

TOWN OF HERTFORD COMPREHENSIVE PEDESTRIAN PLAN



December, 2007 FINAL
Hertford, NC • NCDOT Division of Bicycle & Pedestrian Transportation • The Louis Berger Group, Inc.



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Acknowledgements

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Cover photo of downtown Hertford courtesy of Albemarle Plantation.

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The Visitor's Bureau and Chamber of Commerce in Hertford welcomes visitors and residents alike.

Executive Summary

The Hertford Pedestrian Plan is designed as a guiding document to improve the walkability of the Town through specific projects, policies and programs to make the Town more attractive, comfortable, and safer for pedestrians. The Hertford Pedestrian Plan is funded by a grant from the North Carolina Department of Transportation's Bicycle and Pedestrian Transportation Division (NCDOT DBPT) and matching funds from the Town of Hertford. More than just a project list, the Plan is a tool to create a more pedestrian-friendly atmosphere through recommended programs, policies, projects, and plans. The Plan also provides a description of priorities, partnerships, cost estimates, and funding sources to help the Town implement its recommendations.

The goals of the Hertford Pedestrian Plan were a result of a visioning exercise that the stakeholder committee participated in to identify the top priorities that they would like to see in the Plan. The results were as follows:

1. **Improved pedestrian access** to schools through more pedestrian connections and a stronger sense of safety in the area. More pedestrian connections may include providing a pedestrian connection between the High School and Elementary school, and improving connections to the school from residential areas both in town and outside of town, and also to Hertford's downtown.
2. **Improved health of the community** through programs and projects that may promote walking and more physical activity. Programs may include encouragement at schools (such as a Safe Routes to School program), work places, and recreation facilities. Projects may include greenway facilities and/or an exercise/jogging path.
3. **Capital improvements** which will boost walking and pedestrian safety. Capital improvements may include creating trails that are interesting, attractive, and fun to walk; continuing beautification efforts and improved landscaping; and providing more green areas for rest and relaxation.
4. **Better connectivity** in town to connect residential areas, downtown, businesses, and the recreation center to the south of downtown. Better connectivity is both a result of projects that make walking from one place to another easier but that also improve the experience of walking. Projects that will better connect destinations in the Town may include providing a pedestrian-way from downtown to the recreation center, and a pedestrian-way between the two schools in town. Other projects should focus on improving a pedestrian's sense of security in town and providing more ADA-accessible facilities.
5. **Increased sense of community** measured by more pedestrian traffic downtown, increased use of downtown stores, and more visitors to the town. In addition, the Pedestrian Plan should be the result of a full community involvement effort which

incorporates a variety of viewpoints. Efforts to increase the sense of community should also result in children and residents having a higher regard for their community and downtown in particular.

Plan Process

In December 2006, the Town of Hertford hired the Louis Berger Group, Inc., a national transportation engineering and planning firm, to prepare the Plan. The Plan preparation process was a year-long effort, which was guided throughout by input from a stakeholder committee (made up of town staff and local citizen representatives), Town staff, and other public involvement efforts. The Stakeholder Committee met four times during the course of the planning process, and provided input on the goals for the plan, projects, and programs. Its members were as follows:

- ◆ Juanita J. Bailey, Cooperative Extension
- ◆ Dick Calogero, Historic Hertford, Inc.
- ◆ John Christensen, Town of Hertford
- ◆ JoAnn Morris, Commissioner, Town of Hertford
- ◆ Linda O'Connell, Perquimans Trailblazers
- ◆ Brandon Shoaf, Town of Hertford
- ◆ Dwayne Stallings, Perquimans County Schools
- ◆ Sue Weimar, County Commissioner
- ◆ Chris Wharton, Town of Hertford

In addition to a stakeholder committee, the Plan's public involvement process also included an Open House held on Tuesday, May 8, 2007 and a survey was distributed from March 2007 to May 2007.

Analysis

As part of the Plan preparation process, the Louis Berger Group performed the following analysis of the Town's demographics, community concerns and needs (based on public input), and existing conditions. The demographic analysis addressed characteristics which provide a better understanding of the Town's travel behavior and preferences; these are:

- ◆ Past and Current Population
- ◆ Race
- ◆ Age
- ◆ Educational Attainment
- ◆ Income and Poverty Status
- ◆ Household Vehicle Availability
- ◆ Work Commute
- ◆ To-Work Travel Time

The analysis of the Town's existing conditions evaluated existing roads, sidewalks, land uses and zoning, and major destinations such as schools, shopping areas, and recreation facilities. An inventory of the Town's existing sidewalk was conducted on August 13, 2007; results of the inventory are shown in Figure 1.



Woodard's Pharmacy is a Hertford hallmark, and its distinctive sign contributes to the sense of community in the Town.

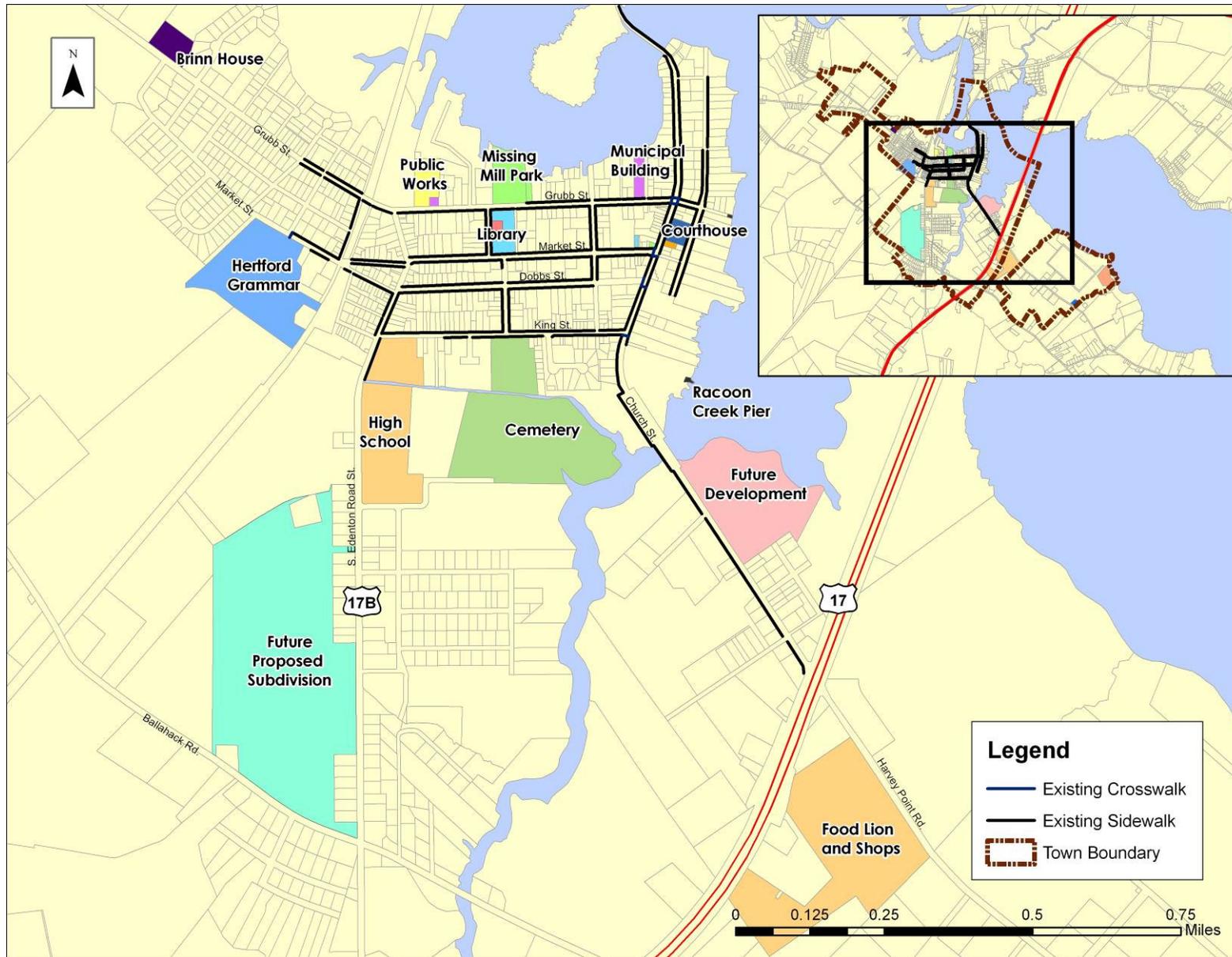


Figure 1. Map of major roads and existing sidewalks and crosswalks in Hertford.

In addition, the Town's existing plans and policies were examined in order to identify potential plan recommendations or adjustments in policies that could strengthen the pedestrian facilities in future construction. The following documents were reviewed:

- ◆ Historic Hertford North Carolina Development Strategic Plan (2001)
- ◆ Hertford Corridor Plan Phase I: US 17 (Draft 2007)
- ◆ NCDOT TIP
- ◆ Town's Subdivision Ordinance

Program and policy recommendations were generated from the analysis of the Town's existing plans and policies, as well as based on the needs that were identified as part of the public input process.

To assist with analysis, five focus areas were created in order to target specific needs in the Town:

- ◆ Intersection of US 17 and Church Street/Harvey Point Road and points south
- ◆ Perquimans High School and Hertford Grammar School
- ◆ Missing Mill Park Area
- ◆ Raccoon Creek Pier and Canoe Launch Area
- ◆ Winfall Connections and S-Bridge

These focus areas are also shown in Figure 2. Project recommendations were developed from these focus areas.

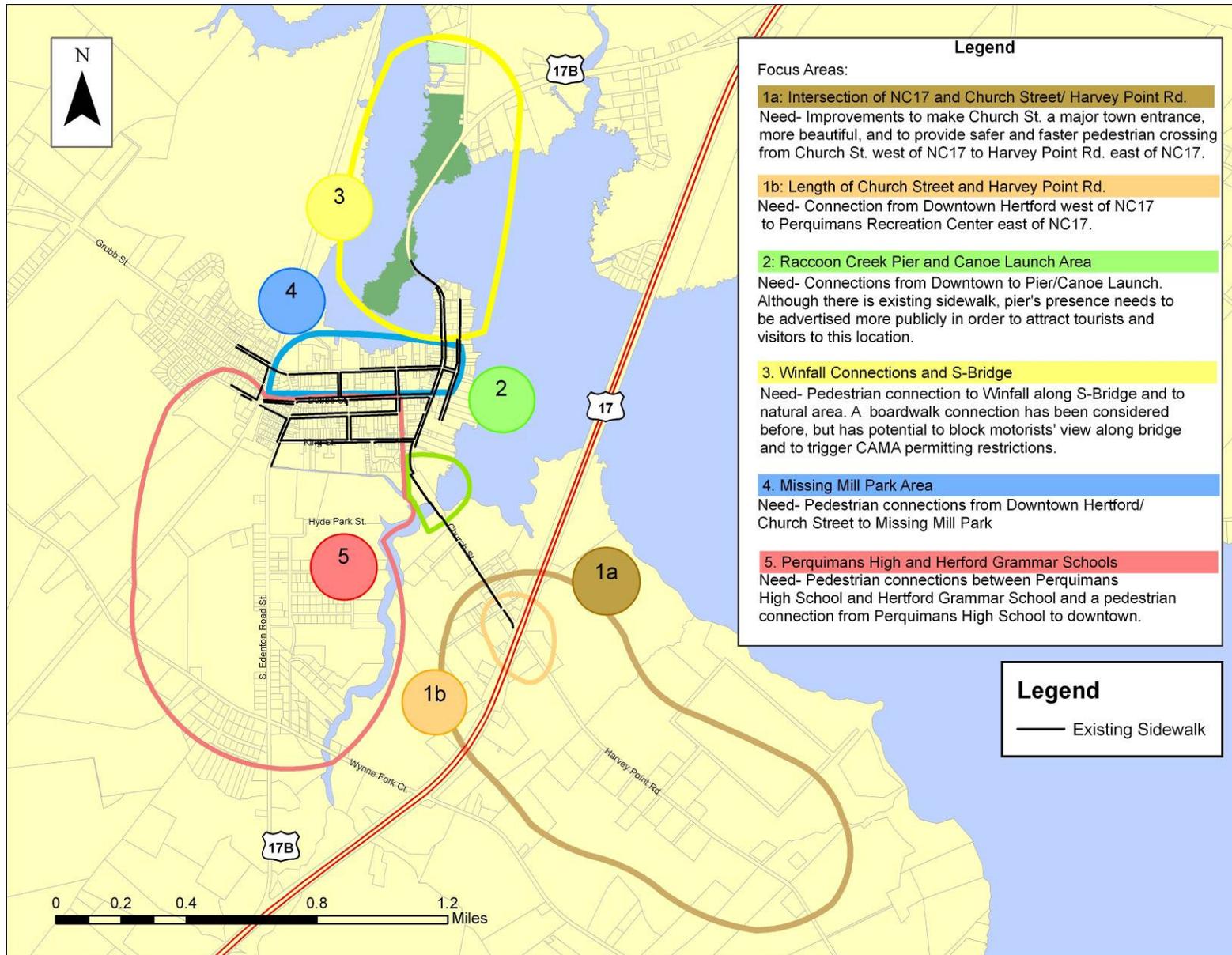


Figure 2. Focus areas for targeted projects in the Pedestrian Plan.

Recommendations

The Plan provides recommendations for projects, programs, and policies which, upon implementation, will make the Town more pedestrian-friendly. Recommendations were divided into short-term (0 – 5 years in the future), mid-term (6 – 10 years), and long-term (more than 10 years in the future) actions. Priorities were developed based on the following criteria:

- ◆ **Safety** – refers to actual or perceived threat to pedestrian safety by nearby traffic. It was qualitatively measured by the crashes near a location or a perceived sense of danger by users as reported by staff, stakeholders, and the public during open houses and the survey.
- ◆ **Demand** – indicates the amount of perceived potential use for the project either as a result of input by the stakeholders, public, or staff, or by observations during field visits. A location had a high level of need/demand if many comments were made about it at the open house or on the survey, or if several pedestrians were observed at the location during field visits.
- ◆ **Accessibility/Connectivity** – refers to the potential for the project to complete a route from one major destination to another. Frequently, the project may connect two existing blocks of sidewalk that were previously separated by a long stretch of missing sidewalk.
- ◆ **Ease of Construction** – qualitatively measures how feasible it will be to construct a project. While this factor takes into account cost to a certain extent, it also includes necessary right-of-way, permitting, and potential negotiations with other agencies and jurisdictions that may be necessary to construct the project.
- ◆ **Cost** – qualitatively measures the potential cost of a project, mostly as a function of length alone.
- ◆ **Recreation vs. Transportation Need** – indicates whether a project will serve a transportation or recreation need. A transportation need is one that connects users from one major destination to another, especially if they are necessary destinations such as schools, grocery stores, banks, and work locations. A project which serves a recreation need is one that is solely intended for use as an exercise route, such as a loop trail around a lake, or a scenic destination.

A summary of the recommendations are provided in the following tables, and Figure 3 shows a map of the proposed projects by priority. As part of an implementation program, the Plan also provides general cost estimates for each project recommendation, and potential partners that the Town could work with to assist with its projects and programs.



Figure 3. Project recommendations identified by priority.

Table 1. Short Term Recommendations (0 - 5 years).

Projects			
Reference Number	Description	Responsibility	Potential Partners
1	Pedestrian Crossing Improvements at Intersection of US 17 and Church Street/Harvey Point Road	Hertford Planning and Public Works Depts.	NCDOT, Chamber of Commerce/Visitor's Bureau, Historic Hertford
3	Sidewalk on the west side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Chamber of Commerce/Visitor's Bureau, NCDOT, Developers
9	Sidewalk on the west side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Hertford Planning and Public Works Depts.	NCDOT, Perquimans County Schools, Developers
10	Sidewalk on the east side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Hertford Planning and Public Works Depts.	NCDOT, Perquimans County Schools, Developers
11	Sidewalk on the north side of Wynne Fork Road from the intersection with Edenton Street Road to US 17	Hertford Planning and Public Works Depts.	NCDOT, Developers
12	Improvements at the intersection of US 17 and Wynne Fork Road to accommodate pedestrians safely	Hertford Planning and Public Works Depts.	NCDOT
13	Sidewalk on both sides of Wynne Fork Road from existing residential development to the intersection with US 17	Hertford Planning and Public Works Depts.	NCDOT, Developers
14	Sidewalk to Missing Mill Park along Church Street from end of existing sidewalk to park entrance	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Parks Facilities
15	Provide pedestrian crossing between Missing Mill Park and Tennis Courts	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Parks Facilities, Hertford Seniors' Center
18	Sidewalk on east side of Church Street from existing sidewalk to the pier	Hertford Planning and Public Works Depts.	Hertford Parks Facilities

Table 1 continued. Short Term Recommendations (0 – 5 years).

Programs		
Description	Responsibility	Potential Partners
Self-guided Walking Tour	Hertford Planning Dept.	Chamber of Commerce/Visitor's Bureau, Historic Hertford, Main Street Program
Safe Routes to School Program	Hertford Planning Dept.	Perquimans County Schools, Hertford Grammar School, PTA, Albemarle COG, NCDOT
Annual 5K	Hertford Trailblazers	Historic Hertford, Chamber of Commerce/Visitor's Bureau
Policies		
Description	Responsibility	Potential Partners
Require new development to construct sidewalk and connect to existing sidewalk.	Hertford Planning Dept. – will require Town Council approval	NCDOT and Developers
Require all new signals to include pedestrian signal heads and crosswalks.	Hertford Planning and Public Works Depts. – will require Town Council approval	NCDOT and developers
Implement policies that will assure safe pedestrian access through parking lots	Hertford Planning Dept. – will require Town Council approval	Developers, Historic Hertford, Chamber of Commerce/Visitor's Bureau

Table 2. Mid-Term Recommendations (6 – 10 years).

Projects			
Reference Number	Description	Responsibility	Potential Partners
2	Sidewalk on the east side of Church Street from existing sidewalk to US 17	Hertford Planning and Public Works Depts.	Developers, Hertford Chamber of Commerce/Visitor's Bureau, NCDOT
8	Greenway linking Perquimans High School to Hertford Grammar School	Hertford Planning and Public Works Depts.	Hertford Parks Facilities, Hertford Chamber of Commerce/Visitor's Bureau, Perquimans County Parks and Recreation
17	Sidewalk on the north side of Church Street from existing sidewalk to Missing Mill Park entrance	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Seniors' Center

Table 2 continued. Mid-Term Recommendations (6 – 10 years).

Projects			
Reference Number	Description	Responsibility	Potential Partners
21	Sidewalk from residential neighborhoods along Grubb Street to connect to Missing Mill Park (Nates Drive to existing sidewalk)	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Seniors' Center
22	Sidewalk on north side of Market Street from the intersection with Grubb Street to the Hertford Grammar School	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau
Programs			
Description	Responsibility	Potential Partners	
Walking/Running Trail with Mile Markers and Exercise Stations	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation	
Connections to the Blueway	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Historic Hertford, Main Street Program	
Police Education	Hertford Police Department	Hertford Police, NCDOT	
Pedestrian Counts on Major Thoroughfares	Hertford Planning and Public Works Depts.	NCDOT, Albemarle Commission	
Policies			
Description	Responsibility	Potential Partners	
Require new development to set aside land for greenways	Hertford Planning and Public Works Depts. – will require City Council approval	Developers, Perquimans Trailblazers	
Implement policies that encourage mixed-use and other pedestrian-friendly developments	Hertford Planning Dept. – will require Town Council approval	Developers, Hertford Main Street Program, Chamber of Commerce/Visitor's Bureau	

Table 3. Long Term Recommendations (10 or more years).

Projects			
Reference Number	Description	Responsibility	Potential Partners
4	Sidewalk on the south side of the access road to the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Perquimans County Parks and Recreation
5	Sidewalk on the east side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Perquimans County Parks and Recreation, NCDOT
6	Boardwalk along the Perquimans riverfront from Downtown to the Recreation Center	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Hertford Parks Facilities, NCDOT
7	Greenway along "wetland" behind High School into town (Jenny's Gut area/cemetery)	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Perquimans County Schools
16	Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation
19	Sidewalk along west side of S-bridge and Causeway/US 17	Hertford Planning and Public Works Depts.	NCDOT
20	Boardwalk adjacent to S-bridge and Causeway/US 17	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, NCDOT
Projects			
Reference Number	Description	Responsibility	Potential Partners
23	Sidewalk on both sides of Dobbs St. from Church Street to Covent Garden Street.	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau
Programs			
	Description	Responsibility	Potential Partners
	Sidewalk Construction Program	Hertford Planning and Public Works Depts.	NCDOT, Developers

Table 3 continued. Long Term Recommendations (10 or more years).

Programs		
Description	Responsibility	Potential Partners
Sidewalk Maintenance Program	Hertford Planning and Public Works Depts.	NCDOT, Developers
Intersection Improvement Program	Hertford Planning and Public Works Depts.	NCDOT, Developers
Policies		
Description	Responsibility	Potential Partners
Implement connectivity requirements.	Hertford Planning Dept. – will require Town Council approval	Developers, Chamber of Commerce/Visitor’s Bureau

Summary

Already, Hertford has many of the elements necessary for a more walkable community. With careful planning, deliberate steps, and persistence, Hertford can become a more pedestrian-friendly community. The Town’s next steps should begin to immediately address the short-term priority program, policy, and project recommendations. At the same time, the Town should also start to lay the groundwork for the longer term recommendations by mentioning them to potential partners and starting now to budget for projects in the future. Most importantly, the Town should continue its efforts to raise awareness about the importance of making the community more walkable in order to continue to grow support for more pedestrian improvements and programs. Residents, visitors, and local leaders should be familiar with the economic, health, and environmental benefits of a community in which there is less dependence on autos and more reliance on foot travel as not only a form of recreation, but also as a form of transportation.

Quick Summary: This section provides an introduction to the Town of Hertford Pedestrian Plan. It describes some of the benefits of a more walkable community, the Plan's outline, and some of its goals.



Figure 1-1. The Visitor's Bureau and Chamber of Commerce in Hertford welcomes visitors and residents alike.

Section 1. Introduction and Goals

1.1. Introduction

Located in eastern North Carolina, the Town of Hertford is a small town with big potential. From the scenic Perquimans River to its quaint downtown, Hertford has become an attractive and exciting venue for people to visit. In recent years, the Town has also experienced increased popularity as a place to live and subsequent new growth in population. In order to continue this trend, recent town efforts such as the *Hertford Development Strategic Plan* and *Hertford Corridor Plan Phase 1: US 17* have focused on enhancing the town's appearance to make it more inviting for visitors and residents. At the same time, new efforts have been made to improve the Town's infrastructure, including sidewalk construction along Church Street to NC 17 and the construction of the new Perquimans County Recreation Center. The Hertford Pedestrian Plan was created in this atmosphere of new growth and renewed infrastructure. Combining the movements to enhance the Town's attractiveness to visitors and new residents with improvements to the community's infrastructure and existing facilities, the Hertford Pedestrian Plan serves as a guiding document to improve the walkability of the Town through specific projects to make the Town more attractive, comfortable, and safer for pedestrians.

The Hertford Pedestrian Plan is funded by a grant from the North Carolina Department of Transportation's Bicycle and Pedestrian Transportation Division (NCDOT DBPT) and matching funds from the Town of Hertford. More than just a project list, the Plan is a tool to create a more pedestrian-friendly atmosphere through recommended programs, policies, projects, and plans. The Plan also provides a description of priorities, partnerships, cost estimates, and funding sources to help the town implement its recommendations.

In addition to improving the walkability of the Town, the Pedestrian Plan has several other benefits. First, the Pedestrian Plan will help guide future spending for projects in order to construct a connected pedestrian system in a logical fashion. Second, improved pedestrian facilities will also make it easier, cheaper, and safer for people to move around town – residents and visitors alike. Third, better walking conditions promote more walking, which has several benefits including reduced traffic congestion, improved health and more active lifestyles for local residents, and reduced air pollution. A more walkable town is also frequently more attractive to visitors and new businesses, which in turn can help boost the local economy. As can be seen, although the Pedestrian Plan is specifically focused on improving walking, it has many peripheral benefits which can help improve health, the environment, and the local economy.

1.2. Planning Process

The Hertford Pedestrian Plan was begun in February 2007 and completed in December 2007 (estimated no later than). During this time, the Town hired The Louis Berger Group, Inc. to assist with plan preparation and public involvement. The plan preparation process was guided by a standing stakeholder committee comprised of members who represented a variety of interests in the Town, including the health community, town staff and public works, the school system, and the town's Greenway/Blueway Advocacy group. The Stakeholder Committee members were:

- ◆ Juanita J. Bailey, Cooperative Extension
- ◆ Dick Calogero, Historic Hertford, Inc.
- ◆ John Christensen, Town of Hertford
- ◆ JoAnn Morris, Commissioner, Town of Hertford
- ◆ Linda O'Connell, Perquimans Trailblazers
- ◆ Brandon Shoaf, Town of Hertford
- ◆ Dwayne Stallings, Perquimans County Schools
- ◆ Sue Weimar, County Commissioner
- ◆ Chris Wharton, Town of Hertford

There were a total of four stakeholder meetings. In addition to the stakeholders, the Plan also was guided by public input as a result of a public involvement process. This process included a survey, conducted from March 2007 to May 2007, two public meetings (one as part of a stakeholder committee meeting and a second as part of the Hertford Grammar School Open House), and a presentation to Town Council. For more flyers and information describing the stakeholder meetings, public meetings, and Town Council presentation, please see Appendix 1.

1.3. Goals

The goals of the Hertford Pedestrian Plan were a result of a visioning exercise that the stakeholder committee participated in to identify the top priorities that they would like to see in the Plan. The results were as follows:

1. **Improved pedestrian access** to schools through more pedestrian connections and a stronger sense of safety in the area. More pedestrian connections may include providing a pedestrian connection between the High School and Elementary school, and improving connections to the school from residential areas both in town and outside of town, and also to Hertford's downtown.
2. **Improved health of the community** through programs and projects that may promote walking and more physical activity. Programs may include encouragement at



Figure 1-2. Woodard's Pharmacy is a Hertford hallmark, and its distinctive sign contributes to the sense of community in the Town.

- schools (such as a Safe Routes to School program), work places, and recreation facilities. Projects may include greenway facilities and/or an exercise/jogging path.
3. **Capital improvements** which will boost walking and pedestrian safety. Capital improvements may include creating trails that are interesting, attractive, and fun to walk; continuing beautification efforts and improved landscaping; and providing more green areas for rest and relaxation.
 4. **Better connectivity** in town to connect residential areas, downtown, businesses, and the recreation center to the south of downtown. Better connectivity is both a result of projects that make walking from one place to another easier but that also improve the experience of walking. Projects that will better connect destinations in the Town may include providing a pedestrian-way from downtown to the recreation center, and a pedestrian-way between the two schools in town. Other projects should focus on improving a pedestrian's sense of security in town and providing more ADA-accessible facilities.
 5. **Increased sense of community** measured by more pedestrian traffic downtown, increased use of downtown stores, and more visitors to the town. In addition, the Pedestrian Plan should be the result of a full community involvement effort which incorporates a variety of viewpoints. Efforts to increase the sense of community should also result in children and residents having a higher regard for their community and downtown in particular.

1.4. Summary

The following chapters contain information and recommendations developed to help guide the Town towards meeting its goals for the Pedestrian Plan. In Chapters 2 and 3, the Plan provides information on existing conditions – including demographics and facilities and existing plans. This information serves as the basis for the project, program, and policy recommendations which are discussed in Chapters 4, 6, and 7. Chapter 5 provides recommended best practices for design guidelines and standards for facility design and construction, and Chapter 8 includes recommended scheduling, funding sources, and implementation approaches.

Quick Summary: This section describes the Town’s demographics as well as the community concerns and needs discussed in the survey results and open house. This description, along with the assessment of existing facilities in Section 2, serves as a basis for the recommendations made later in the plan.

Section 2. Demographic Analysis and Survey Results

2.1. Introduction

The Town of Hertford is located on the Perquimans River near the Albemarle Sound in eastern North Carolina. The Town was incorporated in 1758 by North Carolina State Statute from the land owned by Jonathan Phelps and established as a thriving coastal community and the county seat. Since the 1950’s, the Town has grown as a vacation destination – attracting visitors to its quaint downtown and scenic riverfront. The Town has also recently experienced a boom in new residences as it has become more and more popular for retirees and new businesses.

It is important to understand the needs of the people of Hertford as a basis for the recommendations to be made by this Pedestrian Plan. In order to do so, the Plan must assess the Town’s population demographics and future trends; the community’s concerns and needs about pedestrian travel; and, existing facilities such as sidewalks, roads, and recreation facilities. This section describes each of these items and provides a summary of next steps for recommendations.

2.2. Demographic Analysis

For the Pedestrian Plan, there are several important demographics that must be analyzed in order to create an understanding of the town’s residents and their travel behaviors. These demographics are as follows:

- ◆ Past and Current Population
- ◆ Race
- ◆ Age
- ◆ Educational Attainment
- ◆ Income and Poverty Status
- ◆ Household Vehicle Availability
- ◆ Work Commute
- ◆ To-Work Travel Time

Information for these demographic items was collected from the 2000 US Census data. A complete analysis of the Town’s demographics is contained in Appendix 2.

From the demographic analysis contained in Appendix 2, several key characteristics can be inferred about the residents of Hertford. With a US Census population of

What Makes A Great Place to Live?
Relocate-America.com, Money Magazine, and others have for years taken large surveys to answer this question. Parks, recreation, short commutes contribute to a “vibrancy” of place. A look at current and past winners reveals that the small cities frequently share something in common: residents can safely and conveniently walk to most of their daily destinations. This year’s (2007) winner? Asheville, North Carolina.
(Source: relocate-america.com)

2,070 in 2000, the Town is nearly 50 percent Caucasian and 50 percent African-American. This is markedly different from Perquimans County, which is approximately 70 percent Caucasian and 30 percent African-American. The 2000 US Census data showed little information about the Town’s Hispanic population. In general, the Hispanic population is growing in the State of North Carolina, and this will probably be reflected in the Hispanic population in Hertford.

By age, the Town is comparable to the County but has a greater than the State average percent population of elderly. The Town has nearly 30 percent of its population below the age of 19, another third of the population between the ages of 30 – 60, and nearly 20 percent of the population above the age of 70. The State has less than 21 percent of the population below the age of 19, and less than 10 percent of the population is above the age of 70.

In educational attainment, the Town and County are fairly similar, with more than 30 percent of the population having achieved a high school degree, almost 10 percent having achieved a bachelor’s degree, and less than four percent having a graduate or professional school degree. These rates are lower than educational attainment rates in the State, where greater than 15 percent of the population has received a bachelor’s degree, and nearly eight percent has received a graduate or professional school degree.

The Town and County differ in income levels. Both the Town’s median household income and median family income are lower than those of both the County and the State. In addition, nearly 40 percent of the Town’s population live below the poverty line. This is double that of the County, where 17.9 percent of the population live below the poverty line. Of those living below the poverty line in the Town, over 10 percent are under the age of five.

In terms of travel behavior, over a quarter of households have no vehicle available, and nearly 40 percent of households have access to only one vehicle. This is dramatically different than both the county and the state, where less than 10 percent of households have no vehicle available and approximately 30 percent of households have access to one vehicle. In to-work commute, over five percent of the population walked to work in Hertford as of the 2000 US Census – double that of both the County and the State. In addition, almost eight percent of residents work commute was under five minutes – nearly double that of the County and State.

Figure 2-1. Town of Hertford Change Statistics (*indicates forecast or estimate)

Statistic	Census			2007-2012	
	2000	2007*	2012*	Change*	Annual Rate*
Population	2,070	2,254	2,394	140	1.21%
Median Age	39.6	43.3	44.7	1.4	0.64%
Households	877	985	1,060	75	1.48%
Average Household Size	2.29	2.23	2.21	-0.02	-0.18%

Sources: US Bureau of the Census; ESRI Business Analyst*

FINDING: As a result of demographic trends in Hertford, recommendations made in the Pedestrian Plan should provide safer and faster pedestrian access to jobs, schools, and basic needs such as the grocery store and post office.

Hertford's demographic analysis shows that the Town has:

- ◆ A lower income population than the County and State, with a greater percentage of very young children living below the poverty line.
- ◆ More households with no vehicle or access to only one vehicle than the County or State.
- ◆ A lower commute to-work time than County or State, indicating that Hertford's residents live closer to their jobs than those in the County or State.
- ◆ A higher walk to-work rate than the County or State.

These trends show that Hertford has a demand for pedestrian facilities which serve as functional transportation routes in addition to being recreation facilities. As a result, recommendations made in the Pedestrian Plan should provide safer and faster pedestrian access to jobs, schools, and basic needs such as the grocery store and post office.

2.3. Community Concerns and Needs

The best way to generate information about the Town's community concerns and needs is to ask members of the community. For the Hertford Pedestrian Plan, community members were asked to complete surveys and provide input through an open house and steering committee. The following paragraphs briefly describe the results of the survey and open house and provide guidance for the recommendations that should be developed based on the results of these two input processes. Complete results of the survey and open house are contained in Appendix 3.

2.3.1. Survey Results

The Town of Hertford Pedestrian Plan survey was conducted from January to March 2007, and received over 50 survey respondents. In general, more women than men responded to the survey and most respondents were over the age of 50. The survey's questions were designed to ask Hertford residents about their walking behavior and preferences, about destinations they currently walk to, how they feel about walking in Hertford, and about destinations they would like to be able to walk to and specific improvements they recommend.

The results of the questions on the survey about respondents travel preferences are as follows:

- ◆ The most respondents indicated that they walk for visiting family and friends. Many respondents also listed church, shopping, post office, restaurant, and parks as destinations that they walk to. Most respondents rarely indicated that they walk to work or the grocery store. This trend may be due to the age of most respondents, who may be retired and therefore not need to walk to work, and also may prefer to drive to the grocery store in order to use their car to haul their purchases home. In addition, few respondents indicated school as a destination that they walked to – this, too, may be due to the age of the respondents.

Section 2: Demographic Analysis and Survey Results

- ◆ Most respondents indicated that they walk for exercise, and similarly, a park or recreation center was the top priority for destinations which respondents would like to be able to walk to. Other destinations respondents would like to be able to walk to include: friends, church, and the grocery store.
- ◆ In general, respondents feel comfortable walking in Hertford, although in the discussion questions many respondents indicated they would like to see the safety and appearance of the area near King Street improved and stated that they felt uncomfortable walking there.
- ◆ The most respondents indicated that they were most likely not to walk to a destination in Hertford because it was too far, but a close second was because respondents felt that the traffic makes walking unsafe and unpleasant. Another top reason respondents chose not to walk was because there is not continuous sidewalk to the destination.
- ◆ 63% of respondents would prefer money to be spent on sidewalk on existing roads over money spent on greenways.

From this information, some conclusions can be made about respondents needs, and are described in the following list. These conclusions are used to help guide the recommendations and projects in the Pedestrian Plan.

- ◆ Most respondents do not walk to school or work probably because they are older and retired and therefore do not attend school or go to work. This result is in keeping with the age profile of survey respondents as well as the Town as a whole.
- ◆ Traffic and lack of sidewalk connections are one of the major limitations to walking in Hertford. Respondents would walk more if there were improvements to connect more destinations with sidewalk, especially parks and recreation facilities. Improvements should also be made to make respondents feel more comfortable when walking in Hertford.
- ◆ Respondents currently enjoy walking to visit friends and family and attend church. Existing sidewalk and pedestrian connections should be preserved to these destinations and additional connections should be provided.

From survey responses, some of the frequently discussed locations that respondents would like to see sidewalk are:

- ◆ Grubb Street
- ◆ Edenton Road Street
- ◆ Church Street
- ◆ Dobbs Street

FINDING: From the responses to the Hertford Pedestrian Plan survey, recommendations in the Plan should connect more destinations, especially parks and recreation facilities, with sidewalks and create safer crossing conditions.

2.3.2. Open House Results

Open Houses for the Hertford Pedestrian Plan were held on May 8, 2007 and on August 23, 2007. The May 8, 2007 meeting was held in the Perquimans County Courthouse to present the preliminary Draft Plan and recommendations to the public for comment. The August 23, 2007 meeting was held as part of the Hertford Grammar School Open House to distribute information and materials about the Pedestrian Plan as well as gather input. In general, the following comments were made about the plan:

- ◆ Provide a walkway across the S-bridge.
- ◆ Keep downtown area safe and clean. Presently the grass grows too high to keep litter under control.
- ◆ Walkways to Perquimans County High School need to be well-lit and secured to promote resident walks to community events.

These comments were incorporated into the Plan's recommendations.

2.4. Summary

The results of the demographic analysis, Pedestrian Plan survey, and Open House show two very different sets of people in Hertford with two different sets of perceived needs. In the demographic analysis, it is clear that Hertford has a lower income population, with less access to vehicles, who will need pedestrian facilities which provide safe, convenient access to important necessary destinations such as schools, workplaces, and grocery stores. The survey results and comments from the Open House, on the other hand, show an older, more affluent population which enjoys using pedestrian facilities such as sidewalks and greenways for recreation and exercise. The Hertford Pedestrian Plan should be able to provide recommendations that address both sets of needs, and in so doing create a better, safer place to live for all of the Town's residents.

Quick Summary: This section describes the existing pedestrian infrastructure in Hertford as well as major destinations and the condition of pedestrian access to them. This will assist in creating recommendations that address existing needs and inadequacies.

Section 3. Existing Facilities

3.1. Introduction

Besides existing sidewalks, there are a variety of features in a town which are essential to a complete pedestrian environment. Existing roads, nearby land uses, and major destinations are some of these important features. Major destinations can include schools, shopping areas, recreation facilities, and employment centers. The following paragraphs describe the existing facilities in Hertford and discuss how they may influence the recommendations in the Pedestrian Plan.

3.2. Major Roads

Figure 3-1 shows a map of the major roads and existing sidewalks in Hertford. NC 17 is the major access route to Hertford for most visitors traveling east and west. Church Street (formerly NC 17 prior to the bypass construction), is the major north-south route in the Town, connecting the Town to its nearest neighboring municipality of Winfall. The Town is also within 15 miles of Edenton, which can be accessed heading west on NC 17. Other major roads in town include

- ◆ Harvey Point Road, which begins southeast of the intersection of Church Street and NC 17 and provides connections into town from the Perquimans County Recreation Center, the historic Newbold-White House, and Albemarle Plantation.
- ◆ Edenton Road Street, which has residential development and Perquimans High School.
- ◆ Grubb Street, which has the municipal building, Missing Mill Park, residential development, and connects to the area west of town.

3.3. Sidewalk

Figure 3-1 shows a map of existing sidewalk and crosswalks as identified through an inventory of Hertford conducted on August 13, 2007. Sidewalk in Hertford is primarily located within the downtown area bounded by Grubb Street, Front Street, King Street, and Edenton Road Street, however there is some sidewalk in residential neighborhoods beyond the inner core area. Sidewalk was recently constructed on Church Street to extend from the downtown area to the intersection of Church Street and NC 17, thus providing better access to the shopping and residential areas at the intersection to downtown. Major intersections have ADA-compliant curb ramps, although there are some locations where this is not available. Major missing pedestrian connections are located along the S-bridge to connect to Winfall, along Grubb Street to connect from Church Street to Missing Mill Park, and along Harvey Point Road to the Perquimans Recreation Center. Recommendations in the plan should address these missing locations.

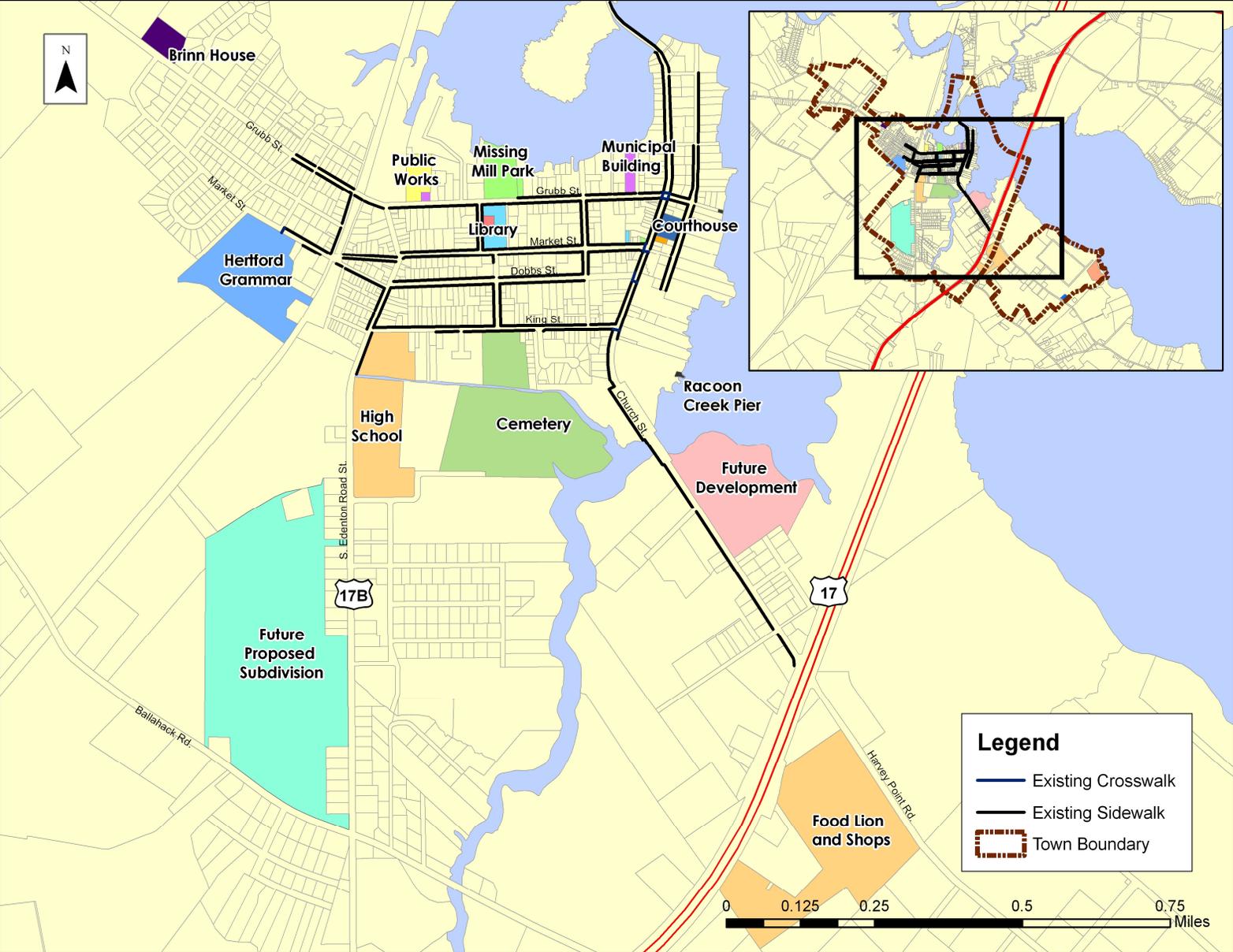


Figure 3-1. Map of major roads and existing sidewalks and crosswalks in Hertford.

3.4. Land Uses and Zoning

Figure 3-2 shows Hertford's current zoning which extends to the limits of the Town's ETJ (extra-territorial jurisdiction), which approximately defines what land uses the Town will allow. Most of the commercial and residential land uses center around the downtown core area. Hertford is in the process of developing a business park to the southeast of Town on Harvey Point Road near to the Perquimans County Recreation Center. Southeast of town is Albemarle Plantation, which has almost 250 existing residential homes and is still under construction. Although this subdivision is beyond town limits, Hertford is the nearest town and therefore the shopping and recreation destination for many of these residents. Hertford is currently experiencing tremendous growth pressure for further residential and commercial construction, however, a moratorium on development has been in effect until adequate water treatment facilities and capacity can be constructed. The Town is currently in the process of constructing a new wastewater treatment plan, which should be finalized in April 2008; the moratorium is expected to be lifted in October 2007.

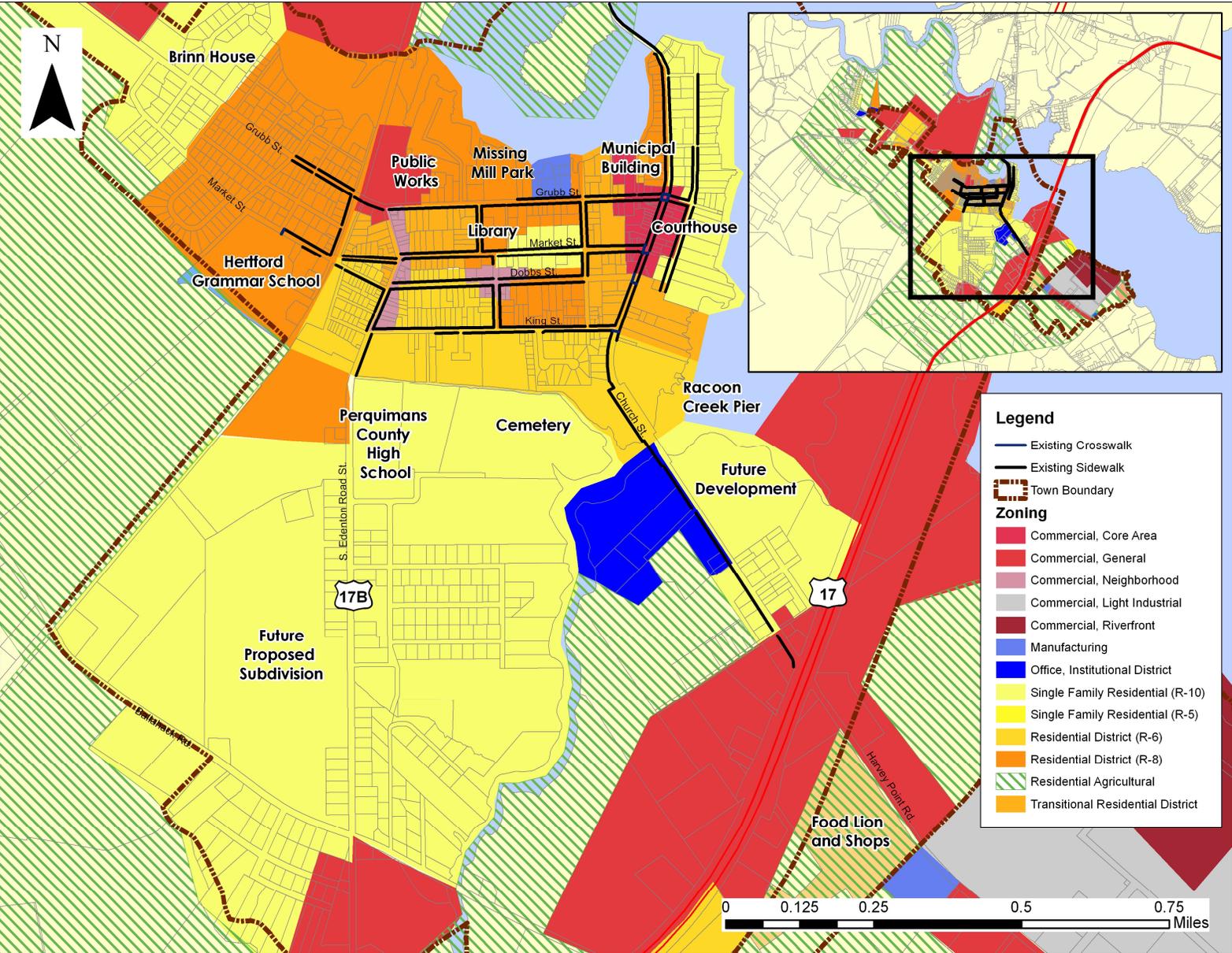


Figure 3-2. A map of zoning in Hertford.

3.5. Major Destinations

It is important to identify major destinations and attractions in Hertford because these are some of the places that people will walk to as pedestrians and should therefore have pedestrian access. Figure 3-3 shows the locations of the major destinations in Hertford as discussed in the following paragraphs. As of December 2007, several new developments are expected to be constructed in Hertford within the next year. Item 8 on Figure 3-3 (Future Mixed-Use Development, tentatively named Hertford Bay) is proposed to entail 120 waterfront townhomes and some mixed use on the road frontage with shops and office space. Item 9, Settler's Landing, has been proposed to have 50 homes initially and then an additional 70 to 80 homes later. Ten homes have already been built on the property.

Shopping and Business

A major destination in Hertford is the downtown area, where there are many businesses, several restaurants, Town Hall, the county courthouse, and the Chamber of Commerce/Visitors Bureau. Beyond downtown, another major destination for shopping and other businesses is the intersection of NC 17 and Church Street/Harvey Point Road where there are several restaurants, businesses, and a nearby grocery store.

Schools

Hertford has two schools, both of which are part of the Perquimans County School district: Perquimans High School and Hertford Grammar School. Students to both schools come from all parts of the County, although Hertford Grammar School does attract students which are within walking distance. Many of the students are from nearby Winfall. In 2007, Hertford Grammar School had an enrollment of 369 students and Perquimans High School had an enrollment of 564 students¹. The schools provide busing service for students, but many arrive by car. Neither school has conducted a Safe Routes to School program.

Recreation Facilities

Hertford has several recreational facilities, including Missing Mill Park and Tennis Courts, both located on Grubb Street and the Perquimans County Recreation Center, located on Harvey Point Road southeast of Town. Hertford also hosts a Senior Center, library, and several boat ramps and water access points. More connections and signage are needed to make these facilities more accessible to visitors and residents.

¹ Source for school enrollment: NC School Report Cards Website, 2003 – 2004 school year.
<http://www.ncreportcards.org>.

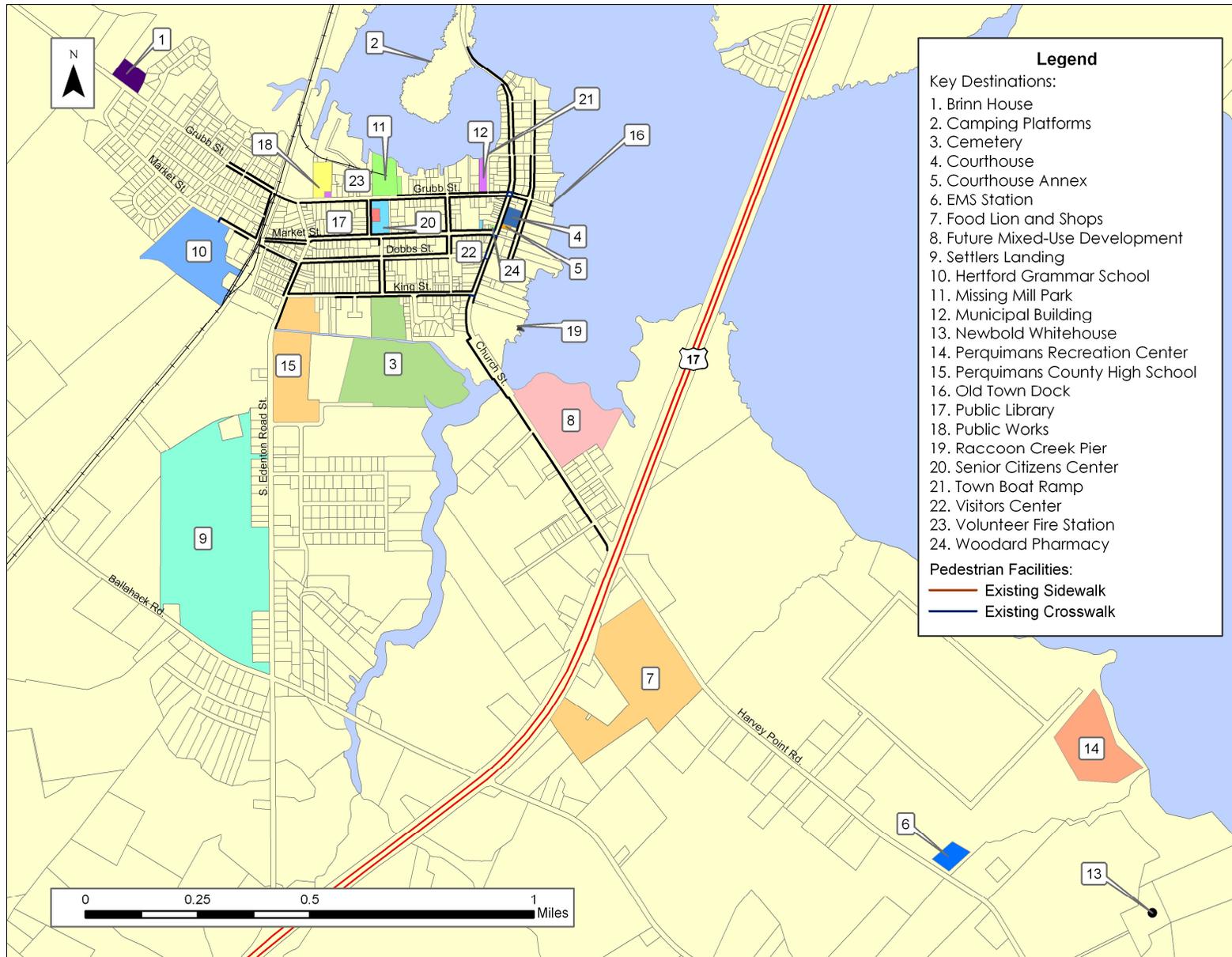


Figure 3-3. A map of some of the major landmarks and destinations in Hertford.

3.6. Summary

It is important to assess pedestrian access to all the major destinations in Hertford in order to develop recommendations that meet all the needs of the community. Although there are adequate sidewalk facilities in downtown Hertford, it is clear that the Town will need pedestrian access from residential areas to major destinations such as the Perquimans County Recreation Center and schools. In addition, it is important that the conditions along the existing facilities be comfortable and convenient for pedestrians – through amenities such as shade, benches, trash cans, and accessible intersection crossings. Not only will improved pedestrian conditions help to make Hertford a better place to live, it will also make it a more inviting place to visit. Recommendations in the Plan will target the need to provide safe, convenient, and comfortable access to major destinations and recreation routes in order to provide better, more enjoyable, walking environments for residents and visitors alike.

Quick Summary: This section provides a summary of the following plans and policies and how they relate to pedestrian facilities in Hertford:

- ◆ Historic Hertford North Carolina Development Strategic Plan (2001)
- ◆ Hertford Corridor Plan Phase I: US 17 (Draft 2007)
- ◆ NCDOT TIP
- ◆ Town's Subdivision Ordinance

Section 4. Existing Plans and Policies

4.1. Introduction

In order to thoroughly address Hertford's pedestrian needs, it is necessary not only to understand the Town's existing physical conditions - sidewalks, greenways, roadway crossings – but also how they are planned and designed. This section describes Hertford's current plans, policies, and programs which relate to the pedestrian system in the Town. These include the *Historic Hertford North Carolina Development Strategic Plan (2001)*, *Town of Hertford Corridor Plan Phase I: US 17 (Draft 2007)*, planned NCDOT projects, and the Town's ordinances.

4.2. Existing Plans

In recent years, Hertford has begun to experience a renewed interest in its downtown. In 2000, the Town became part of the North Carolina Main Street Program which emphasizes a four-point approach to improving downtown by organizing partnerships, promoting services and events, designing aesthetic improvements, and improving marketability. Since then, efforts to redevelop the downtown and the overall appearance of the Town have been underway. As part of this movement, both the Historic Hertford North Carolina Development Strategic Plan (2001) and the Town of Hertford Corridor Plan Phase I: US 17 (Draft 2007) outline future improvement projects for the Town. In addition to the Town's plans, Hertford's pedestrian system is also affected by projects planned by the North Carolina Department of Transportation. The following paragraphs describe in more detail the plans of both the Town and NCDOT.

Historic Hertford North Carolina Development Strategic Plan (2001)

The Historic Hertford North Carolina Development Strategic Plan is as a five-year plan for improvement of the Hertford Heritage Tourism Development Area of the historic Town of Hertford, North Carolina. It was prepared as an extension of the report, *A Vision for Heritage Tourism (1999)*, and incorporates the findings and recommendations of the Town's 2001 planning initiatives, including the Town's initial Main Street Program Report. The Plan provides information on priority improvement areas and major projects, and a strategic action plan which serves as a summary reference of overall actions recommended on all priority areas. The priority areas and major projects are as follows:

Priority areas

- ◆ Business Development and Land Use
- ◆ Streetscape Improvements
- ◆ Sign and Identity System
- ◆ Parking and Connectivity

◆ Waterfront Enhancement

Major Projects

- ◆ Business Development
- ◆ Planning and Zoning
- ◆ Utility Removal and Reconstruction
- ◆ Streetscape Design and Construction
- ◆ Courthouse Grounds Improvements
- ◆ Academy Green Improvements and Dobbs Street Walkway
- ◆ Sign and Identity System
- ◆ Parking Improvements
- ◆ Waterfront Improvement

Four of the five priority areas are directly pedestrian-related, and all but one of the nine major projects also have pedestrian elements.

Some of the projects which are discussed in the Development Strategic Plan are as follows:

- ◆ Potential boardwalk from WRC boat ramp to Missing Mill Park
- ◆ Streetscape improvements along Grubb Street and Academy Street
- ◆ Church Street walkway
- ◆ Major gateway on Church Street on the Winfall side of the S-bridge
- ◆ A walkway along the S-bridge
- ◆ Focal points at Front Street pier and Grubb Street piers
- ◆ Historic walking tour
- ◆ Greenway along Castleton Creek to connect to Church
- ◆ Streetscape enhancement at the following intersections: Market & Church, Dobbs & Church, and King & Church
- ◆ Wayfinding signage system

In addition to discussing projects, the plan also provides design guidance for street enhancements and wayfinding signage. This guidance is discussed more thoroughly in Section 5 of the Pedestrian Plan.

Perquimans County, North Carolina CAMA Core Land Use Plan (2006)

The Coastal Area Management Act (CAMA) requires North Carolina coastal communities in 20 counties to prepare management plans for dealing with the impacts of growth on coastal and coastal tributary ecosystems. The 2006 version ("CAMA Plan") that was reviewed contains a



Figure 4-1. A sign near the Perquimans County Courthouse in Downtown Hertford discusses Hertford's National Register Historic District.

number of general statements relevant to the discussion of the Pedestrian Plan, including the following.

- ◆ Many of the incoming residents are expected to be middle-aged or retirement age, which raises issues with accessibility to services.
- ◆ The County is to continue to educate its citizens about desirable development patterns; however, resource levels are low to technically implement different development standards.
- ◆ The CAMA Plan also discusses an initiative by Trailblazers, a volunteer group that is building a greenways/blueways system plan. The CAMA Plan also stresses the potential importance of ecotourism.
- ◆ In terms of future development, the land use plan component of the CAMA Plan does not specifically speak to pedestrian considerations, although it does encourage.

The CAMA Plan does not carry the weight of regulatory authority, but compliance with the recommendations of the CAMA Plan is often used as a determining factor in State and Federal permitting considerations. In short, the CAMA Plan is focused on preservation of water quality, but also includes recommendations in the Action Plan concerning the development of a county-wide greenway system, revisions to the existing subdivision standards, and creating public access at the Commerce Center. Each of these action steps can be a springboard to include pedestrian accommodations or make travel by walking easier or more enjoyable.

Town of Hertford Corridor Plan Phase I: US 17 (Draft 2007)

The Town of Hertford Corridor Plan Phase 1: US 17 is the first of several plans for corridor improvements to be produced by the Hertford Corridor Project Committee. A result of the strategies identified in the 2001 Hertford Development Strategic Plan, this Committee was created to study land use, transportation, and future development potential for thoroughfare corridors and gateways in Hertford and to develop a plan to protect and improve the appearance of and views from these roadways through Hertford and into the National Register Historic District that includes downtown Hertford. The study area for the Corridor Study was

- ◆ US 17 within Hertford and its ETJ,
- ◆ Lands from Castleton Creek south to US 17 including Church Street, Ballahack Road and Edenton Road Street, and
- ◆ From US 17 south to Commerce Street including the Commerce Center Project and its extension across to Wynne Fork Road including Wynne Fork Road and Harvey Point Road.

As part of the *Town of Hertford Corridor Plan Phase 1: US 17*, the Hertford Corridor Project Committee developed a vision for the US 17 Corridor, which included: “Walkways and bicycle facilities throughout Hertford connecting parks, waterfront access, shopping and neighborhoods support an active lifestyle and a sociable setting for peoples of all ages.” In addition to presenting the Corridor Committee’s vision, the Plan includes an analysis of the issues along the study area, guiding principles for improvements, and a series of recommendations. A Visual Preference Survey was also conducted to identify town residents’ preferences for design features in the Town.

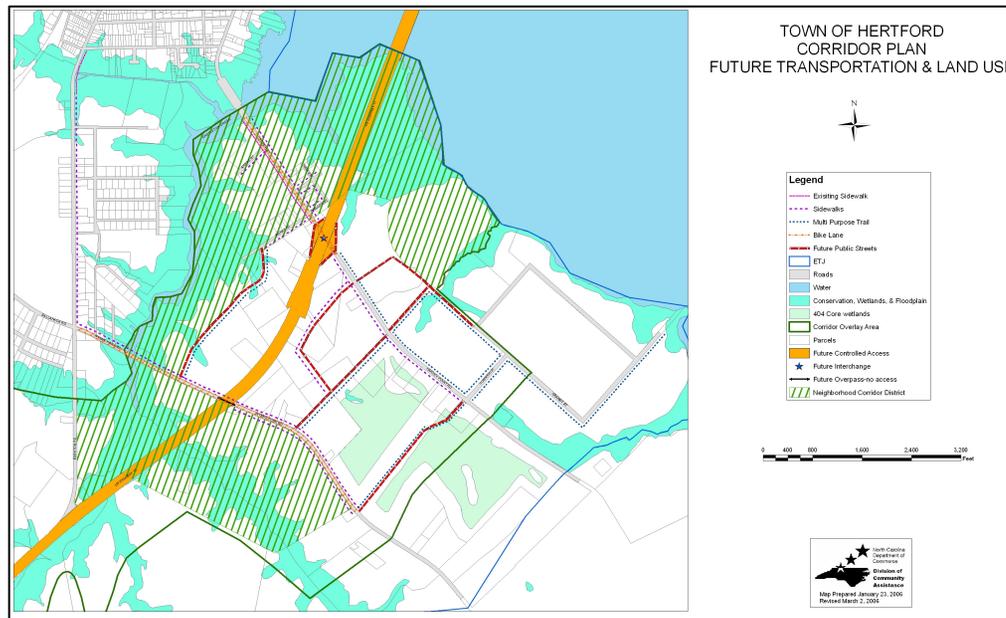


Figure 4-2. 2007 Draft Town of Hertford Corridor Plan Future Transportation & Land Use.

Some of the issues which are identified by the Plan include:

- ◆ Perquimans County is experiencing a boom in development, particularly in areas with waterfront accessibility and water views.
- ◆ The Highway 17 Corridor does not provide clues to motorists that Hertford has an attractive, inviting downtown area on the waterfront.

- ◆ Many current residents in Hertford do not have access to a vehicle and must walk, bike, or rely on others with vehicles for rides. Other residents like to walk or bike for fitness or pleasure.
- ◆ The sidewalk on Church Street is very important because it connects the intersection at US 17 with downtown Hertford and residential areas in Hertford. Residents use the sidewalk for fitness walking and to walk to the grocery store and other shopping located along US 17.
- ◆ The intersection of Church Street and Highway 17 is the prominent entrance way into Hertford and the only access from Town to the only grocery store in Hertford and the Perquimans County Recreation Center. Currently, this intersection is dangerous for pedestrians, which include children walking or bicycling to the Recreation Center. The signal is designed to maximize vehicle through-put along US 17, and therefore there are no pedestrian heads and inadequate time for pedestrians to cross. As more businesses relocate to the US 17 corridor and to the Commerce Center, a safe way for pedestrians to cross US 17 will be even more critical.
- ◆ NCDOT has plans to create a grade-separated interchange at the US 17/Church Street/Harvey Point Road intersection and grade separation with no access to US 17 at the Wynne Fork Road and US 17 intersection. When these changes are made to US 17, local access will be fragmented. The only way to the US 17 corridor area from Town will be along Church Street. Access for pedestrians and bicyclists will become more difficult.
- ◆ Pedestrian facilities should be provided along all streets and within commercial developments in the US 17 corridor area.

The Plan also contains five guiding principles. Of these, three principles addressed items related to pedestrian facilities in the Town:

- ◆ The Highway 17 corridor sets the image of Hertford to motorists, which should therefore be positive.
- ◆ From US 17, one should sense and or learn from way-finding signage that the downtown and historical center of Hertford is nearby and has an inviting charm.
- ◆ The project area should be accessible to all pedestrians, bicyclists, and motorists. It should be safe to cross US 17.

The Corridor Plan makes the following recommendations which relate to pedestrian facilities in the Town:

- ◆ When the proposed interchange at Church Street/Harvey Point Road is completed, a well planned, connected local street network with access to properties in the project area will be critical. The plan should include pedestrian and bicycle facilities along local collectors.

- ◆ Improvements as a result of the Corridor Study should provide the citizen an option of routes between destinations and the opportunity to drive, walk or bike to a destination.
- ◆ Sidewalks are recommended:
 - Along the north east side of Church Street
 - Along streets intersecting Church Street
 - Along the southwest side of Harvey Point Road
 - Along both sides of Wynne Fork Road
 - Along proposed streets into and through the shopping center area south east of US 17
- ◆ Multi-purpose facilities area recommended:
 - On the north east side of Harvey Point Road
 - Along streets in the commerce center
 - On one side of the proposed roadways between Harvey Point Road and Wynne Fork Road
 - On one side of the proposed connector road between Wynne Fork Road and Berry Street
- ◆ The Study recommends two overlay districts: (1) US 17 Overlay District, (2) Neighborhood Corridor District. Both containing recommendations for pedestrian facilities to require sidewalks along street, pedestrian connections between building entrances, sidewalks, and parking areas. Sidewalk connections should also be required between parcels. For the US 17 Overlay district, traffic calming in parking areas is recommended as needed to allow for safe pedestrian crossings.

In addition, some of the findings of the Visual Preference Survey related to pedestrian facilities in the Town and may affect the recommendations made by this Pedestrian Plan. These findings were:

Design Features which rated favorably include:

- ◆ In commercial parking areas, trees and a sidewalk separating bays of parking and leading to business entrance
- ◆ sidewalk separated from the street with vegetation that includes trees
- ◆ wide sidewalks
- ◆ human scaled amenities including benches, flowers, banners, and ornamental lights
- ◆ integrated landscaping and sidewalk
- ◆ ornamental features including lighting, banners, flowers, and street trees

Design Features which rated poorly include:

- ◆ no trees adjacent to sidewalk
- ◆ multi-use facilities for pedestrians and bicycles
- ◆ sidewalk on multilane street with no trees and utilitarian lights on tall poles

- ◆ sidewalk adjacent to the travel lane

Although the Corridor Study focuses primarily on the US 17 Corridor, its recommendations for sidewalks, ordinances which require pedestrian connections, and good design features can be applicable throughout the Town.

NCDOT TIP Plans

According to the *Town of Hertford Corridor Plan Phase I: US 17* (Draft 2007), NCDOT is considering a grade separated interchange at the US 17/Church Street/Harvey Point Road intersection and a grade separation with no access to US 17 at the Wynne Fork Road and US 17 intersection. According to the NCDOT 2007 – 2013 STIP, both of these projects are listed under TIP No. R-4459, which is labeled as programmed for planning and environmental study only at this time.

Additional planned TIP projects in the Hertford area are R-4467 and B-4923, described in the table below. Although listed separately in the TIP, R-4467 and B-4923 will likely be designed and constructed at the same time due. R-4467 includes the causeway in addition to the S-bridge, whereas B-4923 is simply the S-Bridge.

TIP No.	Location	Description	Status
R-4467	US 17 Business/ NC 37 bridge over the Perquimans River	Construct a new roadway on pilings or convert roadway to a bridge.	Programmed for planning and environmental study only.
B-4923	US 17 Business over the Perquimans River	Replace bridge No. 8.	ROW FY 2012 Mitigation FY 2012 Construction FY 2013

4.3. Existing Policies

Currently, the Town’s subdivision ordinance requires that new development have curb-and-gutter on streets, provide underground wiring, and plant street trees that are located on private property. It also requires the dedication of future rights-of-way for streets in accordance with adopted maps and plans. However, the ordinance does not require sidewalks to be installed along public streets, although it does have an optional requirement for pedestrian easements, walkways or alleys if a block is greater than 1,000 feet long. An ordinance revision should be considered to include mandatory sidewalk and greenway easement requirements connecting adjacent properties, as well as sidewalk on one side of all interior streets regardless of length. Converting this language from a Planning Board option to a staff-level option (or requirement)

would strengthen the language (Section 6-2.8). Additionally, the preliminary plat requirements could be expanded to require a vicinity or location map indicating the location of parks, schools, commercial centers, and other pedestrian destinations within a ¼-mile radius (Section 5-2.3 part A).

The Town’s current zoning ordinance (May 19, 1980) covers the Town as well as its Extraterritorial Jurisdiction and has the stated purpose of, among other things, to provide for:

"...light and air; to prevent the overcrowding of land; to avoid undue concentration of population; to facilitate the adequate provision of transportation, water, sewerage, schools, parks and other public requirements; to promote desirable living conditions and the sustained stability of neighborhoods; to protect the property against blight and depreciation; to protect and enhance the visual character and quality of the built environment." (Article 1, Section 1-4)

The zoning ordinance details specific requirements for individual land uses, then proceeds to discuss specific design elements, the following of which are pertinent to pedestrian travel and accommodation:

- ◆ Off-Street Parking (Article V). Number of spaces and requirements are listed, but there is no mention of the location relative to the building. Additional information about parking location, such as that 50% should be required to be placed to the side or rear of the structure, would produce a better walking environment by bringing building faces closer to the street. Requirements for pedestrian connections or, at a minimum, easements, between adjoining commercial and commercial/multi-family residential properties would also facilitate shorter pedestrian travel distances.
- ◆ Landscape Regulations (Article XIII). Updating this section to include graphics and text illustrating the desired relationship between buildings, buffer areas, pedestrian ways, and swales would be a useful step towards creating a best practice for development. Requiring wheel stops to be at least three feet from the nearest pedestrian way would help to ensure that parked cars do not intrude into the pedestrian space.

Generally, the ordinances in Hertford, in conjunction with developing street design standards, would benefit from a close review throughout and making modifications that are integrated. For example, specifying when a sidewalk should be installed (on one side of the street or both) on various hierarchies of streets.

Land-Use/Roadway Functional Classification/ Dwelling Unit	New Urban and Suburban Streets	Existing Urban and Suburban Streets
Commercial & Industrial (All Streets)	Both sides.	Both sides. Every effort should be made to add sidewalks where they do not exist and complete missing links.
Residential (Major Arterials)	Both sides.	Both sides.
Residential (Collectors)	Both sides.	Multifamily - both sides. Single Family dwellings - prefer both sides; require at least one side.
Residential (Local Streets) More than 4 Units Per acre	Both sides.	Prefer both sides; require at least one side
Residential (Local Streets) 1 to 4 Units Per Acre shoulder on both sides required.	Prefer both sides; required at least one side.	One side preferred, at least five feet width.
Residential (Local Streets) Less Than 1 Unit Per Acre	One side preferred, shoulder on both sides.	At least 4-ft. shoulder on both sides required.

NOTES:

- 1) Any local street within two blocks of a school site that would be on a walking route to school - sidewalk required on at least on side.
- 2) Sidewalks may be omitted on one side of new streets where that side clearly cannot be developed and where there are not existing or anticipated uses that would generate pedestrian trips on that side.
- 3) Where there are service roads, the sidewalk adjacent to the main road may be eliminated and replaced by a sidewalk adjacent to the service road on the side away from the main road.
- 4) For rural roads not likely to serve development, a should at least 4 feet in width, preferably 8 feet on primary highways should be provided. Surface material should provide a stable, mud-free walking surface.

Source: Wisconsin DOT, "Wisconsin Pedestrian Planning Guidance: Guidelines for Metropolitan Planning Organizations and Communities in Planning and Developing Pedestrian Facilities." September, 1993

Figure 4-3. Sample Pedestrian Guidelines (adapted from Wisconsin DOT).

4.4. Summary

Hertford's various Plans focus in general on creating a more vibrant downtown area and improving the Town's existing major roadway corridors. Most of the planned projects center around the area along Church Street/US 17 Business, extending from the intersection with Harvey Point Road through town and across the river to Winfall. The Historic Hertford North Carolina Development Strategic Plan calls for a variety of improvements, including those to the streetscape and waterfront; any improvements should always focus on ensuring pedestrian safety and access. Beyond downtown, major planned transportation projects include improvements to the Church Street/US 17 area, the potential replacement of the US 17 Business bridge (the S-Bridge) over the Perquimans River, and the conversion of the intersection of US 17/Harvey Point Road to an interchange. These projects are an excellent opportunity for the Town to provide key links which are currently missing from the existing pedestrian system. As they progress forward in design, they should be coordinated with the recommendations in the Pedestrian Plan in order to maintain existing pedestrian access and create better future pedestrian access.

Quick Summary: This section highlights various areas in Hertford where pedestrian improvements should be made. These areas will form the basis of the project recommendations in Section 8.

Section 5. Focus Areas

5.1. Introduction

Through staff input, the stakeholder committee, and the public involvement process, several "Focus Areas" were developed within Hertford for concentrating pedestrian improvement projects. These areas were identified based on demand for facilities, safety need, and opportunities for connections. The Focus Areas are as follows:

1. Intersection of US 17 and Church Street/Harvey Point Road and points south
2. Perquimans High School and Hertford Grammar School
3. Missing Mill Park Area
4. Raccoon Creek Pier and Canoe Launch Area
5. Winfall Connections and S-Bridge

Figure 5-1 shows a map of Hertford and the general limits of each of the focus areas. The following paragraphs describe the focus areas, the pedestrian needs, and proposed recommendations for each location.

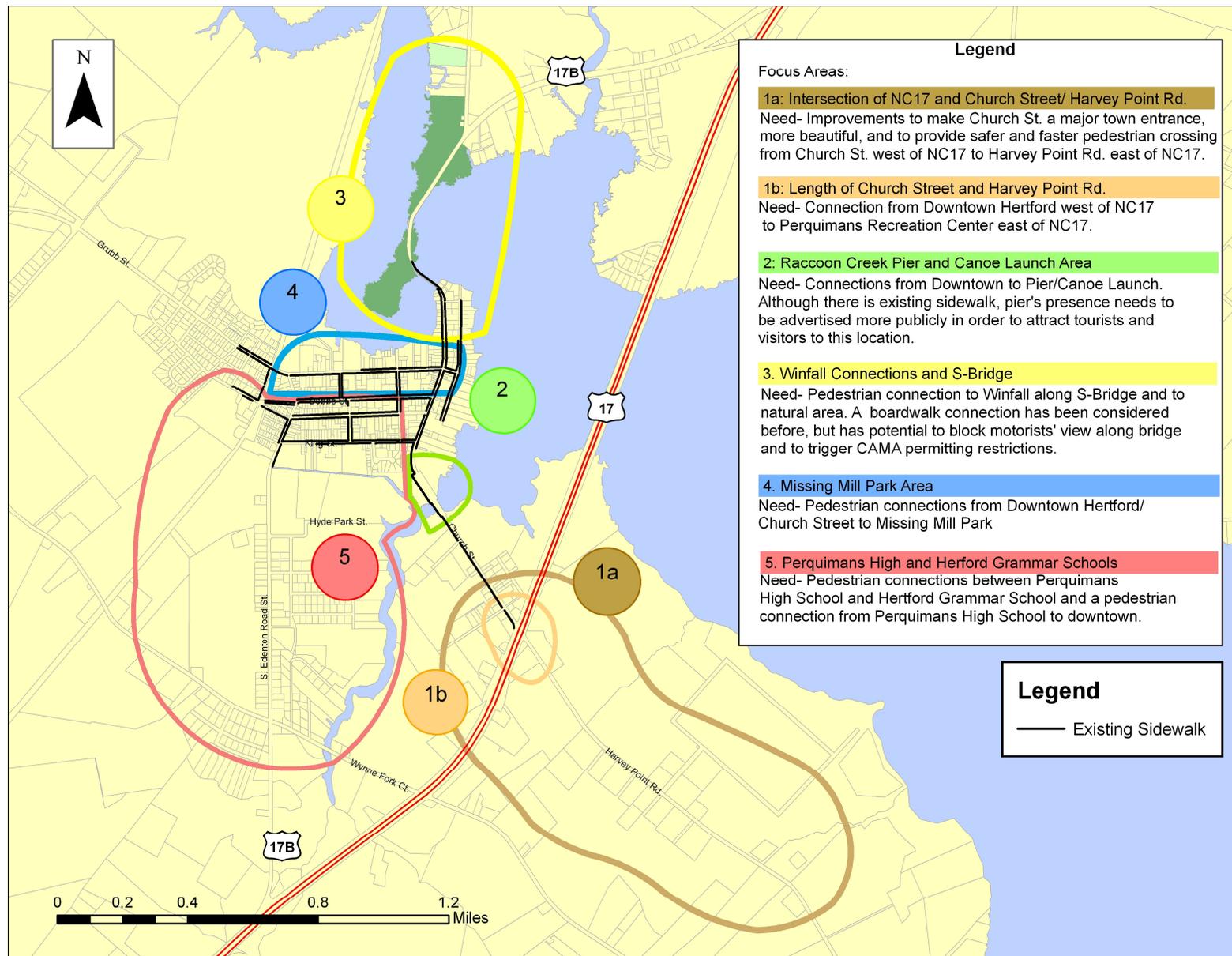


Figure 5-1. Focus areas for targeted projects in the Pedestrian Plan.

5.2. Intersection of US 17 and Church Street/Harvey Point Road and Points South

The intersection of US 17 and Church Street/Harvey Point Road, as well as the area south of this intersection on Harvey Point Road, has been identified in the *Hertford Corridor Plan Phase I: US 17 Study* as a critical pedestrian connection for linking the southeast part of town to Downtown Hertford. The area can best be divided into two subareas: the intersection of US 17 and Church Street/Harvey Point Road, and the length of Church Street and Harvey Point Road from Downtown Hertford to the Perquimans County Recreation Center. The following is a description of the needs and recommendations for each subarea:

Intersection of US 17 and Church Street/Harvey Point Road*Need:*

- ◆ Improvements to make Church Street, a major town entrance, more beautiful and to provide safer and faster pedestrian crossing from Church Street west of US 17 to Harvey Point Road east of US 17

Recommendations:

- ◆ Pedestrian improvements and gateway/appearance enhancements at intersection for US 17/Church Street/Harvey Point Road

Length of Church Street and Harvey Point Road

Need:

- ◆ Connection from Downtown Hertford west of US 17 to Perquimans Recreation Center East of US 17

Recommendations:

- ◆ Examine corridor for pedestrian improvements. May consider a greenway/boardwalk along the waterfront or a wide sidewalk/multi-use path along US 17

5.3. Perquimans High School and Hertford Grammar School

Currently, connections between the Perquimans High School and Hertford Grammar School are blocked by the presence of Norfolk Southern train tracks and a lack of sidewalk connections. There is on-going work on a greenway/boardwalk in the Perquimans High School area along Jenny’s Gut area. Potential connections include extending the greenway/boardwalk to Hertford Grammar School, or constructing an unrelated greenway between the two schools. Better connections between the two schools would promote more walking and bicycling to school, encouraging healthier lifestyles for students and parents.

Need:

- ◆ Pedestrian connections between Perquimans High School and Hertford Grammar School
- ◆ Pedestrian connection from Perquimans High School to Downtown
- ◆ Pedestrian connections from residential areas south of Grammar School and High School on S. Edenton Street Road and Wynne Fork Road

Recommendation:

- ◆ Consider greenway connection between High School and Grammar School to cross railroad tracks
- ◆ Consider greenway connection from Perquimans High School to downtown through the Jenny’s Gut area/Hertford Cemetery
- ◆ Construct a sidewalk along South Edenton Street Road to connect existing and proposed neighborhoods along the road to the Town and the High School.
- ◆ Construct sidewalk along Wynne Fork Road and improve crossing US 17 and Wynne Fork Road



Figure 5-2. Perquimans High School is located along South Edenton Street Road in Hertford, within close proximity of Hertford Grammar School. There is limited sidewalk access between the two schools.



Figure 5-3. Missing Mill Park could be a major pedestrian destination in Town. It should have more signs to alert visitors of its presence and a crosswalk to connect it to the nearby senior center and tennis courts.

5.4. Missing Mill Park Area

Several comments from the Pedestrian Plan Survey and stakeholder input indicate that there is a lack of visibility for Missing Mill Park from the Downtown Hertford area along Church Street. In addition, sidewalk is missing along Grubb Street for easy access to the Park. Wayfinding improvements and better pedestrian facilities – including curb ramps, additional sidewalk, and crossings – would make the park a more popular destination for residents and visitors alike.

Need:

- ◆ Pedestrian connections from Downtown Hertford/Church Street to Missing Mill Park

Recommendations:

- ◆ Complete sidewalk to Missing Mill Park
- ◆ Provide a pedestrian crossing between Missing Mill Park and Tennis Courts/Senior Center
- ◆ Develop wayfinding signage to direct visitors to Missing Mill Park from Downtown
- ◆ Consider a greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building and WRC Boat Ramp



Figure 5-4. The Raccoon Creek Pier area should be highlighted to make it more attractive for visitors and residents.

5.5. Raccoon Creek Pier and Canoe Launch Area

The Raccoon Creek Pier and Canoe Launch is a public boat launch located along Church Street at the westernmost edge of downtown Hertford. The pier and boat launch are located behind public housing, which can make the area seem secluded and isolated. The Town should highlight the presence of this pier to make it more attractive for visitors and residents to use it.

Need:

- ◆ Connections from Downtown to Pier/Canoe Launch. Although there is existing sidewalk, pier's presence needs to be advertised more publicly in order to attract tourists and visitors to this location

Recommendations:

- ◆ Consider developing wayfinding signage within downtown to direct visitors to Pier
- ◆ Consider connecting pier with potential greenway/boardwalk discussed in Focus Area 1

5.6. Connections along the Causeway and S-Bridge

Currently, Hertford and Winfall, although within close proximity to each other, are disconnected due to the lack of pedestrian facilities over the S-bridge, which is the only roadway link between the two towns. Children in Winfall attend both the Perquimans County High School and Hertford Grammar School in Hertford, and many Hertford residents work in Winfall, and *vice-versa*. In

addition, a pedestrian connection between the two towns would serve as a nice attraction for visitors looking for a long scenic walk. There are two options for providing pedestrian facilities – constructing a pedestrian facility (sidewalk or boardwalk) along the S-bridge, or constructing an independent boardwalk/bridge for pedestrian connections unrelated to the S-bridge. Concerns about cost and that either of these options may restrict motorist’s view of the river have hindered progress on improvements in the past. In spite of these objections, a better pedestrian connection is needed between the two towns and should be considered.

Need:

- ◆ Pedestrian connection to Winfall along S-Bridge and to natural areas, schools. A boardwalk connection has been considered before, but has potential to block motorists’ view along bridge and to trigger CAMA permitting restrictions

Recommendations:

- ◆ Reconsider a boardwalk connection to Winfall along S-Bridge, or a new location
- ◆ Provide sidewalk along causeway/N. Church Street where possible beyond bridge
- ◆ Provide wayfinding signage to direct visitors and residents to Winfall as necessary

5.7. Other Proposed Projects

In addition to the focus areas discussed, some recommendations were developed that are not project related, and they are listed as follows. Specific actions to address these items are discussed more thoroughly in *Section 7: Program and Policy Recommendations*.

- ◆ Developing a formalized, signed Historic Walking Tour. Currently, there is an existing self-guided Historic Walking Tour which is marked by painted “turtle” symbols to indicate stops along the route. The Town should reinforce and support this tour, and could even consider developing a related bicycle tour.
- ◆ Other marketing ideas were discussed, including continuing to support the annual Hertford 5K run conducted in conjunction with the Annual Indian Summer Festival and creating a trail with mileage charts/markers for walkers and exercise stations
- ◆ There is an effort in the Town to develop a “blueway” along the Perquimans River. The Town could consider incorporating sidewalk/greenway connections to camping platforms and put-ins developed for the blueway.
- ◆ Identifying ways to make certain areas of the Town feel safer to visitors and residents through better lighting, maintenance, and appearance projects.

5.8. Summary

Each of the focus areas discussed in this section are key locations for pedestrians as either destinations, such as Raccoon Creek Pier, or access routes, such as Church Street to US 17. Section 8 identifies specific project improvements for each of the focus areas and an overall prioritization scheme. By addressing pedestrian improvements in each of these focus areas, the Town will create an overall more pedestrian-friendly environment for residents and visitors.

Quick Summary: This section provides a set of design guidelines for best practices to make Hertford more pedestrian-friendly.

Section 6. Design Guidelines and Standards

6.1. Introduction

This section provides guidance for the Town of Hertford as they construct new pedestrian facilities and reconstruct existing pedestrian facilities to meet better standards. This section is divided into the following topics:

- ◆ legal rights of pedestrians
- ◆ pedestrian facilities and their design
 - sidewalks
 - crossings: signalized or unsignalized
 - greenways
- ◆ ADA requirements
- ◆ downtown area standards
- ◆ school standards
- ◆ sidewalk construction policy and maintenance
- ◆ parking lots

This section of the Plan is important because it provides a consistent set of guidelines within the Town to help create a uniform appearance to Hertford's sidewalks and a more connected system.

6.2. Legal Rights of Pedestrians

It is important to understand the legal rights of pedestrians because these guide and define how pedestrian facilities are constructed and provided. Some of the legal rights of pedestrians are defined in Sections 20-172 through 20-175.2 of the North Carolina General Statutes. Some of the items which should be considered are the following:

- ◆ Drivers must yield to pedestrians (or cyclists) crossing a driveway, alley exit, or parking garage exit on a sidewalk. (§20-173)
- ◆ Pedestrians crossing any roadway other than at a marked crosswalk must yield to vehicles.
- ◆ Pedestrians should cross at street intersections or in marked crosswalks.
- ◆ If there are sidewalks, pedestrians are not to walk in the roadway. Where sidewalks are not provided, any pedestrian walking along the roadway will walk to the extreme left, facing in the direction of approaching traffic.
- ◆ Every driver must consider pedestrians at all times, especially exercising care in the presence of children or incapacitated persons on the roadway. (§20-174)
- ◆ Special emphasis on leaving adequate crossing room at intersections is noted for visually handicapped persons. (§20-175.2)

More information can also be found in the NC Bike/Pedestrian Laws Guidebook, available at the NCDOT's Division of Bicycle and Pedestrian Transportation webpage: <http://www.ncdot.org/transit/bicycle/laws/resources/BikePedLawsGuidebook-Part-1.pdf>.

In addition, pedestrian access is also governed by the requirements of the Americans with Disabilities Act of 1990, a civil rights law which prohibits discrimination against people with disabilities in all aspects of life. As done throughout the US, Hertford must provide transportation facilities, including sidewalks and other pedestrian facilities, which comply with the guidelines set forth in the ADA Accessibility Guidelines (ADAAG) in order to meet the standards of the American Disabilities Act. Some of the major items related to pedestrian facilities that are addressed by ADAAG include curb ramps and cross-slopes. The following bullets describe ADAAG-compliant design for these items:

- ◆ **Curb ramps: design and placement.**
 - *DESIGN:* Curb ramps are a significant and required feature of accessible pedestrian transportation systems, and must be designed carefully to fulfill their function and the requirements of the Americans with Disabilities Act. Curb ramps should not have a slope greater than 1:12, meaning that for every foot of travel, the slope should not rise more than one inch. To provide a tactile warning to the visually impaired, raised truncated domes with a color contrast to the background material (typically concrete) should be used, with measurements shown in Figure 6-1.i The *ADA Accessibility Guidelines for Buildings and Facilities* (<http://www.access-board.gov/adaag/html/adaag.htm#A4.29.2>) has an easy-to-use format for locating specific design criteria related to curb ramps, rise/run restrictions on ramps, and figures illustrating basic concepts.ⁱⁱ
 - *PLACEMENT:* Curb ramps should be placed entirely within the area of a marked crosswalk, so that a pedestrian can enter the ramp space at an angle perpendicular to the direction of travel. Generally, the standard is to have separate curb ramps on each corner; if a shared (sometimes called corner or diagonal) curb ramp is constructed, then the width and radius should accommodate the user so that entry onto the ramp is parallel to the direction of travel. Figure 6-2 provides examples of the acceptable relationship between crosswalk and curb ramp location/widths.

- ◆ **Cross-Slopes.** Cross-slopes, or a slope along the travelway surface which is perpendicular to the direction of travel, can often make it very difficult for wheelchair travel. In addition, it can also make for treacherous walking conditions for individuals with problems with their balance and coordination. Cross-sloping most frequently occurs in conditions in which a driveway meets a sidewalk, but can also occur in other

situations. In order to minimize the risk of a dangerous and difficult travel condition for some, cross-sloping is regulated by ADAAG such that cross-slopes should not exceed two percent, and preferably not exceed 1.5 percent where possible. Figure 6-3 indicates the preferred (top), conditionally acceptable (middle), and unacceptable (bottom) design solutions for new driveways as they interface with sidewalks.

For a complete guide to ADA requirements, please see the National Access Board's website: www.access-board.gov.

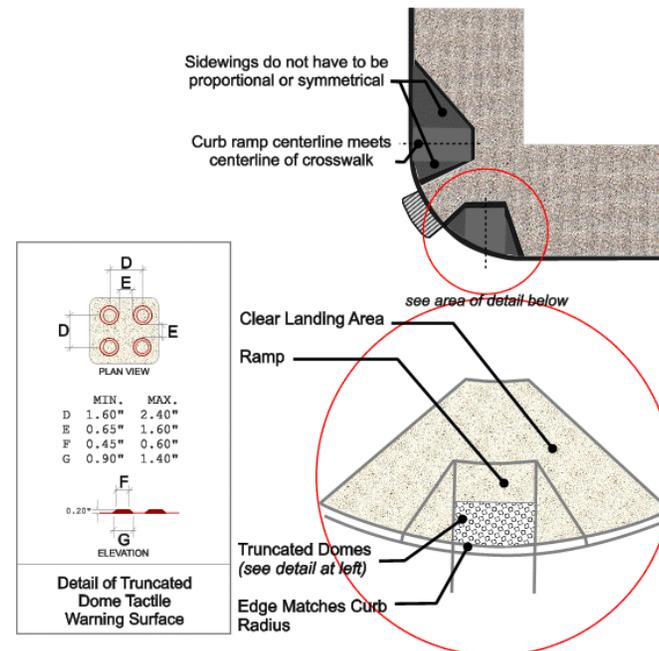


Figure 6-1. Detail of an ADA-compliant curb ramp design with truncated dome measurements.

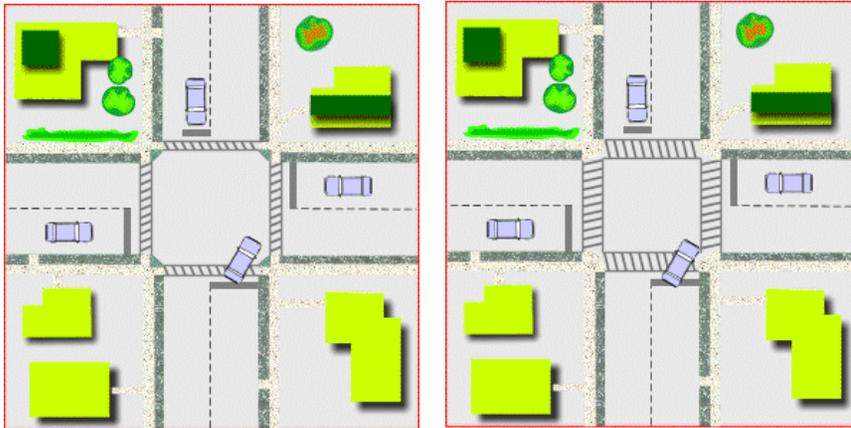
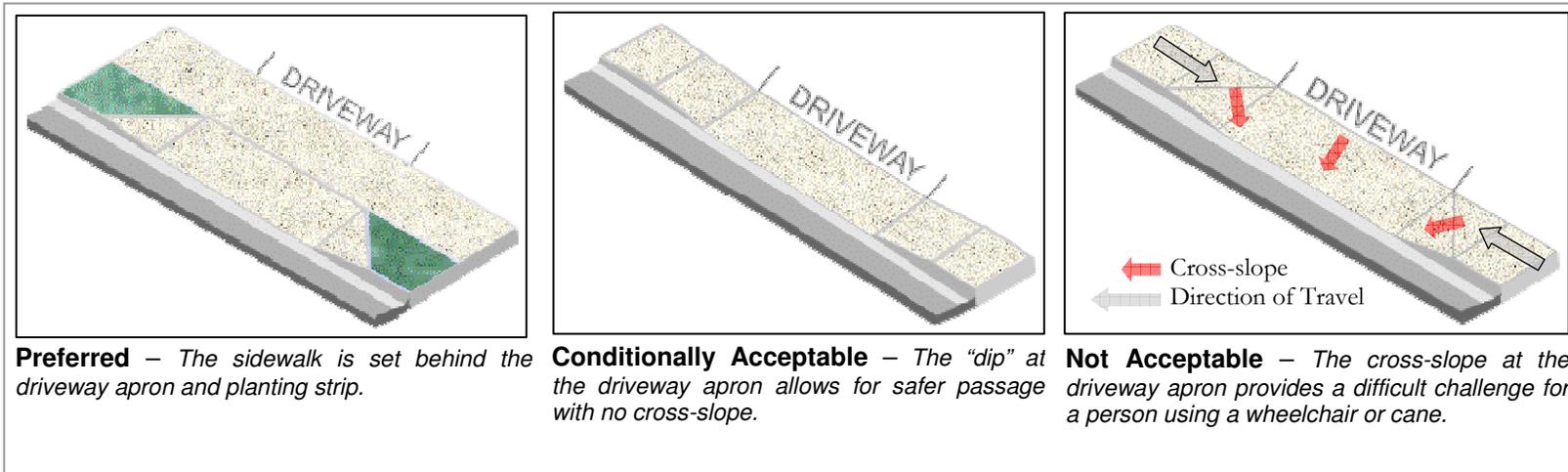


Figure 6-2. Drawing of appropriate curb ramp placements. In each image, the curb ramps are located completely within the crosswalk limits.



Preferred – The sidewalk is set behind the driveway apron and planting strip.

Conditionally Acceptable – The “dip” at the driveway apron allows for safer passage with no cross-slope.

Not Acceptable – The cross-slope at the driveway apron provides a difficult challenge for a person using a wheelchair or cane.

Figure 6-3. Examples of acceptable and unacceptable design solutions for minimizing cross-sloping at a driveway and sidewalk interface.

6.3. Pedestrian Facilities and their Design

There are a variety of sources for design guidance for pedestrian facilities, including the following:

- ◆ NCDOT (Draft, 1997)
- ◆ The American Association of State Highway and Transportation Officials' *Guide for the Planning, Design, and Operation of Pedestrian Facilities* (AASHTO, 2004)
- ◆ Manual on Uniform Traffic Control Devices (MUTCD), frequently updated
- ◆ Federal Highway Administration (FHWA)

The North Carolina Department of Transportation adheres to the design guidelines provided in the AASHTO and MUTCD guidebooks. In general, pedestrian facilities can be described in the following categories:

- ◆ sidewalks
- ◆ crossings
- ◆ greenways

The Town currently does not have its own standards for pedestrian facilities. The following paragraphs provide national standards and best practices for pedestrian facilities by category.

6.3.1. Sidewalks

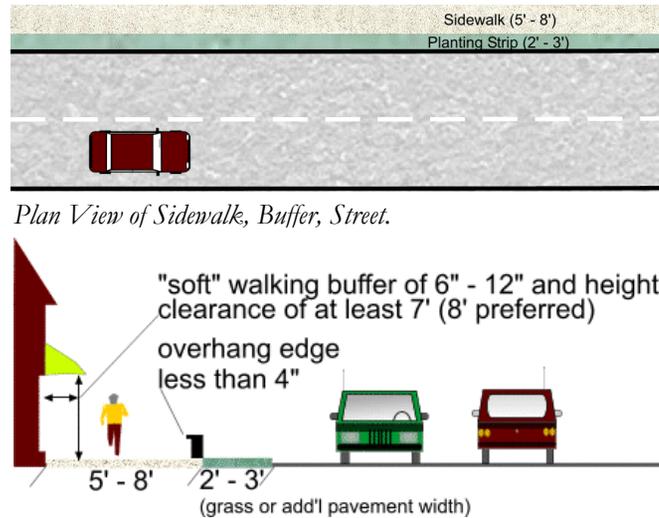
Standard sidewalk is usually five feet minimum in width, concrete, and placed along roadways with curb and gutter. In general, the width of sidewalks should accommodate two persons walking past one another, a width generally recognized to be five feet, at a minimum. Other circumstances that may require additional sidewalk width are: (1) to accommodate the overhang of parked vehicles from off-street or angled on-street parking areas; and (2) additional buffer from traffic when a planting strip cannot be installed.

Additional design considerations for on-street sidewalk facilities include the following:

- ◆ Eliminating both high and low contact points with tree branches, mast-arm signs, overhanging edges of amenities or furniture, and
- ◆ Providing clear space between walls on one side of the walkway and amenities, parking overhang, or plantings on the curb side of the walkway (see Figure 6-4 which diagrams the relationships between pedestrian features, building facades, and roadway).

In general, standard sidewalks should be concrete, which is more durable than asphalt. A more flexible material, such as rubberized paving, can be considered in situations in which there is the

potential for tree roots to crack and lift the concrete. Using these types of materials can reduce the risk of a tripping hazard, and also lower maintenance costs. More permeable materials, such as porous pavers, can also be considered for all pedestrian-ways, and in particular for greenways near streams, in order to reduce run-off from storm events. Caution should be used to consider total, lifecycle costs for alternative materials. For example, porous pavements are more expensive initially to install, but will also usually lose their porosity if the air spaces in the pavement are not regularly cleaned.



Plan View of Sidewalk, Buffer, Street.

Profile View of Sidewalk, Buffer, Street.

Figure 6-4. A diagram of the relationship between pedestrian features, building facades, and the roadway.

6.3.2. Crossings

Crossings are a critical feature in a well-connected pedestrian system because they provide the linkages between one segment of sidewalk to another as a pedestrian may cross a street, connect to another existing piece of sidewalk, or pass to a new development. A well-placed crossing can dramatically reduce pedestrian travel time and improve pedestrian safety – greatly increasing the convenience of walking as a mode of travel. Crossings can be both signalized and unsignalized, as well as located at intersections or at mid-block locations.

There are a variety of designs for unsignalized crossings, including striped crosswalks, zebra crosswalks, and raised platform crosswalks. There are also a variety of designs for signalized crossings, including:

- ◆ Pedestrian Signals and crosswalks
- ◆ Pedestrian Signals, crosswalks, and audible signals
- ◆ Pedestrian Signals, crosswalks, and countdown signals
- ◆ High Intensity Activated Crosswalk (HAWK) Signal

It is recommended that for all signalized intersections in the Town, the Town should have signals for pedestrians. In the Downtown area, pedestrian signals should include audible and visible cues and perhaps even countdown displays.



Figure 6-5. Examples of pedestrian-activated signalized mid-block crossings. **Top image:** An example of a pedestrian-activated signalized mid-block crossing. **Bottom image:** An at-site guide for pedestrians to assist them in understanding the meaning of the push-button signals.

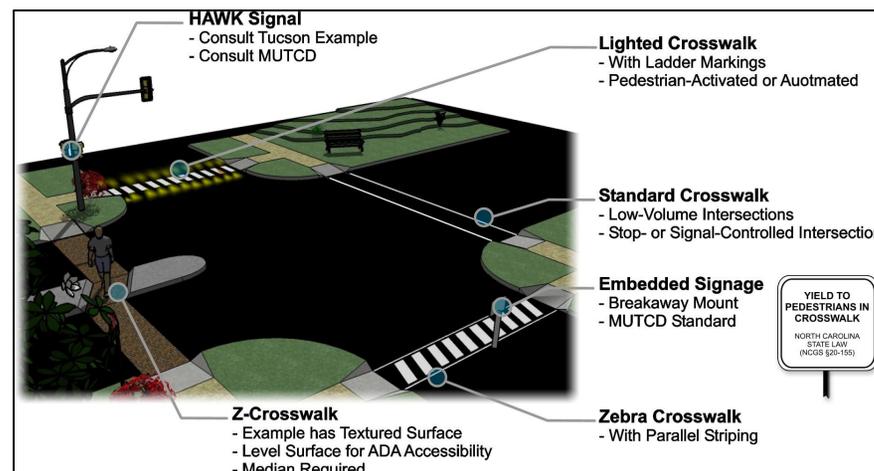


Figure 6-6. A diagram of various crossing treatments Hertford can consider to improve pedestrian accessibility and safety crossing the street.

While no mid-block crossings have been proposed in this Plan, standards and guidelines are provided should a certain condition or situation arise where this type of crossing would be suitable. For mid-block crossings, there is still no national consensus for when a crossing should be created mid-block, and when the mid-block crossing should be signalized. The City of Charlotte Department of Transportation has created a set of guidelines for assessing mid-block crossings, based in part on the work of FHWA and Charles Zegeer of the Pedestrian and Bicycle Information Center. In addition to numbers of pedestrians, vehicle speed, and vehicle volume on the roadway, there are a variety of other considerations which must be accounted for when determining whether to construct a mid-block crossing. These considerations include: lighting conditions, sight distance, numbers of lanes, and roadway width. Figure 6-7 shows the “solution space” identified by the City of Charlotte for considering a mid-block crossing. Table 1 shows the decision matrix created by the City of Charlotte for determining when to construct a mid-block crossing and identifying appropriate treatments.

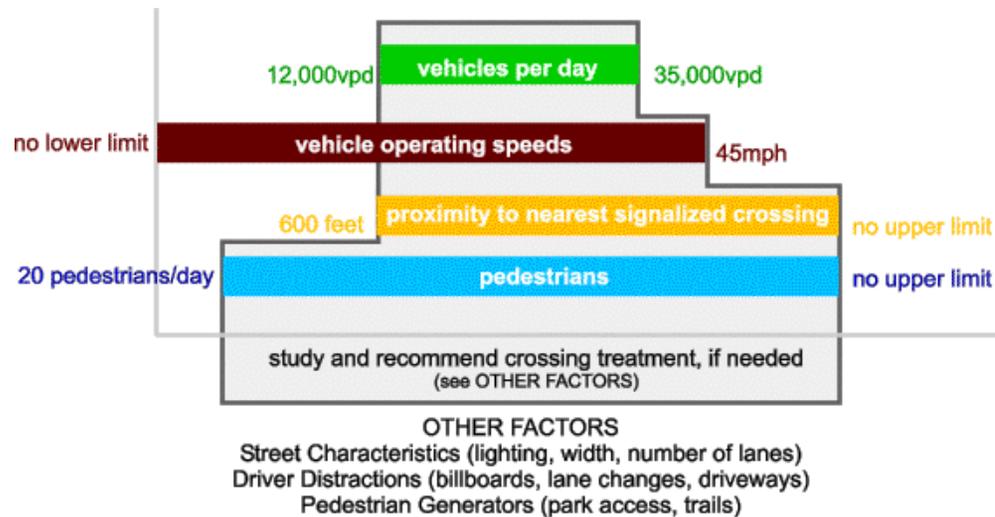


Figure 6-7. The City of Charlotte’s solution space for considering when to apply signalized mid-block pedestrian crossings.

Source: Charlotte Department of Transportation

Table 6-1. Mid-Block Crossing Treatment Design Criteria (Charlotte DOT, 2005).

**Note: MUTCD recommends pedestrian volumes of at least 400 for a four-hour period. **A HAWK (High-Intensity Activated Crosswalk) signal is a pedestrian-activated system used for high-volume crossings found to be useful in increasing the rate of driver responses to pedestrian crossings, especially in Tucson, AZ where they have been utilized extensively.ⁱⁱⁱ*

Pedestrian Mid-block Crossing Treatment	AADT	Operating Speed	Approx. Cost
Signs	5,000 – 35,000	Less than 45 mph	\$250 - 350
High-Visibility Markings	5,000 – 12,000	Less than 35 mph	\$500 – 1,500
Colored and Textured Markings	5,000 – 12,000	Less than 35 mph	\$5,000+
Curb Extensions	5,000 – 12,000	Less than 35 mph	\$5,000 – 25,000
Raised Crosswalks	5,000 – 15,000	Less than 30 mph	\$2,000 – 15,000
Refuge Island	12,000 – 30,000	Less than 40 mph	\$10,000 – 40,000
Median	15,000 – 35,000	35 - 45 mph	Varies greatly
In-Pavement Illumination	5,000 – 15,000	Less than 35 mph	\$40,000
Pedestrian-Only Signal*	15,000 – 35,000	35 – 45 mph	\$40,000 – 75,000
HAWK Signal**	15,000 – 35,000	35 – 45 mph	\$35,000 – 60,000

Given the sensitive nature of mid-block crossings, every new mid-block crossing treatment will require a specific investigation by the Town prior to initiating design and construction. In spite of this, mid-block treatments can be useful in improving safety in areas with fairly high pedestrian crossings and low numbers of vehicles and vehicle speeds, if located and designed properly.

6.3.3. Signage

In addition to sidewalks and crossings, pedestrian facilities also include signage along major pedestrian routes. Signs serve primarily to notify motorists and others of the presence of pedestrians. The intended effect is to cause motorists drive more cautiously and reduce their speeds, thereby improving the safety for pedestrians in the given area.

Signs can be used in a variety of places, including at crosswalks, at intersections, in-street, and near schools. National standards for sign placement and use can be found in the Manual for Uniform Traffic Control Devices (MUTCD). The MUTCD provides guidance for warning signs which can be used at both crosswalks, or along the roadway:

“Nonvehicular signs may be used to alert road users in advance of locations where unexpected entries into the roadway or shared use of the roadway by *pedestrians*, animals, and other crossing activities might occur.” (Page 2C – 21, 2003 Edition)

The following are some recommended signs which Hertford should consider installing. For more signs and more detailed guidelines for sign installation and use, Hertford should consult the MUTCD.

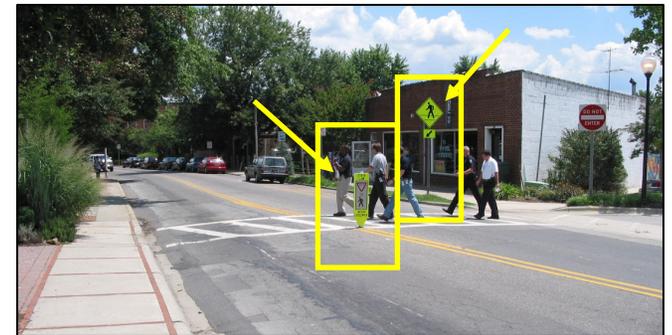


Figure 6-8. An example of two types of signs used to notify motorists of a pedestrian crossing.



Figure 6-9. Example standard pedestrian warning signs. The first sign is usually installed within the street to warn motorists to yield to pedestrians in a crosswalk - it does not have to be near a school. The second and third signs are common general pedestrian warning signs, while the fourth and fifth signs notify motorists of specific instances to watch for pedestrians. The fourth sign, “Turning Traffic”, is usually placed at intersections to warn motorists that are turning right or left to yield to pedestrians in crosswalks. For the fifth sign, the top sign can either be combined with the smaller “ahead” sign or the arrow symbol to indicate the presence of a crosswalk to motorists in a school zone. The signs at far right are examples of typical wayfinding signage to help direct cyclists at major decision points along a route.

6.3.4. Greenways

Greenways, sometimes also called multi-use trails, are one of the most popular pedestrian facilities, especially for recreation. Greenways can be both paved or unpaved paths, often unassociated with a roadway. They can be used by pedestrians, cyclists, and other non-motorized users. Greenways are typically no less than 10 feet wide and asphalt if paved. Figure 6-10 provides a diagram of a general greenway design.

Additional guidance on greenway design and standards can be found at:

www.ncdot.org/transit/bicycle/projects/project_types/Multi_Use_Pathways2.pdf

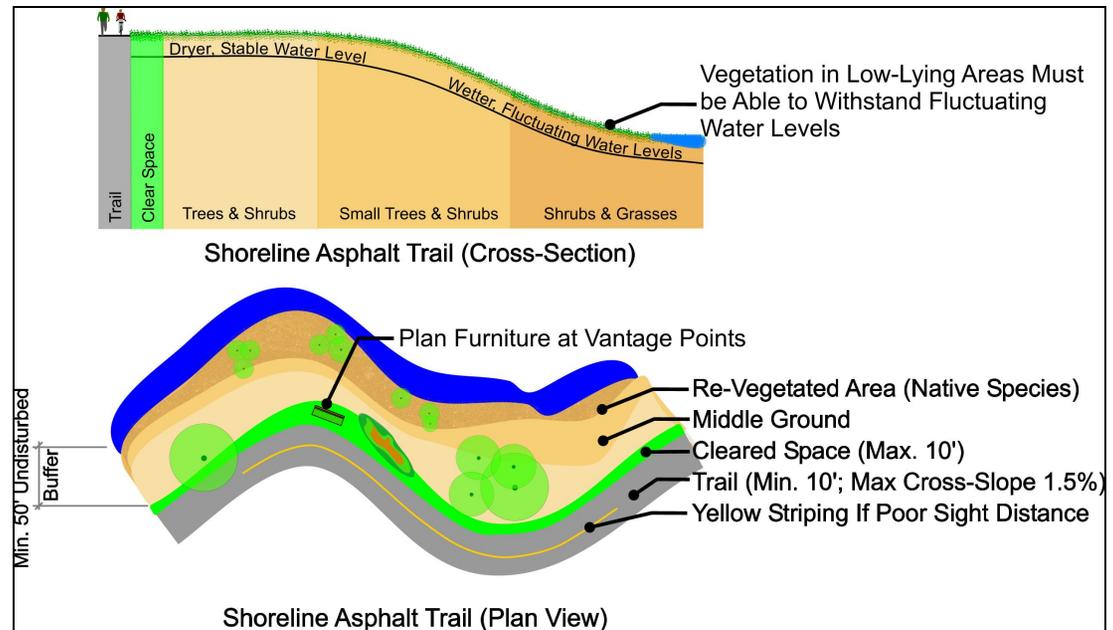


Figure 6-10. A diagram of general greenway design near to a coastal shoreline.

6.4. Downtown Area Standards

Downtown Hertford is already one of the most pedestrian-friendly areas in the Town. In order to maintain its pedestrian-oriented nature, and also to enhance the area's attractiveness and visual appeal, the Downtown area should have certain standards which may or may not be required beyond the downtown area. Some of these recommendations are as follows:

- ◆ **Maintain and require theme as outlined in Historic Hertford Plan.** The Historic Hertford Plan has outlined specific themes for brick sidewalk design, streetlights, and wayfinding signage in Hertford. The Pedestrian Plan supports this theme and recommends that it should be maintained throughout downtown and required whenever new sidewalk is constructed. Figure 6-11 shows an example of this brick and concrete theme.
- ◆ **Provide wide sidewalk.** New sidewalk, or reconstructed sidewalk, should be kept at a minimum of five feet and between five and ten feet, if not wider, in the downtown area. This width is necessary to accommodate the numbers of pedestrians expected in a Town such as Hertford which can have many visitors. Pedestrians need space to window shop, stroll, walk side-by-side with their families, and even stop for a rest in the sidewalk space. The Town should also consider accommodating restaurants or cafes interested in creating outdoor, on-street seating, which is often a major booster to making a street look more popular and pedestrian-friendly. It also attracts even more visitors and potential shoppers and diners.
- ◆ **Provide many pedestrian amenities.** In addition to sidewalk width, the Town should also provide pedestrian amenities such as benches, trash cans, and water fountains to make walking in downtown more comfortable for the numerous visitors that come to Hertford each year. The Town should even consider adding street trees or other means for shade. The more pedestrian amenities available in a particular area, the more inviting the area for pedestrians and visitors.
- ◆ **Provide frequent pedestrian crossings.** In order to maintain the accessibility of the downtown area, crosswalks should be required at various intervals along major streets that are uninterrupted by intersections.
- ◆ **Require pedestrian signals with audible and visible signals as well as countdown displays at all intersections.** Pedestrian signals should be required with audible and visible signals at all intersections, especially within the downtown area. The Town may also wish to consider pedestrian signals with countdowns in order to assist with safer crossings.

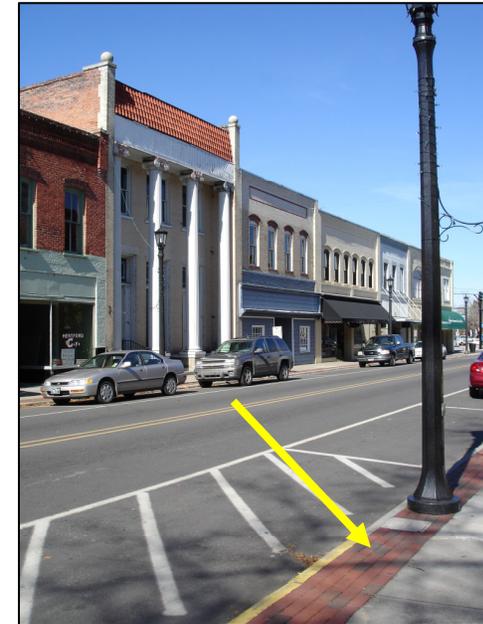


Figure 6-11. A view of Church Street in downtown Hertford. The arrow indicates where the sidewalk has been treated with a brick and concrete theme.

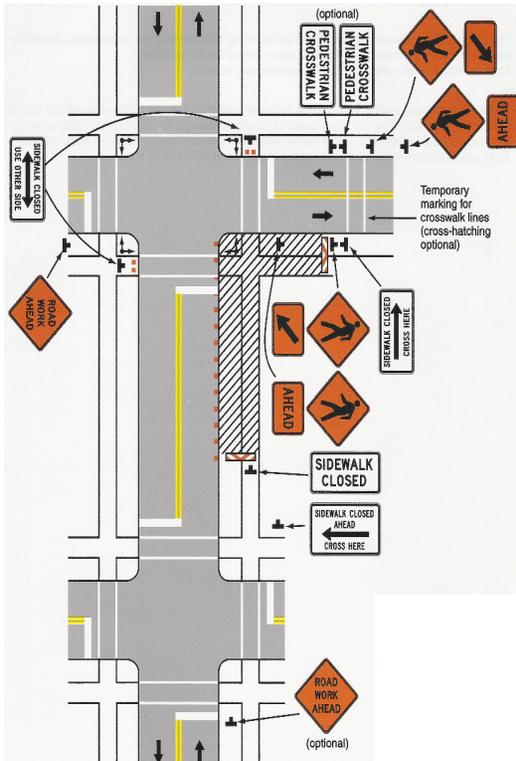


Figure 6-12. A sample signage plan to assist pedestrians using detours in construction zones. Adapted from Sample Signage Plan for Temporary Pedestrian Closure (MUTCD, Figure 6H - 29).

6.5. Schools

In addition to downtown, another area in Hertford that merits special treatment is the area around schools. Schools require special treatment because of the presence of both children and very high levels of traffic during drop-off and pick-up. Especially during drop-off and pick-up, traffic near schools can be heavy and incredibly varied - consisting of small and large personal vehicles, school and other activity buses, pedestrians, and cyclists. Specific design features should be required around schools to improve safety for all in the area. Some of these design features include:

- ◆ Requiring sidewalks on both sides of the street;
- ◆ Placing crosswalks and pedestrian signals at all intersections near the school;
- ◆ Reducing speed limits along adjacent streets; and
- ◆ Providing signage to warn drivers of the school's presence and the potential for children in the street.

6.6. Construction Zones

Given the pressure for more development in Hertford, the Town anticipates that there will be more construction in the future. It is important that during this construction, convenient and safe pedestrian access to destinations remain open and accessible. During the construction or expansion of private development, as well as roadways and utilities, the entity responsible for the construction is also responsible for providing adequate pedestrian access through or around the site as well as signage that provides advance warning to pedestrians and motorists of the closure. Both the MUTCD (Manual on Uniform Traffic Control Devices)^{iv}, NCDOT's Planning and Designing Local Pedestrian Facilities^v, and the ADA (Americans with Disabilities Act)^{vi} stipulate that safe passage should be maintained throughout a temporary closure unless it occurs during an extreme situation such as a natural or man-made emergency. During private construction within Town limits, it is the responsibility of the Town of Hertford to ensure compliance with these rules by regular monitoring.

The following should be considered whenever a sidewalk or trail will be closed temporarily:

- ◆ *Accessibility for Mobility Impaired Citizens.* At least one accessible route should be provided to transportation or transit facilities; accessible parking areas/spaces; public streets/sidewalks; and public parking areas to an accessible entrance of the building. This route(s) will comply with all other accessibility provisions contained in the ADA regardless of whether they are temporary or permanent. A barrier shall be placed across the full width of the sidewalk or trail to be detectable by a visually impaired person using a cane. An audible information device may be needed in cases where there are especially high traffic volumes challenging a visually impaired person making a street crossing.

- ◆ *Temporary Obstructions.* Parked construction equipment, erosion control fencing, storage of materials/construction debris, and other potential obstructions should be kept away from roadside pedestrian access and pedestrian or multi-use trails so as to keep a permanent passageway open for pedestrians crossing the site. Signs and other devices should not protrude more than 4" into the pedestrian passageway and 7' or less above a sidewalk (8' min. preferred).
- ◆ *Advance Warning and Signage.* Advance warnings may consist of a single sign to a flashing strobe, depending on the nature of the construction or context (such as vehicular volumes) of the work area. Advance signage should be placed so that pedestrians have an opportunity to read the sign and make a safe crossing at a street intersection to the opposite side of the roadway. Smaller, mid-block closures will require fewer treatments, but will still retain the "Sidewalk Closed Ahead Cross Street" advance warning at an appropriate and safe crossing point in advance of the closure, at a minimum.
- ◆ *Route Design.* Temporary traffic barriers like jersey barriers (although not intermittent short sections of jersey barriers) and breakaway bollards should be considered as tools to help delineate a buffer from moving vehicles in areas with high pedestrian traffic volumes and/or to help ensure worker safety.

6.7. Parking Lot Design

Everyone becomes a pedestrian once they park their car, but there are many examples of poor parking lot design. Poor parking lot design at the least will deter customers that may be walking or riding transit to a store, and at the most can create a dangerous safety hazard by increasing pedestrian-vehicle interaction. The most common design issue is that the primary carriageway for vehicles in the parking lot happens to coincide with where the greatest number of pedestrians cross: directly in front of the main entrance. Other issues include poor sight lines to spot pedestrians; bad transition areas from the public domain (e.g., streets) to the private parking area; and inconvenient pedestrian access between parking areas, shops, and adjacent communities. Figure 6-14 indicates a preferred set of suggestions to overcome these common problems. The larger the parking lot, the more vehicles and pedestrians, and therefore the more important it is to carefully design treatments to minimize vehicle-pedestrian interaction. Some suggested treatments:

1. **Parking in the rear.** One way to attract pedestrians to a store and to reduce pedestrian-vehicle interaction is to minimize the amount of parking lot that a pedestrian must walk through to get to the store entrance. This can be done by placing parking in the rear which will reduce travel time for pedestrians approaching the store from the street-front and sidewalk. It will also minimize pedestrian-vehicle interaction by keeping pedestrian customers

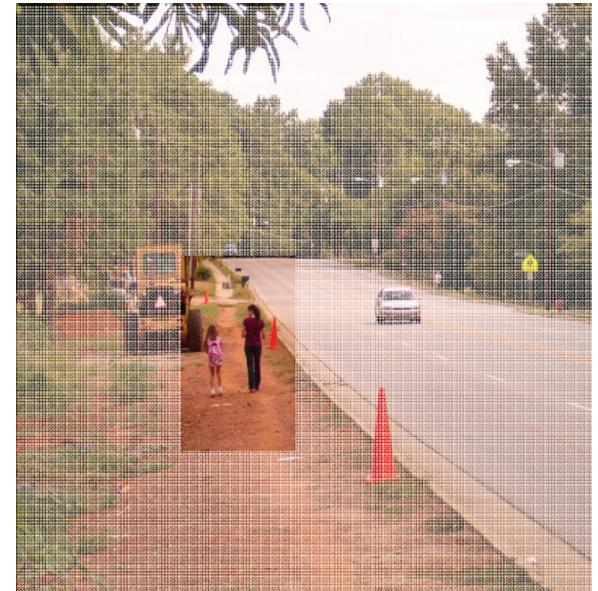


Figure 6-13. Poor pedestrian access at a construction site in Cary, NC.

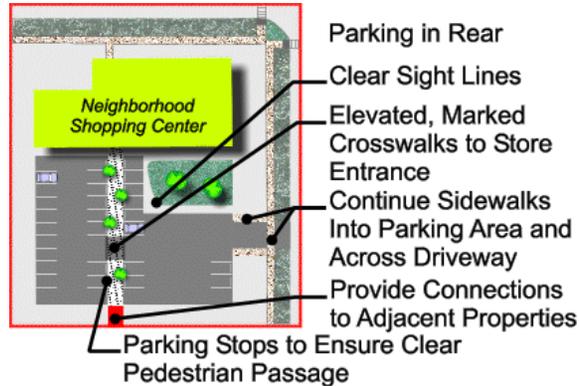


Figure 6-14. An example of pedestrian-friendly parking lot design.

- separate from vehicles by allowing the pedestrian customers to access the store directly from the sidewalk rather than through a parking lot. Parking lots in the rear also create a more attractive streetscape – something that also encourages pedestrian use.
2. **Create safe “landing areas”.** Provide continuous transitions from the street into a safe “landing” area in the parking lot; don’t just “dump” pedestrians into the throat of a driveway.
 3. **Maintain good sight lines** at major turning points inside the parking area.
 4. **Provide well-marked pedestrian access perpendicular to store fronts.** Whenever possible, provide perpendicular pedestrian access into the front of a high volume land use such as major retail uses. The final crossing to the store entrance(s) should be well-marked, preferably with a raised crosswalk and/or colored demarcations to provide good visual cues to the driver. Moving the main parking aisle away from the principal entrance is another option.
 5. **Supply adequate, pedestrian-scale lighting.** Adequate lighting is often perceived as a personal security issue in many large parking areas, and should be provided while avoiding disabling glare (looking into a direct light source and being partially blinded) or causing light pollution to adjoining properties. In order to make customers and pedestrians feel more comfortable, lighting should also be provided at a pedestrian scale. This means lowering the height of some light poles and providing lighting at key locations, such as the entrances and exits to stores, and not just in the parking lots.
 6. **Provide awnings.** Especially for some “big box” stores, it is important that the transition for customers from inside the store to the outside be gradual and protected as much as possible from conflicts with vehicles. By providing awnings, a store protects its customers from the rain while allowing for a more comfortable pedestrian environment for customers to window shop and wait for rides or a bus to arrive. This can make a store seem much more comfortable while encouraging customers to remain within the protected awning area and out of conflict with vehicles in the travelway.

Hertford has several shopping centers and areas with large parking lots, and others may be on the way. It is important that the Town keep the pedestrian’s access and safety in mind when reviewing development proposals. Through better design and better design review, the Town will be able to create parking lots that are both convenient for a car and comfortable for a pedestrian.

6.8. Traffic Calming Considerations

Traffic calming is the term used to describe a toolbox of improvements that can be used to “calm”, or slow, traffic along a street, usually in a neighborhood or similar area with low traffic speeds and relatively lower traffic volumes. Although not directly pedestrian-related, traffic calming efforts can help to create a safer, more comfortable pedestrian environment by reducing

vehicle speeding. Traffic calming comes in a variety of forms. Some of the most common techniques are described in the paragraphs below.

6.8.1. Curb Extensions (Bulb-Outs) and Curb Radii

The primary purpose of bulb-outs is to shorten the distance that pedestrians must travel to cross a street. In addition, they may encourage motorists to drive slower by narrowing the travel lane and reducing vehicular speeds during turning movements at intersections. Motorists will travel more slowly around corners with smaller curb radii even without the use of curb extensions. Landscaping and other aesthetic treatments such as special paving textures should be carefully designed to avoid hazards to drivers and visually-impaired citizens while minimizing maintenance costs. Figure 6-15 shows an example image bulb-out placement to reduce curb radii and make an intersection more pedestrian-friendly.

6.8.2. Medians and Refuge Islands

Figure 6-16 indicates the design and markings associated with refuge islands. Note that pavement markings delineate the approach to the islands; that the islands are “split” to allow for a level platform for wheelchair use; and that in cases where there are wide roads and high traffic volumes, a push-button pedestrian signal may be mounted in the refuge area to allow a pedestrian to split their trip into two halves as they cross the street. Note that the crosswalk on the right side of the diagram is configured at a skewed angle as it crosses the median. This allows pedestrians to have a better angle of sight as they approach and cross each side of the street. In all cases, a minimum 10-foot travel lane is maintained. Sensitivity to large vehicles (buses, trucks and fire equipment) dictates some elements of the median design, curb style, and placement. Median-controlled roadways reduce the number of turning conflicts and are generally preferred for both pedestrians and cyclists over a two-way, left-turn lane (TWLTL) roadway. Note that a refuge island is recommended in Section 8 as part of improvements for a pedestrian crossing at the intersection of US 17 and Church Street/Harvey Point Road.

6.9. Summary

Pedestrian facility use is a function of a variety of factors, including the connectivity of the facilities, their safety, their convenience, and their comfort. For this reason, pedestrian facility design should be thoughtful and sensitive to the needs of its users. By following the guidelines provided in this section for sidewalk, crossing, and trail design, as well as other items associated with pedestrian facilities, Hertford should be able to create a built environment that will promote walking and increase the number of pedestrians in the Town.

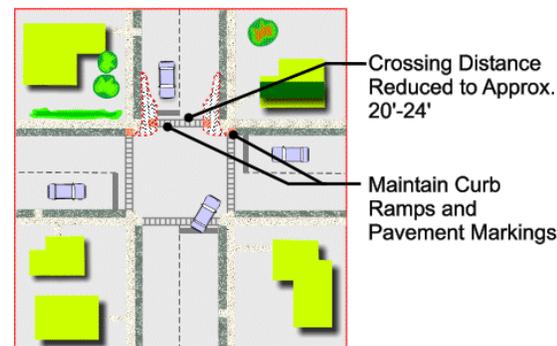


Figure 6-15. An example of bulb-out placement to reduce curb radii and shorten travel distance for pedestrians crossing at an intersection.

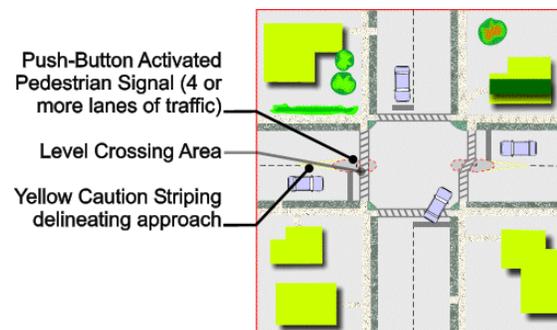


Figure 6-16. An example of well-designed median and refuge islands.

¹ Vanguard Company, accessed November, 2005

<http://www.vanguardonline.com/downloads.asp>)

ⁱⁱ United States Access Board, ADA Accessibility Guidelines Homepage, accessed November, 2005.

<http://www.access-board.gov/adaag/html/adaag.htm#A4.29.2>)

ⁱⁱⁱ James W. Glock, Letter Correspondence to Regina McElroy, Director, FHWA Office of Transportation Operations, January 11, 2006.

^{iv} *Manual on Uniform Traffic Control Devices for Streets and Highways*, 2003 Edition. Federal Highway Administration, 2003. Especially Sections 6B-1, 6D, 7, and Figures 6H-28, 6H-29, 7A-1, and 7B-4.

^v *Planning and Designing Local Pedestrian Facilities*, North Carolina Department of Transportation Office of Bicycle and Pedestrian Transportation. February, 1997, Chapter 10.

^{vi} Americans with Disabilities Act, US Code 28 CFR Part 36: ADA Standards for Accessible Design. Page 496 (www.usdoj.gov/crt/ada/adastd94.pdf).

Quick Summary: This section provides program and policy recommendations to assist in making Bryson City a more pedestrian-friendly community. Recommendations cover the E's of pedestrian planning: Encouragement, Education, Enforcement, and Engineering.

Section 7. Program and Policy Recommendations

7.1. Introduction

In addition to physical facilities, it is also important to have strong programs and policies which encourage making the Town more pedestrian-friendly. Programs often fall into one of the four E's of pedestrian planning: Encouragement, Enforcement, Education, and Engineering, and can encourage pedestrian-friendly behavior and more walking in the Town. Similarly, policies can encourage pedestrian-friendly development and design. The following section provides recommendations for some of the programs and policies which the Town can consider implementing to become more pedestrian-friendly.

7.2. Program Recommendations

Pedestrian facilities alone do not make a town pedestrian-friendly; a variety of programs can also be implemented to create and support a pedestrian-friendly culture. A pedestrian-friendly culture has several different characteristics, including the behavior of the people in the town, the attitude of motorists in the town towards pedestrians, and the role of police and other law officials to enforce pedestrian safety. For this reason, programs are often created to fit within the three non-construction E's of pedestrian planning: education, encouragement, and enforcement. Education programs teach others about safe pedestrian behaviors, the benefits of walking, and are necessary to assist people in feeling more comfortable with their "new" mode of travel. Education programs can also be used to teach motorists how to interact safely with pedestrians. Encouragement programs, like education programs, can also teach about the benefits of walking and serve to promote walking and pedestrian-friendly behavior through activities and incentives. Finally, enforcement programs provide the "teeth" of creating a safe and legal pedestrian environment. When law enforcement officers and other officials protect pedestrians and encourage walking, this sends a clear message that the presence of pedestrians is a legitimate and permanent condition in the Town's transportation network.

The following items are some programs the Town should consider implementing to create a more pedestrian-friendly environment.

- ◆ **Self-guided Walking Tour.** (*Encouragement*)

Walkabouts and Historic Walking Tours are very useful tools for encouraging walking in a Town, and also for improving the marketability and attraction of the Town to visitors. Already the Town has developed a "Turtle Walking Trail" which is advertised at the Visitor's Bureau/Chamber of Commerce. The Turtle Walking Trail was designed to take pedestrians past a variety of historic and notable locations in Hertford, including historic

homes, the waterfront, and landmarks. The Walking Trail is designated by turtle-shaped stencils painted onto the ground. Participants in the Trail follow these stencils from one stop to the next. It is recommended that Hertford continue to support this activity and advertise it as best possible to visitors through the Visitor Bureau, local restaurants, and other potential locations for visitors. The Town should make sure that the Trail connects to locations such as Missing Mill Park and a nice view location for the S-Bridge. After more sidewalk and intersection improvements have been made, the Town could even consider extending the Trail out to the shopping area at the intersection of Church St. and US 17, and beyond to the Recreation Center.

◆ **Safe Routes to School Program.** (*Education and Encouragement*)

According to the Federal Highway Administration's website for Safe Routes to School, in 1969, about half of all students walked or bicycled to school. Today, however, over half of all children arrive at school in private automobiles and only 15 percent of all school trips are made by walking or bicycling¹. As a response to these dramatic statistics, the Safe Routes to School Program was designed to create and promote safe walking and cycling to school. Its goals are to improve safety near schools, promote active lifestyles, and reduce pollution and congestion caused by school traffic.

In North Carolina, the North Carolina Department of Transportation (NCDOT), in conjunction with the Federal Highway Administration, has established the North Carolina Safe Routes to School program, which is a state-wide program to promote safe walking and bicycling to elementary and middle schools in North Carolina. The North Carolina Safe Routes to School program provides opportunities for schools to apply for funding for both programs and capital improvement projects to encourage walking and cycling to school. The program also conducts Safe Routes to School training sessions to be at interested schools to assist with planning and preparation of an extended Safe Routes to School effort at the school. This free training session brings together school administrators, faculty, staff, and representatives from related agencies such as health departments, law enforcement, engineering, and town planning, to educate them about Safe Routes to School, its purpose, and techniques that can be used to create a successful program. Once the training session has been conducted, the school will be prepared to start an effective Safe Routes to School program and is eligible to apply for grants for both programs and capital improvements.

It is recommended that Hertford join with Perquimans County Public Schools to start a Safe Routes to School program at Hertford Grammar School, located within town limits.

¹ FHWA Safety: Safe Routes to School Program. <http://safety.fhwa.dot.gov/saferoutes/>



Figure 7-1. The Indian Summer Festival is an annual Hertford event. A 5K usually accompanies it.



Figure 7-2. Parents at a Safe Routes to School workshop in New Jersey discuss improvements to their school. Safe Routes to School workshops are being conducted across the nation to promote children's health and a better environment.

This program could help to target some of the school's much needed projects, such as connections to the High School. Although high schools are not funded by the national Safe Routes to School Program, the Town should also consider evaluating ways to make it safer for students to walk to Perquimans County High School as well.

- ◆ **Annual 5K Walk/Run.** *(Education and Encouragement)*
An annual 5K is conducted in Hertford as part of the Indian Summer Festival. The Town should continue to support this event because it raises awareness about walking and running in Hertford. It also attracts visitors to the downtown Hertford area and promotes exercise and a more active lifestyle. The Annual 5K could be used as a tool to annually evaluate the running/walking conditions of downtown Hertford.
- ◆ **Walking/Running Trail with Mile Markers and Exercise Stations.** *(Education and Encouragement)*
The Perquimans Trailblazers group, a Hertford-based organization whose mission is to enhance the quality of life in Perquimans County by developing a system of land and water trails, has been working to develop a permanent walking/running trail which would have mile markers and exercise stations. The mile markers would be useful for runners and walkers to measure the distance they have traveled. The exercise stations may attract more runners and walkers who are interested in a more varied workout. The Town should consider helping the Perquimans Trailblazers Group to develop this facility because it would provide another recreation facility for residents, encourage residents to be more physically active, and it would also be an additional attraction for visitors.
- ◆ **Connections to the Blueway.** *(Encouragement)*
In addition to the walking/running trail, the Perquimans Trailblazers is also working on creating a blueway trail along the Perquimans River. The Town should consider reinforcing pedestrian connections to put-ins along the blueway so that paddlers who have stopped for the day can walk to destinations in town. This will encourage visitors, and will also encourage more walking in Town. The Town should consider sidewalk connectivity as well as intersection safety and convenience when making improvements.
- ◆ **Police Education.** *(Enforcement)*
A little enforcement can go a long way, but not if the enforcer is unaware of the appropriate behavior in a given situation. It is recommended that Hertford consider providing a short course for police officers to brush up on their pedestrian rights and safe behaviors so that they can enforce such behavior within town. Thus, educated officers will become ambassadors to teach Hertford residents and visitors the appropriate

pedestrian behavior. In addition, Hertford should be sure to distribute information about pedestrian laws in North Carolina to both local residents and police officers. The laws and a guidebook for them are available at the following websites:

Laws: www.ncdot.org/transit/bicycle/laws/laws_pedlaws.html

Guide: www.ncdot.org/transit/bicycle/laws/resources/BikePedLawsGuidebook-Full.pdf

◆ **Pedestrian Counts on Major Thoroughfares.** *(Evaluation)*

The Town should develop a program to take annual pedestrian counts at specific locations in order to measure the increase in volume of pedestrian over time as the town implements the Pedestrian Plan and its improvements and programs. This will help the Town to evaluate the success of its program and its improvements as well as to determine if there are additional improvements that are needed. Recommended locations for pedestrian counts include:

1. Intersection of Church and Market Street
2. Intersection of Church and US 17
3. Intersection of Church and Berry Street
4. Intersection of King Street and Hyde Park Street
5. Grubb Street in front of Missing Mill Park
6. S. Edenton Road Street in front of the High School
7. Dobbs Street in front of Hertford Grammar School

In addition, counts along the S-bridge should be made once sidewalk has been installed.

◆ **Sidewalk construction program.** *(Engineering)*

Many municipalities develop an on-going sidewalk construction program to assist in closing the gaps in their sidewalk connectivity. This program is annually funded by the Town's budget, and builds small pieces of sidewalk which may be missing from major routes to connect destinations. Hertford should consider developing a similar program to accrue over time a budget for installing sidewalk and greenways to provide additional necessary connections between existing destinations.

◆ **Sidewalk maintenance program.** *(Engineering)*

In addition to a sidewalk construction program, many municipalities also have an annual sidewalk maintenance program which is included in the Town budget. The maintenance program should address improving existing sidewalk connections by replacing cracked sidewalk, re-striping crosswalks, and removing debris and obstacles from sidewalk. A sidewalk maintenance program is important because it helps to make the existing pedestrian facilities more comfortable and convenient for pedestrians.

- ◆ **Intersection improvement program.** *(Engineering)*
Similar to the sidewalk construction program, the intersection improvement program should be an annually budgeted program to upgrade intersections to make them more pedestrian-friendly. At un-signalized intersections, crosswalks should be striped and signs should be installed to alert motorists to the presence of pedestrians. At signalized intersections, pedestrian signals, crosswalks, and amenities such as pedestrian countdown signals should be installed. The intersection improvement program, along with the sidewalk construction and maintenance programs, should provide Hertford with a methodical and funded way to build necessary pedestrian facilities.

7.3. Policy Recommendations

In addition to programs, policies are also useful to help encourage pedestrians. The following are some policy recommendations which the Town should consider implementing to make itself more pedestrian-friendly.

- ◆ **Require new development to construct sidewalk and connect to existing sidewalk as well as adjacent developments.** While Hertford has had limited new development in the past, there is still the potential for future development given the Town's growth as a major visitor destination. When development does occur, the Town should have ordinances in place which will require the development to have pedestrian amenities including sidewalks, crosswalks, and connections to existing sidewalk. This will encourage a more walkable form of development which will both promote a healthier, more active lifestyle among residents and visitors, and also reduce traffic congestion and air pollution. In particular the guidelines in the two overlay districts, the US 17 Overlay District and the Neighborhood Corridor District, should be especially robust in requiring pedestrian facilities and accommodations.
- ◆ **Consider requiring new development to set aside land for greenways.** Many municipalities require developers to set aside land for greenways as part of their development impact fee for parks and recreation facilities. After the Town adopts a formal pedestrian plan, it should consider doing the same. In addition, the Town should require developers to provide connections to existing greenways as they are constructed to allow for better connectivity in the Town's pedestrian system.
- ◆ **Require all new signals to include pedestrian signal heads and crosswalks.** One of the major barriers to pedestrian connectivity in any town is the ability to cross the street. The Town should require all new signals to have pedestrian signal heads and

crosswalks to accommodate pedestrians and plan for future conditions in which pedestrians may be walking through the intersection.

- ◆ **Assure Safe Pedestrian Access through Parking Lots.** Parking lots can be some of the most unfriendly locations for pedestrians due to their very nature of being a place where both vehicles and pedestrians mix. When new development is constructed, or existing development is renovated, parking lots should be evaluated for appropriate pedestrian-safe design. Some features that should be included are pedestrian refuges, sidewalk, and pedestrian-only access from the parking lot to store fronts. Parking lot requirements should be flexible enough to allow for unconventional locations for off-street parking, such as behind a building, or shared with another commercial development. For a more thorough description of appropriate parking lot design, see *Section 5.7 Parking Lot Design*.

- ◆ **Encourage Mixed-Use and Other Pedestrian-Friendly Developments.** While the capacity of Hertford to grow is somewhat limited in geographic range, the Town will continue to experience new, private-sector development of homes, businesses, and retail centers. The way in which these uses are arranged, their proximity to one another, and their design are critical to the promotion of walking as a viable means of transportation. The “push-pull” diagram at right (Figure 3) illustrates development policies that hinder or promote well-designed pedestrian spaces; many of these factors support crime prevention and public safety goals. Excellent resources that the Town can utilize to develop management guidelines include the following:
 - **Project for Public Spaces (www.pps.org).** PPS is a non-profit center founded on the principal of making public spaces better places to be in for pedestrians (and users of all modes of travel). They provide technical and training resources.
 - **Leadership in Energy and Environmental Design Neighborhood Development (www.usgbc.org/DisplayPage.aspx?CMSPageID=148).** Still a pilot program, the U.S. Green Building Council offers a criteria manual that can serve as a checklist for Hertford to help private development create great neighborhoods for people. Location, car dependence, conservation, walkable streets, and compact design are some of the elements that can serve as a menu for a point system to enhance new developments, rather than a prescriptive, one-size-fits-all ordinance requirement.
 - **Connectivity Requirements (for example, www.vtpi.org/tdm/tdm116.htm).** The foundation concept behind any walkable area is the number of places that a person can reach (by foot) in a given amount of time. By

Push (Promote Walking)	Pull (Hinder Walking)
Interconnected Neighborhoods	Cul-de-sac development
Mix of Land Uses	Single-Purpose Neighborhoods
Maximum Block Lengths of 500 feet	Long, “superblock” Streets More than 1,000 Feet in Length
Pedestrian Cross-Access	Disconnected Developments
Well-Lit Spaces Open to View from Passersby	Dark Spaces Hidden from View
Minimum Driveway Spacing Standards	Uncontrolled Driveways Spaced Frequently
Attractive Street Design and Streetscaping	Blank Walls, Large Parking Areas, and Uninteresting Spaces
High-Speed Right Turns	Small Corner Radii

Figure 3. Factors that Promote or Hinder Walking in a Community.

requiring a high level of connectivity of new developments for surrounding streets, Hertford can ensure that their public spaces remain not only walkable, but will have redundant access to emergency services; produce lower fuel use and emissions; and preserve roadway capacity by allowing walking and vehicular trips to occur more on local streets instead of congested arterials.

7.4. Summary

By implementing these programs and policies, along with the recommended projects, Hertford should be on its way to becoming a more pedestrian-friendly location. While the projects will provide the facilities necessary to walk safely in the Town, the programs and policies will work to encourage more pedestrians and increase walking in the Town. The dual approach of projects, plus programs and policies, will be a strong influence in making Hertford a pedestrian-friendly place.

Quick Summary: This section provides a detailed analysis of the benefits and constraints of specific project recommendations based upon the needs of the Focus Areas described in Section 5. The section also prioritizes these recommendations into short-term, mid-term, and long-term priorities.

Section 8. Project Recommendations

8.1. Introduction

Sections 1 – 4 of this Plan outline the existing conditions in the Town of Hertford and identify the pedestrian needs that should be addressed and potential focus areas where improvements should be made. This section builds upon the findings in the first sections of the Plan to identify a set of specific projects to improve the pedestrian system in Hertford. Section 8.2 discusses each focus area and its specific recommendations and projects. Section 8.3 provides general cost estimates for each project, and Section 8.4 specifies recommended priorities.

8.2. Projects

The following paragraphs describe the projects which could address the recommendations for each Focus Area identified in Section 5. For each Focus Area, the potential benefits and constraints for each recommendation identified in Section 5 are discussed, and then projects are identified to match the recommendation. Each project is assigned a reference number for easier identification on maps and in cost estimating. **The reference numbers do not indicate priority.**

8.2.1. Intersection of US 17 and Church Street/Harvey Point Road and points south

Intersection of US 17 and Church Street/Harvey Point Road

For this location, the recommendation in Section 5 was to provide pedestrian improvements and gateway/appearance enhancements at the intersection for US 17/Church Street/Harvey Point Road. Due to the complexity of this intersection, a more thorough analysis will be necessary to determine the ideal improvements; however, some possibilities are listed in the table below. The Town should consider providing a pedestrian refuge mid-way across the intersection due to the size of the intersection. Using this refuge, pedestrians could cross one set of traffic lanes, allow the signal to cycle, and then cross a second set of traffic lanes. This will reduce the travel distance for pedestrians with each signal cycle, and reduce signal delay for traffic along US 17.

Benefits	Constraints
<ul style="list-style-type: none"> ◆ Would make the connection between downtown and Recreation Center/Newbold-White House safer, faster, and easier for pedestrians to cross ◆ May improve appearance of one of the main entrances into Hertford and Downtown 	<ul style="list-style-type: none"> ◆ Pedestrian improvements may reduce through-put of the intersection. ◆ An underpass or overpass would be expensive and may not be used. ◆ A mid-block crossing away from the intersection may cause pedestrian and vehicle confusion.

Proposed Projects

Reference Number	Project Description
1	<ul style="list-style-type: none"> ◆ Pedestrian crossing improvements at the intersection of US 17 and Church Street/Harvey Point Road that include: <ul style="list-style-type: none"> ▪ Pedestrian signals in all four directions ▪ Crosswalks in all four directions ▪ Pedestrian refuge for pedestrians crossing US 17 along Church Street/Harvey Point Road

Length of Church Street and Harvey Point Road

The recommendation here was to examine the corridor for pedestrian improvements, and to consider a greenway/boardwalk along the waterfront or a wide sidewalk/multi-use path along US 17. The potential benefits and constraints to improvements along this location are as follows:

Benefits	Constraints
<ul style="list-style-type: none"> ◆ Would connect Recreation Center to downtown – provide a walking alternative to reach it ◆ Create an attraction for visitors ◆ Would link the south side of Hertford (South of US 17) to the rest of town ◆ Could be tied into a “gateway” design at US 17 ◆ Combined with other projects, could become a town-wide greenway system which would encourage tourism/economy 	<ul style="list-style-type: none"> ◆ Improvements for crossing US 17 may meet with resistance from NCDOT because they may be perceived as hampering traffic flow, especially if US 17 is a major evacuation route for hurricanes ◆ Some property owners south of US 17 who may be resistant to a sidewalk/multi-use path along roadway or boardwalk along river’s edge. Construction would have to occur within the right-of-way ◆ Due to the length, sidewalk along roadway would be very expensive. Boardwalk along river front would also be very expensive. ◆ CAMA rules may constrain the design of a boardwalk along river’s edge

Proposed Projects

Reference Number	Project Description
2	◆ Sidewalk on the east side of Church Street from existing sidewalk to US 17
3	◆ Sidewalk on the west side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center
4	◆ Sidewalk on the south side of the access road to the Perquimans Recreation Center
5	◆ Sidewalk on the east side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center
6	◆ Boardwalk along the Perquimans riverfront from Downtown to Recreation Center

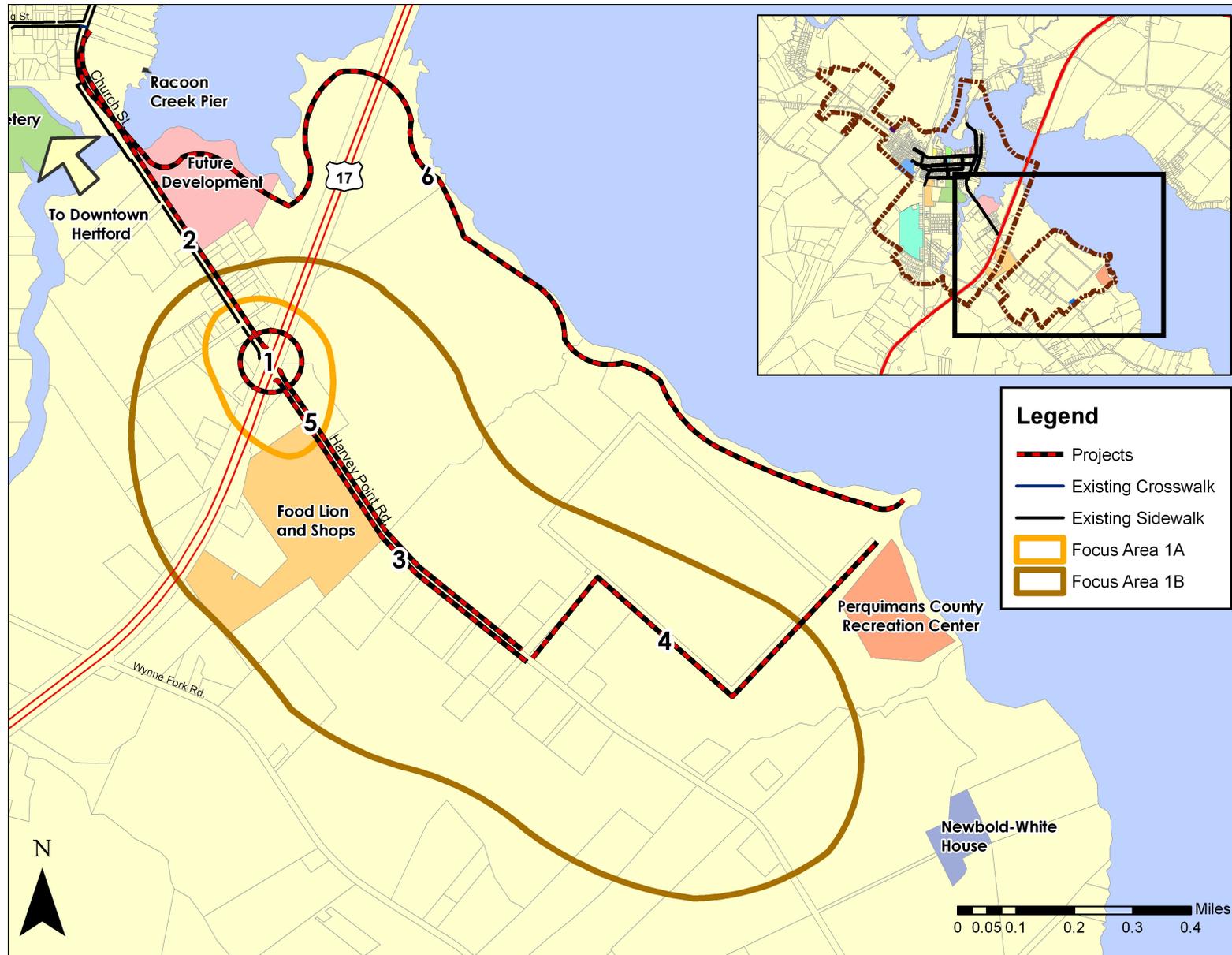


Figure 8-1. Map of proposed projects for the Church Street/Harvey Point Road/US 17 Area.

8.2.2. Perquimans High School and Hertford Grammar School

The benefits and constraints of implementing recommendations around Perquimans High School and Hertford Grammar School are shown below.

Recommendation	Benefits	Constraints
<p>Consider greenway connection from Perquimans High School to downtown through the Jenny’s Gut area/Hertford Cemetery</p>	<ul style="list-style-type: none"> ◆ Could connect to current wetland restoration project to the east of the High School and future wetland restoration project north of S. Edenton Road/the high school to extend south into downtown ◆ Already a sewer easement available which the Town could work to convert into a sewer/greenway easement ◆ Would provide a needed connection between Church Street multi-use path/downtown/and the school 	<ul style="list-style-type: none"> ◆ Appearance/attractiveness and stagnant water in Jenny’s Gut may become unattractive in the summer and the nature of the neighborhoods around the greenway may make some people feel uncomfortable to use the greenway. The cemetery may also contribute to this issue. ◆ Safety of area around Jenny’s Gut may deter users
<p>Consider greenway connection between High School and Grammar School to cross railroad tracks.</p>	<ul style="list-style-type: none"> ◆ Creates a connection between schools ◆ Extends greenway from Jenny’s Gut/downtown area ◆ Has received public support from the Town’s Blueway/Greenway group 	<ul style="list-style-type: none"> ◆ Train tracks are in the way – will need to negotiate them ◆ Limited public property ◆ May have limited use due to public perception of safety
<p>Connection from High School to residential neighborhoods south of the school, both along Edenton Road Street and Wynne Fork Road</p>	<ul style="list-style-type: none"> ◆ Provide safe passage for children walking to school ◆ Additional connection for adults into downtown 	<ul style="list-style-type: none"> ◆ Length of project is significant ◆ May be right-of-way constrained ◆ Sidewalk should be constructed greater than six feet wide with a buffer of three feet or more from the road for ideal walking conditions for children.

Section 8: Project Recommendations

Proposed Projects

Reference Number	Project Description
7	◆ Greenway along "wetland" behind High School into town (Jenny's Gut area/cemetery)
8	◆ Greenway linking Perquimans High School to Hertford Grammar School
9	◆ Sidewalk on the west side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School
10	◆ Sidewalk on the east side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School
11	◆ Sidewalk on the north side of Wynne Fork Road from the intersection with Edenton Street Road to US 17
12	◆ Improvements at the intersection of US 17 and Wynne Fork Road to accommodate pedestrians safely
13	◆ Sidewalk on both sides of Wynne Fork Road from existing residential development to the intersection with US 17
22	◆ Sidewalk on north side of Market Street from the intersection with Grubb Street to the Hertford Grammar School

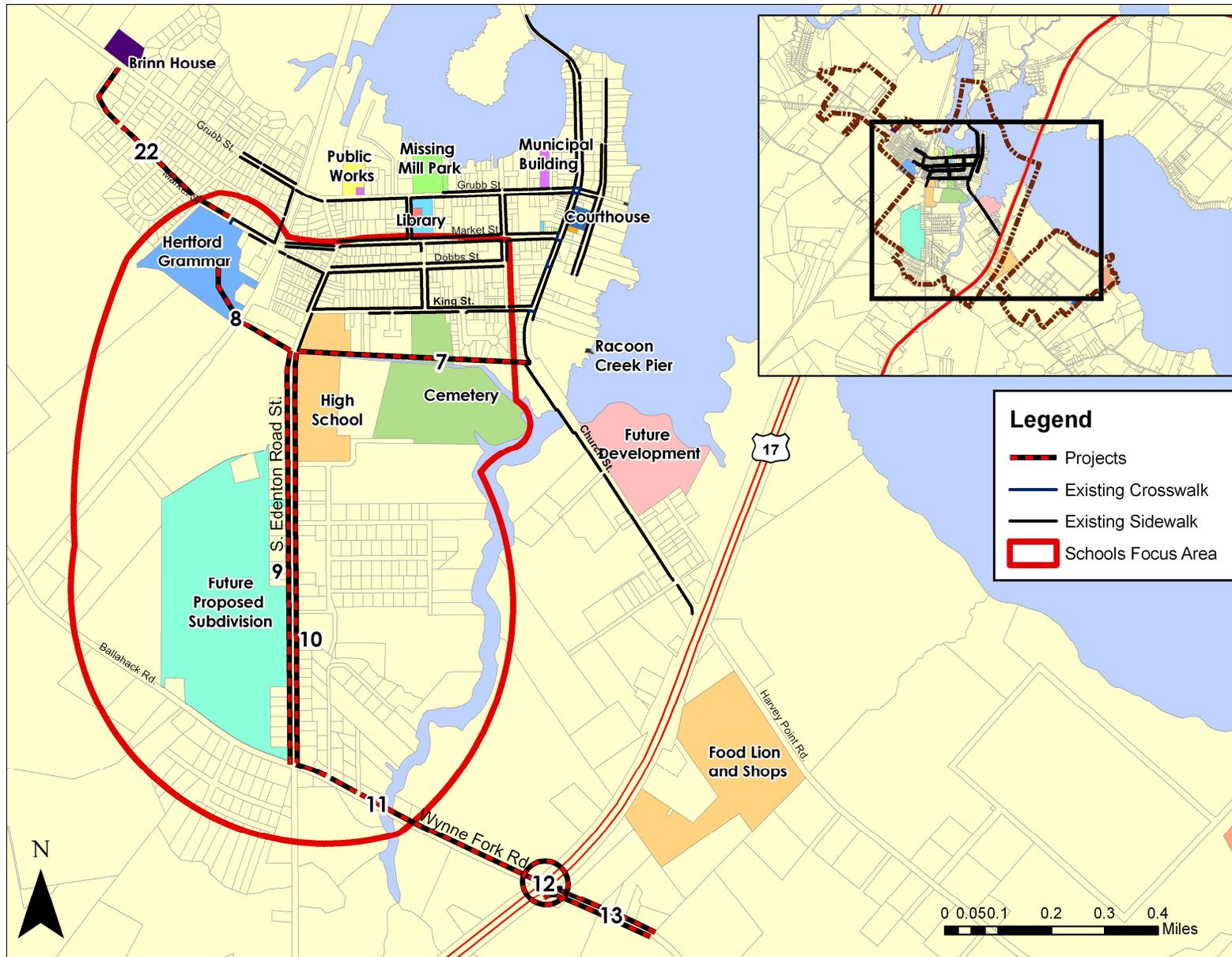


Figure 8-2. A map of the proposed projects to provide pedestrian connections to the schools in Hertford from residential areas.

8.2.3. Missing Mill Park Area

The Missing Mill Park area also had several recommendations related to providing access and improving crossing safety. They are described below:

Recommendation	Benefits	Constraints
Complete sidewalk to Missing Mill Park	<ul style="list-style-type: none"> ◆ Will create pedestrian connection to Missing Mill Park from downtown ◆ Ramp will provide access for wheelchairs, strollers 	<ul style="list-style-type: none"> ◆ May have limited right of way
Provide a pedestrian crossing between Missing Mill Park and Tennis Courts/Senior Center	<ul style="list-style-type: none"> ◆ Improves pedestrian safety and connectivity between two recreation areas ◆ May slow traffic and serve as a traffic calming device 	<ul style="list-style-type: none"> ◆ May have limited right of way ◆ By slowing traffic, may reduce throughput.
Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp	<ul style="list-style-type: none"> ◆ Will create an attraction for visitors to walk or ride bicycles, and look at scenic view ◆ Maximize use of waterfront for economic development 	<ul style="list-style-type: none"> ◆ May be a struggle to receive CAMA permitting ◆ Construction and property acquisition/easement negotiations may be expensive and time-intensive

Proposed Projects

Reference Number	Project Description
14	<ul style="list-style-type: none"> ◆ Sidewalk to Missing Mill Park along Church Street from end of existing sidewalk to park entrance
15	<ul style="list-style-type: none"> ◆ Provide pedestrian crossing between Missing Mill Park and Tennis Courts
16	<ul style="list-style-type: none"> ◆ Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp
17	<ul style="list-style-type: none"> ◆ Sidewalk on the north side of Church Street from existing sidewalk to Missing Mill Park entrance
21	<ul style="list-style-type: none"> ◆ Sidewalk from residential neighborhoods along Grubb Street to connect to Missing Mill Park (Nates Drive to existing sidewalk)
23	<ul style="list-style-type: none"> ◆ Sidewalk on both sides of Dobbs St. from Church Street to Covent Garden Street



Figure 8-3. Map of proposed projects for the Missing Mill Park area.

8.2.4. Raccoon Creek Pier and Canoe Launch

For the Raccoon Creek Pier and Canoe Launch area, the recommendation was to consider connecting the Pier area to Church Street in order to draw attention and attract more use.

Benefits	Constraints
◆ Highlight Raccoon Creek Pier and draw attention from residents and visitors	◆ Secluded location may make it difficult to attract attention

Proposed Project

Reference Number	Project Description
18	◆ Sidewalk on east side of Church Street from existing sidewalk to the pier

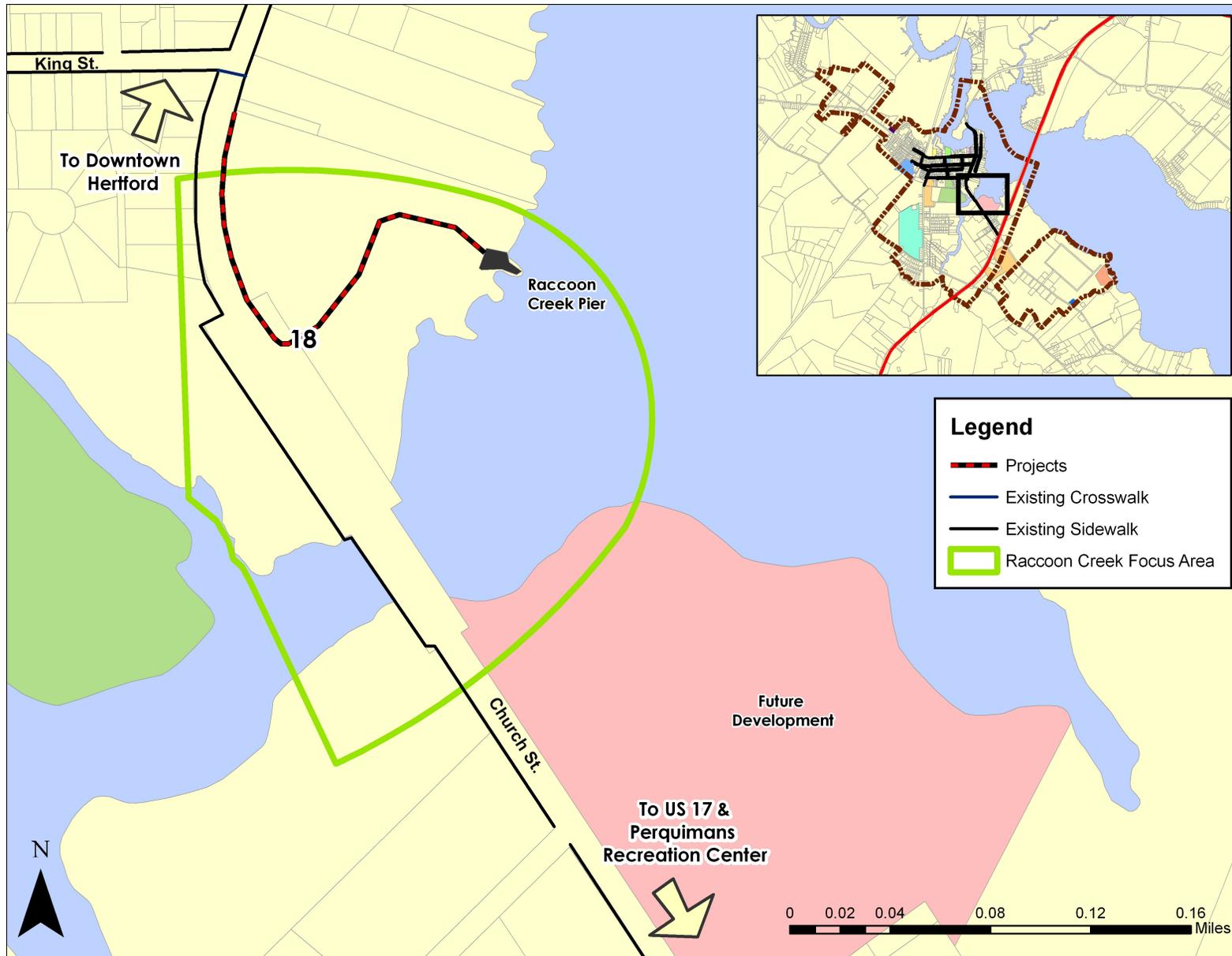


Figure 8-4. Map of proposed project for a connection to the Raccoon Creek Pier from downtown Hertford.

8.2.5. Connections along the Causeway and S-Bridge

The major recommendation for the Causeway and S-bridge area was to provide a pedestrian facility of some kind along the length of it to improve pedestrian connectivity between Hertford and Winfall, and to develop an attraction for visitors. The following describe the potential benefits and constraints.

Project	Benefits	Constraints
Greenway along Causeway/US 17	<ul style="list-style-type: none"> ◆ Would create a connection (walking and cycling) to Winfall for residents (especially students) and visitors ◆ Provide attraction for tourists 	<ul style="list-style-type: none"> ◆ CAMA has refused permitting for new connection in the past ◆ Bridge railings for pedestrian bridge in S-Bridge ROW were feared to block view for motorists

Proposed Projects

Two major project options were discussed: 1) to provide a sidewalk along the Causeway and S-Bridge, or 2) to construct a separate structure/facility that would be a boardwalk/multi-use path near to the Causeway and S-Bridge, but not part of them. When identify the next steps for this Focus Area, the Town should select one of these projects.

Reference Number	Project Description
19	◆ Sidewalk along west side of S-bridge and Causeway/US 17
20	◆ Boardwalk adjacent to S-bridge and Causeway/US 17

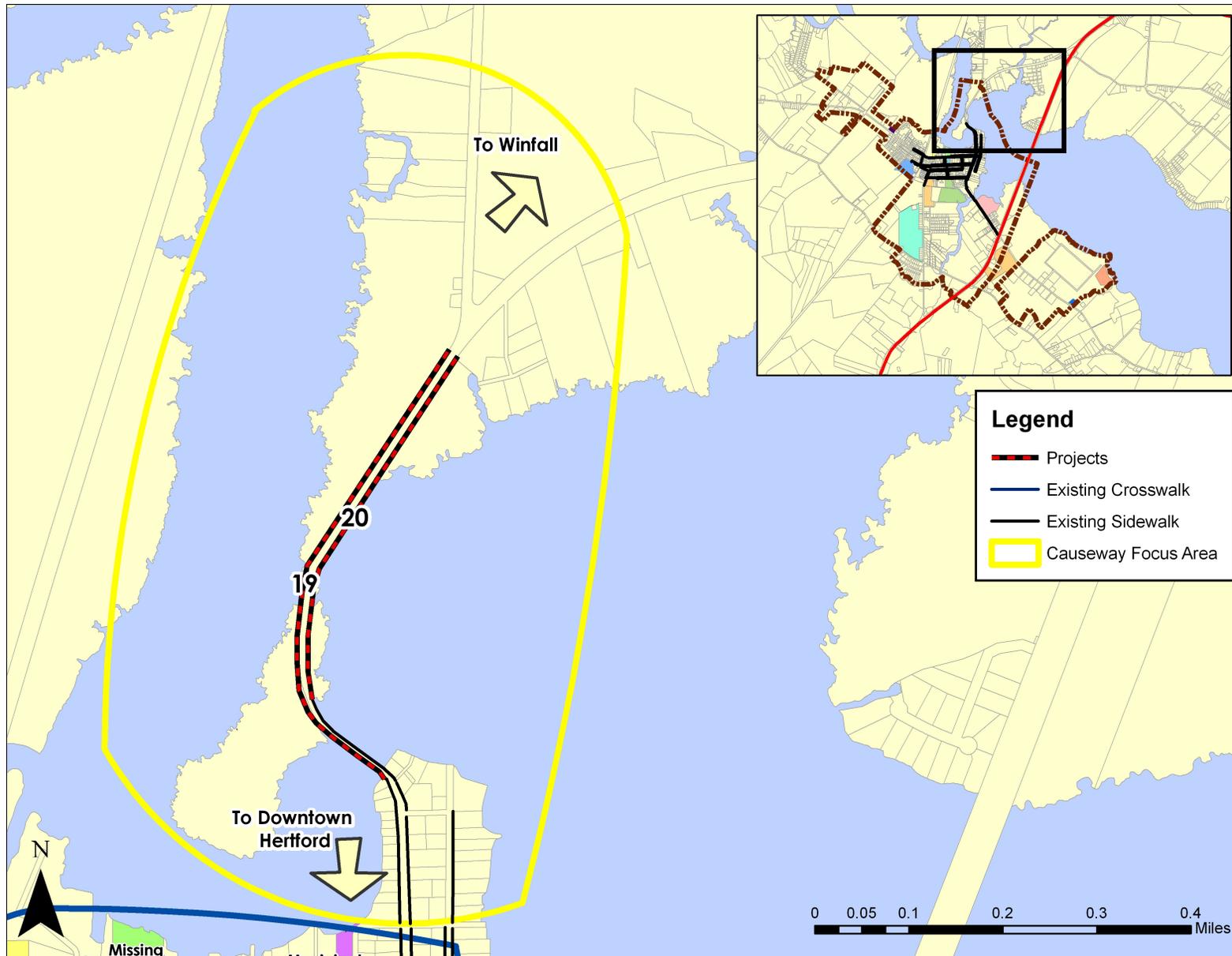


Figure 8-5. Proposed projects along the Causeway for a potential connection to Winfall and scenic route.

8.3. Cost Estimates

Table 8-1 provides very general cost estimates for the projects identified for each focus area. Cost estimates were based on an estimated unit cost for each project as follows:

For Sidewalk Projects: \$55 per linear foot
For Greenway Projects: \$400,000 per mile
For Boardwalk Projects: \$800,000 per mile
Cost of adding sidewalk to an existing bridge: \$170 per linear foot

Cost of installation of one pair of pedestrian signals to existing traffic signals: \$7,000
Cost of installation of one high-visibility crosswalk: \$1,200
Minimum cost of design and construction for new traffic signals (including pedestrian signal) or intersection upgrade: minimum \$150,000

Cost estimates also include a 40 percent mark-up for mobilization, utilities, and labor for structures and a 60 percent mark-up for all other types of facilities. Cost estimates do not take into account purchase of right-of-way or structure construction. These cost estimates were based on an average of cost estimates for the State of North Carolina, and may change with time, location, and cost of raw materials. **Prior to commitment to funding of any of these projects, the Town should conduct a detailed analysis to identify more accurate cost estimates.**

Table 8-1. Cost Estimates for Proposed Projects by Focus Area.

Reference Number	Project	Estimated Length (miles)	Estimated Cost
<i>Focus Area: Intersection of US 17 and Church Street/Harvey Point Road and points south</i>			
1	Pedestrian Crossing Improvements at Intersection of US 17 and Church Street/Harvey Point Road	NA	Starting at \$150,000
2	Sidewalk on the east side of Church Street from existing sidewalk to US 17	0.63	\$183,500
3	Sidewalk on the west side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	0.66	\$183,552
4	Sidewalk on the south side of the access road to the Perquimans Recreation Center	0.85	\$191,088
5	Sidewalk on the east side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	0.64	\$246,646
6	Boardwalk along the Perquimans riverfront from Downtown to the Recreation Center	2.13	\$187,050
<i>Focus Area: Perquimans High School and Hertford Grammar School</i>			
7	Greenway along "wetland" behind High School into town (Jenny's Gut area/cemetery)	0.44	\$174,142
8	Greenway linking Perquimans High School to Hertford Grammar School	0.22	\$89,792
9	Sidewalk on the west side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	0.78	\$225,814
10	Sidewalk on the east side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	0.77	\$223,757
11	Sidewalk on the north side of Wynne Fork Road from the intersection with Edenton Street Road to US 17	0.48	\$139,477
12	Improvements at the intersection of US 17 and Wynne Fork Road to accommodate pedestrians safely	NA	Starting at \$150,000

Table 8-1 Continued

Reference Number	Project	Estimated Length (miles)	Estimated Cost
<i>Focus Area: Perquimans High School and Hertford Grammar School (continued)</i>			
13	Sidewalk on both sides of Wynne Fork Road from existing residential development to the intersection with US 17	0.42	\$122,269
22	Sidewalk on north side of Market Street from the intersection with Grubb Street to the Hertford Grammar School	0.38	\$150,000
<i>Focus Area: Missing Mill Park Area</i>			
14	Sidewalk to Missing Mill Park along Church Street from end of existing sidewalk to park entrance	0.09	\$26,947
15	Provide pedestrian crossing between Missing Mill Park and Tennis Courts	NA	\$1,200
16	Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp	0.29	\$234,870
17	Sidewalk on the north side of Church Street from existing sidewalk to Missing Mill Park entrance	0.18	\$51,900
21	Sidewalk from residential neighborhoods along Grubb Street to connect to Missing Mill Park (Nates Drive to existing sidewalk)	0.20	\$58,166
23	Sidewalk on both sides of Dobbs St. from Church Street to Covent Garden Street.	0.15	\$44,500
<i>Focus Area: Raccoon Creek Pier and Canoe Launch Area</i>			
18	Sidewalk on east side of Church Street from existing sidewalk to the pier	0.21	\$61,437
<i>Focus Area: Connections along the Causeway and S-Bridge</i>			
19	Sidewalk along west side of S-bridge and Causeway/US 17	0.54	\$157,609
20	Boardwalk adjacent to S-bridge and Causeway/US 17	0.41	\$328,392

8.4. Prioritization

Table 8-2, Table 8-3, and Table 8-4 prioritize the proposed projects for each Focus Area based on a short-term, mid-term, and long-term schedule. **Short-term** represents a project which should be addressed within the next five years from the Plan's completion. **Mid-term** projects should be addressed within six to ten years, and **long-term** projects are those that are most likely to happen beyond ten years of the Plan's completion. Prioritization for projects was based on a series of criteria, including the following:

- ◆ **Safety** – refers to actual or perceived threat to pedestrian safety by nearby traffic. It was qualitatively measured by the crashes near a location or a perceived sense of danger by users as reported by staff, stakeholders, and the public during open houses and the survey.
- ◆ **Demand** – indicates the amount of perceived potential use for the project either as a result of input by the stakeholders, public, or staff, or by observations during field visits. A location had a high level of need/demand if many comments were made about it at the open house or on the survey, or if several pedestrians were observed at the location during field visits.
- ◆ **Accessibility/Connectivity** – refers to the potential for the project to complete a route from one major destination to another. Frequently, the project may connect two existing blocks of sidewalk that were previously separated by a long stretch of missing sidewalk.
- ◆ **Ease of Construction** – qualitatively measures how feasible it will be to construct a project. While this factor takes into account cost to a certain extent, it also includes necessary right-of-way, permitting, and potential negotiations with other agencies and jurisdictions that may be necessary to construct the project.
- ◆ **Cost** – qualitatively measures the potential cost of a project, mostly as a function of length alone.
- ◆ **Recreation vs. Transportation Need** – indicates whether a project will serve a transportation or recreation need. A transportation need is one that connects users from one major destination to another, especially if they are necessary destinations such as schools, grocery stores, banks, and work locations. A project which serves a recreation need is one that is solely intended for use as an exercise route, such as a loop trail around a lake, or a scenic destination.

Items that had the greatest priority were those that would meet an immediate safety need or alleviate some hazardous situation, were easily constructible, and met an existing demand for facilities. Projects were also given more priority if they served a specific transportation need or completed a necessary route to a destination. As a result, short-term projects are those that can

be quickly completed or alleviate an immediately hazardous situation. Long-term projects were those designed more for recreation over transportation, and may require additional planning to acquire right-of-way and adequate budget. Figure 8-6 shows a map of Hertford with the projects labeled by priority.

Table 8-2. Short Term Priorities (0 – 5 years).

Reference Number	Project	Criteria
1	Pedestrian Crossing Improvements at Intersection of US 17 and Church Street/Harvey Point Road	Safety Demand Accessibility/Connectivity Transportation Needs
3	Sidewalk on the west side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Safety Accessibility/Connectivity Transportation Needs
9	Sidewalk on the west side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Safety Demand Accessibility/Connectivity Transportation Needs
10	Sidewalk on the east side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Safety Demand Accessibility/Connectivity Transportation Needs
11	Sidewalk on the north side of Wynne Fork Road from the intersection with Edenton Street Road to US 17	Safety Accessibility/Connectivity Transportation Needs
12	Improvements at the intersection of US 17 and Wynne Fork Road to accommodate pedestrians safely	Safety Accessibility/Connectivity Transportation Needs
13	Sidewalk on both sides of Wynne Fork Road from existing residential development to the intersection with US 17	Safety Accessibility/Connectivity Transportation Needs

Table 8-2 Continued

Reference Number	Project	Criteria
14	Sidewalk to Missing Mill Park along Church Street from end of existing sidewalk to park entrance	Safety Accessibility/Connectivity Ease of Construction
15	Provide pedestrian crossing between Missing Mill Park and Tennis Courts	Safety Accessibility/Connectivity Ease of Construction
18	Sidewalk on east side of Church Street from existing sidewalk to the pier	Safety Accessibility/Connectivity Ease of Construction

Table 8-3. Mid-Term Priorities (6 – 10 years).

Reference Number	Project	Criteria
2	Sidewalk on the east side of Church Street from existing sidewalk to US 17	Safety Accessibility/Connectivity
8	Greenway linking Perquimans High School to Hertford Grammar School	Demand Accessibility/Connectivity Transportation Need (School access)
17	Sidewalk on the north side of Church Street from existing sidewalk to Missing Mill Park entrance	Safety Demand Accessibility/Connectivity Recreation
21	Sidewalk from residential neighborhoods along Grubb Street to connect to Missing Mill Park (Nates Drive to existing sidewalk)	Safety Demand Accessibility/Connectivity Recreation
22	Sidewalk on north side of Market Street from the intersection with Grubb Street to the Hertford Grammar School	Safety Accessibility/Connectivity Transportation Need (School access) Cost

Table 8-4. Long-Term Priorities (10 or more years).

Reference Number	Project	Criteria
4	Sidewalk on the south side of the access road to the Perquimans Recreation Center	Accessibility/Connectivity Recreation
5	Sidewalk on the east side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Safety Accessibility/Connectivity
6	Boardwalk along the Perquimans riverfront from Downtown to the Recreation Center	Recreation Potentially Complicated Construction
7	Greenway along "wetland" behind High School into town (Jenny's Gut area/cemetery)	Accessibility/Connectivity Recreation Potentially Complicated Construction
16	Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp	Recreation Potentially Complicated Construction
19	Sidewalk along west side of S-bridge and Causeway/US 17	Demand Accessibility/Connectivity Potentially Complicated Construction
20	Boardwalk adjacent to S-bridge and Causeway/US 17	Demand Accessibility/Connectivity Potentially Complicated Construction
23	Sidewalk on both sides of Dobbs St. from Church Street to Covent Garden Street	Demand Accessibility/Connectivity Project Cost

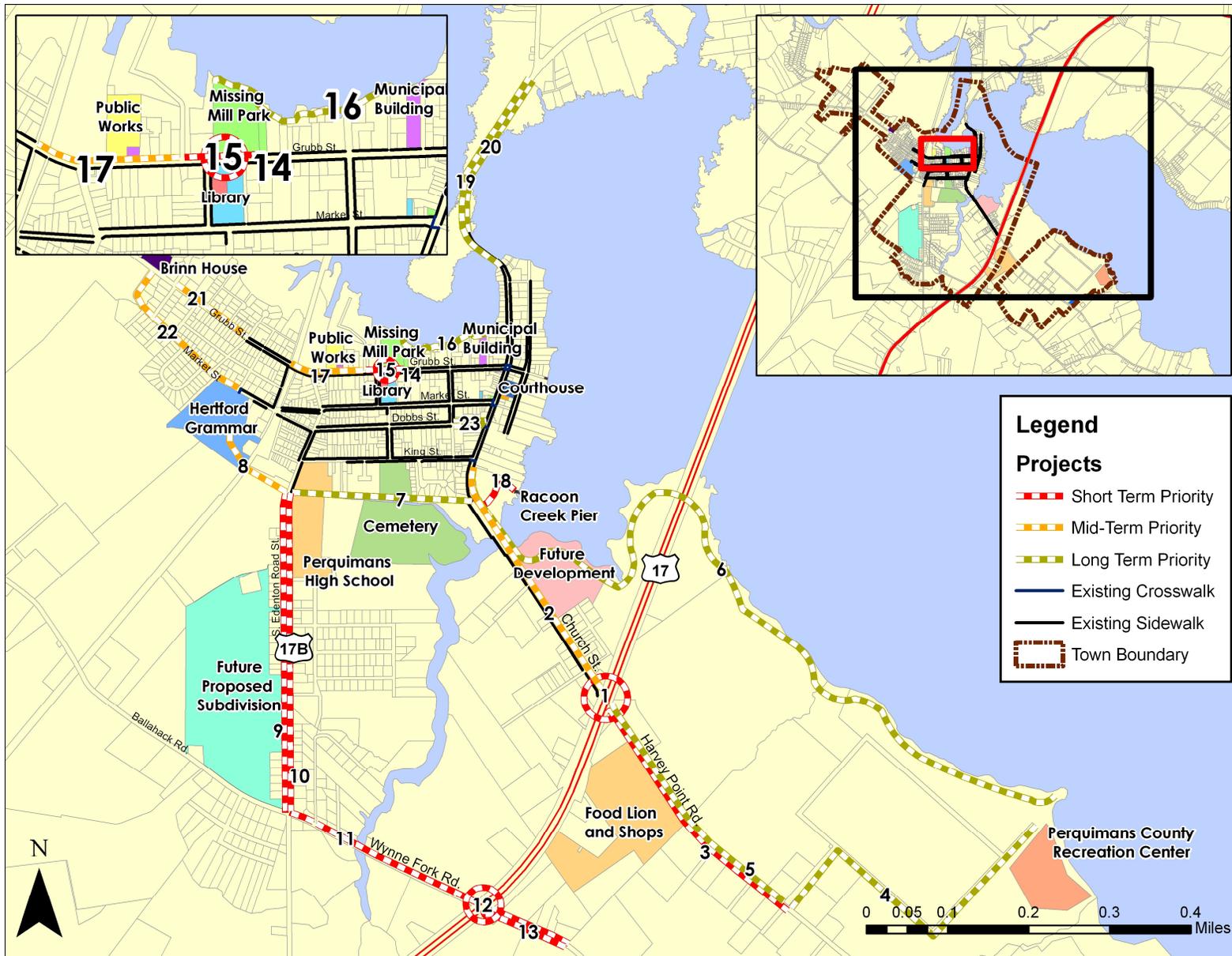


Figure 8-6. Projects for Hertford by priority.

8.5. Summary

Combined, the projects recommended in this Section should work to make Hertford overall a safer, more comfortable place for people to walk and visit. Design for each of the recommendations should follow the guidelines described in *Section 6: Design Guidelines*, as well as receive a more detailed engineering analysis prior to construction. In order to create the most successful implementation of the pedestrian plan, these projects should be coupled with the recommendations for program and policy changes. The following section, *Section 9: Implementation*, provides a better look at funding and potential partnerships for all of the recommendations including programs and policies, and also how the Town should start to move forward with them.

Quick Summary: This section addresses the steps for the Town to take in order to implement the recommendations contained in the Plan. It includes a schedule for projects, recommended partnerships and potential funding sources.

Section 9. Implementation

9.1. Introduction

The preceding sections have outlined project, program, and policy recommendations that will help to create Hertford as a more pedestrian-friendly community. Section 9 provides a recommended implementation plan to help Hertford move forward with putting the Plan's recommendations into action. To be useful, an implementation plan must identify potential funding sources, provide a general timeline for when the Town should begin to enact recommendations, and also potential partners that to which the Town should reach out for help. The following section contains all these items, and detailed "next steps" for actions immediately after the Plan has been adopted.

9.2. Funding

Pedestrian facilities are constructed – and therefore funded – through a number of avenues. Funding can be divided into four categories: local, state, national, and private funding. The following paragraphs describe some of the more prominent sources in each category. Hertford should tap into all of these sources, and search for others as well, in order to take advantage of the funds available.

9.2.1. Local Funding

Currently, Hertford does not have an annual budget line item specifically for pedestrian improvements; however, in the past pedestrian facilities have been constructed as an ancillary to another project – such as curb ramps at Church Street and Market Street as part of a signalization upgrade. In the future, Hertford may wish to consider creating a specific annual budget item to set aside funds for improving pedestrian facilities. A specific budget item is the most direct way to ensure that funding for pedestrian facilities is available, but sometimes a town's budget may be too limited to finance this. Another way for pedestrian facilities to be built is to ensure that they are constructed with any new town projects or improvements, such as parks and recreation facilities, libraries, schools, and new roads. In addition, future private development should be reviewed for adequate pedestrian access and connections. As discussed in the policy recommendations of *Section 7: Programs and Policy Recommendations*, this may mean the Town should require developers to install sidewalk with new construction. The Town should also consider teaming with other organizations that may have their own projects in Hertford, such as the Albemarle Commission (Council of Governments), the Hertford Main Street Program, Historic Downtown Hertford Program, the Visitor's Bureau, and the Chamber of Commerce.

9.2.2. State and National Funding

Hertford should also consider reaching out to state and national funding sources for assistance in constructing pedestrian facilities. State and national funding are a combined category because many of the state entities administer national funds. The North Carolina Department of Transportation (NCDOT) is the single largest source of funding available to Hertford for pedestrian facilities, with the following potential funding sources:

- ◆ State Transportation Improvement Program
 - Incidental Projects
 - Independent Projects
- ◆ Transportation Enhancement Program
- ◆ Spot Improvement Program
- ◆ Small Urban Funds
- ◆ Hazard Elimination Program
- ◆ Governor’s Highway Safety Program
- ◆ Statewide Discretionary Funds

NCDOT also administers the Safe Routes to School Program, which is federally funded. The following paragraphs provide a more thorough description of each of the various funding sources.

- ◆ **State Transportation Improvement Program (STIP)** – This program is the overall funding source for study, design, and construction of major transportation projects, including pedestrian facilities, in the state. Frequently, projects funded by the STIP are also partly funded by other sources, including matching funds from local municipalities. Pedestrian facilities are eligible for funding from this program as independent projects, such as greenways or multi-use paths which are separate from a roadway construction, widening, or some other sort of roadway work. However, one of the most cost-effective and efficient ways to gain funding for pedestrian facility construction is to incorporate them as incidental to a larger project. Overall, most pedestrian accommodations within the state are made as incidental improvements. For more information on the TIP process, see:
www.ncdot.org/transit/bicycle/funding/funding_TIP.html.
For NCDOT’s Pedestrian Policy Guidelines, please see:
www.ncdot.org/transit/bicycle/laws/laws_pedpolicy.html.
For NCDOT’s Greenway Policy, please see the following link at the Division of Bicycle and Pedestrian Transportation’s website:
www.ncdot.org/transit/bicycle/laws/laws_greenways_admin.html.
- ◆ **Transportation Enhancement Program** - Transportation enhancements are transportation-related activities that are designed to strengthen the cultural, aesthetic, and

environmental aspects of transportation systems. The transportation enhancements program provides for the implementation of non-roadway capacity improvement projects, including bicycle and pedestrian facilities; landscaping; and aesthetic improvements. Various forms of pedestrian facilities, such as sidewalks, separate walking paths and trails, and pedestrian safety training and related materials are eligible for funding.

- ◆ **Spot Improvement Program** - The NCDOT Bicycle and Pedestrian Transportation Division budgets \$500,000/year for “spot” safety improvements throughout the State. These improvements include items such as signing, grate replacement, bike rack installations, hazard remediation at skewed railroad crossings, and other small-scale improvements. The Spot Improvement Program is used only for bicycle and pedestrian projects; however, it should not be viewed as a priority source for funding identified projects. It is typically used for small-scale and special-situation projects that are not of a significantly large enough scale to merit being a TIP project. Taking these requirements into consideration, proposals for projects should be submitted directly to the Bicycle & Pedestrian Transportation Division.
- ◆ **Small Urban Funds** – Small Urban Funds are available for small improvement projects in urban areas. Each NCDOT Highway Division has \$2 million of small urban funds available annually. Although not commonly used for bicycle facilities, local requests for small bicycle projects can be directed to the NCDOT Highway Division office for funding through this source. A written request should be submitted to the Division Engineer providing technical information such as location, improvements being requested, timing, etc. for thorough review.
- ◆ **Hazard Elimination Program** – This program focuses on projects intended for locations that should have a documented history of previous crashes. Bicycle and pedestrian projects are eligible for this program, although the funds are not usually used for this purpose. This program is administered through the NCDOT Division of Highways. Similar to the Small Urban Funds, it is a significantly limited funding source.
- ◆ **Powell Bill Funds** – Powell Bill Funds can be used for streets which are not state-maintained. Sidewalk and bikeway construction are eligible activities as well. For more information, see www.ncdot.org/financial/fiscal/ExtAuditBranch/Powell_Bill/powellbill.html.
- ◆ **Governor’s Highway Safety Program (GHSP)** – This program provides funding for projects which show that they will make substantial progress in reducing crashes, injuries and fatalities at a specific location. All funding is considered to be “seed money” to get programs started – the grantee is expected to provide a portion of the project costs and to

continue the program after GHSP funding ends. Projects are only approved for one full or partial federal fiscal year at a time; however, projects may be funded for up to three consecutive years. Amounts of GHSP funds vary from year to year, according to the specific amounts requested.

- ◆ **Statewide Discretionary Funding** - The Statewide Discretionary Fund consists of \$10 million and is administered by the Secretary of the Department of Transportation. This fund can be used on any project at any location within the state. Primary, urban, secondary, industrial access, and spot safety projects are eligible for this funding. To request funding, an agency must submit a written request to the NCDOT Highway Division office with a clear description of project and project justification.
- ◆ **Sidewalk Program** – Each year, a total of \$1.4 million in STP-Enhancement funding is set aside for sidewalk construction, maintenance and repair. Each of the 14 highway divisions across the state receives \$100,000 annually for this purpose. Funding decisions are made by the district engineer. Prospective applicants are encouraged to contact their district engineer for information on how to apply for funding. A listing of divisions and district engineers is available at:
https://apps.dot.state.nc.us/srmu/directory/PDF_Report/ReportEngListing.aspx
- ◆ **Safe Routes to School Program** – The federal Safe Routes to School Program is intended to create and promote safe walking and cycling in order to improve safety near schools, promote active lifestyles, and reduce pollution and congestion caused by school traffic. In North Carolina, NCDOT, in conjunction with the Federal Highway Administration, has established the North Carolina Safe Routes to School program. The North Carolina Safe Routes to School program provides opportunities for schools to apply for funding for both programs and capital improvements projects to encourage walking and cycling to school. For more information about the Safe Routes to School Program, please see the North Carolina Safe Routes to School’s webpage at: www.ncdot.org/programs/saferoutes.
- ◆ **Community Development Block Grants (CDBG)** – CDBG funding is intended to help communities provide housing, create suitable living environments, and expand economic opportunities primarily in low- and medium-income areas. Hertford could use these grant funds for recreation facilities and planning. It should be noted that CDBG Funds are highly competitive and the requirements are extensive. For more information, please see:
www.hud.gov/offices/cpd/communitydevelopment/programs

9.2.3. Private Funding

Hertford can consider a variety of private funding sources. Contrary to NCDOT and federal funding, most private funding sources offer limited grants which are aimed at establishing programs and conducting projects on a smaller scale to encourage more walking. Many of the funding sources, such as the Blue Cross Blue Shield Fit Together Grants and the Robert Wood Johnson Foundation Active Living By Design Awards, relate more to encouraging healthy lifestyles, which can be fostered through a more pedestrian-friendly town. The following paragraphs provide descriptions of the some of the private funding sources that Hertford can consider.

- ◆ **Blue Cross Blue Shield Fit Together Grants** – The FitCommunity Program is one of the programs established by the Fit Together partnership of Blue Cross Blue Shield and the NC Health and Wellness Trust Fund. The Fit Together partnership was created to promote physical activity and healthy eating in an effort to combat the recent dramatic increase of obesity in North Carolina. The FitCommunity Program is a designation and grant program to recognize and reward municipality and county efforts to promote physical activity, healthy eating and tobacco-free programs, policies, environments and lifestyles. A municipality or county is eligible for grant money once it has received a FitCommunity designation. Potential grant money could be used for starting a Safe Routes to School program, Walk-to-Work week, or another community event to promote walking.

9.3. Recognition Programs

The following are two examples of some of the recognition programs that are available to Hertford. Similar to funding sources, recognition programs can be both public and private entities. Although recognition programs may not include funding, through highlighting award recipients they provide free marketing which will make the town more attractive to visitors, businesses, and future residents.

- ◆ **Robert Wood Johnson Foundation Active Living By Design Awards** - Active Living by Design is a national program of the Robert Wood Johnson Foundation and is administered by the UNC School of Public Health. The program establishes innovative approaches to increase physical activity through community design, public policies and communications strategies. Active Living by Design is funding 25 community partnerships across the country to demonstrate how changing community design will impact physical activity. Although funding is currently not available for additional communities, the Town of Hertford should continue to monitor Active Living by Design as a potential funding source should the Town chose to make a commitment to healthy living. For more information, please see: www.activelivingbydesign.org/.

9.4. Implementation

A useful implementation plan is one that identifies a timeframe for when projects should be begun and what partners the Town should consider working with to move a project forward. In general, the Town should consider working with a wide range of partners – from Perquimans County Departments to civic organizations such as the Perquimans County Trailblazers. Table 9-1, Table 9-2, and Table 9-3 present a potential schedule for the project, program, and policy recommendations in this plan and potential partners the town should consider. The table should be used by the Town as a flexible framework for implementing the recommendations in the Plan – recognizing that it is important to capitalize on unexpected opportunities while also pursuing long term goals.

Table 9-1. Short Term Actions (0 - 5 years)

Projects			
Reference Number	Description	Responsibility	Potential Partners
1	Pedestrian Crossing Improvements at Intersection of US 17 and Church Street/Harvey Point Road	Hertford Planning and Public Works Depts.	NCDOT, Chamber of Commerce/Visitor's Bureau, Historic Hertford
3	Sidewalk on the west side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Chamber of Commerce/Visitor's Bureau, NCDOT, Developers
9	Sidewalk on the west side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Hertford Planning and Public Works Depts.	NCDOT, Perquimans County Schools, Developers
10	Sidewalk on the east side of Edenton Street Road from the intersection with Ballahack Road to the Perquimans High School	Hertford Planning and Public Works Depts.	NCDOT, Perquimans County Schools, Developers
11	Sidewalk on the north side of Wynne Fork Road from the intersection with Edenton Street Road to US 17	Hertford Planning and Public Works Depts.	NCDOT, Developers
12	Improvements at the intersection of US 17 and Wynne Fork Road to accommodate pedestrians safely	Hertford Planning and Public Works Depts.	NCDOT
13	Sidewalk on both sides of Wynne Fork Road from existing residential development to the intersection with US 17	Hertford Planning and Public Works Depts.	NCDOT, Developers

Table 9-1 continued

Projects			
Reference Number	Description	Responsibility	Potential Partners
14	Sidewalk to Missing Mill Park along Church Street from end of existing sidewalk to park entrance	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Parks Facilities
15	Provide pedestrian crossing between Missing Mill Park and Tennis Courts	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Parks Facilities, Hertford Seniors' Center
18	Sidewalk on east side of Church Street from existing sidewalk to the pier	Hertford Planning and Public Works Depts.	Hertford Parks Facilities
Programs			
Description	Responsibility	Potential Partners	
Self-guided Walking Tour	Hertford Planning Dept.	Chamber of Commerce/Visitor's Bureau, Historic Hertford, Main Street Program	
Safe Routes to School Program	Hertford Planning Dept.	Perquimans County Schools, Hertford Grammar School, PTA, Albemarle COG, NCDOT	
Annual 5K	Hertford Trailblazers	Historic Hertford, Chamber of Commerce/Visitor's Bureau	
Policies			
Description	Responsibility	Potential Partners	
Require new development to construct sidewalk and connect to existing sidewalk.	Hertford Planning Dept. – will require Town Council approval	NCDOT and Developers	
Require all new signals to include pedestrian signal heads and crosswalks.	Hertford Planning and Public Works Depts. – will require Town Council approval	NCDOT and developers	
Implement policies that will assure safe pedestrian access through parking lots	Hertford Planning Dept. – will require Town Council approval	Developers, Historic Hertford, Chamber of Commerce/Visitor's Bureau	

Table 9-2. Mid-Term Actions (6 – 10 years)

Projects			
Reference Number	Description	Responsibility	Potential Partners
2	Sidewalk on the east side of Church Street from existing sidewalk to US 17	Hertford Planning and Public Works Depts.	Developers, Hertford Chamber of Commerce/Visitor's Bureau, NCDOT
8	Greenway linking Perquimans High School to Hertford Grammar School	Hertford Planning and Public Works Depts.	Hertford Parks Facilities, Hertford Chamber of Commerce/Visitor's Bureau, Perquimans County Parks and Recreation
17	Sidewalk on the north side of Church Street from existing sidewalk to Missing Mill Park entrance	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Seniors' Center
21	Sidewalk from residential neighborhoods along Grubb Street to connect to Missing Mill Park (Nates Drive to existing sidewalk)	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau, Hertford Seniors' Center
22	Sidewalk on north side of Market Street from the intersection with Grubb Street to the Hertford Grammar School	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau
Programs			
	Description	Responsibility	Potential Partners
	Walking/Running Trail with Mile Markers and Exercise Stations	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation
	Connections to the Blueway	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Historic Hertford, Main Street Program
	Police Education	Hertford Police Department	Hertford Police, NCDOT
	Pedestrian Counts on Major Thoroughfares	Hertford Planning and Public Works Depts.	NCDOT, Albemarle Commission

Table 9-2 continued

Policies		
Description	Responsibility	Potential Partners
Require new development to set aside land for greenways	Hertford Planning and Public Works Depts. – will require City Council approval	Developers, Perquimans Trailblazers
Implement policies that encourage mixed-use and other pedestrian-friendly developments	Hertford Planning Dept. – will require Town Council approval	Developers, Hertford Main Street Program, Chamber of Commerce/Visitor's Bureau

Table 9-3. Long-Term Actions (10 or more years)

Projects			
Reference Number	Description	Responsibility	Potential Partners
4	Sidewalk on the south side of the access road to the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Perquimans County Parks and Recreation
5	Sidewalk on the east side of Harvey Point Road from the intersection of US 17 to the access road for the Perquimans Recreation Center	Hertford Planning and Public Works Depts.	Perquimans County Parks and Recreation, NCDOT
6	Boardwalk along the Perquimans riverfront from Downtown to the Recreation Center	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Hertford Parks Facilities, NCDOT
7	Greenway along "wetland" behind High School into town (Jenny's Gut area/cemetery)	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, Perquimans County Schools
16	Greenway/boardwalk along riverfront from Missing Mill Park to Town Municipal Building/Boat Ramp	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation
19	Sidewalk along west side of S-bridge and Causeway/US 17	Hertford Planning and Public Works Depts.	NCDOT
20	Boardwalk adjacent to S-bridge and Causeway/US 17	Hertford Planning and Public Works Depts.	Perquimans Trailblazers, Perquimans County Parks and Recreation, NCDOT

Table 9-3 continued

Projects			
Reference Number	Description	Responsibility	Potential Partners
23	Sidewalk on both sides of Dobbs St. from Church Street to Covent Garden Street.	Hertford Planning and Public Works Depts.	Historic Hertford, Chamber of Commerce/Visitor's Bureau
Programs			
Description		Responsibility	Potential Partners
Sidewalk Construction Program		Hertford Planning and Public Works Depts.	NCDOT, Developers
Sidewalk Maintenance Program		Hertford Planning and Public Works Depts.	NCDOT, Developers
Intersection Improvement Program		Hertford Planning and Public Works Depts.	NCDOT, Developers
Policies			
Description		Responsibility	Potential Partners
Implement connectivity requirements.		Hertford Planning Dept. – will require Town Council approval	Developers, Chamber of Commerce/Visitor's Bureau

9.5. Summary

Using this implementation plan as a guide and the recommendations contained in this Plan, the Town of Hertford should be able to create a better, safer network of sidewalks, greenways, trails, paths, and crossings for pedestrians in the Town. The Town's next steps should begin to immediately address the short-term priority program, policy, and project recommendations. At the same time, the Town should also start to lay the groundwork for the longer term recommendations by mentioning them to potential partners and starting already to budget for projects in the future. Most importantly, the Town should continue its efforts to raise awareness about the importance of making a community more walkable in order to continue to cultivate support for more pedestrian improvements and programs. Residents, visitors, and local leaders should be familiar with the economic, health, and environmental benefits of a community in which there is less dependence on autos and more reliance on foot travel as not only a form of recreation, but also as a form of transportation.

Already, Hertford has some of the elements necessary to be a more walkable community. With careful planning, deliberate steps, and dedication, Hertford can become an even more pedestrian-friendly community.

Appendix 1: Public Involvement Materials

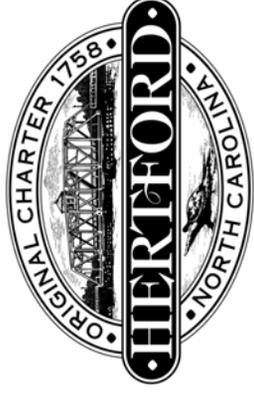
Announcing: the Hertford Pedestrian Plan!

What is the Hertford Pedestrian Plan? The Hertford Pedestrian Plan is intended to provide guidance for the future of Hertford's pedestrian facilities by making recommendations for future projects, programs, and policies that will encourage pedestrians to walk in the Town. The Plan is made possible through a grant from the North Carolina Department of Transportation's Division of Bicycle and Pedestrian Planning and matching funds from the Town of Hertford.

Why a Pedestrian Plan? It is important to encourage pedestrians in Hertford for a variety of reasons, including:

- ◆ health,
- ◆ economic/tourism benefits,
- ◆ safety,
- ◆ congestion management.

Hertford is a Town which is ripe for pedestrian improvements to boost its already walkable downtown, and increase its popularity as a vacation destination. By improving pedestrian facilities in the Town, it will become easier for both the long-time resident and newcomer to get around safely.



Where would we make recommendations? Some of the potential study areas for the Plan include Hertford's downtown, schools, recreation areas, and connections to the proposed nearby East Coast Greenway. The Plan can also provide recommendations for enhancing the pedestrian environment for visitors through improved wayfinding, pedestrian amenities, and better street crossings. The Town already has a walking tour and several annual festivals – the Plan should incorporate these into its recommendations as well.

When? There will be an Open House for the Plan coming up this spring on May 8, 5-7 PM to gather information about walking in Hertford. A Draft Plan will be available no later than June 2007.

Contact Information:

Brandon Shoaf, Town Planner, will serve as the local contact for this project. The Town has also contracted with the Louis Berger Group, Inc. to lead the pedestrian plan preparation process. The contact for the Louis Berger Group, Inc. is Jennifer Lewis, El. If you have any additional questions, please feel free to contact either person.



Brandon Shoaf

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The Louis Berger Group
Project Manager
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Cary, NC 27511
Phone: (919) 467 – 3885 ext. 19
Email: jlewis@louisberger.com

**Want more info?
Come to the Open
House!**
May 8, 5-7 PM
Perquimans County
Courthouse

Thank you for your interest in the Hertford Pedestrian Plan. Your efforts will help to make Hertford a better place to live, work, and visit!

Please tell us the roads where you would like to see sidewalks:

Road Name	Starting Point	Ending Point
(example) Academy St.	Covent Garden St.	Church St.

Please tell us the roads or greenways where there is sidewalk that needs repair or is obstructed:

Road Name, Start, End
(example) Church St. between Dobbs and Market St.
Cracked pavement from tree roots. Dangerous for wheelchairs & strollers.

Please tell us about any intersections where you would like to see improvements for pedestrians. Improvements could include adding a crosswalk, new pedestrian signals, pedestrian warning signs, curb ramps, or audible pedestrian signals.

Intersecting Roads	Problem	Improvement
(example) Church St. and Market St.	Have to wait a long time to cross the street.	Please provide a pedestrian signal.

Please provide us with any additional comments you may have:

Additional Optional Information:

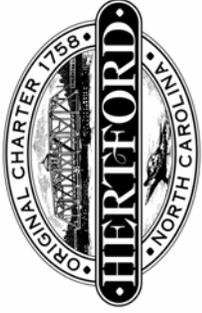
Name: _____

Address: _____

For more information about the Pedestrian Plan,
 please attend the up-coming
Open House,
May 8 from 5—7PM at
the Perquimans County Courthouse

Thank you for taking the Town of Hertford
 Pedestrian Survey! You can return this survey to
 Town Hall when you pay your utility bill, or mail it to
 the following address:

The Hertford Pedestrian Plan Survey
 C/O Jennifer Lewis
 The Louis Berger Group, Inc.
 1513 Walnut St, Ste. 250
 Cary, NC 27511



HERTFORD PEDESTRIAN PLAN OPEN HOUSE INFORMATION SHEET

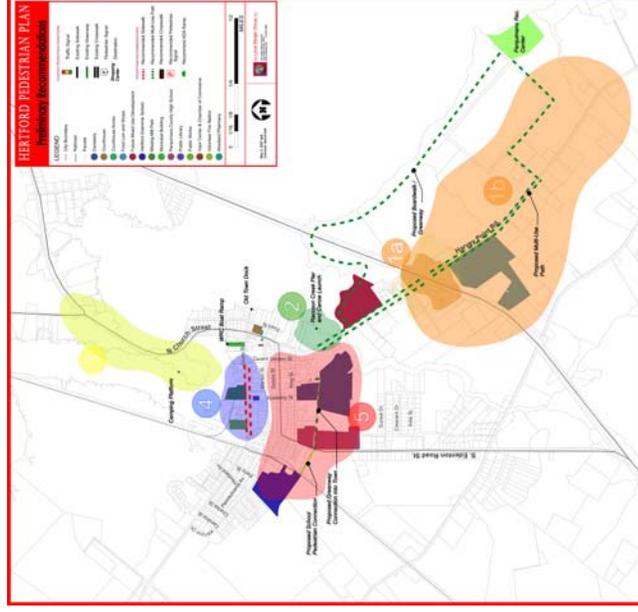
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Why a Pedestrian Plan? It is important to encourage pedestrians in Hertford for a variety of reasons, including health, economic/tourism benefits, safety, and congestion management. Hertford is a Town which is ripe for pedestrian improvements to augment its already walkable downtown, and boost its popularity as a vacation destination. By improving pedestrian facilities in the Town, it will become easier for both the long-time resident and newcomer to get around safely.

Potential Focus Areas: Some of the potential study areas for the Plan include Hertford's downtown, schools, recreation areas, and connections to the proposed nearby East Coast Greenway. The Plan can also provide recommendations for enhancing the pedestrian environment for visitors through improved wayfinding, pedestrian amenities, and better street crossings. The Town already has a walking tour and several annual festivals – the Plan should incorporate these into its recommendations as well.

Some of the potential focus areas are:

1. Intersection of NC 17 and Church Street/Harvey Point Road and points south
2. Raccoon Creek Pier and Canoe Launch Area
3. Winfall Connections and S-Bridge
4. Missing Mill Park Area
5. Perquimans High School and Hertford Grammar School



Note: Larger Image on the Back.

When? There will be two general opportunities for public involvement in the Pedestrian Plan, including this Open House Meeting. The next opportunity will be in late August or September, when a Draft Plan will be presented.

Contact Information:

Brandon Shoaf, Town Planner, is the local contact for this project. The Town has also contracted with the Louis Berger Group, Inc. to lead the pedestrian plan preparation process. The contact for the Louis Berger Group, Inc. is Jennifer Lewis, EI. If you have any additional questions, please feel free to contact either person.



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Thank you for your participation in the Hertford Pedestrian Plan. Your efforts will help to make Hertford a better place to live, work, and visit!

HERTFORD PEDESTRIAN PLAN

Preliminary Recommendations

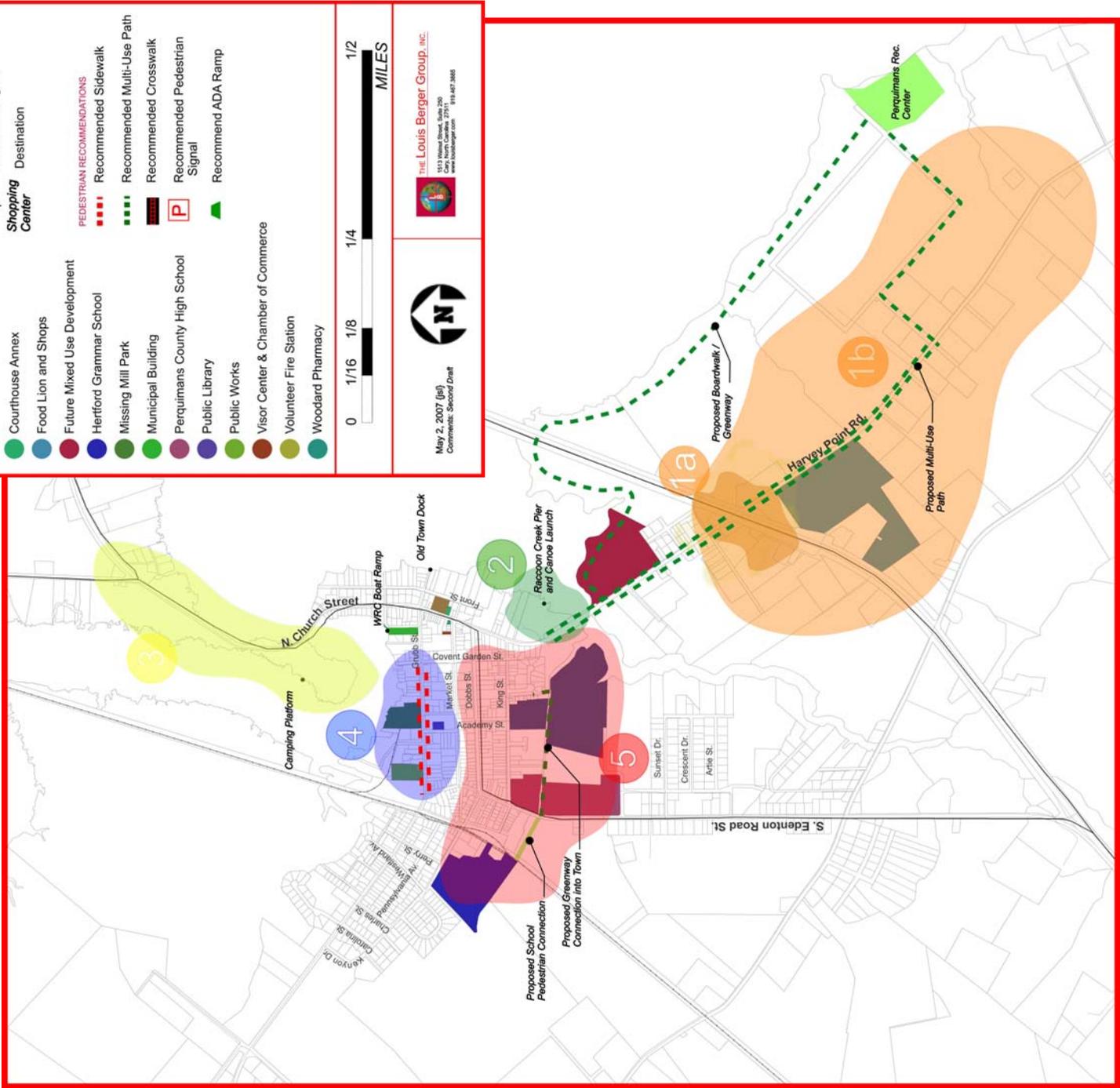
EXISTING PEDESTRIAN CONDITIONS	
	City Boundary
	Railroad
	Parcels
	Cemetery
	Courthouse
	Courthouse Annex
	Food Lion and Shops
	Future Mixed Use Development
	Hertford Grammar School
	Missing Mill Park
	Municipal Building
	Perquimans County High School
	Public Library
	Public Works
	Visor Center & Chamber of Commerce
	Volunteer Fire Station
	Woodard Pharmacy
	Traffic Signal
	Existing Sidewalk
	Existing Greenway
	Existing Crosswalk
	Pedestrian Signal
	Shopping Center
	Destination

PEDESTRIAN RECOMMENDATIONS	
	Recommended Sidewalk
	Recommended Multi-Use Path
	Recommended Crosswalk
	Recommended Pedestrian Signal
	Recommend ADA Ramp

MILES

May 2, 2007 (jg)
Comments: Second Draft

THE Louis Berger Group, INC.
1133 Walnut Street, Suite 200
Cary North Carolina 27511
919.687.3665



Appendix 2: Demographic Analysis

The following tables provide Census 2000 data for various demographic characteristics that are useful to the Town of Hertford's Pedestrian Plan. All data was collected from the US Census 2000 website, excepting where noted.

Population

	Town of Hertford	Perquimans County	North Carolina
1990 Census Population:	-n/a-	10,447	6,628,367
2000 Census Population	2,070	11,368	8,049,313
Percent Change:		8.8%	21.4%
2005 Estimate Population*	2,127	12,154	

Census Summary File 1, P1 (2000) and P1 (1990)

**North Carolina State Data Center*

Race

	Town of Hertford	Perquimans County	North Carolina
Total:	2,070	11,368	8,049,313
<i>By Percent</i>			
White alone	48.2%	70.8%	72.0%
Black or African American alone	50.4%	28.0%	21.5%
American Indian and Alaska Native alone	0.1%	0.2%	1.2%
Asian alone	0.2%	0.2%	1.3%
Some other race alone	0.0%	0.1%	2.3%
Population of two or more races	0.6%	1.0%	2.3%
Hispanic or Latino*	0.9%	0.6%	4.6%

Census Summary File 1, P3 & P7

*Note: The US 2000 Census measures "Hispanic/Latino" as an ethnicity and not a race.

Hispanic/Latino population estimates are therefore considered separate from racial estimates.

Age

	Town of Hertford	Perquimans County	North Carolina
Total:	2,070	11,368	8,049,313
<i>By Percent</i>			
9 years and younger	12.9%	11.1%	13.7%
10 - 19 years	16.6%	14.1%	6.7%
20 - 29 years	9.6%	8.9%	14.6%
30 - 39 years	11.4%	12.5%	15.8%
40 - 49 years	13.0%	14.2%	15.1%
50 - 59 years	11.0%	13.8%	11.2%
60 - 69 years	7.9%	11.8%	7.6%
70 - 79 years	9.8%	8.8%	5.6%
80 years and older	7.8%	4.8%	2.9%

Census Summary File 1, P12

Educational Attainment

	Town of Hertford	Perquimans County	North Carolina
Total:	2,070	11,368	8,049,313
<i>By Percent</i>			
No Schooling	0.0%	1.0%	1.2%
Eight Grade or Below	7.4%	8.4%	7.8%
Some High School	21.6%	18.6%	14.0%
High School Graduate	35.3%	33.0%	28.5%
Some College	19.8%	21.3%	20.5%
Associate Degree	5.1%	5.3%	6.8%
Bachelor's Degree	7.6%	8.6%	15.3%
Graduate or Professional School Degree	3.2%	3.7%	7.2%

Summary File 3, P37

Income

	Town of Hertford	Perquimans County	North Carolina
Median Household Income (1999)	\$19,681	\$29,538	\$39,184
Median Family Income (1999)	\$24,524	\$35,122	\$46,335

Summary File 3, P53 & P77

Total Population	2,070	11,368	8,049,313
Percent Population Below Poverty Line	39.2%	17.9%	12.3%
<i>Percent Under Age 5</i>	<i>11.6%</i>	<i>6.6%</i>	<i>9.9%</i>
<i>Percent Over Age 65</i>	<i>8.9%</i>	<i>16.8%</i>	<i>12.8%</i>

Summary File 3, P87

Household Vehicle Availability

	Town of Hertford	Perquimans County	North Carolina
Total:	865	4,465	3,132,013
<i>By Percent</i>			
No vehicle available	26.6%	9.3%	7.5%
1 vehicle available	37.3%	30.8%	32.3%
2 vehicles available	26.0%	41.9%	39.9%
3 vehicles available	7.5%	14.5%	14.9%
4 vehicles available	2.1%	2.9%	4.0%
5 or more vehicles available	0.5%	0.6%	1.4%

Summary File 3, H44

Work Commute (for workers over 16 years old)

	Town of Hertford	Perquimans County	North Carolina
Total:	659	4,434	3,837,773
<i>By Percent</i>			
Single Occupancy Vehicle	68.4%	76.2%	79.39
Carpooled:	22.9%	15.6%	14.03
Public transportation:	1.1%	0.7%	0.91
Bicycle	0.0%	0.0%	0.18
Walked	5.6%	2.3%	1.93
Other means	0.0%	0.9%	0.89
Worked at home	2.0%	4.4%	2.68

Summary File 3, P30

To-Work Travel Time (for workers over 16 years old)

	Town of Hertford	Perquimans County	North Carolina
Total:	659	4,434	3,837,773
<i>By Percent</i>			
Less than 5 minutes	7.4%	4.6%	2.9%
5 minutes to a half hour	56.1%	51.6%	64.8%
Half hour to an hour	19.7%	24.1%	24.0%
Hour to an hour and a half	3.9%	4.9%	3.4%
More than an hour and a half	10.8%	10.4%	2.2%
Worked at Home	2.0%	4.4%	2.7%

Summary File 3 P31