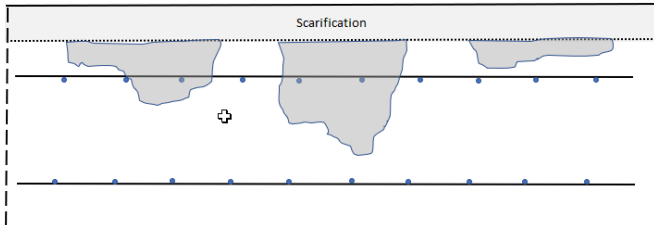


# STRUCTURE BULLETIN

## NCDOT Construction Unit

[Website](#) [email](#)

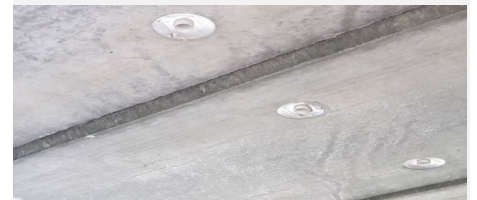


### Current Issues: Payment for PPC overlays

From time to time the question comes up of how to pay for PPC overlay items, specifically Class II repairs. Usually we think of Class II as ones where the bottom of the patch is below the top mat of rebar. On PPC overlays the intent is to pay for any repair necessary below scarification and above the bottom mat as Class II. In the image above all three gray areas would be paid as Class II Surface Preparation. This item is compensation for "taking out the bad". Next we must fill the patch. If the patch is filled during the overlay placement with the paver, we do not pay for this. If filling the patch is a separate operation performed by hand, we pay for Concrete Deck Repair for PPC Overlay. Last, we pay for the material that goes in the patch as Polyester Polymer Concrete Materials. In a nutshell, for repairs that are necessary below scarification we pay for three items: taking out the bad, the work of putting good material back in, and the material itself. The language in the PPC Special Provisions is going to be cleaned up to make this more clear.



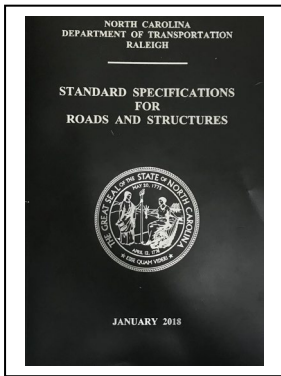
1. Current Issues
2. Updates
3. Specification Questions
4. Training



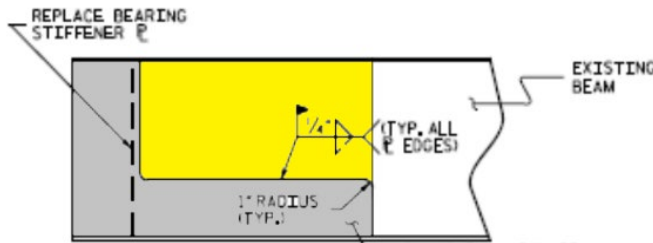
### Odds and Ends:

#### Box Beam Void Drains

The holes in the picture are on the bottom of box beams. They are supposed to be there and do not need to be patched. They are there in order to drain the void formed inside the beam in case any water finds its way inside. They have a sticker over the pipe to keep grout out during casting. Sometimes this sticker is still there when they arrive on the job, or sometimes the sticker fails and the hole is blocked with grout. In either case the hole should be cleared, not patched, so it can perform as designed.



## Questions:



**Question:** Can the contractor change the shape of a steel girder repair?

**Answer:** Sometimes. The "L" shaped repairs shown above in grey are common in girder repairs. While this shape may capture the area needing repair it may involve cutting sections with two or sometimes three radii. Some contractors will ask to square up the repair to eliminate these radii, resulting in a repair area of both the grey and yellow above. While it costs them more in material it the savings in time and fit up is worth it to them. This is allowable with the approval of the engineer. Just remember we only pay for the pounds of material in the original repair area (grey) and not the addition (yellow).

## Area Construction Engineers:

Div	Contact	Phone
1&2	<a href="#">Randy Hall</a>	282-402-9957
3&4	<a href="#">David Candela</a>	910-524-4931
5	<a href="#">Troy Brooks</a>	336-972-4627
6&8	<a href="#">John Partin</a>	336-847-1226
7	<a href="#">Aaron Griffith</a>	336-215-9170
9	<a href="#">Vickie Davis</a>	704-202-0945
10	<a href="#">Darin Waller</a>	980-521-5176
11&12	<a href="#">Brian Skeens</a>	828-803-1461
13&14	<a href="#">Aaron Powell</a>	828-694-7971

## Training:

Plans change. Looks like we are going to have to delay the 1.5 day long training session for a year. We are currently assessing the training for this winter and will keep you posted. **Managers**, please remember, if you have specific structure training needs we would be glad to talk to you about holding a customized training session for your office. Contact [Cameron](#) or [Aaron](#) if you are interested.

**Structure Bulletins** are now archived on the [Construction Unit](#) website under [Construction Resources](#).

If you have a topic you would like to see addressed in a future edition of the Structure Bulletin please [email](#) us at either [acochran@ncdot.gov](mailto:acochran@ncdot.gov) or [aeerwood@ncdot.gov](mailto:aeerwood@ncdot.gov)