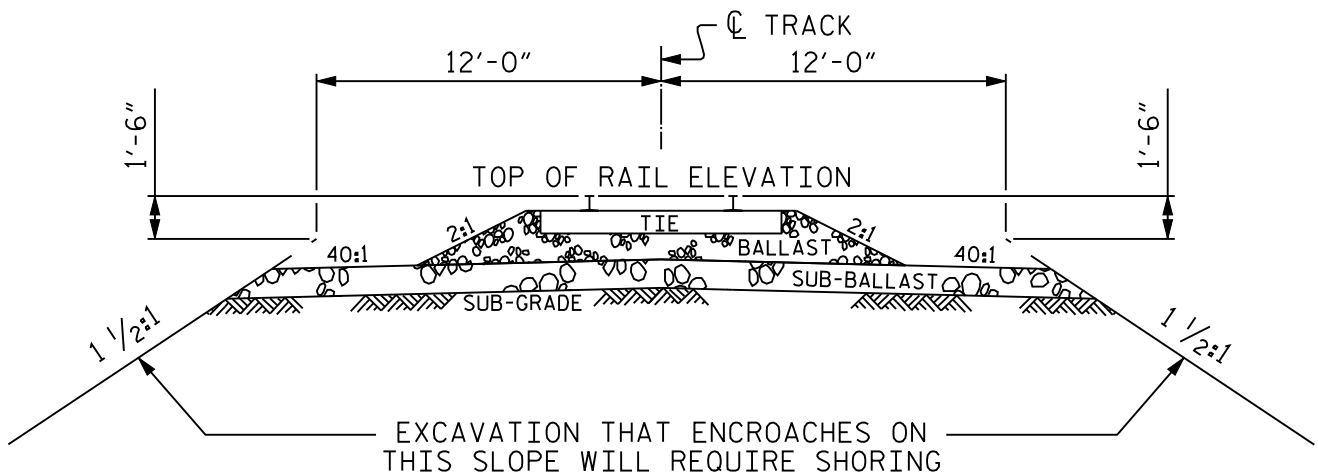


- ZONE 1 EXCAVATION WITHIN ZONE 1 WILL REQUIRE SHORING FOR THE PROTECTION OF THE RAILROAD.
 - ZONE 2 EXCAVATION WITHIN ZONE 2 WILL REQUIRE SHORING CONSISTING OF INTERLOCKING SHEETING FOR THE PROTECTION OF THE RAILROAD.
 - ZONE 3 NO EXCAVATIONS WILL BE ALLOWED IN ZONE 3. ZONE 3 LIMITS DEFINE THE MINIMUM DISTANCE FROM CENTERLINE OF TRACK TO FACE OF SHEETING, IF REQUIRED.
- NOTE: TEMPORARY SHORING WITHIN ZONE 1 AND ZONE 2 MUST BE DESIGNED FOR TRAIN SURCHARGE LOADS. EXCAVATIONS OUTSIDE OF ZONE 1 MAY STILL REQUIRE SHORING FOR SAFETY, BUT SUCH SHORING NEED NOT BE DESIGNED FOR TRAIN SURCHARGE LOADS.

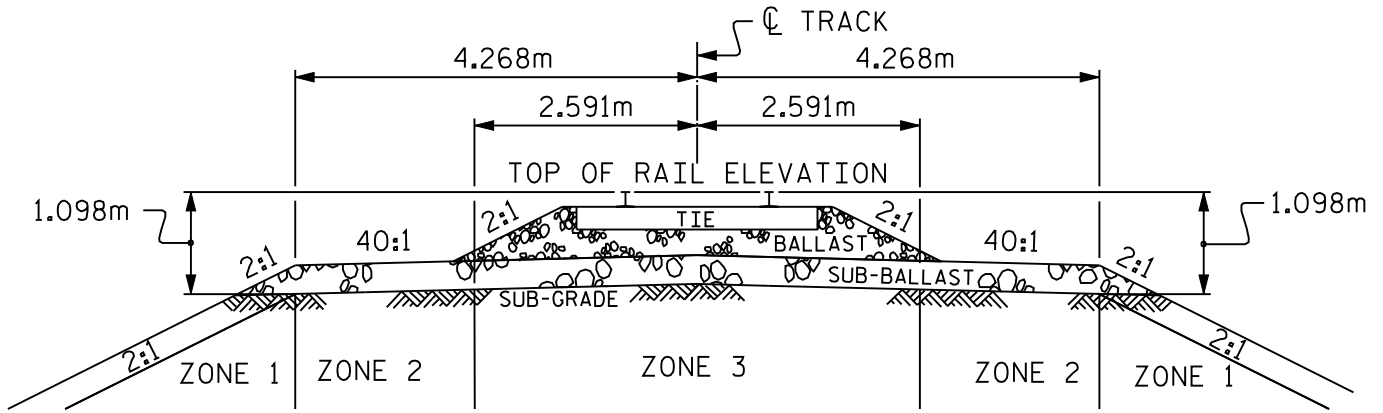
NORFOLK SOUTHERN



CSX

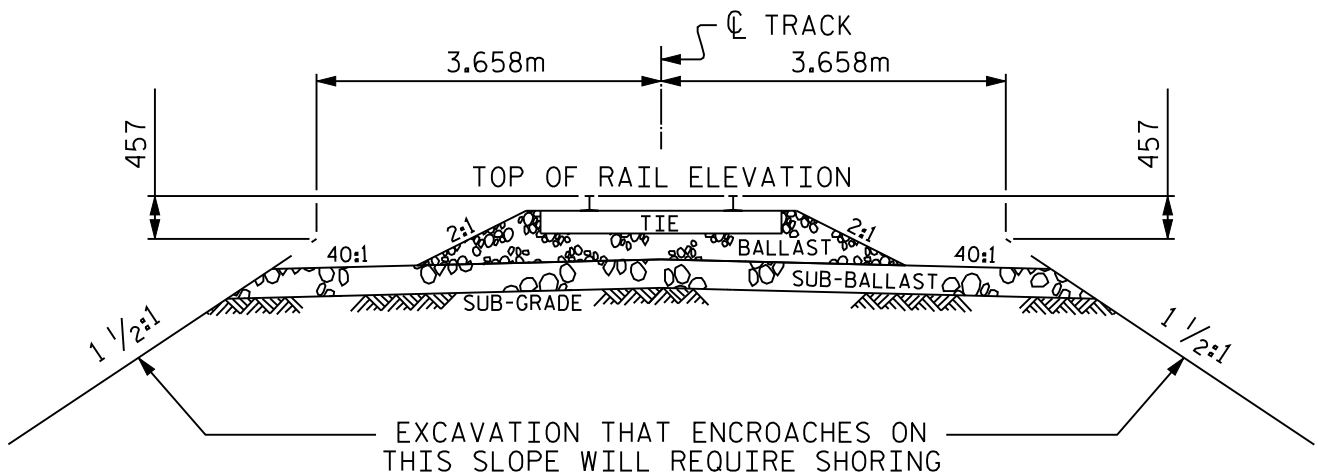
RAILROAD SHORING REQUIREMENTS

FIGURE 12 - 27



- ZONE 1 EXCAVATION WITHIN ZONE 1 WILL REQUIRE SHORING FOR THE PROTECTION OF THE RAILROAD.
 - ZONE 2 EXCAVATION WITHIN ZONE 2 WILL REQUIRE SHORING CONSISTING OF INTERLOCKING SHEETING FOR THE PROTECTION OF THE RAILROAD.
 - ZONE 3 NO EXCAVATIONS WILL BE ALLOWED IN ZONE 3. ZONE 3 LIMITS DEFINE THE MINIMUM DISTANCE FROM CENTERLINE OF TRACK TO FACE OF SHEETING, IF REQUIRED.
- NOTE: TEMPORARY SHORING WITHIN ZONE 1 AND ZONE 2 MUST BE DESIGNED FOR TRAIN SURCHARGE LOADS. EXCAVATIONS OUTSIDE OF ZONE 1 MAY STILL REQUIRE SHORING FOR SAFETY, BUT SUCH SHORING NEED NOT BE DESIGNED FOR TRAIN SURCHARGE LOADS.

NORFOLK SOUTHERN



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RAILROAD SHORING REQUIREMENTS

FIGURE 12 - 27 M