



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

MICHAEL F. EASLEY
GOVERNOR

LYNDO TIPPETT
SECRETARY

MEMORANDUM TO: Project Engineers
Project Design Engineers
FROM: T. V. Rountree, P. E.
State Bridge Design Engineer
DATE: February 14, 2001
SUBJECT: REVISED CSX RAILROAD GUIDELINES

Attached are revised English and Metric Design Manual Figures 7-33 for use in detailing bridges over railroads. The revised figures reflect recent changes in CSX Railroad guidelines.

For multi-column piers, CSX now requires that the crashwall extend 2'-6" (760mm) beyond each exterior column which is equivalent to Norfolk Southern Railroad. CSX now specifies that the crashwall thickness for multi-column piers shall match the column diameter but shall never be less than 2'-6" (760mm).

This policy is effective immediately. The Design Manual will be updated at a later date.

TVR/JAD/crl

Attachments

[1.English "Pier Crash Wall for Railroad Overheads".](#)

[2.Metric "Pier Crash Wall for Railroad Overheads".](#)



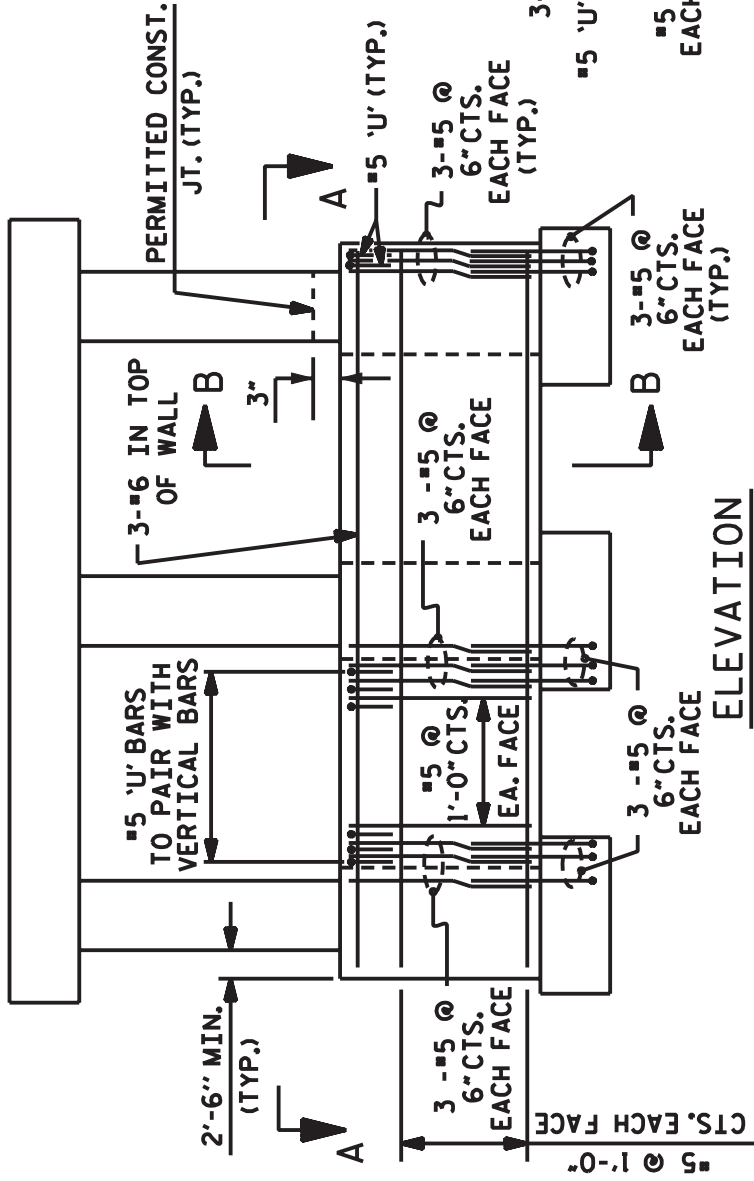
[Back to Structures Main Page](#)

Last Updated: 02/14/2001 by:Randy Bisette

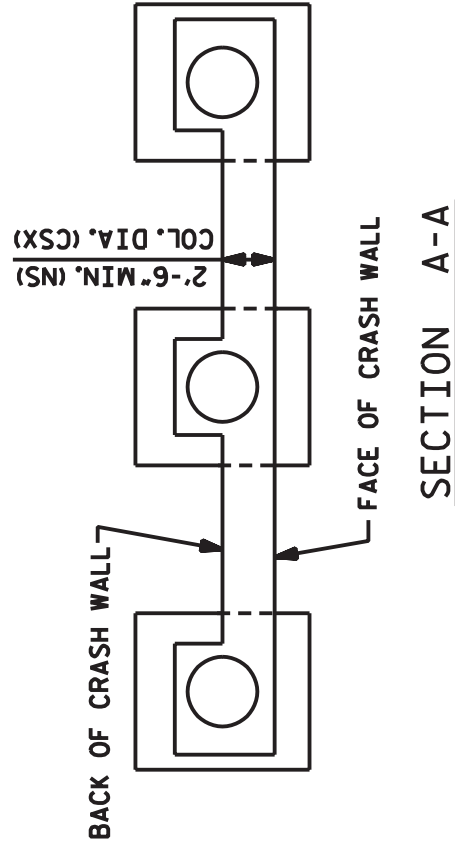
[E-mail Structure Design](#)

NOTE TO DETAILER

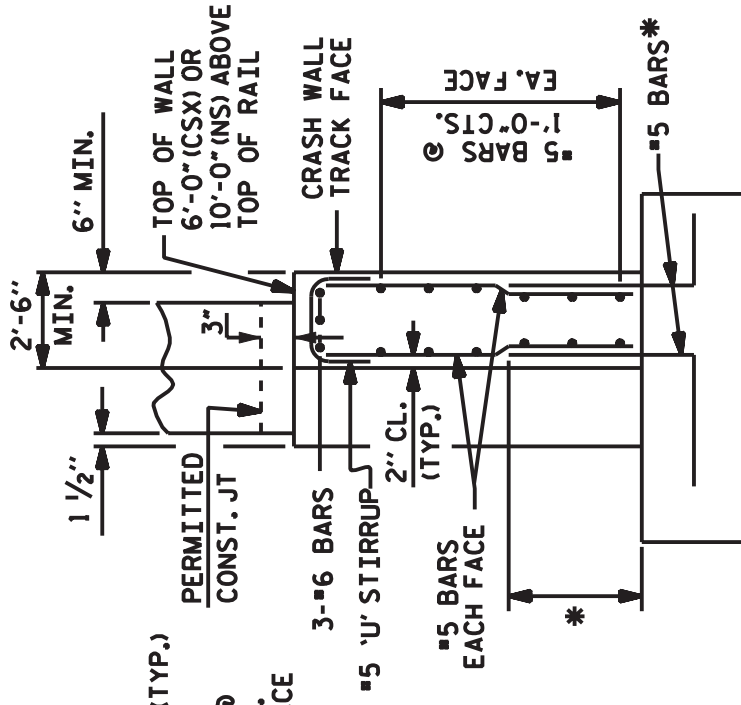
* ADD 3'-0" TO SPLICE LENGTH IF SPREAD FOOTINGS ARE USED TO TAKE CARE OF POSSIBLE ADJUSTMENTS TO FOOTING ELEVATION.



ELEVATION



SECTION A-A



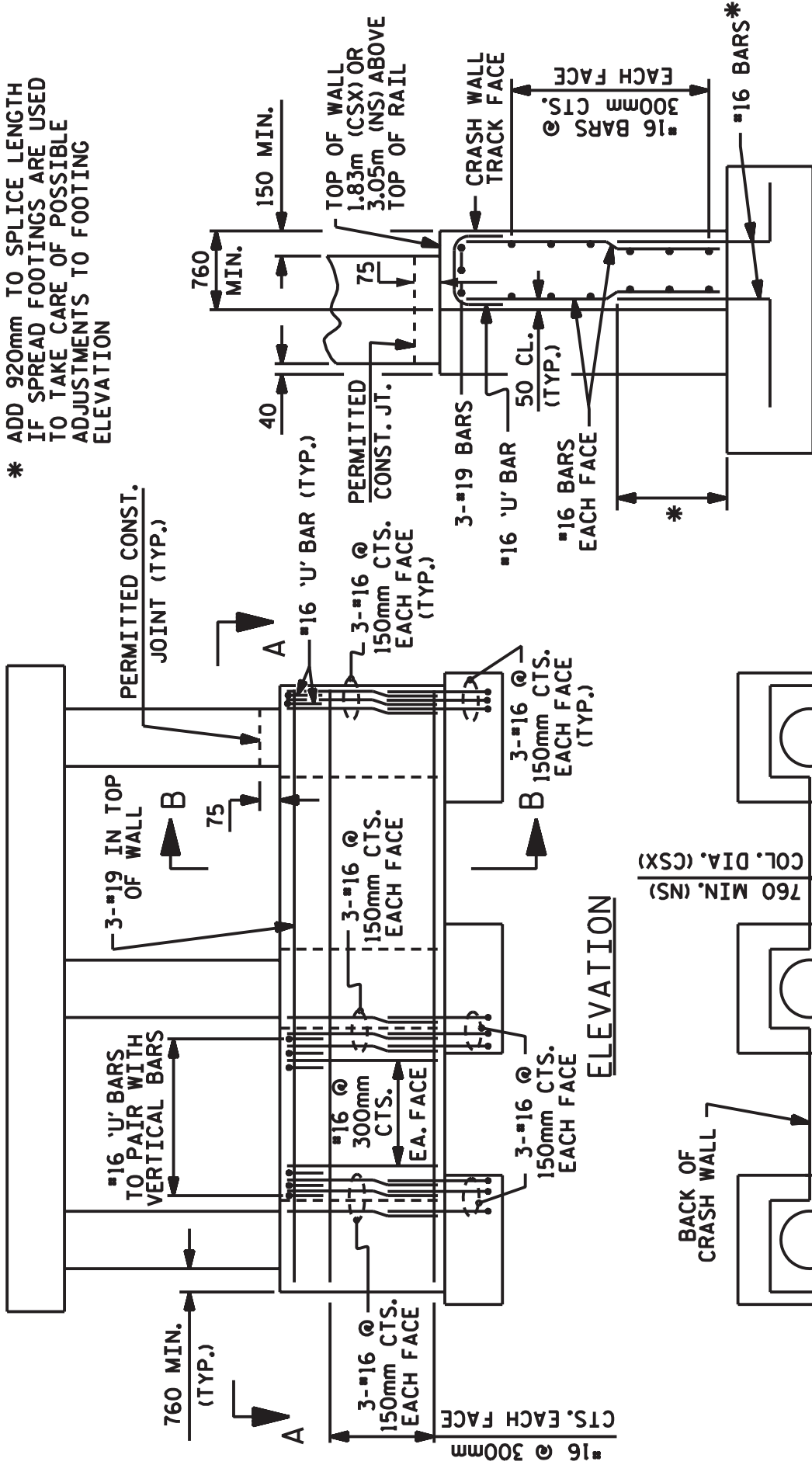
SECTION B-B

NS - NORFOLK SOUTHERN
CSX - CSX

PIER CRASH WALL FOR RAILROAD OVERHEADS
(FOR MULTICOLUMN BENTS)

NOTE TO DETAILER

* ADD 920mm TO SPLICE LENGTH IF SPREAD FOOTINGS ARE USED TO TAKE CARE OF POSSIBLE ADJUSTMENTS TO FOOTING ELEVATION



ELEVATION

SECTION B-B

NS - NORFOLK SOUTHERN
CSX - CSX

SECTION A-A

PIER CRASH WALL FOR RAILROAD OVERHEADS

(FOR MULTICOLUMN BENTS)