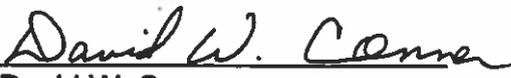


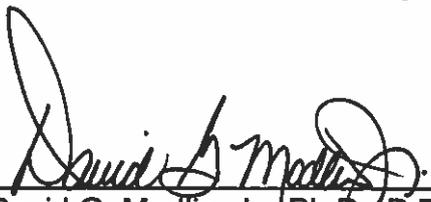
**FEASIBILITY STUDY**

**Greensboro  
Intersection Revisions at  
High Point Road (SR 4121) and Veasley Street  
Guilford County**

**U-3320**

Prepared by  
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Date

## **U-3320**

### **Greensboro Intersection Revisions at High Point Road (SR 4121) and Veasley Street Guilford County**

#### **I. General Description**

This feasibility study describes proposed improvements at the intersection of High Point Road (SR 4121) and Veasley Street in Greensboro. The improvements include realignment of the intersection to remove the existing offset of the Veasley Street approaches and widening the northbound, southbound, and eastbound approaches to add additional lanes. The project location is shown on Figure 1. The existing intersection configuration is shown on Figure 2. The proposed configuration is shown on Figure 3.

Additional right-of-way will be required for this project; however, it is not anticipated that any residences or businesses will be relocated.

The total cost including construction and right-of-way is estimated to be \$ 1,300,000.

This study is the initial step in the planning and design process for this project and is not the product of exhaustive environmental or design investigations. The purpose of this study is to describe the proposed project including costs, and identify potential problems that may require consideration in the planning and design phases.

#### **II. Existing Conditions**

The purpose of this project is to increase the traffic carrying capacity and safety of the subject intersection.

In the North Carolina Functional Classification System, High Point Road is classified as an Urban Principal Arterial. Veasley Street (a non-system roadway) is not classified.

The northeast, northwest, and southwest quadrants of the intersection are heavily developed commercially. The southeast quadrant is vacant at this time.

In the area of Veasley Street, High Point Road is generally a 7-lane, 77-foot (23.5 m) wide (face-to-face) curb-and-gutter roadway. Veasley Street is generally a 2-lane roadway which has been widened at the intersection to facilitate turning movements. Each of the High Point Road approaches include a combination right-turn/through lane, 2 through lanes, a left-turn lane, and 3 lanes exiting the intersection. The southbound Veasley Street approach includes a combination right-turn lane and one lane exiting the intersection. The northbound approach of Veasley Street includes a combination right-turn/left-turn lane, a right-turn lane, and 1 lane exiting the intersection.

Based on projected turning movements, it is estimated that the intersection is currently operating at a level of service C which will deteriorate to a level D by the design year without improvements. With the improvements, the level of service will improve such that it will be operating at a Level C and approaching D by the design year.

During the period from April 1, 1991, through March 31, 1994, there were 105 accidents reported at this intersection. None of the accidents resulted in fatal injuries; however, 33 of the accidents resulted in 57 non-fatal injuries. The most prevalent type accidents were Rear-end (37%), Left-Turn (29%), and Angle (18%). The proposed improvements should lessen the congestion at this intersection, and reduce the potential for these type accidents.

### **III. Recommendations**

It is recommended to re-align the intersection of High Point Road (SR 1421) and Veasley Street, in Greensboro, to remove the existing off-set of the Veasley Street approaches and to widen the northbound, southbound, and eastbound approaches to add additional lanes. The project location is shown on Figure 1. The existing intersection configuration is shown on Figure 2. The proposed configuration is shown on Figure 3.

The southbound approach of Veasley Street will be shifted eastward to be more in alignment with the northbound approach.

The northbound approach of Veasley Street will be widened to include a left-turn lane, a through lane, a combination through/right-turn lane and one lane exiting the intersection. The southbound approach of will include a left-turn lane, a through lane, a right-turn lane, and two lanes exiting the intersection.

The eastbound approach of High Point Road will be widened to include a left-turn lane, two through lanes, a combination through/right-turn lane, a right-turn lane, and three lanes exiting the intersection. The westbound approach will

remain with a left-turn lane, two through lanes, a combination through/right-turn lane, and three lanes exiting the intersection.

The existing traffic signal will be upgraded.

Additional right-of-way will be required; however, it is not anticipated that any residences or businesses will be relocated.

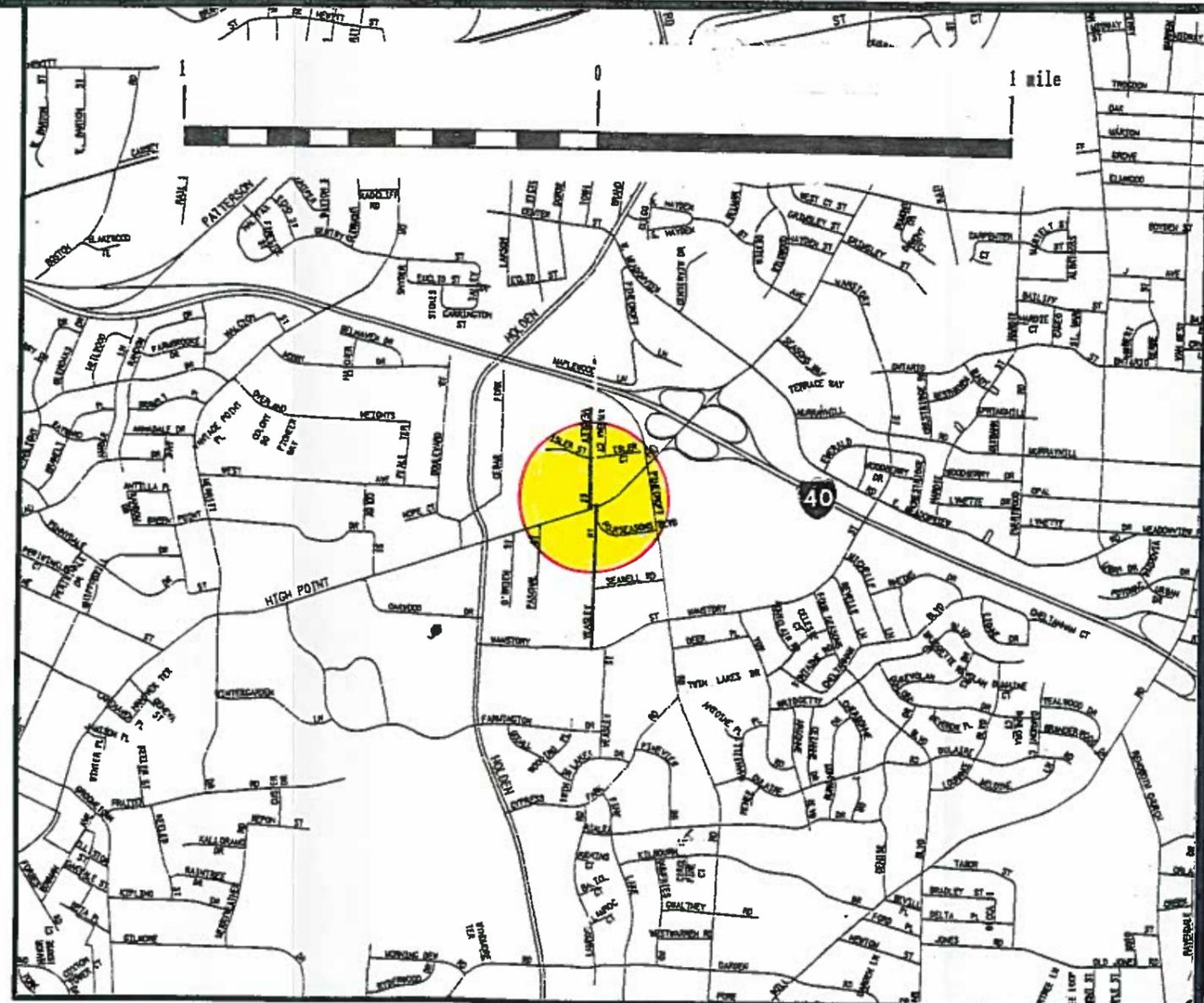
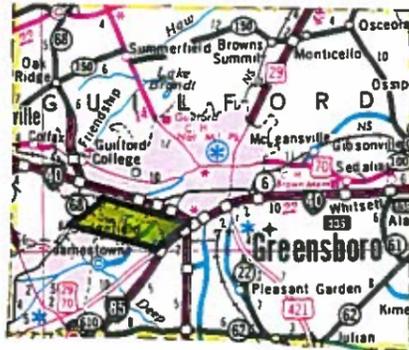
The total project cost including construction and right-of-way is estimated to be \$ 1,300,000 as follows:

Construction	.....	\$ 300,000
Right-of-Way	.....	1,000,000
Total Cost	.....	\$ 1,300,000

#### IV. Other Comments

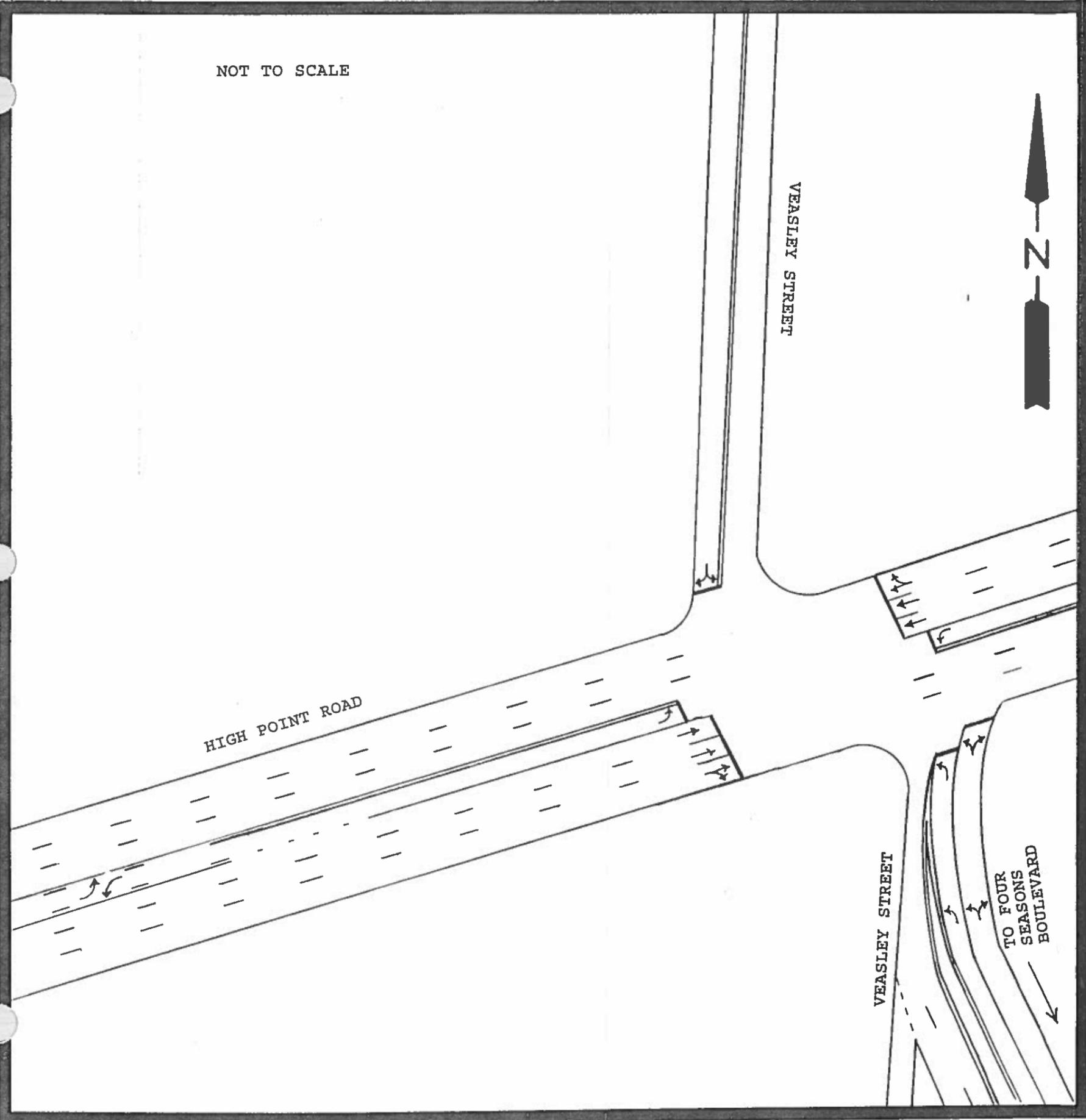
An environmental screening was not conducted for this study.

The proposed project includes 11-foot (3.3-m) wide lanes. Standard lane width is 12 feet (3.6 m). To utilize lanes wider than 11 feet (3.3 m) would entail acquisition of a greater amount of already very expensive right-of-way and possibly create business relocatees.



FEASIBILITY STUDIES UNIT		
Project Location		
Intersection Revisions High Point Road at Veasley Street		
Greensboro, Guilford County		
U-3320	Division 7	Figure 1

NOT TO SCALE



FEASIBILITY STUDIES UNIT		
Existing Configuration		
Intersection Revisions		
High Point Road at Veasley Street		
Greensboro, Guilford County		
U-3320	Division 7	Figure 2

NOT TO SCALE

VEASLEY STREET



HIGH POINT ROAD

VEASLEY STREET

TO FOUR SEASONS BOULEVARD

FEASIBILITY STUDIES UNIT		
Recommended Configuration		
Intersection Revisions		
High Point Road at Veasley Street		
Greensboro, Guilford County		
U-3320	Division 7	Figure 3