

PLAN

BEGIN APPROACH SLAB SHOWN,  
END APPROACH SLAB SIMILAR

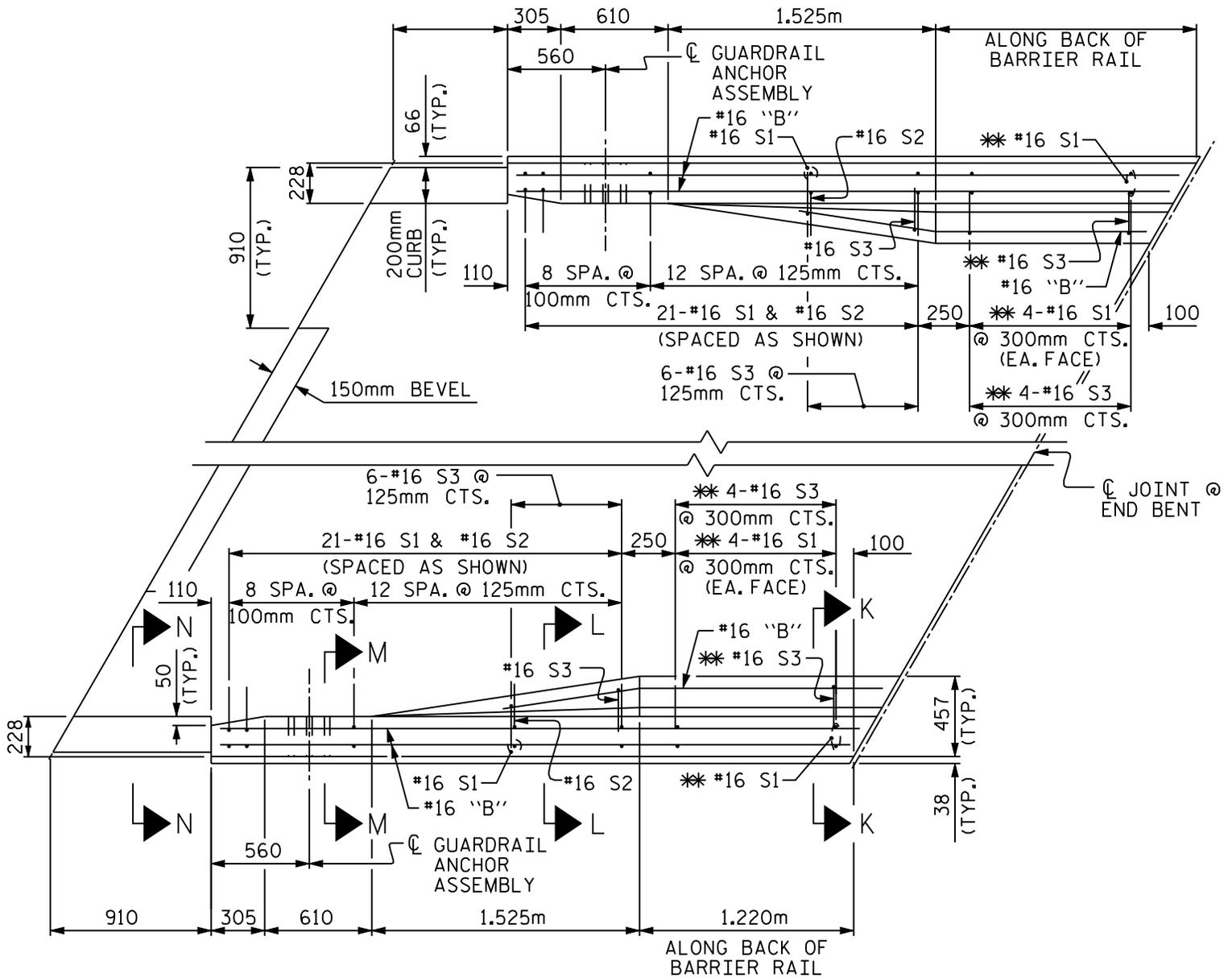
NOTE TO DETAILER:

THE #5 S1 AND S3 BARS IN THE FULL BARRIER WIDTH SECTION SHALL BE SPACED AT A MAXIMUM OF 1'-0" CENTERS. THESE BARS ARE TO BE ADHESIVELY ANCHORED WHEN THE JOINTS ARE SAWED. IF AN ARMORED JOINT IS USED, CHANGE THE #5 S1 BARS IN THE FRONT OF THE RAIL TO #5 S2 BARS AND DO NOT ADHESIVELY ANCHOR BARS.

DETAILS SHOWN ARE FOR FLEXIBLE PAVEMENT WITH SAWED JOINTS. MODIFICATIONS ARE REQUIRED WHEN USED WITH RIGID PAVEMENT, CORED SLABS, OR ARMORED JOINTS.

BARRIER RAIL TRANSITION DETAILS

FIGURE 12 - 7



PLAN

BEGIN APPROACH SLAB SHOWN,  
END APPROACH SLAB SIMILAR

NOTE TO DETAILER:

THE #16 S1 AND S3 BARS IN THE FULL BARRIER WIDTH SECTION SHALL BE SPACED AT A MAXIMUM OF 300mm CENTERS. THESE BARS ARE TO BE ADHESIVELY ANCHORED WHEN THE JOINTS ARE SAWED. IF AN ARMORED JOINT IS USED, CHANGE THE #16 S1 BARS IN THE FRONT OF THE RAIL TO #16 S2 BARS AND DO NOT ADHESIVELY ANCHOR BARS.

DETAILS SHOWN ARE FOR FLEXIBLE PAVEMENT WITH SAWED JOINTS. MODIFICATIONS ARE REQUIRED WHEN USED WITH RIGID PAVEMENT, CORED SLABS, OR ARMORED JOINTS.

BARRIER RAIL TRANSITION DETAILS

FIGURE 12 - 7 M