated, parking lots should be evaluated for appropriate pedestrian-safe design. Some features that should be included are pedestrian refuges, sidewalk, and pedestrian-only access from the parking lot to store fronts. Parking lot requirements should be flexible enough to allow for unconventional locations for off-street parking, such as behind a building, or shared with another commercial development. For a more thorough description of appropriate parking lot design, see *Section 5.7 Parking Lot Design*.
- ◆ **Encourage mixed-uses and other pedestrian-friendly developments.** The Bryson City Land Use Plan and any zoning ordinances should require development

patterns that are conducive to walking. Although growth in Bryson City is likely to be limited, the Town should implement policies which guide growth, no matter how large or small, such that new developments are within walkable distances to other destinations and encourage walking, over driving a vehicle, in the community.

6.4. Summary

By implementing these programs and policies, along with the recommended projects, Bryson City should be on its way to becoming a more pedestrian-friendly location. While the projects will provide the facilities necessary to walk safely in the Town, the programs and policies will work to encourage more pedestrians and increase walking in the Town. The dual approach of projects, plus programs and policies, will create a strong effort to make Bryson City a pedestrian-friendly place.

This section contains a summary of the recommended projects contained in the Plan, preliminary cost estimates, and recommended prioritization approaches.

Section 7. Project Development

7.1. Introduction

The first half of this Plan outlines the existing conditions in Bryson City and identifies the pedestrian needs that should be addressed and potential focus areas where improvements should be made. This section builds upon these findings to identify a set of specific projects to improve the pedestrian system in Bryson City. Section 7.2 discusses each focus area and its specific recommendations and projects. Section 7.3 provides general cost estimates for each project, and Section 7.4 specifies recommended priorities.

7.2. Projects

In Section 4, recommendations were made for specific focus areas throughout Bryson City. The following paragraphs match projects to those recommendations in order to address the needs of the focus area. Each project is assigned a reference number for easier identification on maps and in cost estimating. **The reference numbers do not indicate priority.**

7.2.1. Downtown Bryson City Area

Recommendations in Section 4 for the downtown Bryson City area addressed improving pedestrian safety and convenience at several major intersections, and developing better pedestrian connections between Island Park and other major destinations. In specific, the recommendations were:

- ◆ Improve the intersection of Everett and Main Street and the intersection of Academy Street and Rector Streets for better pedestrian convenience and safety
- ◆ Provide sidewalk along Academy Street
- ◆ Provide sidewalk along Fryemont Street
- ◆ Provide pedestrian connection from Island Park to the Train Depot and Island Park to Downtown

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The following projects have been identified to address these recommendations:

Reference Number	Project	Opportunities	Constraints
1	Improve signal timing for pedestrian crossings at the intersection of Everett and Main Streets	♦ Provide a longer time for pedestrians to cross, thereby creating a more convenient crossing	♦ may have an effect on traffic flow on the intersection
2	Install crosswalk striping for pedestrian crossings at the intersection of Academy and Rector Streets	♦ Raise visibility of pedestrians and the potential for pedestrian crossings at this location	♦ None excluding cost
3	Construct pedestrian connection from Island Park to the Train Depot by constructing sidewalk along Ramseur Street and Bryson Street	♦ Provide a more visible connection for visitors to draw them to other attractions in the town besides the Train Depot ♦ Create a safer route from the Train Depot to Island Park by separating the pedestrian pathway from the vehicle travelway	♦ May require additional right of way purchase
4	Install sidewalk along Academy Street	♦ Create safe pedestrian access along the street	♦ May require additional right of way purchase
5	Install sidewalk along Island Street	♦ Create safe pedestrian access along the street ♦ Highlight connection between downtown and Island Park	♦ Parts of Island Street are in a flood zone – depending on requirements this may limit construction
6	Construct sidewalk along Fryemont Street	♦ Create safe pedestrian access along the street	♦ May require additional right of way purchase



Figure 7-1. Missing sidewalk on Academy Street.

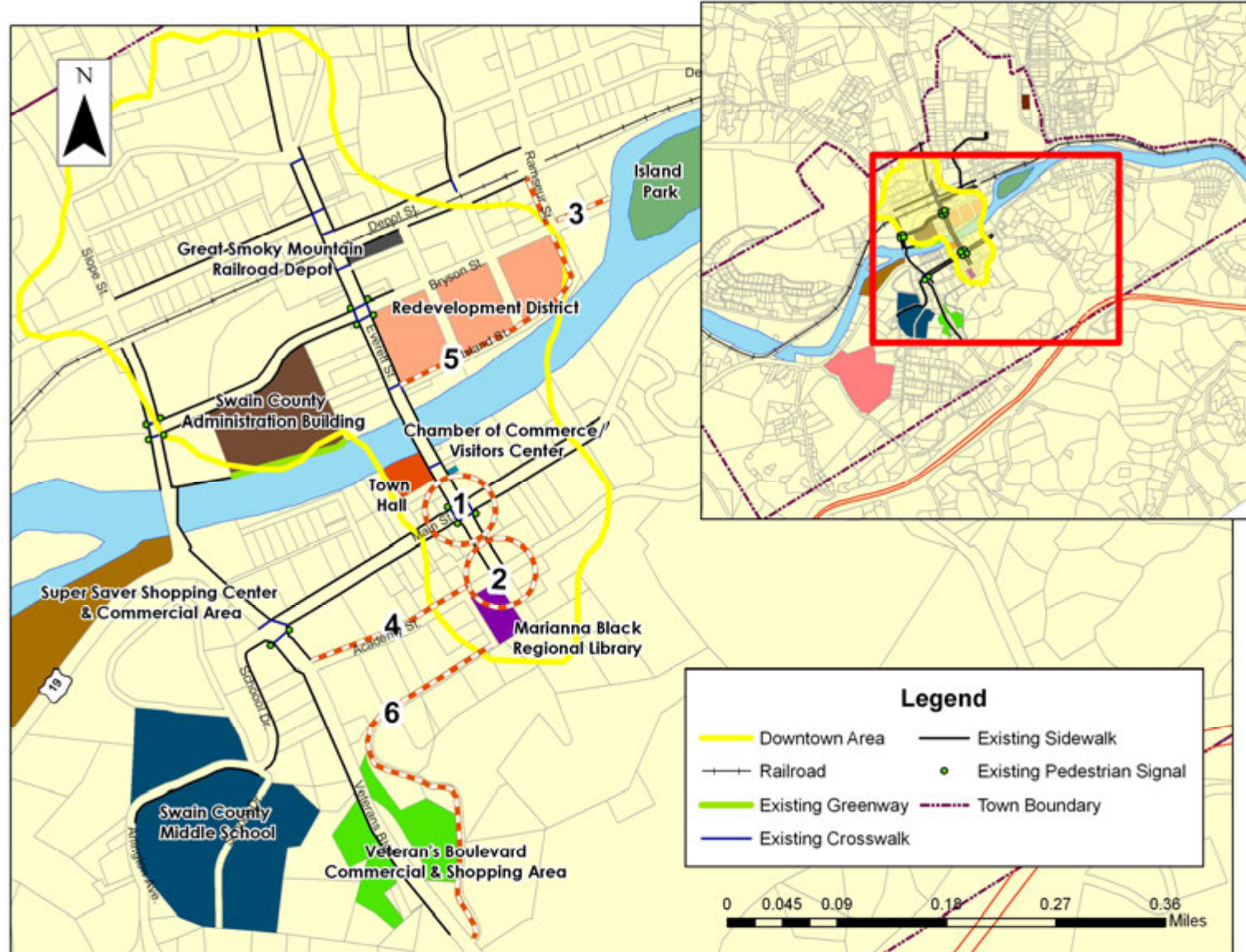


Figure 7-2. Map of proposed projects in the Downtown Bryson City Focus Area.

7.2.2. Super Saver Commercial Area

Recommendations for the Super Saver Commercial Area focused on improving pedestrian access from downtown by providing safer, more convenient connections and intersections. The following are the specific recommendations from Section 4:

- ◆ Provide a direct pedestrian connection from downtown to Super Saver Shopping Center
- ◆ Improve pedestrian safety and accessibility at the intersection of Slope Street/Veteran's Boulevard and HWY 19

The following projects have been identified to address these recommendations:

Reference Number	Project	Opportunities	Constraints
7	Construct sidewalk from downtown to Super Saver Shopping Center	◆ Create safe pedestrian access along the street	◆ May require additional right of way purchase
8	Provide pedestrian signals for intersection of Slope Street/Veteran's Boulevard and HWY 19	◆ Provide safer, more convenient pedestrian crossing for access from downtown to the commercial area nearby	◆ None excluding cost



Figure 7-3. The view approaching the Super Saver shopping Center. The sidewalk ends after the Pizza Hut.

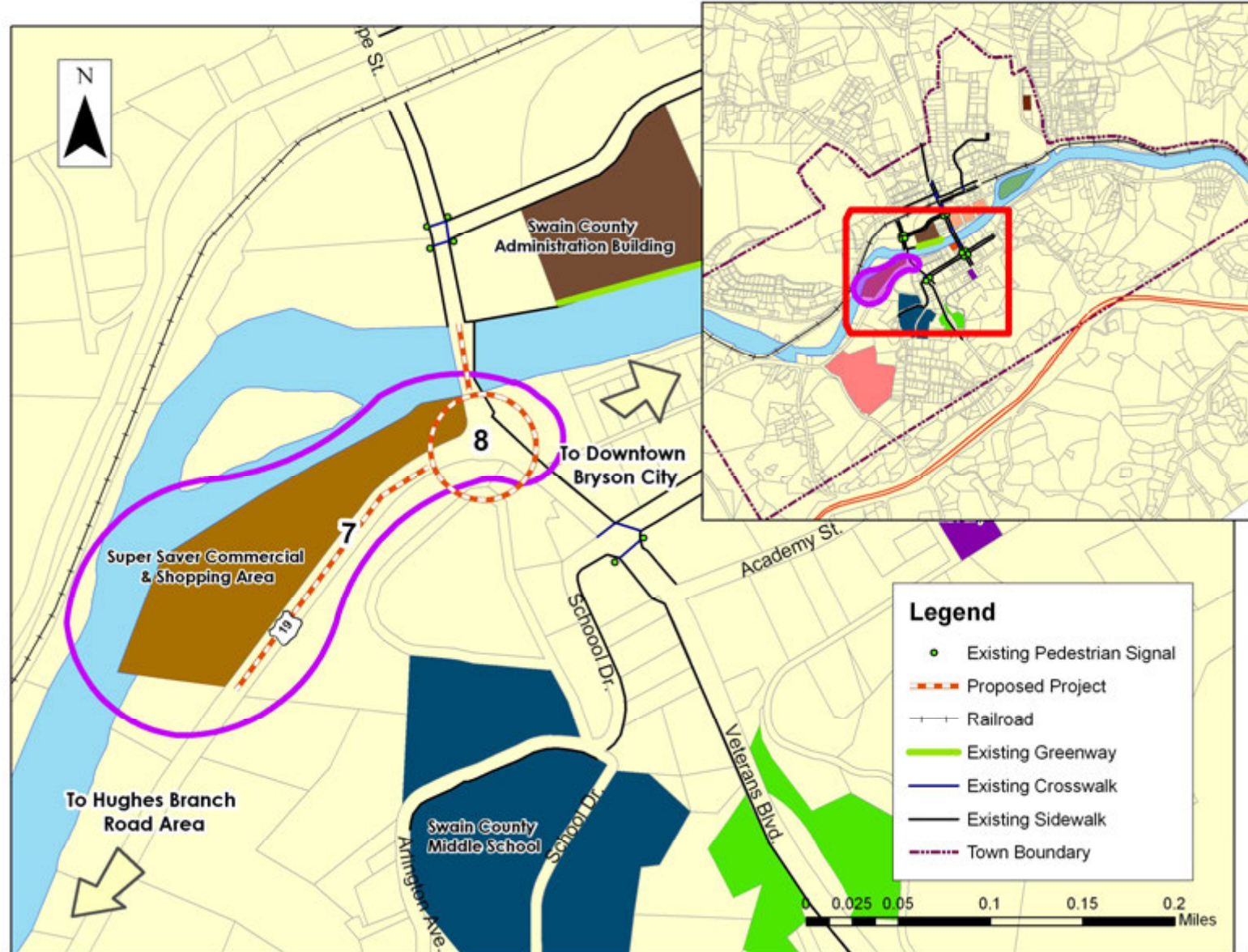


Figure 7-4. Map of proposed projects in Super Saver Commercial and Shopping Area for Bryson City.

Bryson City Pedestrian Plan

Section 7: Project Development

7.2.3. Veterans Boulevard Area

Section 4's recommendations for this area focused on improving pedestrian safety and connectivity to destinations along busy Veteran's Boulevard:

- ◆ Provide pedestrian signals and crosswalks at Veteran's Boulevard and Main Street – pedestrian signals exist for crossing Veteran's Boulevard on the south side of Main Street but should be provided in all four directions
- ◆ Improve pedestrian safety along Veteran's Boulevard and for pedestrian crossing the Boulevard

Specific projects to address these recommendations are:

Reference Number	Project	Opportunities	Constraints
9	Provide pedestrian signals and crosswalks in all directions at intersection of Veteran's Boulevard and Main Street	◆ Provide safer, more convenient pedestrian crossing for access from downtown to Veteran's Boulevard commercial development and the Schoolhouse Hill area	◆ None excluding cost
10	Provide Pedestrian warning signage along Veteran's Boulevard	◆ Notify motorists of presence of pedestrians to create a safer environment for pedestrians	◆ None excluding cost
11	Consider signalized mid-block crossing along Veteran's Boulevard	◆ Provide safer, more convenient pedestrian crossing from one side of Veteran's Boulevard to another	◆ None excluding cost
12	Construct sidewalk on the northeast side of Veteran's Boulevard	◆ Provide safer pedestrian access for the northeast side of Veteran's Boulevard.	◆ May require additional right of way

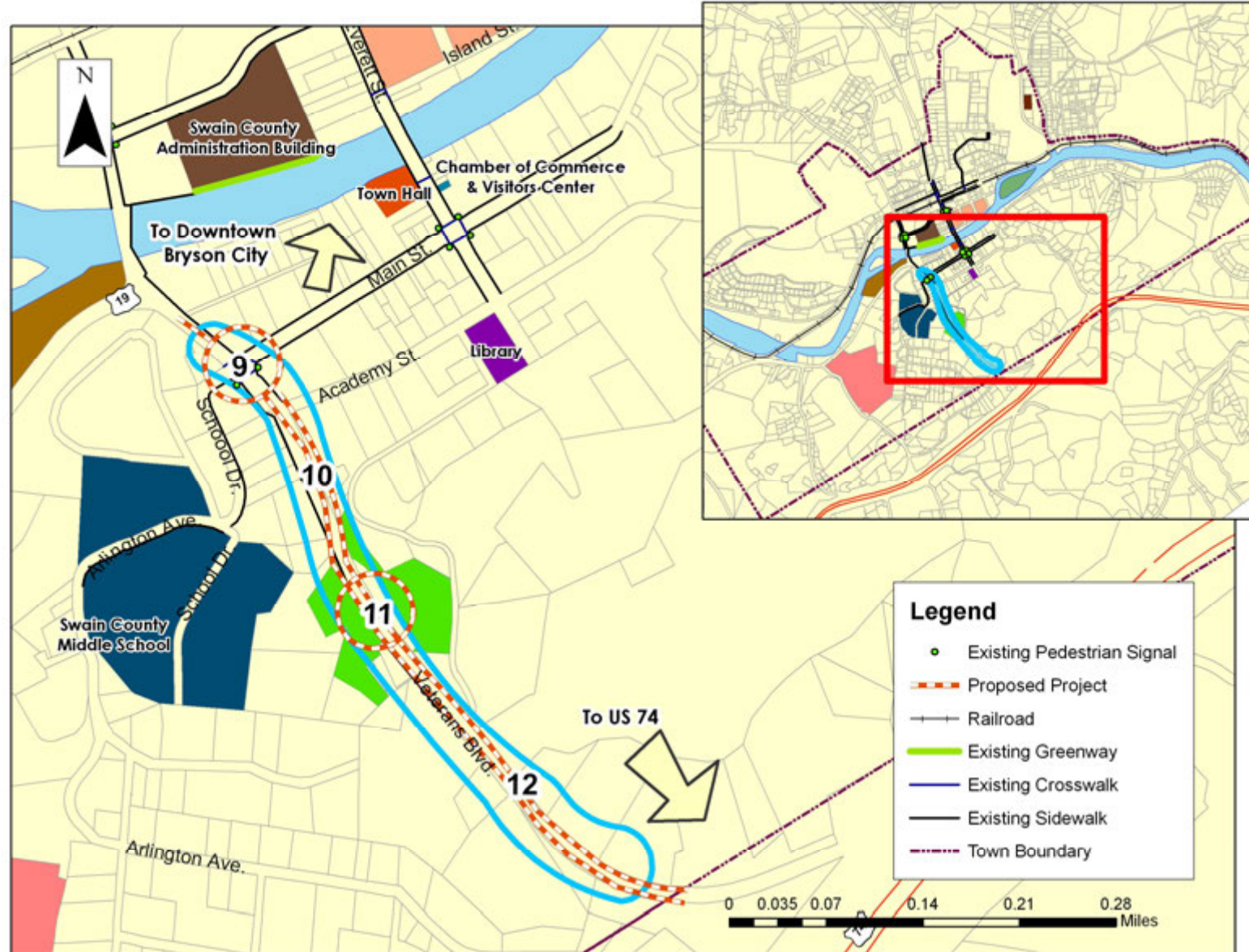


Figure 7-5. Map of proposed projects along Veteran's Boulevard in Bryson City.

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Section 7: Project Development

7.2.4. Schools Area: Swain County Middle School and Swain County High School

Recommendations for this area focused on improving pedestrian safety for students and parents around Swain County Middle School and Grace Academy. In specific, the recommendations for this area are:

- ◆ Provide safer pedestrian access to Swain County Middle School
- ◆ Improve pedestrian access and safety along School Drive
- ◆ Provide complete access along Fontana Road to Swain County High School

Projects to address these recommendations are:

Reference Number	Projects	Opportunities	Constraints
13	Provide signed and marked pedestrian crossings from sidewalk on School Drive for access to Swain County Middle School	◆ Raise visibility for children crossing the street, thereby improving safety	◆ None excluding cost.
14	Widen existing sidewalk along School Drive to Swain County Middle School	◆ Will accommodate greater volumes of pedestrians walking on the sidewalk to allow for a more comfortable walking environment	◆ May require additional right of way purchase. Also constrained by steep slopes and potentially rugged terrain
15	Construct sidewalk from Downtown Bryson City to Swain County High School along Fontana Drive	◆ Provide pedestrian access from Bryson City to Swain County High School	◆ May require additional right of way purchase ◆ May be constrained by steep slopes

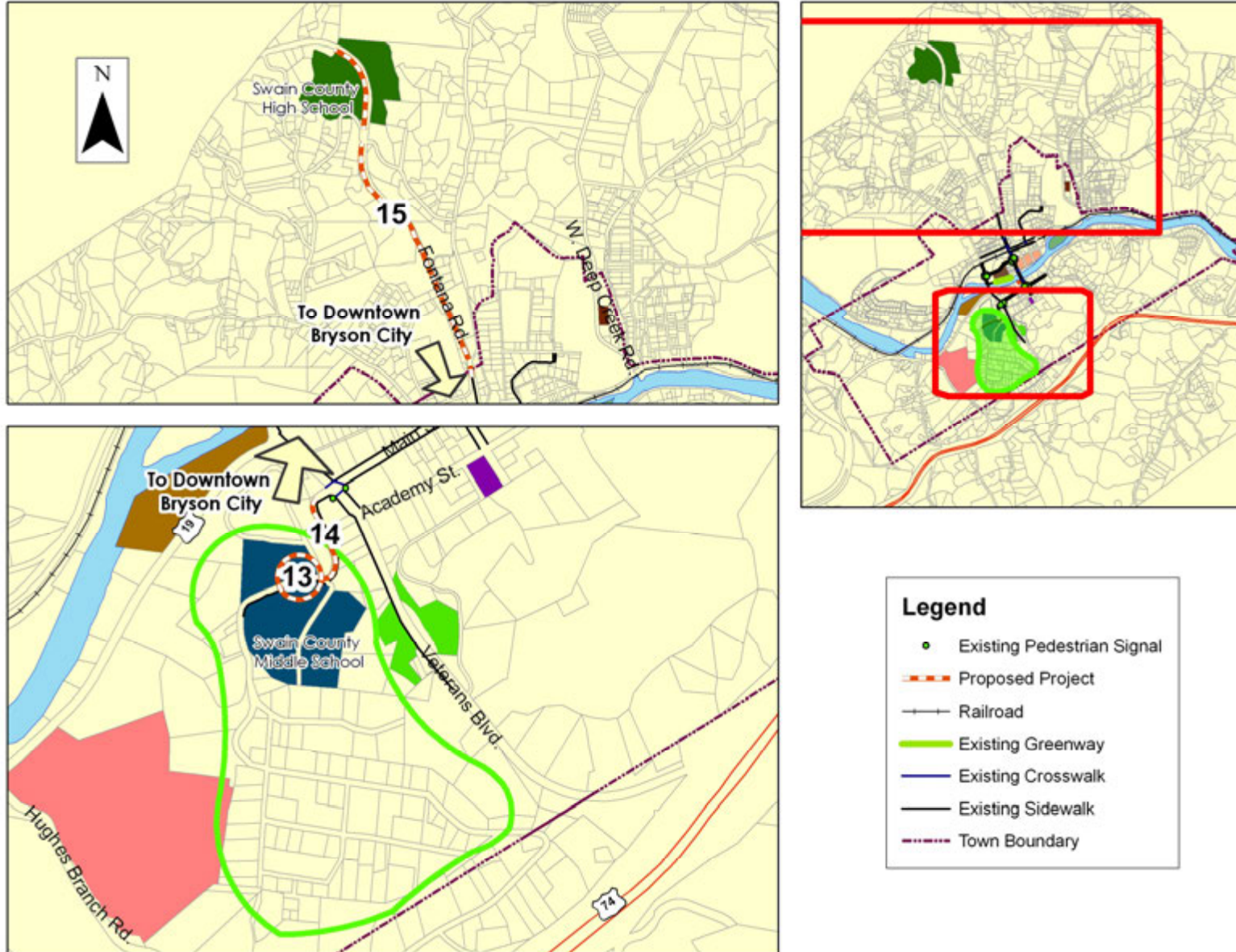


Figure 7-6. Map of proposed projects near schools in Bryson City.

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7.2.5. Hughes Branch Road/HWY 19 Intersection Area

Recommendations from Section 4 for the Hughes Branch Road/HWY 19 Intersection focus on improving pedestrian safety and access to the area from downtown. The recommendation stated in Section 4 is as follows:

- ◆ Provide safe pedestrian connection and crossings from Downtown to the intersection of Hughes Branch Road and US 19 and the Ingles/commercial area nearby

Projects to address this recommendation are:

Reference Number	Project	Opportunities	Constraints
16	Provide Sidewalk along Hughes Branch Road and US 19 to Ingles/commercial area	◆ Provide safe, convenient pedestrian access from downtown to Ingles Shopping Center and Commercial Area	<ul style="list-style-type: none"> ◆ Length may make additionally costly ◆ May require additional right-of-way purchase ◆ One side of US 19 may have steep slopes and be close to the Tuckaseegee River, thereby making sidewalk construction difficult
17*	Install pedestrian signals at the intersection of Hughes Branch Road and US 19	◆ Provide safer, more convenient pedestrian crossing of US 19 to the Ingles Shopping Center and Commercial Area	◆ Currently the intersection is unsignalized, although it is under study to be signalized. Pedestrians signals should not be installed until traffic signals are installed

*Note: NCDOT TIP Project No. R-4751 is planned for US 19 from the intersection of US 19 and Hughes Branch Road to US 441 in Cherokee.

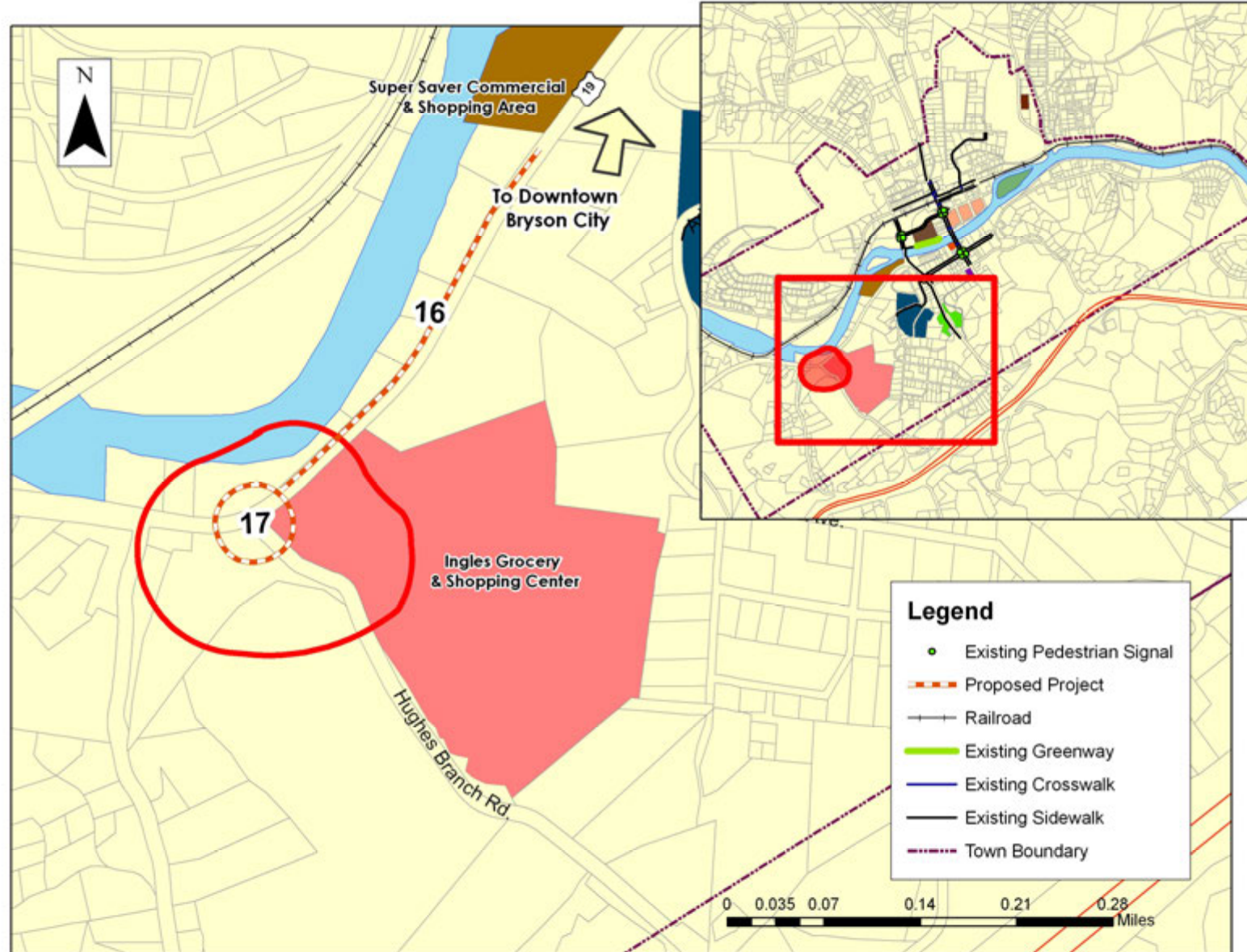


Figure 7-7. Map of proposed projects in the Hughes Branch Road Focus Area for Bryson City.

7.2.6. *Deep Creek Recreation Center Area*

Similar to the recommendations for the other focus areas, the recommendation for the Deep Creek Recreation Center area addresses improving pedestrian safety and access from downtown:

- ◆ Provide safe pedestrian connection and crossings from Downtown to the Deep Creek Recreation Center

Specific projects to address this recommendation are:

Reference Number	Project	Opportunities	Constraints
18	Construct sidewalk connection to Deep Creek Recreation Center to Downtown	<ul style="list-style-type: none">◆ Provide safe pedestrian access from downtown to recreation center	<ul style="list-style-type: none">◆ May require additional right of way purchase◆ May have steep slopes on one side of roadway

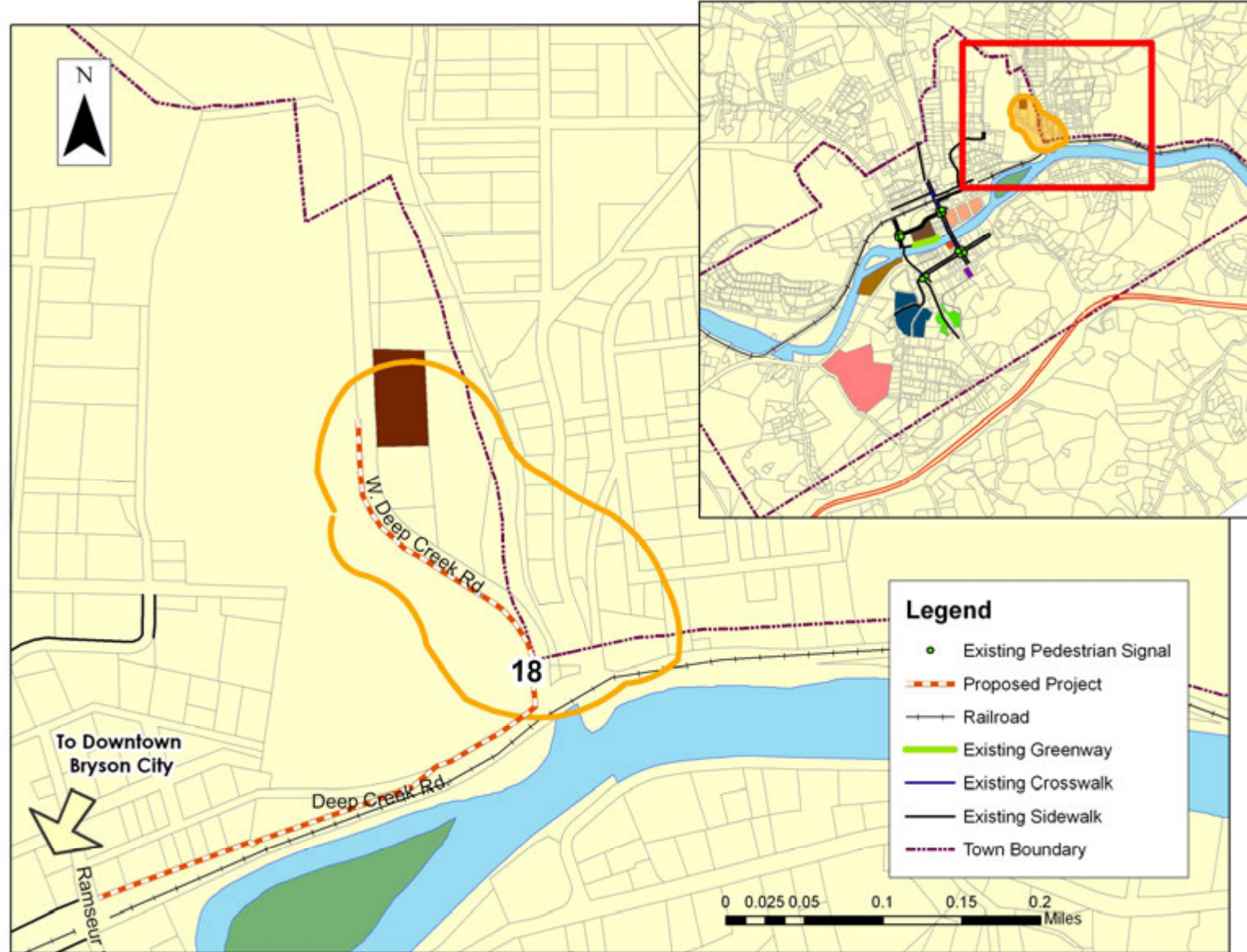


Figure7-8. Map of proposed project in Deep Creek Focus Area in Bryson City.

7.3. Cost Estimates

Table 7-1 provides very general cost estimates for the projects identified for each focus area. Cost estimates were based on an estimated unit cost for each project as follows:

For Sidewalk Projects: \$55 per linear foot
For Greenway Projects: \$400,000 per mile

Cost of installation of one pair of pedestrian signals: \$7,000
Cost of installation of one high-visibility crosswalk: \$1,200

Cost estimates also include an almost 70 percent mark-up for mobilization, utilities, and labor. Cost estimates do not take into account purchase of right-of-way or structure construction. These cost estimates were based on an average of cost estimates for the state of North Carolina, and may change with time, location, and cost of raw materials. **Prior to commitment to design or construction of any of these projects, the Town should conduct a thorough analysis to identify more current cost estimates.**

Table 7-1. Cost Estimates for proposed projects.

Reference Number	Project	Estimated Length (miles)	Estimated Cost	
Focus Area: Downtown Bryson City Area				
1	Improve signal timing at Everett & Main Streets	NA	Minimal*	\$
2	Crosswalk striping at Academy & Rector Streets	NA	\$4,000	\$
3	Sidewalk from Island Park to the Train Depot along Ramseur Street and Bryson Street	0.09	\$44,000	\$
4	Sidewalk along Academy Street.	0.14	\$70,500	\$\$
5	Sidewalk along Island Street	0.21	\$100,700	\$\$
6	Sidewalk along Fryemont Street.	0.35	\$170,000	\$\$\$
Focus Area: Super Saver Commercial Area				
7	Construct sidewalk from downtown to Super Saver Shopping Center	0.31	\$148,941	\$\$\$
8	Provide pedestrian signals for intersection of Slope Street/Veteran's Boulevard and HWY 19	NA	\$27,347	\$
Focus Area: Veteran's Boulevard Area				
9	Provide pedestrian signals at intersection of Veteran's Boulevard and Main Street;	NA	\$27,347	\$
10	Provide Pedestrian warning signage along Veteran's Boulevard;	NA	\$27,347	\$
11	Consider signalized mid-block crossing along Veteran's Boulevard	NA	\$27,347	\$
12	Construct sidewalk on the southwest side of Veteran's Boulevard	0.49	\$240,000	\$\$\$

*Signal timing improvements should be coordinated with NCDOT Division and are usually conducted at no or minimal cost to the municipality.

**This project will require additional study and design before a representative cost estimate can be determined.

Table 7-1 continued. Cost Estimates for proposed projects.

Reference Number	Project	Estimated Length (miles)	Estimated Cost	
Focus Area: Schools Areas - Swain County Middle School & Swain County High School				
13	Provide signed and marked pedestrian crossings from sidewalk on Arlington Road and School Drive for access to Swain County Middle School;	NA	\$13,674	\$
14	Widen existing sidewalk along School Drive to Swain County Middle School	*	*	*
15	Construct sidewalk from Downtown Bryson City to Swain County High School along Fontana Drive.	1.15	\$556,000	\$\$\$
Focus Area: Hughes Branch Road/HWY 19 Intersection Area				
16	Provide Sidewalk along Hughes Branch Road and US 19 to Ingles/commercial area; and,	0.31	\$152,000	\$\$\$
17	Install pedestrian signals at the intersection of Hughes Branch Road and US 19.	NA	\$27,347	\$
Focus Area: Deep Creek Recreation Center				
18	Construct sidewalk connection to Deep Creek Recreation Center to Downtown; and,	0.54	\$260,000	\$\$\$

7.4. Priority

Tables 2, 3, and 4 prioritize the proposed projects for each Focus Area based on a short-term, mid-term, and long-term schedule. Short-term represents a project which should be addressed within the next five years from the Plan's completion. Mid-term projects should be addressed within six to ten years, and long-term projects are those that are most likely to happen beyond ten years of the Plan's completion. Prioritization for projects was based on a series of criteria, including the following:

- ♦ **Safety** – refers to actual or perceived threat to pedestrian safety by nearby traffic. It was qualitatively measured by the crashes near a location or a perceived sense of danger by users as reported by staff, stakeholders, and the public during open houses and the survey.

- ◆ **Demand** – indicates the amount of perceived potential use for the project either as a result of input by the stakeholders, public, or staff, or by observations during field visits. A location had a high level of need/demand if many comments were made about it at the open house or on the survey, or if several pedestrians were observed at the location during field visits.
- ◆ **Accessibility/Connectivity** – refers to the potential for the project to complete a route from one major destination to another. Frequently, the project may connect two existing blocks of sidewalk that were previously separated by a long stretch of missing sidewalk.
- ◆ **Ease of Construction** – qualitatively measures how feasible it will be to construct a project. While this factor takes into account cost to a certain extent, it also includes necessary right-of-way, permitting, and potential negotiations with other agencies and jurisdictions that may be necessary to construct the project.
- ◆ **Cost** – qualitatively measures the potential cost of a project, mostly as a function of length alone.
- ◆ **Recreation vs. Transportation Need** – indicates whether a project will serve a transportation or recreation need. A transportation need is one that connects users from one major destination to another, especially if they are necessary destinations such as grocery stores, banks, and work locations. A project which serves a recreation need is one that is solely intended for use as an exercise route, such as a loop trail around a lake, or a scenic destination.

Items that had the greatest priority were those that would meet an immediate safety need or alleviate some hazardous situation, were easily constructible, and met an existing demand for facilities. Projects were also given more priority if they served a specific transportation need or completed a necessary route to a destination. As a result, short-term projects are those that can be quickly completed or alleviate an immediately hazardous situation. Long-term projects were those designed more for recreation over transportation, and may require additional planning to acquire right-of-way and adequate budget. Figure 7-9 shows a map of the projects and their priorities.

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Section 7: Project Development

Table 7-2. *Short Term Priorities (0 - 5 years)*

Reference Number	Project	Criteria
1	Install striping and pedestrian signals for pedestrian crossings at the intersection of Everett and Main Streets	Safety Demand Accessibility/Connectivity
2	Install striping for pedestrian crossings at the intersection of Academy and Rector Streets	Safety Demand Accessibility/Connectivity
7	Construct sidewalk from downtown to Super Saver Shopping Center and commercial area	Safety Demand Accessibility/Connectivity
8	Provide pedestrian signals at intersection of Veteran's Boulevard and Slope Street	Safety Demand Accessibility/Connectivity
9	Provide pedestrian signals at intersection of Veteran's Boulevard and Main Street	Safety Demand Accessibility/Connectivity
16	Sidewalk along US 19 to Hughes Branch Road from Downtown	Safety Demand Accessibility/Connectivity
17	Install pedestrian signals at the intersection of Hughes Branch Road and US 19	Safety Demand Accessibility/Connectivity

Table 7-3. *Mid-Term Priorities (6 - 10 years)*

Reference Number	Project	Criteria
3	Construct pedestrian connection from Island Park to the Train Depot with sidewalk along Ramseur Street and Bryson Street	Demand Accessibility/Connectivity
10	Provide pedestrian warning signage along Veteran's Boulevard	Safety
13	Provide signed and marked pedestrian crossings from sidewalk on School Drive for access to Swain County Middle School	Safety
18	Construct sidewalk connection to Deep Creek Recreation Center to Downtown; and,	Safety Transportation Accessibility/Connectivity

Table 7-4. *Long-Term priorities (10 or more years)*

Reference Number	Project	Criteria
4	Sidewalk along Academy Street	Safety Accessibility/Connectivity
5	Sidewalk along Island Street	Safety Accessibility/Connectivity
6	Sidewalk along Fryemont Street	Safety Accessibility/Connectivity
11	Consider signalized mid-block crossing along Veteran's Boulevard	Safety Accessibility/Connectivity
12	Construct sidewalk on the northeast side of Veteran's Boulevard	Safety Accessibility/Connectivity
14	Widen existing sidewalk along School Drive to Swain County Middle School	Safety Accessibility/Connectivity
15	Construct sidewalk from Downtown Bryson City to Swain County High School along Fontana Drive.	Safety Accessibility/Connectivity

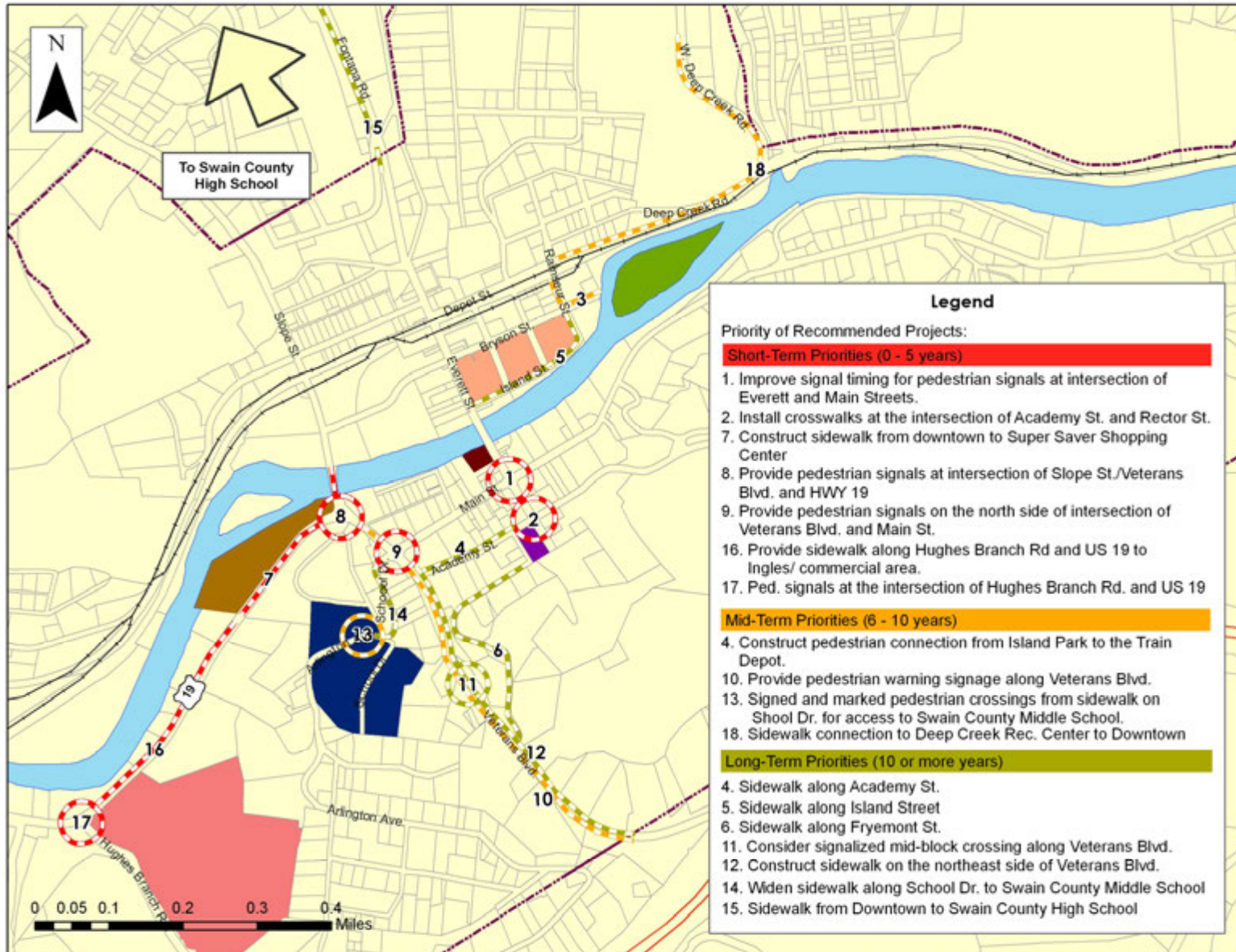


Figure 7-9. Map of Bryson City showing projects and their priorities.

7.5. Summary

There are many short-term priorities, however many of the projects are easy and inexpensive to complete. Others, such as the a mid-block crossing on Veteran's Boulevard, are more complex, but should remain as urgent priorities due to the safety concerns at the location. While the priorities should help guide Bryson City's approach to making improvements, the Town should not overlook the opportunity to make progress on long-term projects even if it may occur before the completion of short-term projects.

This section provides a brief review of potential funding sources for Bryson City to consider, and an implementation plan with short-, mid-, and long-term actions for the recommendations in the Plan.

Section 8. Implementation and Funding

8.1. Introduction

In order for Bryson City to move forward with the next steps in becoming a more pedestrian-friendly town, it must have the priorities and the funding available for it to proceed. This section addresses potential funding sources that the Town could consider for some of the proposed projects. An implementation plan is also provided which identifies potential partners for the recommendations, and a schedule for implementing them.

8.2. Funding

Pedestrian facilities are constructed – and therefore funded – through a number of avenues. Funding can be divided into four categories: local, state, national, and private funding. The following paragraphs describe some of the more prominent sources in each category. Bryson City should tap into all of these sources, and search for others as well, in order to take advantage of the funds available.

8.2.1. Local Funding

Currently, Bryson City does not have an annual budget line item specifically for pedestrian improvements. In the future, Bryson City may wish to consider creating a specific annual budget item to set aside funds for improving pedestrian facilities. A specific budget item is the most direct way to ensure that funding for pedestrian facilities is available, but sometimes a town's budget may be too limited to finance this. Another way for pedestrian facilities to be built is to ensure that they are constructed with any new town projects or improvements, such as parks and recreation facilities, libraries, schools, and new roads. In addition, future private development should be reviewed for adequate pedestrian access and connections. As discussed in the policy recommendations of *Section 6: Programs and Policy Recommendations*, this may mean the Town should require developers to install sidewalk with new construction. The Town should also consider teaming with other organizations that may have their own projects in Bryson City, such as the Partnership for Bryson City, the Great Smoky Mountain Railroad, the Visitor's Bureau, and the North Carolina Department of Transportation.

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Section 8: Implementation and Funding

8.2.2. State and National Funding

Bryson City should also consider reaching out to state and national funding sources for assistance in constructing pedestrian facilities. State and national funding are a combined category because many of the state entities administer national funds.

The North Carolina Department of Transportation (NCDOT) is the single largest source of funding available to Bryson City for pedestrian facilities, with the following potential funding sources:

- ◆ State Transportation Improvement Program
 - Incidental Projects
 - Independent Projects
- ◆ Transportation Enhancement Program
- ◆ Spot Improvement Program
- ◆ Small Urban Funds
- ◆ Hazard Elimination Program
- ◆ Governor's Highway Safety Program
- ◆ Statewide Discretionary Funds

NCDOT also administers the Safe Routes to School Program, which is federally funded. The following paragraphs provide a more thorough description of each of the various funding sources.

- ◆ **State Transportation Improvement Program (STIP)** – This program is the overall funding source for study, design, and construction of major transportation projects, including pedestrian facilities, in the state. Frequently, projects funded by the STIP are also partly funded by other sources, including matching funds from local municipalities. Pedestrian facilities are eligible for funding from this program as independent projects separate from a roadway construction, widening, or some other sort of roadway work, but one of the most cost-effective and efficient ways to gain funding for pedestrian facility construction is to incorporate them as incidental to a larger project. Overall, most pedestrian accommodations within the state are made as incidental improvements. For more information on the TIP process, see:

www.ncdot.org/transit/bicycle/funding/funding_TIP.html.

For NCDOT's Pedestrian Policy Guidelines, please see:

www.ncdot.org/transit/bicycle/laws/laws_pedpolicy.html.

For NCDOT's Greenway Policy, please see the following link at the Division of Bicycle and Pedestrian Transportation's website:

www.ncdot.org/transit/bicycle/laws/laws_greenways_admin.html.

- ◆ **Transportation Enhancement Program** - Transportation enhancements are transportation-related activities that are designed to strengthen the cultural, aesthetic, and environmental aspects of transportation systems. The transportation enhancements program provides for the implementation of non-roadway capacity improvement projects, including bicycle and pedestrian facilities; landscaping; and aesthetic improvements. Various forms of pedestrian facilities, such as sidewalks, separate walking paths and trails, and pedestrian safety training and related materials are eligible for funding.
- ◆ **Spot Improvement Program** - The NCDOT Bicycle and Pedestrian Transportation Division budgets \$500,000/year for "spot" safety improvements throughout the State. These improvements include items such as signing, grate replacement, bike rack installations, hazard remediation at skewed railroad crossings, and other small-scale improvements. The Spot Improvement Program is used only for bicycle and pedestrian projects; however, it should not be viewed as a priority source for funding identified projects. It is typically used for small-scale and special-situation projects that are not of a significantly large enough scale to merit being a TIP project. Taking these requirements into consideration, proposals for projects should be submitted directly to the Bicycle & Pedestrian Transportation Division.
- ◆ **Small Urban Funds** – Small Urban Funds are available for small improvement projects in urban areas. Each NCDOT Highway Division has \$2 million of small urban funds available annually. Although not commonly used for bicycle facilities, local requests for small bicycle projects can be directed to the NCDOT Highway Division office for funding through this source. A written request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, etc. for thorough review.
- ◆ **Hazard Elimination Program** – This program focuses on projects intended for locations that should have a documented history of previous crashes. Bicycle and pedestrian projects are eligible for this program, although the funds are not usually used for this purpose. This program is administered through the NCDOT Division of Highways. Similar to the Small Urban Funds, it is a significantly limited funding source.

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- ◆ **Powell Bill Funds** – Powell Bill Funds can be used for streets which are not state-maintained. Sidewalk and bikeway construction are eligible activities as well. For more information, see:
www.ncdot.org/financial/fiscal/ExtAuditBranch/Powell_Bill/powellbill.html
- ◆ **Governor's Highway Safety Program (GHSP)** – This program provides funding for projects which show that they will make substantial progress in reducing crashes, injuries and fatalities at a specific location. All funding is considered to be “seed money” to get programs started – the grantee is expected to provide a portion of the project costs and to continue the program after GHSP funding ends. Projects are only approved for one full or partial federal fiscal year at a time; however, projects may be funded for up to three consecutive years. Amounts of GHSP funds vary from year to year, according to the specific amounts requested.
- ◆ **Statewide Discretionary Funding** - The Statewide Discretionary Fund consists of \$10 million and is administered by the Secretary of the Department of Transportation. This fund can be used on any project at any location within the state. Primary, urban, secondary, industrial access, and spot safety projects are eligible for this funding. To request funding, an agency must submit a written request to the NCDOT Highway Division office with a clear description of project and project justification.
- ◆ **Sidewalk Program** – Each year, a total of \$1.4 million in STP-Enhancement funding is set aside for sidewalk construction, maintenance and repair. Each of the 14 highway divisions across the state receives \$100,000 annually for this purpose. Funding decisions are made by the district engineer. Prospective applicants are encouraged to contact their district engineer for information on how to apply for funding.
- ◆ **Safe Routes to School Program** – The federal Safe Routes to School Program is intended to create and promote safe walking and cycling in order to improve safety near schools, promote active lifestyles, and reduce pollution and congestion caused by school traffic. In North Carolina, NCDOT, in conjunction with the Federal Highway Administration, has established the North Carolina Safe Routes to School program. The North Carolina Safe Routes to School program provides opportunities for schools to apply for funding for both programs and capital improvements projects to encourage walking and cycling to school. For more information about the Safe Routes to School Program, please see the North Carolina Safe Routes to School's webpage at: www.ncdot.org/programs/saferoutes.

- ♦ **Community Development Block Grants (CDBG)** – CDBG funding is intended to help communities provide housing, create suitable living environments, and expand economic opportunities primarily in low- and medium-income areas. Bryson City could use these grant funds for recreation facilities and planning. It should be noted that CDBG Funds are highly competitive and the requirements are extensive. For more information, please see:
www.hud.gov/offices/cpd/communitydevelopment/programs

8.2.3. *Private Funding*

Bryson City can consider a variety of private funding sources. Contrary to NCDOT and federal funding, most private funding sources offer limited grants which are aimed at establishing programs and conducting projects on a smaller scale to encourage more walking. Many of the funding sources, such as the Blue Cross Blue Shield Fit Together Grants and the Robert Wood Johnson Foundation Active Living By Design Awards, relate more to encouraging healthy lifestyles, which can be fostered through a more pedestrian-friendly town. The following paragraph provides a description of one of the private funding sources that Bryson City can consider.

- ♦ **Blue Cross Blue Shield Fit Together Grants** – The FitCommunity Program is one of the programs established by the Fit Together partnership of Blue Cross Blue Shield and the NC Health and Wellness Trust Fund. The Fit Together partnership was created to promote physical activity and healthy eating in an effort to combat the recent dramatic increase of obesity in North Carolina. The FitCommunity Program is a designation and grant program to recognize and reward municipality and county efforts to promote physical activity, healthy eating and tobacco-free programs, policies, environments and lifestyles. A municipality or county is eligible for grant money once it has received a FitCommunity designation.

8.3. **Recognition Programs**

The following are two examples of some of the recognition programs that are available to Bryson City. Similar to funding sources, recognition programs can be both public and private entities. Although recognition programs may not include funding, through highlighting award recipients they provide free marketing which will make the town more attractive to visitors, businesses, and future residents.

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- ♦ **Robert Wood Johnson Foundation Active Living By Design Awards** - Active Living by Design is a national program of the Robert Wood Johnson Foundation and is administered by the UNC School of Public Health. The program establishes innovative approaches to increase physical activity through community design, public policies and communications strategies. Active Living by Design is funding 25 community partnerships across the country to demonstrate how changing community design will impact physical activity. Although funding is currently not available for additional communities, the Town of Bryson City should continue to monitor Active Living by Design as a potential funding source should the Town chose to make a commitment to healthy living. For more information, please see: <http://www.activelivingbydesign.org/>.

8.4. Implementation

As identified in Section 1, overall the main goal of the Pedestrian Plan is to provide sidewalk on all roads in Bryson City in the long term. To assist with this, the Town has also identified priorities to projects – in addition to putting safety improvements and improvements near schools first, it was also identified that project improvements should first be made within the town core and then work out to Bryson City's outlying areas. A useful implementation plan is one that incorporates all of the Town's goals into its priority system and identifies a timeframe for when projects should be begun and what partners the Town should consider working with to move a project forward.

Table 1 – 3 present a potential schedule for the project, program, and policy recommendations in this plan and potential partners the town should consider. The table should be used by the Town as a flexible framework for implementing the recommendations in the Plan – recognizing that it is important to capitalize on unexpected opportunities while also pursuing long term goals. In general, the Town should consider working with a wide range of partners – from government entities such as the Swain County School System to local organizations such as the Partnership for the Future of Bryson City and Swain County.

Table 1. Short-Term Actions (0 - 5 years)

Projects			
Reference Number	Description	Primary Responsible Party	Potential Additional Partners
1	Install striping and pedestrian signals for pedestrian crossings at the intersection of Everett and Main Streets	NCDOT/Town Manager and Public Works	Partnership**
2	Install striping for pedestrian crossings at the intersection of Academy and Rector Streets	NCDOT/Town Manager and Public Works	Partnership
7	Construct sidewalk from downtown to Super Saver Shopping Center and commercial area	NCDOT/Town Manager and Public Works	Partnership
8	Provide pedestrian signals at intersection of Veteran's Boulevard and Slope Street	NCDOT/Town Manager and Public Works	Partnership
9	Provide pedestrian signals at intersection of Veteran's Boulevard and Main Street	NCDOT/Town Manager and Public Works	Partnership
16	Sidewalk along US 19 to Hughes Branch Road from Downtown*	NCDOT/Town Manager and Public Works	Partnership
17	Install pedestrian signals at the intersection of Hughes Branch Road and US 19*	NCDOT/Town Manager and Public Works	Partnership
<p>*Note: NCDOT TIP Project No. R-4751 is planned for US 19 from the intersection of US 19 and Hughes Branch Road to US 441 in Cherokee.</p> <p>"Partnership": Partnership for the Future of Bryson City and Swain County</p>			

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Table 1 Continued. *Short-Term Actions (0 – 5- years)*

Programs		
Description	Primary Responsible Party	Potential Additional Partners
Walkabout/Historic Walking Tour	Town Manager and Visitor's Bureau/Chamber of Commerce	Partnership
Safe Routes to School Program	Town Manager	SCSS
Spot Improvement and Maintenance Programs	Town Manager/Public Works	NCDOT
Policies		
Description	Primary Responsible Party	Potential Partners
Require new development to construct sidewalk and connect to existing sidewalk.	Town Manager/Planning Board	Developers
Require all new signals to include pedestrian signal heads and crosswalks.	Town Manager/Planning Board/NCDOT/Public Works	Developers
Coordinate with Land Use Plan and future ordinances: As part of the policy recommendation to encourage mixed-use and pedestrian-friendly development, ensure that the recommendations of the Pedestrian Plan are incorporated into the Land Use Plan and other future recommendations.	Town Manager/Planning Board	Land Use committee and Planning Board
Assure safe pedestrian access through parking lots: as part of the policy recommendation to assure safe pedestrian access through parking lots, the Town should begin immediately to review future development plans to evaluate their pedestrian-friendliness and encourage more pedestrian-friendly design.	Town Manager/Planning Board	Developers
<i>"Partnership": Partnership for the Future of Bryson City and Swain County</i> <i>SCSS: Swain County School System</i>		

Table 2. Mid-Term Actions (6 - 10 years)

Projects			
Reference Number	Description	Primary Responsible Party	Potential Partners
3	Construct pedestrian connection from Island Park to the Train Depot with sidewalk along Ramseur Street and Bryson Street	Town Manager and Public Works	Partnership Visitor's Bureau Chamber SC Rec. Depart.
10	Provide pedestrian warning signage along Veteran's Boulevard	NCDOT /Public Works	Town Manager
13	Provide signed and marked pedestrian crossings from sidewalk on School Drive for access to Swain County Middle School	Town Manger and Public Works	Partnership SCSS SC Rec. Depart.
18	Construct sidewalk connection to Deep Creek Recreation Center to Downtown	NCDOT/Public Works/Town Manager	Partnership NCDOT SC Rec. Depart.
Programs			
Description		Primary Responsible Party	Potential Partners
Crosswalk/Traffic Safety Stings		Police Dept.	Town Manager Chamber
Promotional/Educational Materials		Town Manger	Visitor's Bureau Partnership SC Rec. Depart.
Policies			
Description		Primary Responsible Party	Potential Partners
Consider requiring new development to set aside land for greenways		Planning Board/Town Manager	Developers
<i>"Partnership": Partnership for the Future of Bryson City and Swain County</i> <i>"Chamber": Chamber of Commerce</i> <i>"SC Rec. Depart.": Swain County Recreation Department</i>			

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Table 3. Long-Term Actions (10 or more years)

Projects			
Reference Number	Description	Primary Responsible Party	Potential Partners
4	Sidewalk along Academy Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
5	Sidewalk along Island Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
6	Sidewalk along Fryemont Street	Town Manager/Public Works	NCDOT Chamber Visitor's Bureau Partnership
11	Consider signalized mid-block crossing along Veteran's Boulevard	NCDOT/Public Works	
12	Construct sidewalk on the northeast side of Veteran's Boulevard	NCDOT/Public Works	
14	Widen existing sidewalk along School Drive to Swain County Middle School	Public Works	NCDOT SCSS
15	Construct sidewalk from Downtown Bryson City to Swain County High School along Fontana Drive.	NCDOT/Public Works	SCSS
Programs			
Description		Potential Partners	
None.			
Policies			
Description		Potential Partners	
None.			
"Chamber": Chamber of Commerce "Partnership": Partnership for the future of Bryson City and Swain County "SCSS": Swain County School System			

8.5. Conclusion

Using this plan as a guide, the Town of Bryson City should be able to create a better, safer network of sidewalks, greenways, trails, paths, and crossings for pedestrians in the Town. The Town's next steps should begin to immediately address the short-term priority program, policy, and project recommendations. At the same time, the Town should also start to lay the groundwork for the longer term recommendations by mentioning them to potential partners, such as the Great Smoky Mountain Railroad and the Partnership for the Future of Bryson City, and starting to budget for projects in the future. Most importantly, the Town should continue its efforts to raise awareness about the importance of making a community more walkable in order to continue to cultivate support for more pedestrian improvements and programs. Residents, visitors, and local leaders should be familiar with the economic, health, and environmental benefits of a community in which there is less dependence on autos and more reliance on foot travel as not only a form of recreation, but also as a form of transportation.

As a small town but a major tourist attraction, Bryson City is in an ideal situation to develop a more walkable community. The Town should capitalize on its location and its attractions, such as the Great Smoky Mountain Railroad, to reinforce its existing pedestrian infrastructure with new projects and improvements. With careful planning, deliberate steps, and persistence, Bryson City can become a more pedestrian-friendly community.

Appendix 1. Demographics

The following tables provide Census 2000 data for various demographic characteristics that are useful to the Town of Bryson City's Pedestrian Plan. All data was collected from the US Census 2000 website, excepting where noted.

Population

	Town of Bryson City	Swain County	North Carolina
1990 Census Population:	1,145	11,268	6,628,637
2000 Census Population	1,411	12,968	8,049,313
Percent Change:	18.85%	13.11%	17.65%

Census Summary File 1, P1 (2000) and P1 (1990)

*North Carolina State Data Center

Race

	Town of Bryson City	Swain County	North Carolina
Total:	1,411	12,968	8,049,313
<i>By Percent</i>			
White alone	90.93%	66.33%	72.0%
Black or African American alone	1.98%	1.70%	21.5%
American Indian and Alaska Native alone	4.96%	29.03%	1.2%
Asian alone	0.35%	0.15%	1.3%
Some other race alone	0.64%	0.49%	2.3%
Population of two or more races	1.13%	2.28%	2.3%
Hispanic or Latino*	1.70%	1.47%	4.71%

Census Summary File 1, P3 & P7

*Note: The US 2000 Census measures "Hispanic/Latino" as an ethnicity and not a race. Hispanic/Latino population estimates are therefore considered separate from racial estimates.

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Appendix 1: Demographics

Age

	Town of Bryson City	Swain County	North Carolina
Total:	1411	12968	8,049,313
<i>By Percent</i>			
9 years and younger	9.71%	13.02%	13.7%
10 - 19 years	10.06%	14.24%	6.7%
20 - 29 years	10.42%	11.48%	14.6%
30 - 39 years	11.98%	13.19%	15.8%
40 - 49 years	12.76%	14.47%	15.1%
50 - 59 years	14.10%	13.10%	11.2%
60 - 69 years	8.29%	9.72%	7.6%
70 - 79 years	11.27%	6.77%	5.6%
80 years and older	11.41%	4.00%	2.9%

Census Summary File 1, P12

Income

	Town of Bryson City	Swain County	North Carolina
Median Household Income (1999)	\$23,232	\$28,608	\$39,184
Median Family Income (1999)			
<i>Summary File 3, P53 & P77</i>			
Total Population	1,411	12,968	8,049,313
Percent Population Below Poverty Line	19.18%	18.31%	12.28%
Percent Under Age 18	27.35	35.65	32.45
Percent Age 65 and over	24.36	15.22	12.75

Summary File 3, P87

Household Vehicle Availability

	Town of Bryson City	Swain County	North Carolina
Total:	584	5,137	3,132,013
<i>By Percent</i>			
No vehicle available	15.07%	6.95%	7.51%
1 vehicle available	41.27%	35.14%	32.27%
2 vehicles available	30.99%	38.74%	39.93%
3 vehicles available	8.73%	14.52%	14.91%
4 vehicles available	2.74%	3.21%	3.97%
5 or more vehicles available	1.20%	1.44%	1.41%

Summary File 3, H44

Work Commute (for workers over 16 years old)

	Town of Bryson City	Swain County	North Carolina
Total:	495	5,230	3,839,250
<i>By Percent</i>			
Single Occupancy Vehicle	68.89%	77.07%	79.36%
Carpooled:	18.99%	15.56%	14.02%
Public transportation:	0.40%	0.04%	0.91%
Bicycle	0.00%	0.00%	0.13%
Walked	0.00%	0.00%	0.18%
Other means	6.67%	2.47%	1.93%
Worked at home	1.01%	1.40%	0.79%

Summary File 3, P30

Bryson City Pedestrian Plan
Appendix 1: Demographics

To-Work Travel Time (for workers over 16 years old)

	Town of Bryson City	Swain County	North Carolina
Total:	495	5,230	3,837,773
<i>By Percent</i>			
Less than 5 minutes	16.77%	6.08%	2.93%
5 to 9 minutes	24.65%	17.30%	10.20%
10 min - Less than 30 min	41.62%	50.88%	54.55%
30 min - Less than 45 min	8.69%	14.61%	17.96%
45 min - Less than 1 hr	2.63%	4.00%	6.09%
An hour or more	1.62%	3.67%	5.59%

Summary File 3 P31

Appendix 2. Public Involvement Materials

Bryson City Pedestrian Plan

Stakeholder Committee Information Sheet

What is the Bryson City Pedestrian Plan? The Bryson City Pedestrian Plan is intended to provide guidance for the future of Bryson City's pedestrian facilities by making recommendations for future projects, programs, and policies that will encourage pedestrians to walk in the Town. The Plan is made possible through a grant from the North Carolina Department of Transportation's Division of Bicycle and Pedestrian Planning and matching funds from the Town of Bryson City and the Partnership for the Future of Bryson City/Swain County.

Why a Pedestrian Plan? It is important to encourage pedestrians in Bryson City for a variety of reasons, including health, economic/tourism benefits, safety, and congestion management. Bryson City is a Town which is ripe for pedestrian improvements to augment its already walkable downtown, and boost its popularity as a vacation destination. By improving pedestrian facilities in the Town, this will make it easier for both the long-time resident and newcomer to get around safely and easily.

Where would we make recommendations? Some of the potential study areas for the Plan include the downtown/train depot area, pedestrian and greenway connections to Island Park, pedestrian safety improvements along Veteran's Boulevard, and pedestrian improvements along Arlington Road leading to Swain County Middle School.



What does a Stakeholder do? As a stakeholder, your responsibility will be to guide the Plan towards the appropriate project and program recommendations for the Town. Through a series of meetings, together we will create a vision of what Bryson City should look like in the future as a pedestrian-friendly place, and then identify those recommendations that will make this happen. Last, the stakeholders will review the final Plan document. While your role as a stakeholder will require little more than attending a few meetings and reading the final Plan, your participation is critical in order to create a plan that fits the town and all of its needs.

When? As part of the NCDOT grant process, the Plan must meet specific requirements described in the attached outline. Our meetings will be generally structured around this outline. Our expected schedule remains flexible; however, meetings are estimated to be held in March and again in May, with final approval of the plan in July.

Contact Information:

Larry Callicutt, Town Manager for Bryson City, will serve as the local contact for this project. The Town has also contracted with the Louis Berger Group, Inc. to lead the pedestrian plan preparation process. The contact for the Louis Berger Group, Inc. is Jennifer Lewis, EI. If you have any additional questions, please feel free to contact either person.



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Thank you for your participation in the Bryson City Pedestrian Plan. Your efforts will help to make Bryson City a better place to live, work, and visit!

Announcing: The Bryson City Pedestrian Plan Open House!

When: Monday, April 30, 5 – 6 PM

Where: Bryson City Municipal Building/Town Hall on Everett Street

What: An Open House to discuss plans to make Bryson City more pedestrian-friendly. The purpose of this Plan is to identify projects, policies, and programs which can be implemented to improve pedestrian safety and access in Bryson City. The Town seeks input during this Drop-In Open House on locations for pedestrian improvements, potential projects, and programs to make the Town more pedestrian-friendly. Food and drink will also be available.



Why: The Town of Bryson City, through a grant from the NCDOT Division of Bicycle and Pedestrian Transportation and matching funds from the Partnership for Bryson City, is in the process of preparing its first Comprehensive Pedestrian Plan. The Town needs public input to make this plan the most effective and useful it can be!

Please come out and talk to us!

For more information, contact:

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